



**PLANNING COMMISSION  
WEDNESDAY, MAY 10, 2017  
6:00 PM**

**AGENDA**

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**I. 6:00 PM CALL TO ORDER - ROLL CALL**

Jerry Greenfield, Chair      Eric Postma, Vice Chair Peter Hurley  
Al Levit      Kamran Mesbah      Phyllis Millan  
Simon Springall

**PLEDGE OF ALLEGIANCE**

**CITIZEN'S INPUT**

*This is the time that citizens have the opportunity to address the Planning Commission regarding any item that is not already scheduled for a formal Public Hearing tonight. Therefore, if any member of the audience would like to speak about any Work Session item or any other matter of concern, please raise your hand so that we may hear from you now.*

**CONSIDERATION OF THE MINUTES**

I. A. Consideration Of The Minutes

Documents:

[I. A. Consideration Of The Minutes.pdf](#)

**II. 6:15 PM LEGISLATIVE PUBLIC HEARING**

II. A. Transit Master Plan (Brashear) (90 Minutes)

Documents:

[II. A. Transit Master Plan \(Brashear\) \(90 Minutes\).Pdf](#)

TMP RECORD - EXHIBIT A

Documents:

[Exhibit A. Transit Master Plan \(2017\).Pdf](#)

TMP RECORD - EXHIBIT B

Documents:

[Exhibit B. TMP Findings Report.pdf](#)

TMP RECORD - EXHIBIT C

Documents:

[Exhibit C. BFM Articles Combined.pdf](#)

TMP RECORD - EXHIBIT D

Documents:

[Exhibit D. Electronic News Updates Combined.pdf](#)

TMP RECORD - EXHIBIT E

Documents:

[Exhibit E. Spokesman Notices Combined.pdf](#)

TMP RECORD - EXHIBIT F

Documents:

[Exhibit F. Public Comments On Draft TMP Combined.pdf](#)

TMP RECORD - EXHIBIT G

Documents:

[Exhibit G. Public Comments From Surveys.pdf](#)

TMP RECORD - EXHIBIT H

Documents:

[Exhibit H. TMP Public Involvement Plan.pdf](#)

TMP RECORD - EXHIBIT I

Documents:

[Exhibit I. Planning Commission Meeting Minutes Combined.pdf](#)

TMP RECORD - EXHIBIT J

Documents:

[Exhibit J. City Council Meeting Minutes Combined.pdf](#)

TMP RECORD - EXHIBIT K

Documents:

[Exhibit K. TMP Task Force Meeting Minutes Combined.pdf](#)

**III. 7:45 PM INFORMATIONAL**

III. A. City Council Action Minutes (Staff) (15 Minutes)

Documents:

[III. A. City Council Action Minutes \(Staff\) \(15 Minutes\).Pdf](#)

III. B. Basalt Creek Concept Plan (Bateschell) (10 Minutes)

Documents:

[III. B. Basalt Creek Concept Plan \(Bateschell\) \(10 Minutes\).Pdf](#)

III. C. 2017 Planning Commission Work Program (Bateschell) (5 Minutes)

Documents:

[III. C. 2017 Planning Commission Work Program \(Bateschell\) \(5 Minutes\).Pdf](#)

#### IV. 8:15 PM ADJOURNMENT

*Time frames for agenda items are not time certain.*

##### Public Testimony

*The Commission places great value on testimony from the public. People who want to testify are encouraged to:*

- *Provide written summaries of their testimony*
- *Recognize that substance, not length, determines the value of testimony*
- *Endorse rather than repeat testimony of others*

*Thank you for taking the time to present your views.*

For further information on Agenda items, call Tami Bergeron, Planning Administrative Assistant, at (503) 570-1571 or e-mail her at [bergeron@ci.wilsonville.or.us](mailto:bergeron@ci.wilsonville.or.us).

Assistive Listening Devices (ALD) are available for persons with impaired hearing and can be scheduled for this meeting.

The City will also endeavor to provide the following services, without cost, if requested at least 48 hours prior to the meeting:

- \*Qualified sign language interpreters for persons with speech or hearing impairments
- \*Qualified bilingual interpreters.

To obtain services, please call the Planning Administrative Assistant at (503) 682-4960



# PLANNING COMMISSION

## WEDNESDAY, MAY 10, 2017

- I. **CONSIDERATION OF THE MINUTES**
  - A. Consideration of the April 12, 2017 Planning Commission minutes.

**PLANNING COMMISSION  
WEDNESDAY, APRIL 12, 2017  
6:00 P.M.**

**Wilsonville City Hall  
29799 SW Town Center Loop East  
Wilsonville, Oregon**

*DRAFT MINUTES  
For approval at  
the 5/10/17 PC  
Meeting*

**Minutes**

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**I. CALL TO ORDER - ROLL CALL**

Chair Greenfield called the meeting to order at 6:03 p.m. Those present:

Planning Commission: Jerry Greenfield, Eric Postma, Peter Hurley, Phyllis Millan, Simon Springall, and Kamran Mesbah. Al Levit arrived after the Consideration of the Minutes.

City Staff: Chris Neamtzu, Amanda Guile-Hinman, Jeanna Troha, Miranda Bateschell, Dwight Brashear, Eric Loomis, Michelle Marston, and Nicole Hendrix.

**PLEDGE OF ALLEGIANCE**

The Pledge of Allegiance was recited.

**CITIZEN'S INPUT**

There was none.

**CONSIDERATION OF THE MINUTES**

A. Consideration of the March 8, 2017 Planning Commission minutes  
The March 8, 2017 Planning Commission minutes were accepted as presented.

Off the record, Commissioner Levit corrected the third paragraph on Page 9 of 17, to state, "...was already a potential project in the County Transit ~~Transit~~ **Transportation System Plan.**"

**II. WORK SESSIONS**

A. Transit Master Plan (Brashear)

Commissioner Levit arrived at 6:06 pm.

Chris Neamtzu, Planning Director, reminded the public hearing on the Transit Master Plan had been continued from last month to a date certain of May 10, 2017.

Dwight Brashear, SMART Transit Director, acknowledged SMART Staff members Eric Loomis, Michele Marston, and Nicole Hendrix as key members of the team. He noted the last update to the Master Plan was done in 2008. The Executive Summary was intended to provide a true sense of direction and tone as SMART moved forward. The Master Plan should be part of the City and reflect what SMART could do as transit professionals to work with the City's planning efforts. He noted transit professionals in Washington DC liked Wilsonville's transit system because SMART was nimble. When a transit need arose, SMART figured out how to get people where they needed to go. He shared conversations he had with lawmakers in Salem and transit professionals in Washington DC who saw SMART as special and unique. It was important to make SMART viable for the next generation and keep it responsive to residents' needs.

- He noted the public involvement involved with Master Plan's update, noting SMART had been responsive to public comments. He was passionate about providing people freedom through public transit, particularly

those who did not have any choice about their lack of transportation and needed to go buy groceries or attend doctor appointments.

- The team focused on making the document flow better by eliminating redundancy and making it better organized. The previous 274-page Master Plan had been reduced to 154 pages; however, no significant changes were made. The document had been streamlined and the proposed routes were still based on the input received and what SMART believed were the most efficient routes. None of the budgeted numbers had changed. Increased costs had been a huge concern for the Chamber of Commerce and others.
- The Master Plan was designed to discuss what things were available to do, what SMART would love to do, and what transit professionals across the country were doing to attract riders. He wanted to focus on getting people out of their cars and using transit by creating a good, reliable, safe transit system that rivaled driving a car. Such ideas included providing lunch shuttles that would provide transportation between Wilsonville's business centers and city restaurants and making shuttles available for the employees of larger employers with meetings in Portland.
- SMART understood the transit system was able to serve thousands of people annually because businesses were so generous. SMART wanted to be part of the solution and help Wilsonville be the best it could. He even envisioned streetcar-type trolleys circulating regularly within the Town Center area.

Eric Loomis, SMART Operations Manager, emphasized SMART being a nimble organization, noting SMART was considering changes in response to comments received since last month's Planning Commission meeting about specific routes in the Master Plan. Concerns had been expressed about transit stops in different business complexes that were not reflected in the Master Plan. In response, timetables and transfers were being reviewed to determine what would better serve the customer base without impacting the budget.

Mr. Brashear stated SMART's team had reviewed the Master Plan page-by-page and route-by-route, which ultimately resulted in a more concise, organized document that spoke to the role SMART would play as the City grows and develops. SMART understood the challenges of the increasing population in the region, and the worst bottleneck was the Boone Bridge in Wilsonville. With Wilsonville's strategic location between Salem and Portland, the challenge for both SMART and the City was to prepare for upcoming opportunities. The document discussed the role SMART would like to play in Wilsonville's growth.

Key discussion items and responses to Commissioners' questions were as follows:

- The hope was that in 30 years, this Master Plan would be considered the foundation for public transit to which further building blocks would be added. The Plan would likely be updated in the next 5 to 8 years, but like prior master plans, this Master Plan was expected to support and help move transit forward, even 30 years from now.
- Perhaps the lunch bus, which was briefly attempted about 10 years ago, could be tried again since traffic was quite different now.
- The Master Plan encouraged businesses to get involved and develop transportation plans.
- Bus windows were dark, which was intimidating and might prevent people from riding the bus because they could not see who was on the bus.
- Several routes were in and out, when a looped route could be possible. For example, the route to Villebois currently turned around at the traffic circle on Grahams Ferry Rd, but buses could return on Kinsman when the extension opened to cover more of Villebois. Similarly, a looped route in Charbonneau would cover the entire area.
- SMART was open to such changes, but route planning came down to efficiencies and route timing. Looped routes might cover more area, but they were longer, so the bus would come less frequently due to increased costs. Straight in-and-out lines increased frequency and hopefully, ridership. Public comments revealed people were willing to walk a bit farther to a higher frequency route, which was a consideration especially in Villebois.
- In the legend on Page 13 of 158, Dot #7 was mislabeled Courtside Park; it was actually Town Center Park.

- On Page 34 of 158, the orange line in the graph did not correspond to the numbers above in Chart & Graph A. It would be useful to use the same colors in the bar graph on Page 35 as those used on Page 34.
- On Page 36, SMART's percentages for 2006 in the Oregon Payroll Tax Rates table did not agree with the verbiage on Page 35.
- On Page 40 of 158, the last sentence in the Sales Tax paragraph seemed awkward where it stated, "...the building of a new waste water proceeds...".
- Having some cost predictability for businesses was important. The Master Plan only projected costs to 2017. Fuel costs were volatile and PERS and benefit costs were difficult to control. What would trigger SMART to consider other revenue sources, such as raising taxes, fares, or grant funding, and what might that mean for businesses? Perhaps that trigger point was unknown, but the conversation would be needed.
- Transit involved a certain level of unpredictability. Some cities used the General Fund to operate transit. Others used a penny or half-cent sales tax and funding was not an issue. The majority of transit systems did not have a dedicated funding source, which was an ongoing issue. The state was working on a transportation tax package that would provide security long into the future for most transit systems, especially those the size of SMART, if approved. The Master Plan was not looking for further funding from business. SMART would always look for additional funding sources at both the state and federal level. Only after Mr. Brashear was certain SMART was operating as efficiently as possible would he look at local funding sources. SMART was financially healthy now and it was his job to manage that as far into future as possible. SMART was scalable and would try to grow with the city. With a balanced approach to growth, payroll tax revenues would keep pace so SMART would not have a problem in the next 3 to 5 years.
- SMART had already started the dialogue with TriMet about the business growth expected in Basalt Creek and Coffee Creek. Wilsonville would be well-positioned for making a strong argument about SMART providing transit services in those areas. Having those discussions prior to those areas being developed provided for greater opportunities for SMART to service Basalt Creek and Coffee Creek and use those payroll tax revenues.
- Many people did not realize SMART's service was free. A marketing campaign might be beneficial, especially in targeting middle- and high-schoolers as they need to move around town.
- The Emergency Ride Program was a national program where transit personnel commit to being available during operating hours to provide rides in emergency situations, which addressed concerns about needing a car for emergencies.
- The Master Plan discussed having advertising on the sides of busses as a funding source, but the art piece was important and added to what Wilsonville was all about.
- In recognizing employers for being transit advocates, perhaps more could be done with smaller businesses to increase ridership, such as partnering with fast food restaurants whose employees had lower salaries. Some outreach was being done now to work directly with businesses.
- The survey showed people wanted intercity connections, and light rail was more accessible from the Tigard area. With regard to the Tigard versus Barber Blvd issue, the ability to make connections to light rail needed to be emphasized.
- Reducing the geographic area for out-of-town medical was troubling. Many people had insurance plans that required certain doctors in certain areas. This service should be safeguarded as SMART looked at its revenue forecasts and considered where to cut funds.
- For this Master Plan to be a significant and transformational 30-year plan, language and substance reflecting Mr. Brashear's enthusiasm needed to be throughout the document. The Plan spoke to the costs which continue to be a topic of discussion. It needed to speak more quantitatively and graphically to SMART's specific benefits. A meaningful dialogue was needed now, especially since SMART was in good shape financially, so when difficult questions arise, SMART would not have to educate people about its benefits. It would have already been discussed for five years using actual numbers.

- The time would come when the community would have to decide whether to have SMART paid for by the General Fund and a cost-benefit analysis would be done to identify what benefits taxpayers were getting. That information needed to be in the Master Plan.
- In the Madison metro area, a business subsidized a bus route to get its employees from their preferred place of residence to work and the line had one of the heaviest riderships. Something similar could be considered in Wilsonville; SMART's system could be tailored to benefit businesses more directly if that ridership could be quantified.
- One survey in the Master Plan showed 20 percent of respondents worked and lived in Wilsonville, X percent worked in Portland, etc., but there was no specific tie into the current ridership. Surveying all employees in Wilsonville businesses was suggested to see what was keeping the 20 percent who work in Wilsonville and those commuting into the city from using transit. Connectivity to Tualatin would be addressed as Basalt Creek began development and it was time to connect TriMet and SMART.
- The employer benefits needed to be analyzed thoroughly, even as a separate addendum or study. Smaller businesses were probably the most difficult to analyze, but they were supporting the infrastructure that made Wilsonville a community. Everyone benefitted and SMART needed to show some picture of those benefits. There were not even any numbers about the benefits regarding air quality and numbers of cars, etc. The Master Plan needed to move more toward quantitative goal setting and monitoring.
- There could be more in the Plan about how transit infrastructure could be integrated with land use planning to create a synergistic, collaborative plan to allow for redevelopment at a higher density or mixed-use that would not occur without a nearby transit stop. The many traffic concerns people keep raising with regard to the Town Center redesign should be partially addressed through the redesign of the transit system.
- Though not part of this Master Plan, grant opportunities exist through the Federal Transit Administration (FTA) where cities develop public-private partnerships (P-3) to create mixed-use projects. Local matches were required and were often expensive. These P-3 projects were looking for cities with a plan, such as one built around Town Center, and a financial commitment in place. The grants were very competitive, but could be discussed further at a future time.
- Mr. Brashear had spoken with Representative Vial about Siemens' interest in acquiring a campus in Wilsonville, and while he had no specifics, he was optimistic. SMART had already reached out to Siemens, who had a strong transportation division, to make them feel welcome.
- A large number of Xerox and Mentor Graphics employees lived in Sherwood, but no direct transit connection existed. It seemed businesses had to be faced with acquiring land for more parking before getting more creative about getting their employees to work via transit.
- Having TriMet's #96 bus come to a logical terminus in Wilsonville would be hugely effective getting people into Portland. The #4 bus did not go to Graham Oaks Nature Park on every run, apparently because it would disturb the wildlife, but many trucks entered there for drivers to use the restrooms.
- The next Transit Master Plan should probably address autonomous vehicles and their effect on public transit and transit in general.
- When people at the state level talked about what they admired about SMART, the word nimble was used frequently. SMART was set up so minor changes could be done quickly. Providing service without fares was admired, especially since service within the city was not just a simplistic, short circulating bus in a downtown area. Providing a fareless service was a sure sign that transit was supported at all levels, which was also admired by others. A number of cities were looking at what Wilsonville did 30 years ago, even though it could not be replicated in the same way now.

### III. INFORMATIONAL

#### A. 2016 Housing Report (Bateschell)

Miranda Bateschell, Long-Range Planning Manager, presented the Staff report on the 2016 Housing Report included in the meeting packet. This was the City's third Annual Housing Report following the 2013 Housing Needs Analysis and 2014 Wilsonville Residential Land Study, both of which were used to help track



Wilsonville's land use capacity for residential needs and whether the city's housing supply was keeping pace. , She highlighted the background and key findings of the Housing Report, noting that owner-occupied housing continued to become less affordable, which would continue to be an issue. It was important to continue tracking this trend to help inform the Equitable Housing Grant work that would be done through the Metro Community Plan Development Grant the City received earlier this year. Staff sought the Commissions input about considerations for the 2017 Housing Report.

Her responses to Commissioner questions and additional comments from the Commission were as follows:

- Multifamily referred to attached homes, not apartment complex buildings. Anything that was more than two units fell into the definition of multifamily. It was interesting that zero multifamily units were built in 2016.
- After the last cycle of Community Planning and Development Grants at Metro, the region's economy picked up in terms of development and that additional revenue had not been allocated through the grant process. Metro started the Equitable Housing Program and hired someone to see how equitable housing might be addressed and ensure housing options existed for all demographic needs. Through that process, an additional cycle was added for the Community Planning and Development Grants solely focused on equitable housing. About the same time, City Council had discussions with the public about housing affordability issues in the city and had John Gale discuss the different programs available regarding housing affordability, which was when the City became aware of the grant opportunity. She highlighted the scope and objectives of the Equitable Housing Project, which included understanding the gaps and problem areas in terms of providing affordable housing for specific demographics and to consider strategies for adopting new policies and programs to address the need. At the end of the project, six or more strategies would be adopted to address housing affordability and equity in Wilsonville.
- Because the project could relate to Town Center, Staff was working to stagger the projects in the most meaningful and informative way. The Town Center Vision would be established, along with the design alternatives process and going through a preferred land use alternative, prior to getting into the strategies for the Equitable Housing Grant. So, the needs analysis would be done upfront while working through the Town Center planning process. Near the end of the Town Center planning effort, appropriate strategies would be considered for the Equitable Housing Grant.
- The scope of the Equitable Housing Project was fairly small. At this point, it had not been established whether ongoing reporting would be associated with dynamic housing market; however, it could be one of the strategies.
- What was missing was the overall picture. However, no overall trend had been established yet because the City had only been doing the report for three years. What did 2016 do to the overall housing picture for the community? Proper planning was supposed to help predict the future. Housing goals were set, but nothing indicated if the city's housing was moving in the right direction, so having trends was necessary.
- Such snapshots could be done every year, but the report should show how 2016 changed the overall picture of the housing stock, not the market. Doing a yearly market analysis was expensive, unnecessary, and would be outdated too soon. However, a community-wide picture of the housing stock at the same time as the annual report would be good to have.
- Having visual graphics and percentages from the initial and current Housing Report together would be helpful to easily compare and illustrate changes through the years.
- Having rental statistics would also be helpful as *The Spokesman* articles had been grim.
- While the decrease in the ratio of multifamily to single-family was influenced by the market changed with what was developed, it was partially rooted in what the City had planned for, so the City had guided it to some degree. The vast majority of building permits was in Villebois. Although the market ebbed and flowed, housing was going into areas where the City had planned for it, knowing single-family would be needed as well.
- A lot of time was spent in areas like Villebois to get good visual building patterns, but in smaller developments, many of the standards were not being carried through, especially in the Renaissance boat

house development. After all the effort and consulting time being spent for good-looking developments, how could standards be carried over to other developments that did not have a master plan?

- The vacancy percentage of rental properties in Wilsonville was just under 4 percent, which was close to the regional average. Wilsonville was in the top three in the nation.
- State habitability standards ensured that maintenance would be done on existing rental developments.
- There had been an increasing trend over the last several years of out-of-state investment purchases of multifamily properties in Wilsonville, and many of these investors were spending significant money to upgrade the properties around wood decks, siding, roofing, interiors, etc. Such upgrades brought rent spikes, which could be difficult for current residents.
- The City had no official affordable housing program with set goals and policies. The City did offer incentives, such as tax abatement for properties that offered affordable housing, and formed partnerships with non-profits on certain responsive projects, like the Creekside Apartments built for senior citizens after the mobile home park closure.
- The Equitable Housing Project would address broader, citywide goals and policies by identifying various trends and programs to achieve a more holistic approach, community-wide. The problem had to be defined first, otherwise, the market would move toward luxury units, which would displace people, which was a big concern that was occurring in Wilsonville right now with the apartment upgrades and increased rents.
- A market study would be critical as there was clearly a market for more highly-paid working professionals. There was no intent to put these market forces down, but defining the problem that this created was a step in addressing the problem. Without the demographic and market information, a conversation was difficult. An informed dialogue was needed, not one based on assumptions.
- A number of property tax exemptions were approved in the City Council report. Mr. Neamtzu noted the cost of those exemptions would be obtained from the Finance Department and sent to the Commission. The value was not insignificant.
- The employee survey around transit asking why so many worked in Wilsonville but lived outside the city could address some of the questions around housing. The Equitable Housing Project would conduct surveys of employees at large employers throughout the community, which could be tied into inquiries about traffic.
- The housing equity analysis must include both housing and transportation costs. People might live in housing they could afford without considering the cost of commuting to afford that housing. The money used for transportation could be invested into housing located closer to work.

#### B. City Council Action Minutes 03.06.2017 and 3.20.2017 (Staff)

The Planning Commission agreed receiving the Action Minutes was extremely helpful.

Key discussion items and responses to Commissioner questions about Council's Action Minutes were as follows:

- ODOT weighed in on the three alternatives for the French Prairie Bridge and had serious concerns about the one closest to the Boone Bridge because a future widening of the bridge was being considered with a potential seismic upgrade. This was the first time Staff had heard this as a concern from the State. ODOT would not support that alternative.
- Clackamas County would conduct a feasibility study for a bridge across the Willamette at the Canby Ferry site on Mountain Rd that would not be completed until 2018. Most City Staff did not know about the project until just recently.
- Details were provided about the new Residential Parking Permit Program which addressed vehicles being parked long-term in certain neighborhoods. Many residents were happy to see the ordinance approved.
- Mr. Neamtzu provided background and details about Objective 8, Promoting vibrant art, cultural, and heritage programs and facilities. Individual staff members had been assigned to each objective. A booklet would be prepared for public consumption discussing each goal, and the projects implementing those goals. Council's goalsetting was reaching a new level, providing full disclosure to share the vision with the community via printed and written documents. More information would be forthcoming.

- Documenting an inventory of art pieces found throughout the city was suggested, including maintenance of the art as well.
- The communitywide bicycle wayfinding was just a small piece of the wayfinding program under Objective 9, which referred to a bigger program with the City's logo, marketing, and directional signage to business districts, significant park facilities, city buildings, etc., as well as monument signs at freeway off ramps. A plan would be created with design standards and a vision for how the signage would look. The wayfinding would need to be taken seriously in the the Town Center redevelopment.
- The City Manager's written quarterly report tracked the progress made on Council's goals. Mr. Neamtzu wrote a summary of the Planning Commission's meeting that went into the City Manager's monthly report; however, being buried in a report, its effectiveness was unknown. Council and the Commission would also be holding joint meetings, providing an opportunity for the Commission to discuss topics not on the work session's agenda. There were a number of ways the Commission could communicate with Council.
- There had been a redesign with regard to the parking spaces on the new bypass route at the Boone's Ferry and 5<sup>th</sup> St intersection. The final plan retained the existing parking intact to serve the businesses, so bicycles would to use the street.

### C. Basalt Creek Concept Plan Update (Bateschell)

Miranda Bateschell, Long-Range Planning Manager, noted the Staff report and PowerPoint she had presented to City Council on March 20, 2017 were included in the packet. She overviewed the Basalt Creek Concept Plan Update via PowerPoint dated April 12, 2017, summarizing the presentation provided to Council on a recent Tualatin City Council worksession on the Concept Plan during which that council directed Tualatin's staff to change Tualatin's side of the jurisdictional boundary from manufacturing park to residential on the Concept Plan's Land Use Map.

Key comments and responses to Commissioner questions was as follows:

- Ms. Bateschell indicated the location and route of the Basalt Creek Parkway, which was elevated above the railroad but was at grade at Grahams Ferry Rd. Significant grading would be required to maintain a maximum 6 percent grade and the Parkway would be elevated above the canyon, which was about 60 ft deep.
- Having some sort of a transitional buffer from Wilsonville's light industrial to Tualatin's low density residential was the reason for the manufacturing park designation originally. Other iterations showed a transitional office park designation. The intent was to create a better transition, but that was not seen in the last proposal from Tualatin.
- It seemed Tualatin was using its need for industrial land as a wedge until they got the boundary it wanted and then changed their mind about the land use. This also seemed counter to Metro's intention for the entire area.
- Historically, other UGB areas had shifted away from employment uses, which was challenging from a regional perspective, so Metro wanted to avoid that. Metro supported Wilsonville's work on the Development Feasibility Analysis. Wilsonville's City Council shared many of the same concerns as the Commission about the process and believed the boundary conversation was still open since Tualatin had not stayed with the original discussion about the need for employment land. If the two cities could not agree on the boundary, Metro was the final boundary authority.
- The site plan on the left side of Slide 9 was presented by a representative of the property owner from Otak and showed a small amount of commercial versus residential. The entire footprint was much more intensive than the original footprint.
- Up until the Otak site plan was proposed, a manufacturing park designation was still on the land. Tualatin had concerns about it being an employment area because there was too much rock and slope for employment and wanted to do residential.
- Staff recently received an electronic packet, which could be forwarded to the Commission, that included the site plan and a letter from the property owner's representative, along with letters from developers and

engineers reaffirming the steep slopes and basalt in the area, and the area did not appear to be most suitable for employment uses. However, Ms. Bateschell had not seen any further analysis. The bedrock analysis done last year when the City was exploring information for the sanitary sewer system showed the bedrock depth could vary widely within a short distance. That information was provided to the consultant team, who would visit the site to determine feasibility. At this point, the City would look at a range of different employment types and buildings, and what might be feasible on the site, realistically, in the future.

- Staff would discuss next steps with City Council on April 17<sup>th</sup> and hold a work session with Council on the initial Development Feasibility Report on May 1<sup>st</sup>.

#### D. 2017 Planning Commission Work Program

Miranda Bateschell confirmed the joint work session with Council on the Town Center Plan would be May 15<sup>th</sup>, 2017. The Existing Conditions Report to be discussed, but the important items would be the Vision and Goals. The Commission would receive a report on all the public comment received and how it shaped the draft Vision and Goals that would be presented for discussion.

- She noted almost 1,000 responses were received on the first survey, which was the best response rate on any City surveys to date. The consultants were expected to present the data to Staff by the end of the week.
- The “Question of the Month” was continuing and many responses were being received from the community. Responses to last month’s question, “What, if anything, keeps you from spending more time in Town Center?” was tied between not having enough restaurants or shops. This month’s question asked what type of restaurants would provide the variety desired, and next month’s question would be about shops, which would also address scale and building types to plan for.
- She agreed to look into issues reported with the Question of the Month.

Commissioner Springall stated he would be unable to attend the Town Center joint work session.

#### IV. ADJOURNMENT

Chair Greenfield adjourned the regular meeting of the Wilsonville Planning Commission at 8:55 p.m.

Respectfully submitted,

By Paula Pinyerd of ABC Transcription Services, Inc. for  
Tami Bergeron, Administrative Assistant - Planning



**PLANNING COMMISSION**  
**WEDNESDAY, MAY 10, 2017**

- II. LEGISLATIVE PUBLIC HEARING**
  - A. Transit Master Plan (Brashear) (90 Minutes)



## PLANNING COMMISSION STAFF REPORT

|  |   |   |  |
|--|---|---|--|
| <b>Meeting Date:</b> May 10, 2017  |   | <b>Subject:</b> Approval of Transit Master Plan   |  |
|  |   | <b>Staff Member:</b> Dwight Brashear, Transit Director<br><b>Department:</b> South Metro Area Regional Transit  |  |
| <b>Action Required</b>   |   | <b>Advisory Board/Commission Recommendation</b>   |  |
| <input checked="" type="checkbox"/> Motion<br><input type="checkbox"/> Public Hearing Date:<br><input type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date:<br><input type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date:<br><input checked="" type="checkbox"/> Resolution<br><input type="checkbox"/> Information or Direction<br><input type="checkbox"/> Information Only<br><input type="checkbox"/> Council Direction<br><input type="checkbox"/> Consent Agenda |   | <input checked="" type="checkbox"/> Approval<br><input type="checkbox"/> Denial<br><input type="checkbox"/> None Forwarded<br><input type="checkbox"/> Not Applicable |  |
|  |   | <b>Comments:</b> Approve Resolution LP17-003 and recommend to Council the adoption of the Transit Master Plan.  |  |
| <b>Staff Recommendation:</b> Staff recommends that the Planning Commission approve resolution LP17-0003 and recommend adoption of the Transit Master Plan to City Council.   |   |   |  |
| <b>Recommended Language for Motion:</b> I move that the Planning Commission adopt resolution LP17-003 and recommend approval of the Transit Master Plan to City Council.   |   |   |  |
| <b>Project / Issue Relates To:</b> <i>[Identify which goal(s), master plans(s) your issue relates to.]</i>   |   |   |  |
| <input type="checkbox"/> Commission Goals/Priorities   | <input checked="" type="checkbox"/> Adopted Master Plan(s)<br>Transit Master Plan | <input type="checkbox"/> Not Applicable   |  |

**ISSUE BEFORE COMMISSION:** In 2007, the City of Wilsonville adopted a Transit Master Plan (TMP) that enhanced existing service and updated chapters of the Transportation System Plan. Ten years later, the City's population has grown to almost 23,000 residents and employs roughly 20,000. Beginning in 2015, South Metro Area Regional Transit (SMART) staff and project consultants led an extensive outreach process and route analysis to recommend services

that are better tailored for today's businesses and residents. The SMART system provides transportation options within Wilsonville, to Salem, Canby, and north towards Portland.

**EXECUTIVE SUMMARY:**

The purpose of the Transit Master Plan document, as formulated by the Transit Master Plan Citizen Task Force is to provide a roadmap “to providing convenient, safe, reliable, efficient, fiscally and environmentally responsible and friendly transportation services to anyone who wants or needs those services in Wilsonville. The provision of SMART services helps to attract and retain businesses. SMART is a valued community asset that enhances the quality of life for everyone who lives, works, or visits Wilsonville”. The 2017 TMP aims to make route adjustments to better fit the needs of businesses, residents, and visitors of Wilsonville.

The route changes proposed in the TMP are cost-neutral and focus on more frequent and efficient service. The main changes proposed are:

- Switch Route 2X service from Barbur Transit Center to Tigard Transit Center;
- Divide Route 3 so that there is a 3X commuter route to Canby and separate Charbonneau Shuttle;
- Streamline Route 4 and expand evening/Saturday service;
- Route 7 reconfiguration.

**EXPECTED RESULTS:** The adoption of the Transit Master Plan will create a transit roadmap, guiding future decisions while helping the City of Wilsonville to create and maintain a sustainable public transit network.

**TIMELINE:** City Council will hold a public hearing for the Transit Master Plan on June 5, 2017. A City Council adoption date is slated for June 19, 2017.

**CURRENT YEAR BUDGET IMPACTS:** The development of the 2017 Transit Master Plan has been mainly funded by federal and state grants. The recommended route changes in the Transit Master Plan are cost-neutral.

**FINANCIAL REVIEW / COMMENTS:**

Reviewed by:      Date:

**LEGAL REVIEW / COMMENT:**

Reviewed by:      Date:

**COMMUNITY INVOLVEMENT PROCESS:**

To ensure this document update represented the diverse interests of the Wilsonville community, the Transit Master Plan had an extensive and inclusive public engagement process. Outreach efforts were tailored to reach people in practical and convenient ways as to reflect the opinions from the wide spectrum of current and potential system users, the business community, and residents. SMART has completed two rounds of public outreach in which transit riders, local

residents and payroll taxpayers have been invited to share their opinions. A citizen Task Force has worked with SMART staff and consultants for more than a year on this planning process.

SMART staff and outreach consultants provided electronic updates to an interested parties list, published articles in the Boones Ferry Messenger and Wilsonville Spokesman, and update project and City website information to keep interested parties informed and up-to-date. SMART is currently in its final round of outreach by publishing the draft TMP online and in public spaces open for public review until May 24<sup>th</sup>, before the City Council Public Hearing on June 5<sup>th</sup> 2017.

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY** (businesses, neighborhoods, protected and other groups): When implemented, the new plan is expected to improve efficiencies and to reduce traffic congestion by providing commuters an alternative to travel in single-occupant vehicles. Also, adoption of an updated Transit Master Plan may open new avenues of opportunity relative to grant funding for the entire City of Wilsonville.

**ALTERNATIVES:**

**CITY MANAGER COMMENT:**

**ATTACHMENTS:**

- A. Planning Commission Resolution No. LP17-003
- B. Draft Ordinance for City Council
- C. TMP Record Index



# ATTACHMENT A

## PLANNING COMMISSION RESOLUTION NO. LP17-0003

### **A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF WILSONVILLE RECOMMENDING THE CITY COUNCIL ADOPT A 2017 TRANSIT MASTER PLAN FOR THE CITY OF WILSONVILLE.**

WHEREAS, the Planning Commission of the City of Wilsonville (“City”) has the authority to review and make recommendations to the City Council regarding changes to, or adoption of new elements and sub-elements of, the Comprehensive Plan pursuant to Sections 2.322 and 4.032 of the Wilsonville Code (“WC”); and

WHEREAS, the proposed Transit Master Plan is a support document to the City’s Transportation Systems Plan, which is a support document to the City’s Comprehensive Plan, and the Transit Master Plan is subject to the same rules and regulations as an amendment to the City’s Comprehensive Plan; and

WHEREAS, the Planning Director submitted a proposed Ordinance to the Planning Commission, along with a Staff Report, in accordance with the public hearing and notice procedures that are set forth in WC 4.008, 4.011, 4.012, and 4.198; and

WHEREAS, the Planning Commission conducted a work session on December 22, 2016, a legislative hearing on March 8<sup>th</sup>, 2017 and after providing the required public notice, held a public hearing on May 10, 2017 to review the proposed Transit Master Plan and to gather additional testimony and evidence regarding the Transit Master Plan; and

WHEREAS, the Planning Commission has afforded all interested parties an opportunity to be heard on this subject and has entered all available evidence and testimony into the public record of its proceeding; and

WHEREAS, the Planning Commission has duly considered the subject, including the staff recommendations and all the exhibits and testimony introduced and offered by all interested parties; and

NOW, THEREFORE, BE IT RESOLVED that the Wilsonville Planning Commission does hereby adopt the Staff Report and its attachments (attached hereto as Exhibit A), as presented at the May 10, 2017 public hearing, including the findings and recommendations contained therein, and further recommends the Wilsonville City Council approve and adopt the Transit Master Plan as hereby approved by the Planning Commission; and

BE IT FURTHER RESOLVED that this Resolution shall be effective upon adoption.

ADOPTED by the City of Wilsonville Planning Commission at a regular meeting thereof this 10<sup>th</sup> day of May 2017, and filed with the Planning Administrative Assistant on this date.

---

Wilsonville Planning Commission

ATTEST:

---

Tami Bergeron, Planning Administrative Assistant

SUMMARY OF VOTES:

Chair Greenfield: \_\_\_\_\_  
Commissioner Postma: \_\_\_\_\_  
Commissioner Hurley: \_\_\_\_\_  
Commissioner Levit: \_\_\_\_\_  
Commissioner Mesbah: \_\_\_\_\_  
Commissioner Millan: \_\_\_\_\_  
Commissioner Springall: \_\_\_\_\_

Attachments:

Exhibit A – Staff Report with attachments

# ATTACHMENT B

## ORDINANCE NO. LP17-0003

### **AN ORDINANCE OF THE CITY OF WILSONVILLE ADOPTING AN UPDATED TRANSIT MASTER PLAN AS A SUB-ELEMENT OF THE TRANSPORTATION SYSTEM PLAN, REPLACING ALL PRIOR TRANSIT MASTER PLANS, AND REPEALING ORDINANCE NO. 653**

WHEREAS, the City of Wilsonville (City) currently has a 2008 Transit Master Plan that was adopted by City Council (Ordinance No. 653) on July 7, 2008; and

WHEREAS, ORS 197.175 requires city to prepare, adopt, and implement Comprehensive Plans consistent with statewide planning goals adopted by the Land Conservation and Development Commission; and

WHEREAS, ORS 197.712(2)(e) requires cities to develop and adopt a public facilities plan for areas within the Urban Growth Boundary containing a population greater than 2,500 persons, including rough cost estimates for projects needed to provide sewer, water and transportation uses contemplated in the Comprehensive Plan and Land Use Regulations; and

WHEREAS, the Transit Master Plan is a sub-element of the Transportation System Plan, which is a sub-element of the Comprehensive Plan; and

WHEREAS, an updated Transit Master Plan is needed to account for growth, rising transit costs, and future development; and

WHEREAS, in developing the updated Transit Master Plan, the City has sought to carry out federal, state, and regional mandates, provide for alternative cost-saving and funding solutions to minimize public expense, enhance efficiencies in transit routes, and maintain and potentially expand transit services; and

WHEREAS, the updated Transit Master Plan documents current routes, ridership, costs, and revenue, evaluates current transit deficiencies, estimates future transit demands, and estimates the capital and operation costs needed to meet these future demands; and

WHEREAS, staff of the City's Transit Department, which operates as South Metro Area Regional Transit (SMART), has considered a range of possible financial scenarios and put forward a Master Plan with different alternative level of transit service, depending on available funding; and

WHEREAS, following the timely mailing and publication of required notice, the Planning Commission conducted a public hearing on May 10, 2017, wherein the Commission received public testimony, staff reports and input, and exhibits, and thereafter deliberated and voted to approve Resolution No. LP17-0003 recommending to the City Council the approval of the proposed Transit Master Plan for the City of Wilsonville; and

WHEREAS, a copy of the record of the aforementioned Planning Commission action and recommendation is marked Exhibit A, attached hereto and incorporated by reference herein; and

WHEREAS, following the Planning Commission public hearing, the Planning Director forwarded the recommended Transit Master Plan onto the City Council, along with a staff report and attachments, in accordance with public hearing and notice procedures that are set forth in Sections 4.008, 4.011, 4.012, and 4.198 of the Wilsonville Code (WC); and

WHEREAS, the City Council, after public hearing notices were provided to over \_\_\_\_\_ property owners, a list of interested agencies, emailed to over 1,200 people, and posted in several locations throughout the City and on the City website, held a public hearing on June 5, 2017 to review the proposed Transit Master Plan, and to gather additional testimony and evidence regarding the proposed Transit Master Plan; and

WHEREAS, the City Council has afforded all interested parties an opportunity to be heard on this subject and has entered all available evidence and testimony into the public record of its proceeding; and

WHEREAS, the City Council has duly considered the subject, including the Planning Commission recommendations and all the exhibits and testimony introduced and offered by all interested parties.

NOW, THEREFORE, THE CITY OF WILSONVILLE ORDAINS AS FOLLOWS:

1. FINDINGS.

The above-recited findings are adopted and incorporated by reference herein as findings and conclusions of Resolution No. LP17-003, which includes the staff report. The City Council further finds and concludes that the adoption of the proposed Transit Master Plan is necessary to help protect the public health, safety,

and welfare of the municipality by planning that will help ensure there will continue to be adequate transit services within the City's transportation system.

2. DETERMINATION.

Based on such findings, the City Council hereby adopts the Transit Master Plan, attached hereto and marked as B, and incorporated by reference as if fully set forth herein, which shall replace and supersede all prior Transit Master Plans adopted by ordinance, resolution, or motion. Ordinance No. 653 is hereby repealed.

3. EFFECTIVE DATE OF ORDINANCE.

This Ordinance shall be declared to be in full force and effect thirty (30) days from the date of final passage and approval.

SUBMITTED to the Wilsonville City Council and read for the first time at a meeting thereof on the \_\_\_\_ day of \_\_\_\_\_, 2017, and scheduled for second reading on \_\_\_\_\_, commencing at the hour of 7 p.m. at the Wilsonville City Hall, 29799 SW Town Center Loop East, Wilsonville, Oregon.

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

ENACTED by the City Council on the \_\_\_\_ day of \_\_\_\_\_, 2017, by the following votes: Yes: \_\_\_\_ No: \_\_\_\_

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

DATED and signed by the Mayor this \_\_\_\_ day of \_\_\_\_\_, 2017.

\_\_\_\_\_  
TIM KNAPP, MAYOR

SUMMARY OF VOTES:

Mayor Knapp

Council President Starr

Councilor Stevens

Councilor Lehan

Councilor Akervall

Attachments:

Exhibit A – Planning Commission Resolution and Record (including staff report)

Exhibit B – Transit Master Plan

DRAFT

# ATTACHMENT C

## TRANSIT MASTER PLAN

### RECORD INDEX

Transit Master Plan Records can be accessed <http://www.ci.wilsonville.or.us/AgendaCenter>

Planning Commission Agenda for May 10, 2017

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| EXHIBIT B | FINDINGS REPORT                     |
| EXHIBIT C | BOONES FERRY MESSENGER ARTICLES     |
| EXHIBIT D | ELECTRONIC UPDATES                  |
| EXHIBIT E | SPOKESMAN NOTICES                   |
| EXHIBIT F | PUBLIC COMMENTS ON DRAFT TMP        |
| EXHIBIT G | PUBLIC COMMENTS FROM SURVEYS        |
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# Transit Master Plan

2017



WILSONVILLE  
OREGON

**DRAFT**



**SMART**  
SOUTH METRO AREA REGIONAL TRANSIT



- CONVENIENCE
- SAFETY
- RELIABILITY
- EFFICIENCY
- FISCAL RESPONSIBILITY
- FRIENDLY SERVICE
- EQUITY & ACCESS
- ENVIRONMENTAL RESPONSIBILITY





**Please note that this is a REVISED DRAFT as of May 2<sup>nd</sup>, 2017**

This is the **DRAFT** 2017 Transit Master Plan for the City of Wilsonville. It will remain in **DRAFT** form until adopted by the City Council, after public hearings and deliberation, which means that you can comment on the draft plan and your comments may lead to changes before the plan is finally adopted.

You can make oral comments at any TMP public meetings, which can be found on the TMP project website, [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com). All written comments must be submitted by May 24<sup>th</sup> which can also be completed through the TMP project website. Those comments will be passed along to Planning Commission and City Council for review.

We are grateful for your interest and we welcome your participation.

## Acknowledgements

The City of Wilsonville would like to acknowledge the following for their dedication to the development of this Transit Master Plan. Their insight and outlook toward the future of this City helped create a comprehensive plan that represents the needs of employers, residents and visitors of Wilsonville.

### Transit Master Plan Task Force

Julie Fitzgerald, Chair\*  
Kristin Akervall  
Caroline Berry  
Paul Diller  
Lynnda Hale  
Barb Leisy  
Peter Rapley  
Pat Rehberg  
Jean Tsokos  
Stephanie Yager

### City Council

Tim Knapp, Mayor  
Scott Star, President  
Kristin Akervall  
Charlotte Lehan  
Susie Stevens  
Julie Fitzgerald\*

### Consultants

Jarrett Walker  
Michelle Poyourow  
Christian L Watchie  
Ellen Teninty  
Shannon Ladner

### Planning Commission

Jerry Greenfield, Chair  
Eric Postma, Vice Chair  
Al Levit  
Phyllis Millan  
Peter Hurley  
Simon Springall  
Kamran Mesbah

### City Staff

Dwight Brashear, Transit Director  
Eric Loomis, Operations Manager  
Scott Simonton, Fleet Manager  
Gregg Johansen, Transit Field Supervisor  
Patrick Edwards, Transit Field Supervisor  
Nicole Hendrix, Transit Program Coordinator  
Michelle Marston, Transit Program Coordinator  
Brad Dillingham, Transit Planning Intern  
Keith Katko, Finance Operations Manager  
Susan Cole, Finance Director  
Chris Neamtzu, Planning Director  
Charlie Tso, Assistant Planner  
Tami Bergeron, Planning Administration Assistant  
Amanda Guile-Hinman, Assistant City Attorney  
Stephan Lashbrook, Transit Director\*  
Jen Massa Smith, Program Manager\*\*  
Steve Allen, Operations Manager\*

\*Retired

\*\*Former staff

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# Executive Summary

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Public transit has always been at the forefront of shaping the personality and image of some of this country's greatest cities. The first and most notable modes of public transit include the Staten Island Ferry, which began operating in 1817 and remains operational to this day, the cable cars of San Francisco, the New York subway system and the elevated train in Chicago, affectionately referred to as the "L", which began operating in 1895 and is considered this country's first rapid transit system. So what does all this mean for the City of Wilsonville and what role will the Transit Master Plan play in helping to shape the City of Wilsonville's future? These questions are both relevant and timely, and they are questions that the Transit Master Plan seeks to address in specific detail.

The City of Wilsonville, like most thriving cities and not at all surprisingly, finds itself at a bit of a crossroads. Rapid growth coupled with a desire to maintain a high quality of life for its residents, commuters and visitors has delivered the City of Wilsonville to an intersection; the very same intersection that has produced many great cities. It is important to stress that a crossroad in and of itself is not necessarily a situation to be feared. However, deciding which direction to travel, how that travel will be accomplished and when traveling will be most advantageous to make for a successful journey are all critical components of the directional decision making process. The good news is that the City of Wilsonville was and continues to be forward thinking. The magnificent architects of the City's design knew that developing, maintaining and providing mobility options was/is crucial to the sustainable success of any vibrant city, of which the City of Wilsonville unquestionably qualifies.

Convinced that "necessity is the mother invention" the leaders of the City of Wilsonville created the South Metro Area Regional Transit (SMART). SMART was developed with the mission of providing safe, reliable and cost effective public transit, and after almost three decades of proudly delivering on this mission, SMART finds itself in good health, well liked, well respected, and most importantly, ready to face the future.

So where does SMART go from here? The Transit Master Plan holds the key. Developed by the SMART team with valuable input and cooperation from City staff, a citizen task force, the public, the Mayor and City Council, the Planning Commission and the business community by way of the Wilsonville Area Chamber of Commerce, the Transit Master Plan will serve as a beacon of hope and promise, guiding our decisions while ensuring that SMART always remains true its core mission. To that end, SMART is pleased to present its new multiyear Transit Master Plan. This comprehensive plan is a view into the future and is dedicated to all those who live, work, play or do business in the City of Wilsonville. We have taken great care to ensure that everyone wishing to be heard was in fact heard. Furthermore, we believe it to be imperative that the Transit Master Plan address the needs of all, inclusive of individuals and businesses. SMART understands and appreciates that it is the local business community that makes it possible for SMART to provide mobility options to thousands annually. SMART also understands

and believes that our transit goals can only be achieved through a collaborative effort between all that desire the City of Wilsonville to take its rightful place as one of this country's most livable cities.

In closing, it is our sincere hope that the reader will find the Transit Master Plan to be lucid and well organized. In addition, we are mindful that the only consistent thing in life is change, so we welcome and encourage your input, feedback and questions, not only today or tomorrow but each and every day as we seek to deliver on our promise to all of you.

Respectfully,

SMART

DRAFT

# Introduction

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## Overview

### South Metro Area Regional Transit

Situated on Interstate-5 between Portland and Salem, Wilsonville serves as a transportation hub and has established itself as an important employment center in the Portland metropolitan area. With employment figures almost equal to the City's population, large numbers of employees commute to jobs in Wilsonville from Portland, Salem, Sherwood, Gresham, Vancouver, and many other locations. To aid in the movement of Wilsonville's employees, residents and visitors South Metro Area Regional Transit (SMART), a department of the City of Wilsonville, provides fixed-route service within the City of Wilsonville and connecting service to Canby, Tualatin, Salem, and Portland. SMART also provides an extensive demand-response system (Dial-a-Ride) with priority to ADA-qualified riders, transporting elderly and disabled riders to out-of-town medical appointments.

In addition to fixed-route service, SMART provides businesses, residents and visitors of Wilsonville with the resources to participate in various transportation options such as vanpooling, carpooling, bicycling, walking, and telework through the SMART Options Program.



**THINK SMART OPTIONS**

This program promotes a robust set of travel options to provide the freedom and choice for people to travel while reducing the amount of single-occupancy vehicles on the road.

In order to ensure equitable access, coordinated transit services and enhance regional mobility of transit programs and projects, SMART works cooperatively with state and regional partners, including Oregon Department of Transportation (ODOT), TriMet, Cherriots, Canby Area Transit (CAT), Multnomah, Clackamas and Washington Counties, and Oregon Metro. The value of SMART's services is greatly increased by being well connected with other transportation networks which include neighboring transit systems and bicycle and pedestrian networks. These connections increase the level of choice and overall mobility for people in and around Wilsonville, whether they are traveling to jobs, school, shopping, parks, or recreational events.

## SMART History

Prior to 1989, transit service in Wilsonville was provided by the Tri-County Metropolitan Transportation District of Oregon (TriMet) with one route that operated during peak commute hours. In 1988, the Wilsonville Innovative Transportation Association was formed to look at alternative methods of providing transit service at a better cost/benefit ratio for local businesses. The City of Wilsonville successfully petitioned to withdraw from TriMet's service district, effective January 1<sup>st</sup>, 1989.

For the first two years, the City contracted with Bucks Ambulance Service to provide rides on a demand response basis. In 1991, Wilsonville began to provide demand-response service on its own. Wilsonville also contracted with TriMet to extend Route 96 into Wilsonville, providing Wilsonville residents and employees with transportation between Wilsonville and Portland. The City decided to change its name from Wilsonville Area Rapid Transit (WART) to South Metro Area Rapid Transit (SMART) in 1993 and to South Metro Area Regional Transit in 2007. 1993 was also the year when SMART began providing service to Tualatin Park & Ride, Barbur Transit Center, and Oregon City Transit Center. The Oregon City service was discontinued in 2002 due to low ridership and resources were reallocated to improve services to Portland.

In 1994, SMART began providing the in-town service that had previously been contracted service. Ridership increased dramatically between 1994 and 1995 and new routes were introduced in 1995, 1996, 1998, 2003, 2009 and 2014.

Today, SMART operates seven fixed-routes as well as Dial-a-Ride, non-emergency medical trips, and special shuttle services for older adults and people with disabilities. These services are primarily supported by a Wilsonville payroll tax and grant funding. SMART's payroll tax rate is currently 0.5000 percent and was last increased in October 2008 to help fund service connections with TriMet's commuter rail. Please see the chapter on Funding Sources and Expenditures for SMART's payroll tax history as compared to other transit agencies.

## Transit Master Plan

The Transit Master Plan (TMP) provides a broad look ahead to the type of transit system and supportive transportation options required to meet Wilsonville's mobility needs. This is accomplished by providing proposals for improved transit service as well as strategies to reduce single-occupancy vehicles. With its combined transit and transportation options approaches, the TMP will guide future decision-making for SMART for the next five to seven years.



The development of this plan relied heavily on members of a TMP Task Force; an advisory committee of residents, transit riders, and community organizations who served as a sounding board during the public involvement and update processes. The Task Force proposed the following mission statement and goals for the TMP process:

*To provide convenient, safe, reliable, efficient, fiscally and environmentally responsible and friendly transportation services to anyone who wants or needs those services in Wilsonville. SMART services help to attract and retain businesses. SMART is a valued community asset that enhances the quality of life for everyone who lives, works in, or visits Wilsonville.*

**CONVENIENCE.** SMART provides services that are convenient, comfortable and easy to use.

**SAFETY.** SMART provides a safe and secure environment for our customers, community and employees through maintenance, training, design, enforcement and the allocation of resources.

**RELIABILITY.** Coordinated and reliable, the Wilsonville community depends on SMART to meet mobility needs.

**EFFICIENCY.** SMART's services are provided in an efficient and cost-effective manner.

**FISCAL RESPONSIBILITY.** The SMART system is fiscally responsible to the community.

**FRIENDLY SERVICE.** SMART provides friendly services that add value to Wilsonville's quality of life.

**EQUITY & ACCESS.** SMART services are accessible and they are provided equitably throughout the Wilsonville community.

**ENVIRONMENTAL RESPONSIBILITY.** SMART conducts its operations with regard for long-term environmental consequences.

For SMART to provide residents, employers, employees, and visitors of Wilsonville the service described in the TMP mission and goals, it must be part of an integrated transportation system that considers land use, fixed-route buses, demand-response service, taxis, carpools, vanpools, employer shuttles, bicycles, pedestrians, and innovative strategies to encourage non-single occupancy vehicle travel.

The planning context and policy framework for the Transit Master Plan is dependent upon a variety of local, regional and state plans, policies and regulations; such as the Oregon Transportation Plan (OTP), the Regional Transportation Plan (RTP), Americans with Disabilities Act (ADA), and the Tri-County Coordinated Transportation Plan for Seniors and Persons with Disabilities (CTP). These documents help determine and shape how growth will occur in Wilsonville and what key policies, goals and principles should be considered. Overall, these goals and objectives contribute to providing safe, efficient, accessible, and equitable transportation; providing mobility to people of all ages and incomes; and reducing air pollution, greenhouse gas production, energy use, and traffic congestion. This plan and SMART service aims to further the implementation of the City's Transportation System Plan, a subset of the City Comprehensive Plan. To learn more about these policies and plans, please see Appendix D.



# SMART Existing Services

## SMART Capital Facilities & Fleet

SMART Central Station is Wilsonville's transit hub located on SW Barber Street in Wilsonville. It was built and completed as part of TriMet's Wilsonville WES Station in 2009. The hub includes the WES station, Park & Ride facility, a 12-bay bus transit center, 48 bicycle lockers and a bike repair station to allow for multi-modal trips. SMART Central also features a metal and glass clock tower that has a security and sound system. To allow for quick and convenient transfers, SMART routes are timed to pulse with WES arrivals and departures.

Wilsonville SMART Administration and Fleet Maintenance facility is located at 28879 SW Boberg Road. This building houses the SMART Fleet, the City/SMART Maintenance and Transit Operations Divisions.

The Administration and Fleet Maintenance building was completed in 2013 with guidance from the Mayor and City Council. This state-of-the-art facility was constructed without incurring any debt and includes sustainable features to increase efficiency and reduce environmental impacts. The facility also includes a Compressed Natural Gas (CNG) fueling station for SMART's CNG buses.



### *SMART Transit Fleet*

SMART maintains a wheelchair-accessible fleet of 30 active revenue vehicles. The active fleet consists of a wide range of vehicles including everything from small cutaways used in Dial-a-Ride service to heavy duty 40' buses, used primarily on inter-city routes. SMART fleet also includes a trolley utilized for community events and local service in the warmer months. In addition to these active vehicles, SMART maintains a small contingency fleet of older vehicles that are only used in case of emergency.

### *Bus Stops & Amenities*

SMART currently has more than 150 bus stops within the City of Wilsonville. Some stops are primarily poles and signs while others have full amenities such as; benches, shelters, schedule displays, lights and garbage cans.

## Fixed-Route Service

The current SMART fixed-route system consists of seven routes; two commuter express routes and five local routes serving the community of Wilsonville and surrounding areas. SMART operates its full services Monday through Friday from 5:00 a.m. to 9:15 p.m. and reduced service on two of its routes on Saturdays from 8:30 a.m. to 5:30 p.m.

Routes typically begin and end their runs at SMART Central. SMART Central Station offers intermodal connections between SMART buses and TriMet’s Westside Commuter Rail (WES) and Salem Cherriots. Several major transfer locations outside of Wilsonville allow for transfers from SMART to other providers, including TriMet (Tualatin Park & Ride and Barbur Transit Center), Canby Area Transit (CAT) and Cherriots. In addition, SMART also connects with TriMet Route 96 at Commerce Circle. This TriMet route offers service from Wilsonville to Portland averaging every 30 minutes during peak commute times.

SMART service is free for trips within Wilsonville, including both fixed-route and Dial-a-Ride service. Inter-city service and connections to other transit providers requires a fare payment.

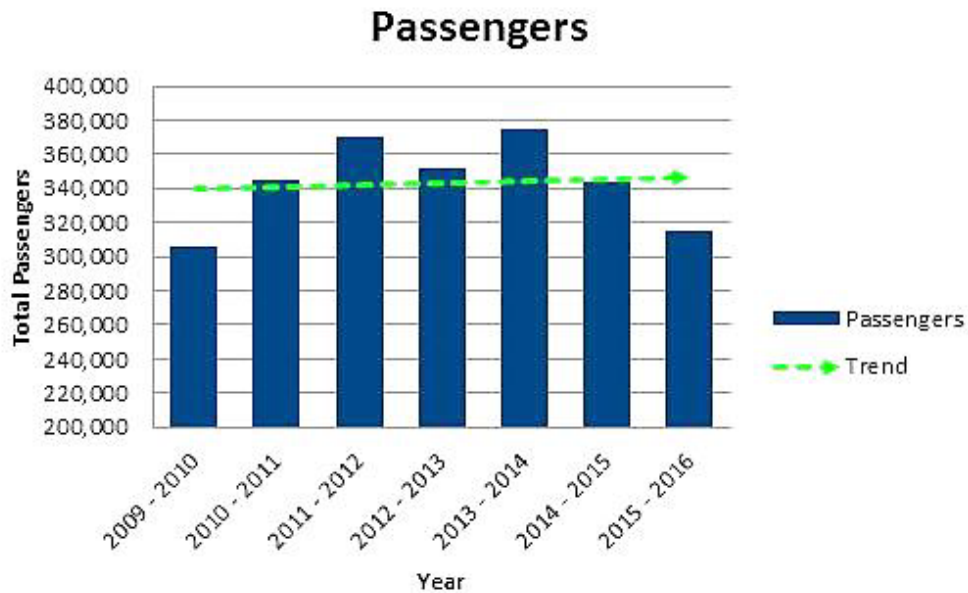
The table below summarizes fixed-route fares and passes for riding SMART routes. Riders can pay cash or purchase a monthly pass for either the 1X or the 2X/3 routes. An “All Pass” allows use of all inter-city routes (1X, 2X, and 3). Reduced-price passes for seniors, people with disabilities, students, and youth must be purchased in person at Wilsonville City Hall. SMART does not accept transfer receipts from other transit providers (other than Cherriots passes on the shared Route 1X) and other providers do not provide a discounted fare for passengers transferring from SMART services.

**Fixed-Route Fares & Passes**

| Fare Category           | Fare Type                       | 1X<br>(Salem)         | 2X (Portland)<br>3 (Canby) | 4,5,6,7<br>(Wilsonville) |
|-------------------------|---------------------------------|-----------------------|----------------------------|--------------------------|
| Adult <sup>1</sup>      | Single Ride (cash)              | \$3.00                | \$1.50                     | Free                     |
|                         | Monthly Pass                    | \$85.00               | \$35.00                    |                          |
|                         | Monthly “All” Pass <sup>3</sup> | \$120.00              |                            |                          |
|                         | 20-Ride Punch<br>Card/Pass      | Requires 2<br>punches | Requires 2<br>punches      |                          |
|                         | 40-Ride Punch<br>Card/Pass      |                       |                            |                          |
| Discounted <sup>2</sup> | Single Ride (cash)              | \$1.50                | \$0.75                     |                          |
|                         | Monthly Pass (One<br>Route)     | \$42.50               | \$17.50                    |                          |
|                         | Monthly “All” Pass <sup>3</sup> | \$60.00               |                            |                          |

Notes: (1) Adult fares are for persons age 18-59. (2) Discounted fares must be purchased in person at City Hall and are for seniors age 60+, persons with disabilities, Medicare card holders, youth age 5-17, and students 18-23 with valid student ID. (3) An “All” Pass is valid on all intercity routes (1X, 2X, and 3X). Source: SMART website.

Over the past five years, SMART system ridership has varied between 325,000 to 350,000 passenger trips per year. Since 2008, when the last TMP was approved and the WES service changes were put in place, SMART’s service hours have increased by over 35 percent, operating cost saw a 16 percent increase while ridership has seen just over a 4 percent increase. The variation may be due to a variety of factors including service changes, employment or shift time fluctuations or data gathering limitations. As SMART moves to a computerized passenger count system in the next couple years, gathering data will become easier and more accurate.

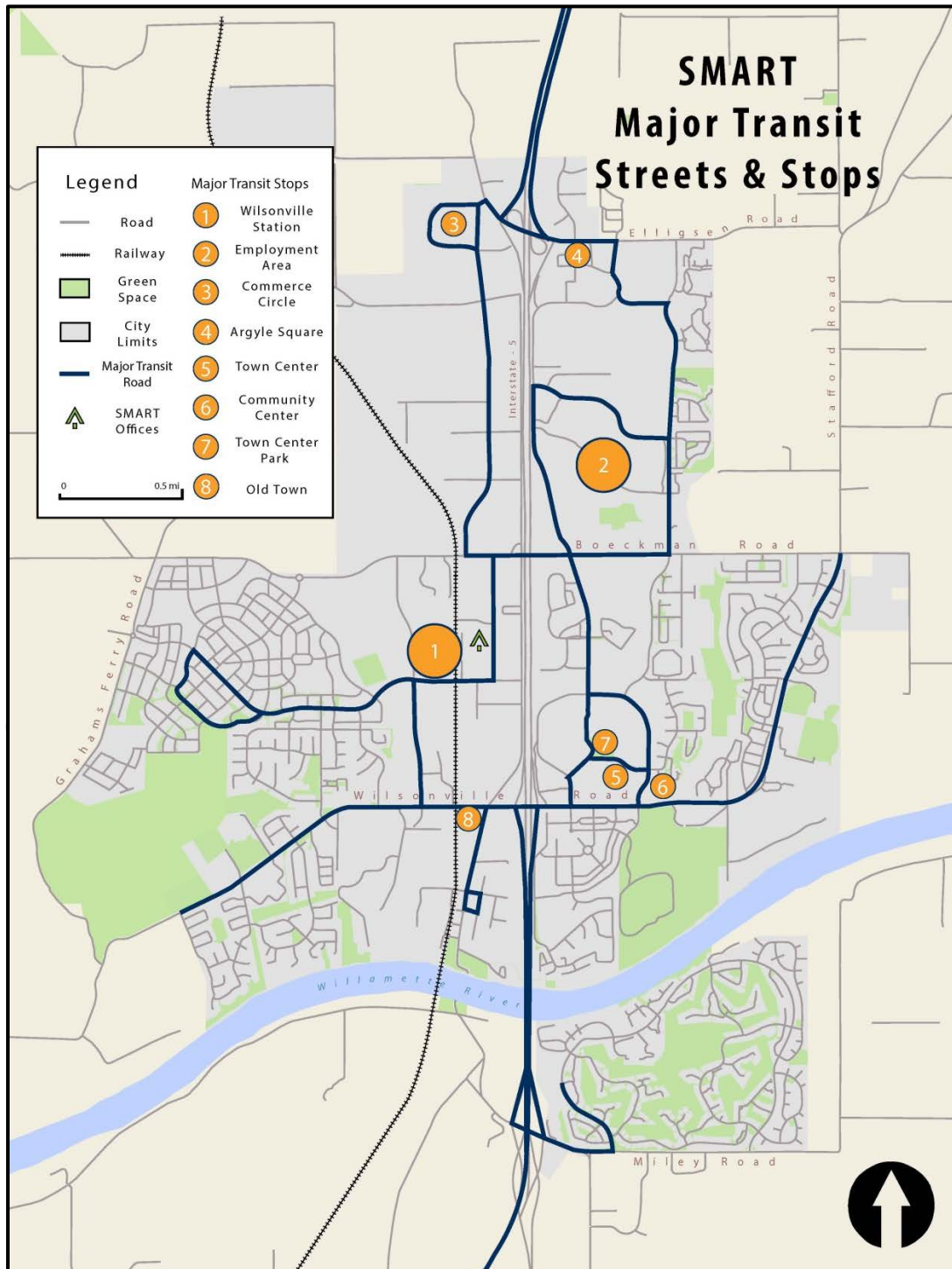


In addition to productivity measurement, SMART is also committed to ensuring that transit services are provided in a fair and balanced manner throughout Wilsonville. To that end, SMART has worked to establish measures that monitor equity issues and ensure compliance with Federal Title VI Civil Rights standards for our fixed-route system. A copy of SMART’s Title VI policy can be found in Appendix C.

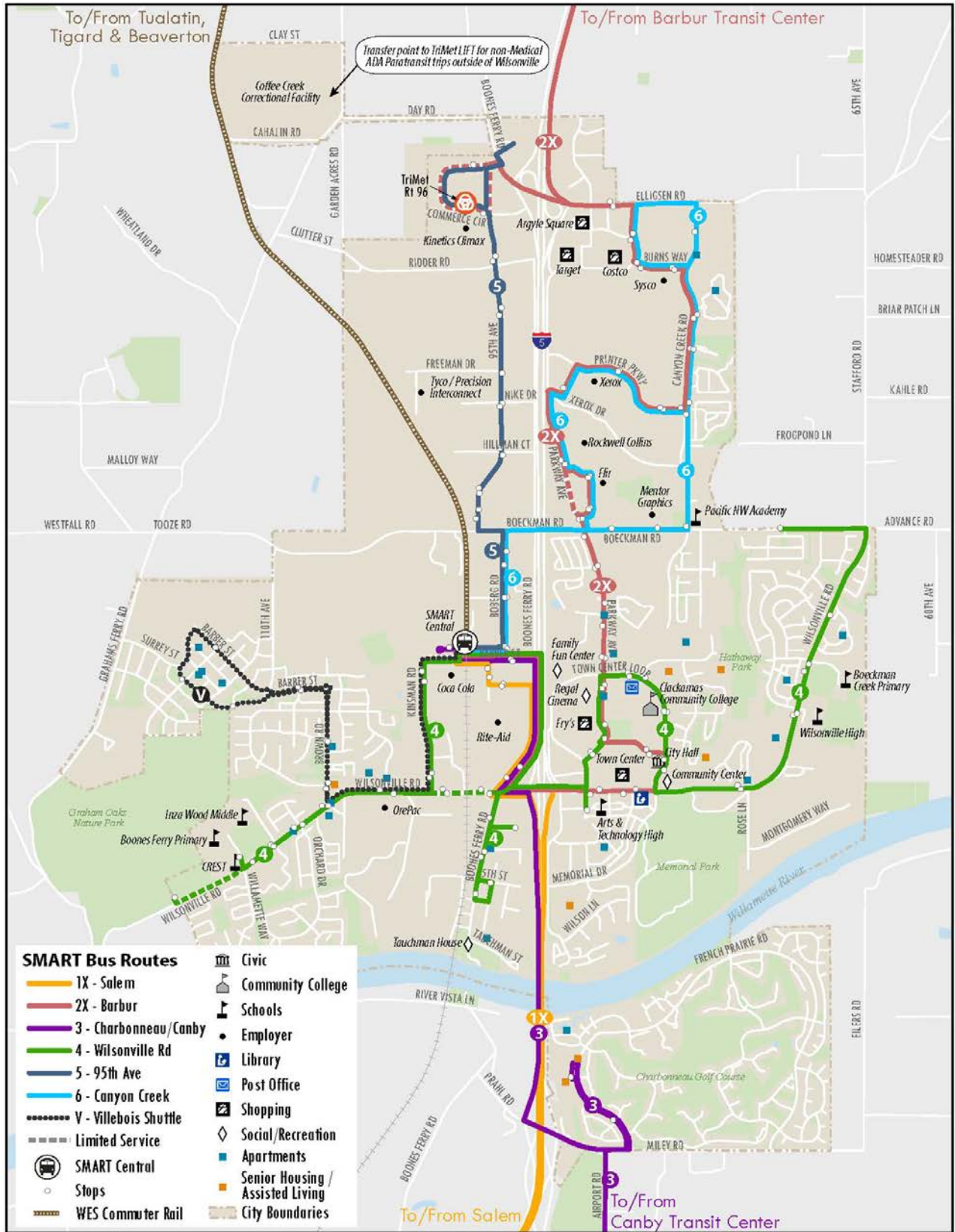
On-time performance is a measure of trips completed as scheduled. A bus is considered on-time if it is within five minutes of the schedule at any time point, late is defined as more than five minutes after the scheduled departure time, and early is defined as anything before the scheduled departure time. SMART has set a standard that at least 85 percent of all trips will run on time for fixed-route service and 90 percent for Dial-a-Ride service.

Service availability and access is a general measure of the distribution of routes within the SMART service area. SMART’s goal is to ensure that 85 percent of City residents live within walking distance (i.e. no more than 1/4 mile) of a bus stop. SMART will continuously make efforts to mirror the growth of the City of Wilsonville with its transportation service so that residents and employers can have equal access to transit.

Major transit streets are a primary corridor for transit, receiving half-hour or better service during peak traffic hours. The designation of major transit streets allows the City to set standards for encouraging land use patterns, development designs, and street and bicycle/pedestrian improvements that support transit. Major stops are those which are located at intersections of two or more bus routes, transfer locations between transit systems, Park & Ride lots, and shopping centers or other major destinations.



## Map of Current Fixed-Route Services



| Route | Service Type | Name                          | Termination(s) / Major Transit Centers   | Other Major Destinations   | Weekday Headway          | Weekday Service Span   | Saturday Headway | Saturday Service Span / |
|-------|--------------|-------------------------------|--|--|--------------------------|--|------------------|-------------------------|
| 1X    | Intercity    | Salem                         | Salem Transit Center   | State Capital  | 30-60 min                | 4:30 a.m.-10:00 a.m.<br>3:30 p.m.-8:00 p.m.<br>No midday service | No Service       | No Service              |
| 2X    | Intercity    | Barbur                        | Tualatin Park & Ride<br>Barbur Blvd Transit Center                                 | Wilsonville City Hall<br>Mentor Graphics<br>Parkway Woods<br>Argyle Square<br>Commerce Circle      | 20-60 min                | 5:00 a.m.-9:45 p.m.  | 60-120 min       | 8:15 a.m. – 5:45 p.m.   |
| 3     | Intercity    | Charbonneau<br>Canby          | Canby Transit Center   | Springridge at<br>Charbonneau  | 60 min                   | 6:00 a.m.-9:15 a.m.<br>3:30 p.m.-7:30 p.m.<br>No midday service  | No Service       | No Service              |
| 4     | Local        | Wilsonville Road<br>East/West | Mentor Graphics<br>Boones Ferry<br>Primary School<br>or Graham Oaks<br>Nature Park | Boeckman School<br>Wilsonville H.S.<br>Civic Center<br>Town Center Loop<br>Inza Wood Middle School | 30-60 min                | 5:00 a.m.-7:45 p.m.  | 60-120 min       | 8:30 a.m.-5:30 p.m.     |
| 5     | Local        | 95 <sup>th</sup> Avenue       | SMART Central  | Commerce Circle  | 30 min                   | 5:30 a.m.-10:15 a.m.<br>3:15 p.m.-7:15 p.m.<br>No midday service | No Service       | No Service              |
| 6     | Local        | Canyon Creek                  | Argyle Square  | Mentor Graphics<br>Parkway Woods<br>Argyle Square  | 30 min                   | 6:30 a.m.-10:05 a.m.<br>3:15 p.m.-7:39 p.m.<br>No midday service | No Service       | No Service              |
| 7     | Local        | Villebois<br>Shopper Shuttle  | Villebois  | Town Center  | 5 a.m. &<br>2 p.m. trips | 6:07 a.m.-6:45 a.m.<br>5:07 p.m.-5:45 p.m.                       | No Service       | No Service              |

## Dial-a-Ride

Dial-a-Ride (DAR) is a door-to-door transportation demand-response service for customers within the City of Wilsonville. SMART is pleased to offer four distinct programs under its Dial-a-Ride service: ADA Complementary Paratransit, General Public, Senior Trips, and Out-of-Town Medical Trips. While ADA eligible customers are given priority scheduling, Wilsonville residents of all ages are welcome to utilize the Dial-a-Ride programs.

SMART proudly offers complementary ADA Paratransit service to individuals who may have difficulties accessing fixed-route services. ADA customers are required to be certified as eligible under the federal ADA guidelines in order to use the service. Reservations can be made up to two weeks in advance and rides are guaranteed during any hours of normal operations.

**Dial-a-Ride Fares**

| Fare Type                          | Local | Intercity |
|------------------------------------|-------|-----------|
| Single Ride<br>(one way)<br>(cash) | Free  | \$3.00    |
| Monthly Pass                       |       | \$50.00   |
| Punch Pass                         | Free  | 2 punches |

Residents of Wilsonville can utilize SMART’s General Public Dial-a-Ride program. Under this program, customers are required to book their ride a day in advance of their desired day of travel. This DAR program is designed to supplement fixed-route service and should be used sparingly to reach areas within Wilsonville that are not frequently served by other transportation modes. Although SMART is pleased to offer DAR services, customers are encouraged to utilize the fixed-route service and travel training program whenever possible.

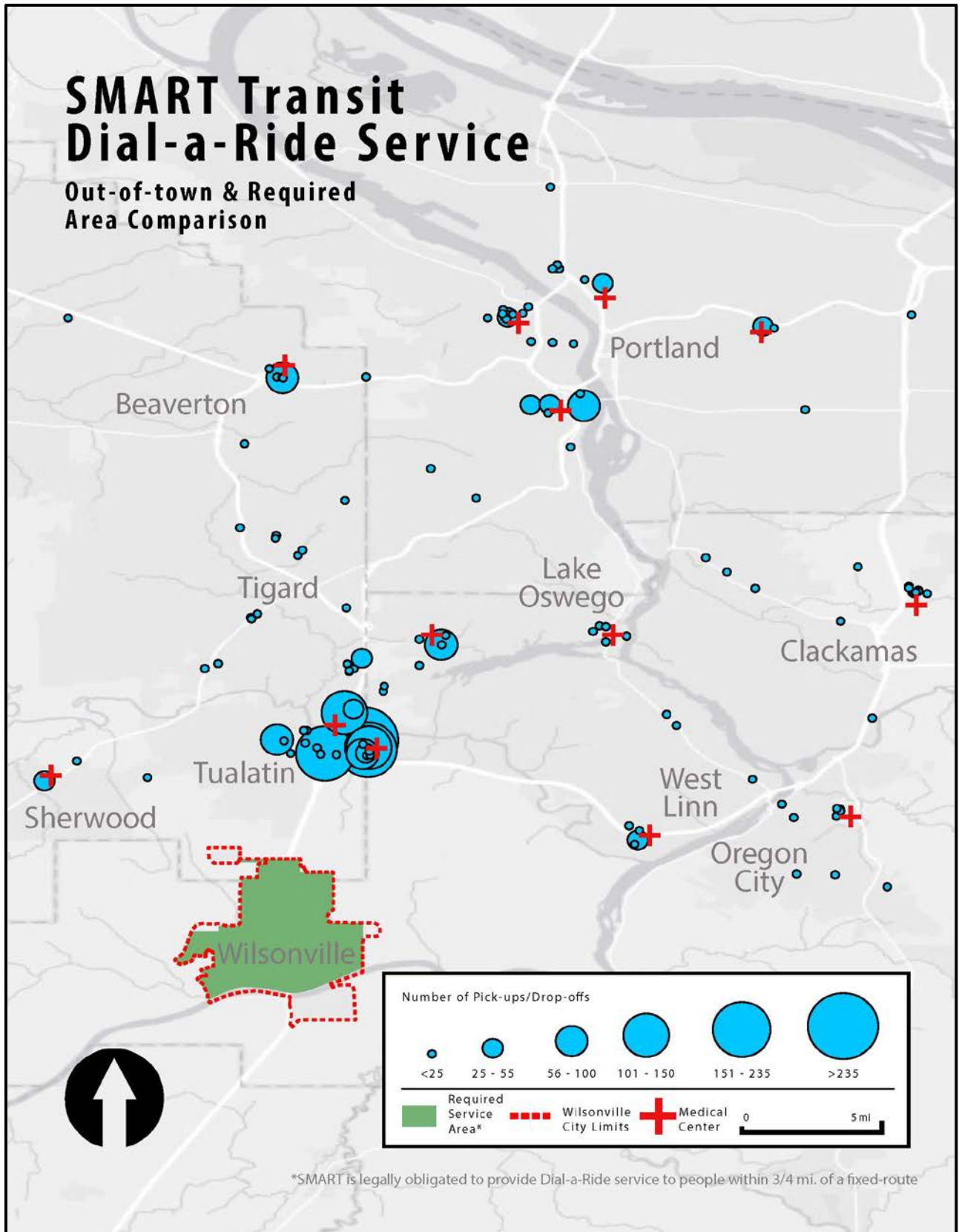
SMART is mindful and fully embraces the reality that Wilsonville has an active senior population and as such, SMART offers the Senior Trips Dial-a-Ride program. This heavily used program was designed to provide seniors with viable mobility options. Through DAR, seniors have unrestricted access to the City and all of its amenities. Seniors who are 60+ are able to make reservations up to two days in advance of their anticipated trip.

Through special transportation funds allocated to transportation providers throughout the region, SMART is able to offer an Out-of-Town Medical Trips. This program allows seniors and persons with disabilities access to the Portland Metropolitan area for medical appointments. SMART competes with other transportation providers for the funding needed to offer this vital program. SMART believes in the concept of equal access, and thus it is important for SMART to provide a bridge between medical services and the people that need them, no matter the distance.

SMART has updated its computer-aided dispatching software to improve efficiencies relative to passenger loads, route planning, and fuel consumption. This update has allowed SMART to increase Dial-a-Ride capacities within all programs as scheduling has become more efficient. The ridership for all Dial-a-Ride programs saw a 53% increase from 2015 to 2016 with no additional services added. This increase is mainly attributed to the new software.

# SMART Transit Dial-a-Ride Service

Out-of-town & Required  
Area Comparison



\*SMART is legally obligated to provide Dial-a-Ride service to people within 3/4 mi. of a fixed-route

**SMART DAR Out-of-Town and Required Service Comparison Map 2016**



## Other Services

SMART offers the following additional services:

### *Travel Training*

SMART has partnered with Ride Connection to bring their RideWise travel training program to Wilsonville. Eligible customers who are unfamiliar with SMART's fixed-route services are encouraged to take part in this program to help understand how to plan your trip and successfully utilize SMART's and surrounding area fixed-route services. The travel trainer also hosts group trips designed to help people feel comfortable with the transit system in a social, relaxed environment.

### *Shopper Shuttle*

SMART operates shopper shuttle service for no charge five days a week to Safeway and Fred Meyer stores in Wilsonville for seniors and supportive housing residents in Villebois.

### *Community Center Senior Lunches*

SMART operates pre-scheduled, door-to-door service to Wilsonville Community Center for senior lunches.

### *Summer Events Trolley*

SMART operates the trolley throughout the summer to community events such as the Farmers Market and Rotary concerts. The trolley also provides free rides around town for special events like Fun in the Park.



# Wilsonville Community Profile and Characteristics

A strategic southern gateway to the Portland Metropolitan area – Wilsonville’s small town lifestyle, coupled with high quality neighborhoods, popular amenities and good jobs have grown the city’s population continuously over time, even during the most recent economic down turn. In a population study completed by Portland State in July 2016, the estimated population for the City of Wilsonville was roughly 23,700 people. Once a small farming community, Wilsonville has transitioned into a well-planned city with a mix of high-tech industries, small businesses, a transportation system, and a variety of housing types.



Within City limits, Wilsonville has many diverse neighborhoods. These neighborhoods offer a broad variety of opportunities for businesses and residents – from the Westside Industrial area to Town Center and Villebois – these distinct neighborhood identities have different transit needs. Neighborhood areas and corresponding analysis are described in Appendix E.

Wilsonville is also home to several major employers and an employee base of more than 18,000. Wilsonville weathered the 2008 recession better than most communities in Oregon and is now seeing increases in employment figures and building permit requests.

Commuter Trends (Longitudinal Employment-Household Dynamics 2014)

|   | Wilsonville          | Tualatin             | Oregon City          | Tigard               | Sherwood           | Canby              |
|---|----------------------|----------------------|----------------------|----------------------|--------------------|--------------------|
| <b>Percent of Workers Who Commute In</b>            | 93%<br>16,957/18,310 | 93%<br>23,742/25,426 | 85%<br>11,718/13,826 | 92%<br>40,731/44,138 | 86%<br>4,521/5,526 | 80%<br>4,743/5,903 |
| <b>Percent of Working Residents Who Commute Out</b> | 85%<br>7,447/8,800   | 87%<br>11,021/12,705 | 87%<br>13,787/15,895 | 86%<br>20,965/24,372 | 92%<br>8,067/8,802 | 85%<br>6,677/7,837 |

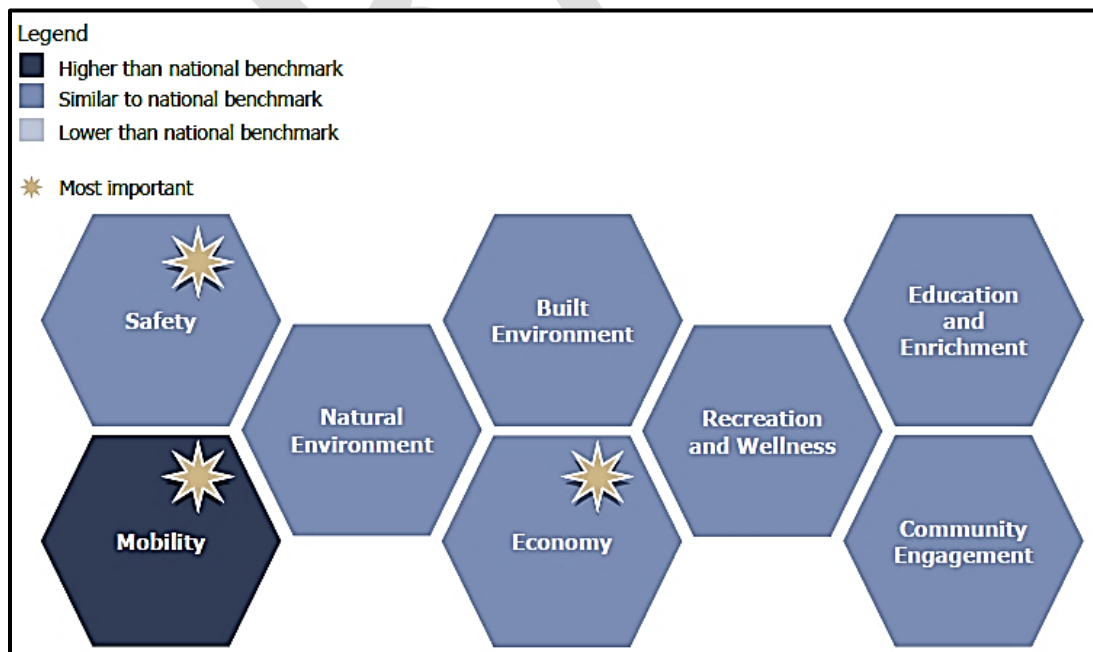
A high percentage of Wilsonville workers and residents are commuters. The majority of workers (93 percent) commute into Wilsonville from other areas of the Portland Metro region. The top cities providing Wilsonville businesses with workers include Portland, Beaverton, and Salem. The commuting trend is also true of local residents. Of the 8,800 working residents of Wilsonville, 85 percent commute to nearby communities. These strong growth trends are

expected to continue over the next 20 years. Portland Metro forecasts indicate that by 2035, population and employment opportunities in Wilsonville will increase by nearly 50 percent.

Those traveling in Wilsonville face two major geographic constraints --The Willamette River and Interstate-5. The combination of these obstacles creates traffic congestion, limiting the economic vitality of the area. On-ramps from Oregon 217 and Interstate 205 funnel into Interstate 5 (north of Wilsonville) creating serious traffic that could be reduced through various transportation demand management strategies to reduce single occupancy vehicles. In addition, weaving motions of vehicles south of the Boone Bridge contribute to congestion by slowing the speed of traffic and resulting in many serious traffic crashes. Traffic on the freeway delays bus times, just as it delays all other traffic, for those heading across or outside of town.

## Quality of Life

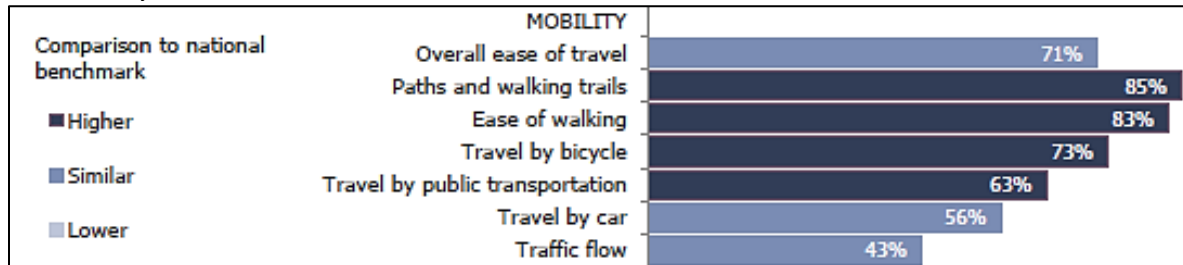
The National Citizen Survey (NCS) gathers opinions of Wilsonville residents regarding various aspects of the City to determine its livability. The NCS defines livability as how desirable a place is to live. Overall, residents rated the quality of life in Wilsonville as good or excellent. The visual below shows eight aspects of the community, the color of each aspect reveals how residents rated the section compared to the national benchmark. Stars on an aspect indicate residents identified it as important to the community. By displaying what facets are most important and their quality, the community can highlight its values and strengths. The mobility sector scored excellent, higher than the national benchmark, and was also labeled as an important aspect to the community. This high satisfaction with mobility is partly a result of SMART's fixed-route and demand response services.



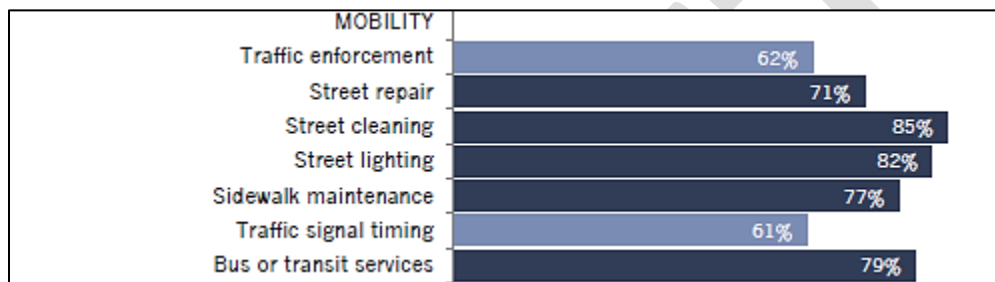
National Citizen Survey. Wilsonville Livability Report. 2016.

More specific ratings related to mobility within the areas of community characteristics, governance, and participation can be seen below:

*Community Characteristics:*



*Governance:*



According to the NCS report, “About 8 in 10 (Wilsonville) residents positively rated the availability of paths and walking trails, ease of walking, bus or transit services, street cleaning and street lighting in Wilsonville as excellent or good...when asked to write in their thoughts about Wilsonville’s biggest priority in the next five years, around 4 in 10 participants responded that traffic, roads and transportation was the top priority for the City”. From this study it can be concluded that although mobility scored very well and is considered a strength of Wilsonville, there are still opportunities for improvement. These improvements include enhancing transportation options like biking and carpooling, as well as making public transportation a more appealing option than driving a personal vehicle.

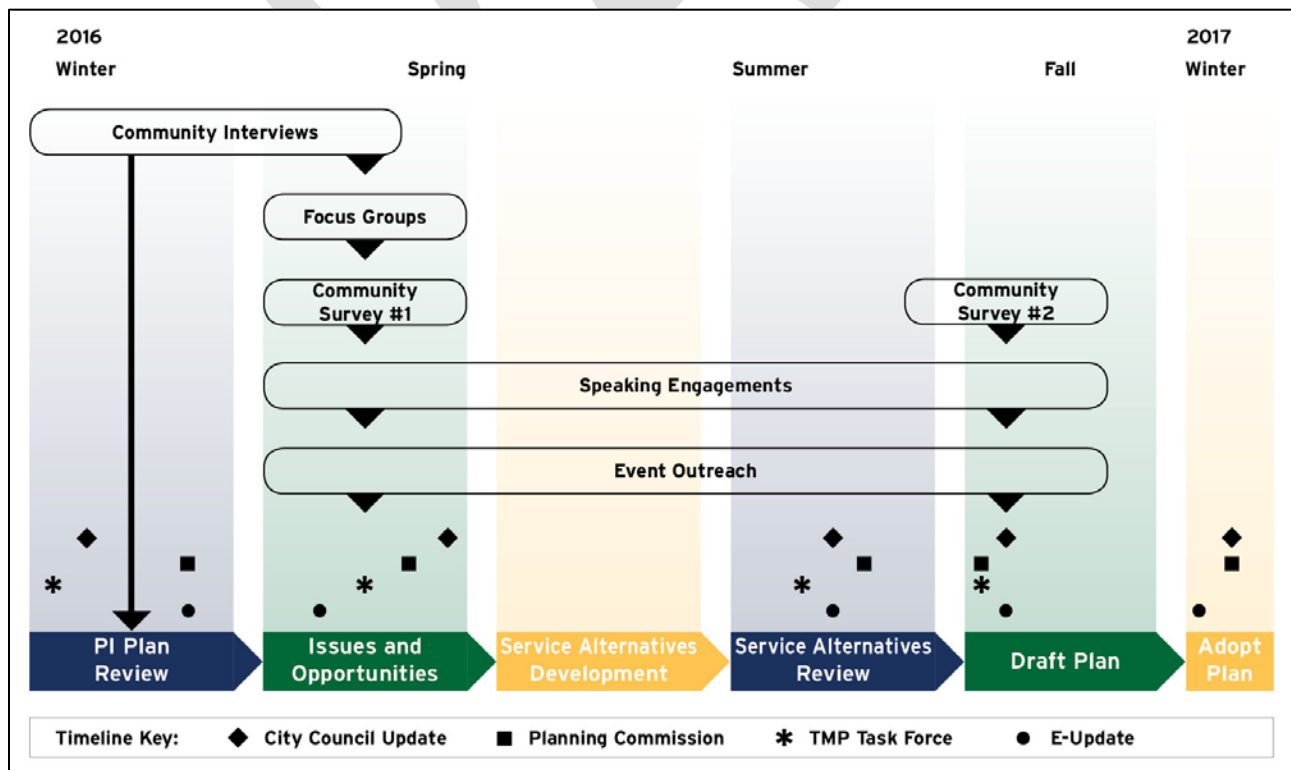
# Public Involvement

To ensure this document represented the diverse interests of the Wilsonville community, the Transit Master Plan (TMP) had an extensive and inclusive public engagement process. Outreach efforts were tailored to reach people in practical and convenient ways as to reflect the opinions from the wide spectrum of current and potential system users, the business community, and residents. Gaining input from employee break rooms, neighborhood and community gatherings, on-board SMART buses and through mobile electronic kiosks are examples of the project’s active public involvement efforts. The complete TMP Public Outreach Summary (including survey results and data) may be found in Appendix F. To guide the TMP public involvement process, the following values were established:

- Realistic:** Be clear about the project constraints, objectives, and parameters.
- Inclusive:** Reach out to everyone including those with limited transit access and other barriers.
- Meaningful:** Provide timely information that is accurate and easily accessible.
- Transparent:** Make decisions public with project materials available on the website.

## Phases of Public Involvement Process

There were three phases of the TMP Public Involvement process: **Issues and Opportunities Identification, Service Alternatives Development, and Service Alternatives Review.** The visual below shows the timeline and relevant actions for the public involvement process.



## Phase One: Issues and Opportunities Identification

SMART and Cogito Partners launched the public input process with the overarching question of how to prioritize service within a fixed budget. This question was framed to help stakeholders determine service priorities and the correlated trade-offs in delivering transit services with available resources.

SMART discussed the project purpose and framework with community partners and stakeholders to understand their perspectives and determine where to focus SMART resources. The first community bi-lingual survey was opened to the public and available in paper and online formats, at electronic kiosks, and community events. The survey posed questions to understand current ridership and the key trade-offs for SMART to consider in service alternatives development including:

| Survey Question  | Survey Response  |
|--|--|
| <b>Who rides SMART?</b>  | Respondents mostly identified as employees and residents of Wilsonville.   |
| <b>What kinds of trips are taken by transit?</b>                                   | Main reasons for using SMART services are for work commute, recreation, and errands.   |
| <b>Type of trips not taken by transit and why?</b>                                 | Inter-city trips, mainly to Portland due to infrequency and transfers.<br><br>More people would use SMART for work, if available to serve a variety of work shifts.  |
| <b>Preference on length of trip, service time, and distance to access transit?</b> | Survey respondents felt transit service being provided throughout the day and week was more important than peak-hour service only.<br><br>Non-riders might be more inclined to use SMART service if it goes to more destinations outside the city and with more frequency. |

*“Regular service makes it more likely that people will use it. The times become well known and people are able to make use of the buses for cross-town trips outside of rush hour. So provides [sic] connectivity improvements in the city, and deals with the east-west divide that still challenges families, especially those with children.”*

SMART and consultants augmented this information with focus groups, display outreach, targeted survey distribution, and a large community workshop. Seven key themes arose from this phase in outreach and are as follows:

- 1) *Support the economy*
- 2) *Enhance connectivity*
- 3) *Expand service times*
- 4) *Provide additional transportation options*
- 5) *Get the word out*
- 6) *Create partnerships*
- 7) *Explore additional funding mechanisms*



The input of the online survey respondents, stakeholder workshop attendees, stakeholder interviewees, and focus groups participants revealed that SMART could shift its priorities slightly on two key trade-offs with public support:

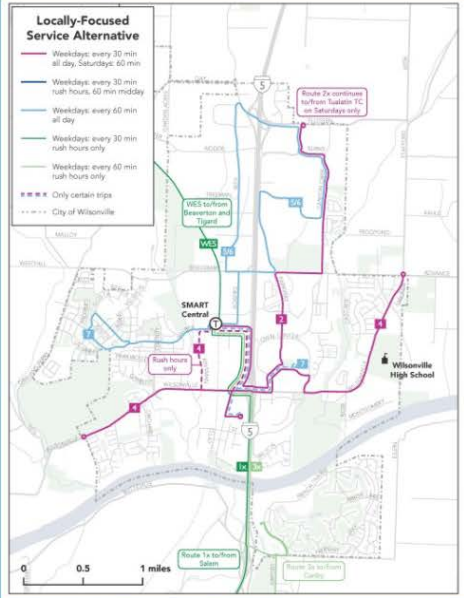

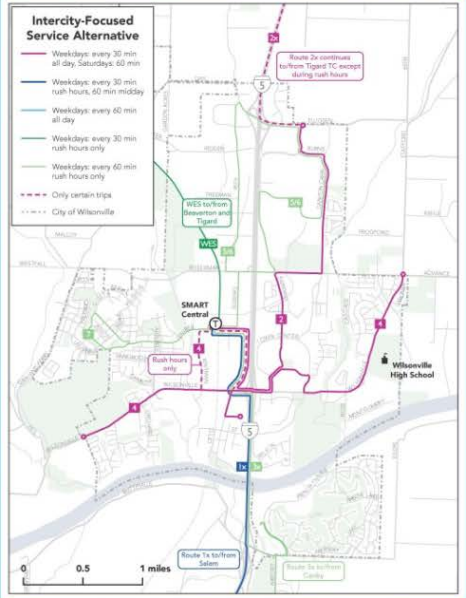
- *More frequent service in places with more activity.*
- *More service throughout the day and week.*

## Phase Two: Service Alternatives Development

Based on input from the Issues and Opportunities phase, SMART service planners and Jarrett Walker & Associates developed service alternatives. To illustrate these service trade-offs, the second phase of outreach presented a spectrum of potential service alternatives to gauge the public’s priorities. SMART staff led the outreach effort, presenting the questions of:

- How to balance ridership and coverage goals?*
- How to balance rush-hour service with service other times of day and week?*
- How to balance local and inter-city (out-of-town) services?*

The table below shows the service alternatives spectrum that the public used to determine how to balance their wants and needs.

| Locally-Focused Alternative  | Common to both Alternatives  | Intercity-Focused Alternative   |
|--|--|---|
| <p><b>Key Distinctions:</b></p> <ul style="list-style-type: none"> <li>Expanded evening service until 11p.m. (Routes 2X &amp; 4).</li> <li>Faster east/west cross town service.</li> <li>Increased service to Villebois.</li> </ul>  | <ul style="list-style-type: none"> <li>More service on weekdays in the midday and evenings with fewer stop locations.</li> <li>More Saturday service with fewer stop locations.</li> <li>Because both alternatives increase how often buses come on existing routes (growing ridership), no additional routes are proposed (covering a greater geographic area). This means that some people would have to walk slightly farther, but in exchange everyone gets a more frequent route and a faster, more direct trip.</li> </ul> <p>For more information, visit:<br/> <a href="http://www.plansmartwilsonville.com">www.plansmartwilsonville.com</a></p>  | <p><b>Key Distinctions:</b></p> <ul style="list-style-type: none"> <li>Expanded service to Salem.</li> <li>New connection to <b>Tigard Transit Center</b> at times when WES is not running.</li> <li>Service will no longer be provided to the Barbur Transit Center, but overall connections to TriMet will be improved.</li> <li>Better timed connections to Canby Transit for Oregon City.</li> </ul>  |



## Phase Three: Service Alternatives Review

The developed service alternatives were then reviewed by community partners and stakeholders via multiple and accessible outreach methods to assess the proposed service alternatives and potential funding strategies.

In the first phase of TMP outreach, small groups of stakeholders representing common interests convened to discuss issues and opportunities. For the Service Alternatives Review phase, groups were combined to foster dialogue and understanding of the inherent trade-offs for transit service. Groups included representatives from business, recreation, retail, education, social service, and City of Wilsonville advisory groups, elected officials, nonprofits, public agencies and private organizations. The key findings from these discussions were:

### *Preference for more inter-city service*

- Focus on rush hour does not apply to many of Wilsonville employer shifts
- Connections to Oregon City remain critical
- Proposed frequencies may be challenging for higher education students

### *Develop transportation options*

- Explore additional options like ridesharing, car and vanpooling, bike sharing, car sharing, and ridesourcing to augment SMART services
- Involve Wilsonville Area Chamber of Commerce in this conversation

In addition to these group discussions, a second community survey was opened to the public. The Service Alternatives Survey administered online, via electronic kiosks, and at community events received over 550 responses. The key question aimed to understand the community's balance between inter-city focused and locally-focused service alternatives. The response data is summarized below.

### *Provide more inter-city service.*

Inter-city service, primarily desired for work commuting, would create more access to neighboring cities such as Tigard, Canby and Salem. This service would use existing resources to better connect with current transit systems like Cherriots and TriMet. Such expansions would support the goals of the Oregon Public Transportation Plan (OPTP) and the Regional Transportation Plan (RTP) to create an interconnected transportation system.

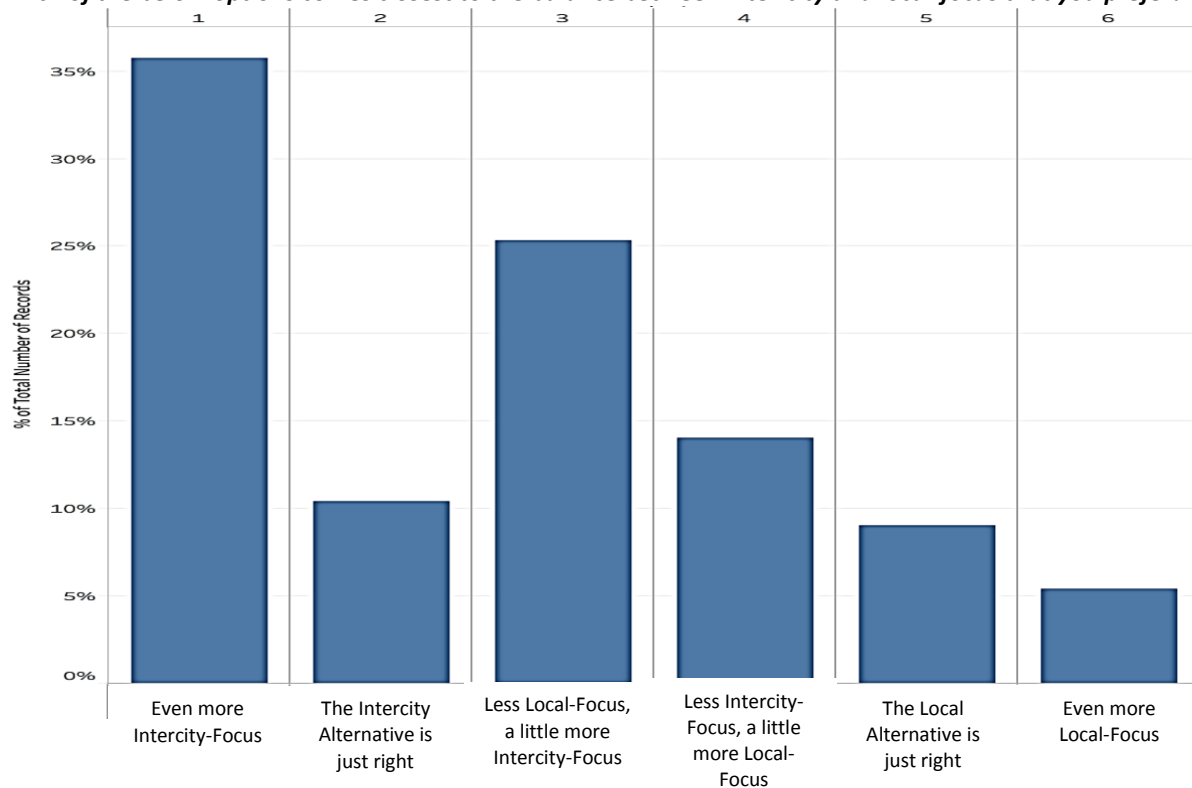
*"...My primary concerns are for those who can't afford cars, or can't drive due to age, to get around. I selected this alternative as it connects more to other transportation, which I think would be better for working adults that don't have cars, and to get cars off the road."*

*More mid-day service.*

Provide transit access to commuters who do not have traditional work schedules or need to get home early. SMART also should play a supportive role in developing transportation options such as carpool or vanpool by providing informational resources to Wilsonville employers.

*“One of the hardest things about trying to live a low-car lifestyle in Wilsonville is the ability to get to other places outside of commute hours. Wilsonville is small enough to bike or walk around for most trips, despite being a car-first town. Getting outside of Wilsonville is where traffic is worst and connections are hard.”*

**Which of the below options comes closest to the balance between inter-city and local-focus that you prefer?**



**TMP Task Force Input**

The Transit Master Plan Task Force strongly preferred expansion options which will improve connections to other transit systems and service to neighboring communities, connecting with TriMet bus lines, WES, and other nearby cities. Such services support goals of the City of Wilsonville’s Transportation System Plan as well as other adopted transportation plans, to create an interconnected and multi-modal transportation system.

## Forms of Outreach

Throughout each phase of the public involvement process, SMART utilized a variety of outreach techniques to be as realistic, inclusive, meaningful, and transparent as possible. SMART serves multiple constituencies with a variety of transportation needs and to ensure that community sub-groups were represented and community priorities were understood, SMART gathered input from:

### *Workshop and Issue-Based Discussions*

To gain insight on key issues and trade-offs for service, SMART conducted a community workshop with 40 community representatives to provide the necessary understanding of transit planning for well-informed decision-making. In addition, there were seven small issue-based discussions that included perspectives from bicycle and pedestrian users, people with disabilities, older adults, under-represented populations, youth and businesses.

### *Media*

SMART used multiple media outlets to inform the Wilsonville community about the project and specific opportunities to give input prior to key decisions and project milestones. This included articles in the Boones Ferry Messenger and Wilsonville Spokesman, e-updates to an interested parties list of over 1,200, social media posts, and distributing over 10,000 informational bookmarks at retailers, public facilities, education institutions, SMART buses, and restaurants.

SMART also had a dedicated TMP project website ([www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)) that allowed for public comments, provided updates, and a timeline of the project.

### *Community Surveys*

SMART administered two surveys in spring and summer of 2016 via the project website, electronic kiosks, distributions of paper surveys, and community event outreach. SMART received over 1,300 responses in total and gained valuable insight into current riders, barriers, and services preferences.



### *Wilsonville Civic Leaders*

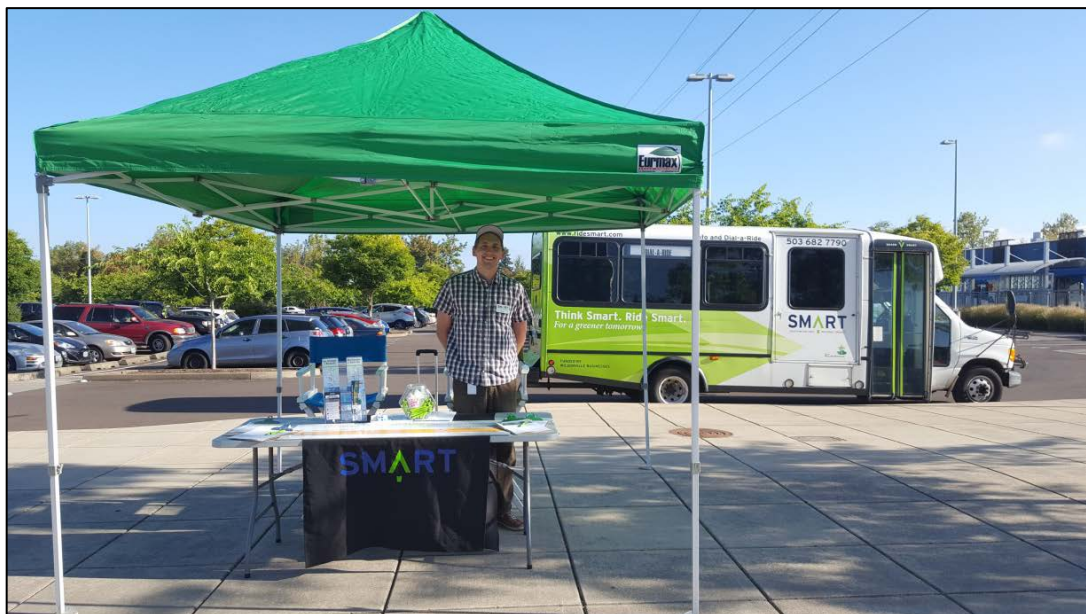
Every community has leaders who connect with a variety of constituencies. Early interviews on how best to reach their networks enhanced the TMP’s public outreach by connecting with those who serve on relevant Wilsonville committees, local service organizations, community non-profits, and business networks. Leaders were part of the TMP Task Force and SMART staff worked with Wilsonville organizations to place information in their newsletters and links to project website at major milestones.

### *Under-Represented Communities (Title VI) / Environmental Justice (EJ) Populations*

A goal of the TMP outreach has been to ensure that the impacts and benefits of the TMP are equitably experienced regardless of race, national origin, gender, disabilities, English language proficiency or income levels. SMART and partners created outreach materials that were available in accessible formats. Partnerships with community organizations to do Spanish outreach helped at specific, identified locations. Meetings and outreach occurred at various times of day and locations to make information available to a broad array of people. There were arranged survey input opportunities at low-income service providers, apartment complexes, and identified shopping venues. Title VI/EJ perspectives were incorporated in focus group discussions. SMART also adopted a formal Title VI program (see Appendix C).

In addition to the outreach forms listed above, SMART also integrated public opinion through:

- Development and adherence to a Public Involvement Plan, vetted by key stakeholders
- Updates to committees and City Council
- Electronic kiosks with bi-lingual information and surveys at public locations
- Guidance from the TMP Task Force
- Consistent, reliable, accessible information
- Representative stakeholder interviews



# Transit Demand

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## Planning for Future Transit Demand

There are many continuously changing factors that influence transit ridership. Due to the variability of these factors, it can be difficult to accurately predict the future demand for transit. The price of gasoline is an example of a dynamic factor that influences ridership, but one that has proven hard to predict. There are direct correlations between the price of oil and transit ridership; when gasoline prices decline, many transit riders return to their private vehicles. Although this factor does greatly sway ridership, it is impossible to accurately predict future oil prices. It is important that SMART is able to provide the proper service even as factors continue to affect and shape transit demand. Efforts to expand transportation services can lead to more freedom and options for people.

In a summary of his book from 2015<sup>1</sup>, Jarrett Walker highlights key characteristics for high transit ridership. Two of these characteristics are frequency and duration of service. Walker states that more frequency of service allows for rider spontaneity and less wait time, giving the individual more freedom. Duration of service allows for people to ride at more times of the day and week such as in the evening and on weekends. This consistency throughout the day and week can promote more ridership because it is a reliable service. An important point Walker makes in his summary, is that sometimes low ridership is necessary to accommodate what the community identifies as important. This could mean providing service that has less frequency but more coverage as to accommodate low-income, disabled, or other populations that may otherwise have no access to transit.

A study in 2005 concluded that, "The most significant ridership increases are generally the result of a combination of initiatives or actions. Seldom does a single initiative result in significant or sustained increases."<sup>2</sup> Other studies found that external factors have the largest effect on ridership; such as parking availability, land use, and population density.

Future trends and developments, along with customer preferences and availability of funding, help to form the basis for planning future service. The following section identifies specific factors that are likely to affect transit demand for the City of Wilsonville and SMART.

1. Walker, Jarrett. *Human Transit. "Explainer: The Transit Ridership Recipe"*. July 2015.
2. Cambridge Systematics, Inc. *TCRP Research Results Digest 69, Evaluation of Recent Ridership Increases*, p.1.

# Factors Likely to Affect Transit Demand

## Population Growth

According to the Portland State University (PSU) Population Research Center, as of July 2016, Wilsonville is at a population of 23,700. Based on preliminary Metro household estimates, Wilsonville's population will be in the range of 30,000 by 2030. This growth will create a higher demand on transit, as more people need options to get around, in and out of town.

## Residential Development

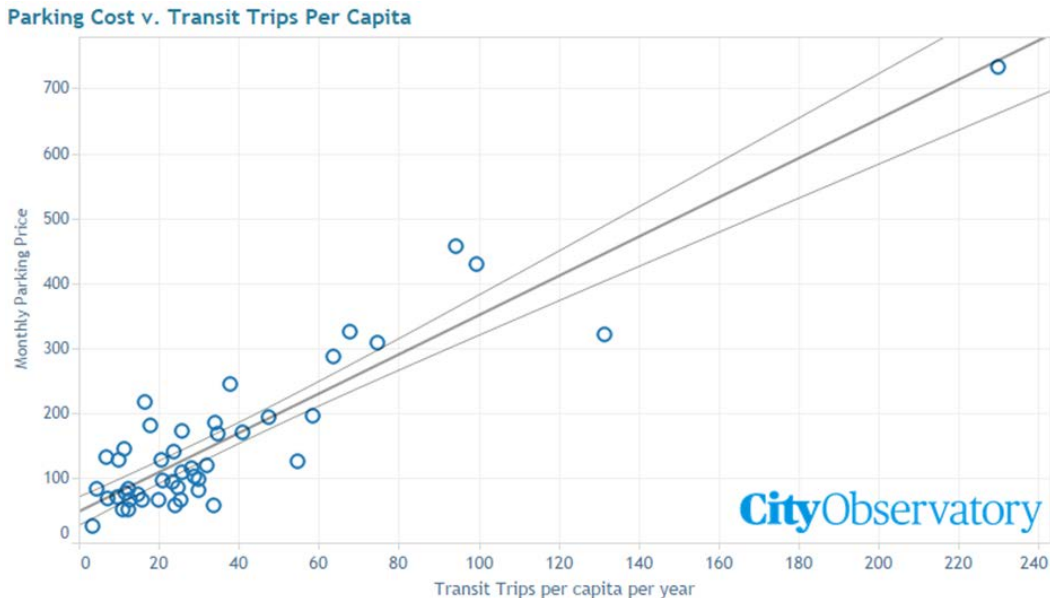
The build out of Villebois will result in a total of more than 2,500 housing units to the west side of Wilsonville. In addition, the City is in the process of developing Frog Pond, a 500-acre residential area that will add many single-family, detached homes and two more public schools to east Wilsonville. These developments will increase demand for bus service.

## Growing Employment

Metro has designated hundreds of acres in the Coffee Creek and Basalt Creek areas just northwest of Wilsonville for future industrial development. This will increase employment in Wilsonville and the need for more travel options. SMART will continue to serve employers to encourage easy transportation that will aid in employee retention and recruitment.

## Cost of Parking

Free and plentiful parking is a powerful incentive to drive rather than using alternatives such as transit or carpooling. The City Observatory, an urban research think tank, found that the correlation between transit ridership and parking cost is very strong. Commuters who drive alone are very sensitive to the price they have to pay; as long as parking is free there is not much incentive for commuters to consider other modes of transportation. In Wilsonville, parking is free and plentiful which means it is more difficult to get people to use transit.



## Commuter Rail

Since early in 2009, TriMet has provided commuter rail service between Beaverton and Wilsonville with the Westside Express Service (WES). Operating on “heavy rail” or freight lines, the WES differs from light rail but similar in that it has the advantage of bypassing and removing traffic from I-5 and Highway 217.

Multiple transit connections exist at each WES stop, with access to MAX trains at the Beaverton Transit Center. WES continues to provide only commute-hour service, leaving a gap during the mid-day and evening hours.

Expanding WES service would lead to more ridership for SMART as many customers transfer from WES. Service changes proposed in this Master Plan will have SMART filling in those gaps when the WES does not operate, allowing more people to have the freedom to take the WES and still be able to get home mid-day.



## Fares

Electronic fares (e-fares) have recently become another payment option for transit agencies to implement. This technology allows riders to pay for fares on their phones or by card and has the potential to create a simple, single way for people to pay. This option is proving to be convenient for many and provide faster ways of payment and transferring between transit systems which may increase regional transit ridership. SMART is currently partaking in a feasibility study to determine the costs and benefits of implementing this technology.

## Aging Population

Based on PSU's 2016 population figures and 2010 U.S. Census data, there are approximately 3,300 (14%) of Wilsonville residents over age 65. The baby boomer generation has reached retirement age and seniors are accounting for a growing portion of the population. This is likely to create increased demand for SMART's Dial-a-Ride and medical trips. Elderly residents may also add to the increased demand for fixed-route transit service. It is important to do further research on SMART's current Dial-a-Ride system to promote maximum efficiency for the expected aging population. SMART will need to find ways to increase efficiencies in services provided to both the aging and disabled populations due to anticipated increasing costs and increasing demands for service.

## **Balancing Needs**

The greatest challenge of transit service planning is finding and maintaining the delicate balance between the many divergent and often conflicting needs of passengers. Although most routes are planned based on a particular passenger need (such as commuting), many routes are also able to effectively accommodate other needs at the same time.

### *Commuter Service vs. Local Service*

The preferred destinations of commuters and local users are often divergent enough that a single route cannot effectively meet both needs. In many cases, the travel times of commuters and local users are very different. Commuter trips tend to be very time-sensitive, with commuters being less tolerant of frequent stops. On the other hand, local bus riders have a variety of trip destinations and require many stops. However, there is considerable overlap in travel times of commuters and students in Wilsonville. This is in part due to the early morning shifts at a number of employment sites that end in the early afternoon. Many employers are now utilizing a wide range of shift times, making it increasingly difficult for SMART to accommodate those workers.

### *Service on Transit Corridors vs. in Neighborhoods*

Limiting transit service to transit corridors ensures a shorter travel time from beginning to end. It also ensures that neighborhoods do not have the traffic and noise impacts of buses traveling down small streets. While bus service into neighborhoods has potential neighborhood impacts, it also ensures the greatest level of access, particularly for those who cannot walk very far.

### *Transit Dependent vs. Choice Riders*

Transit serves an important role in providing mobility to people who do not have access to automobiles, including the young, old, people with disabilities and people with low incomes. SMART's first priority must always be to provide transportation for transit-dependent individuals, particularly those with disabilities. However, in order for transit to effectively reduce automobile trips and the overall demand on the transportation system, SMART must also provide a service which is attractive as an alternative to those who drive, also known as choice riders. Although the needs of transit dependent riders and choice riders are not mutually exclusive, they are likely to have differing priorities. Choice riders are likely to place a greater value on fewer stops and faster travel times, whereas transit dependent passengers may prefer more closely spaced stops that allow for shorter travel distance to the bus stop.

### *Coordinating Route Schedules*

Transit relies heavily on connections to other systems to be efficient and provide the most options for users. If a route schedule is designed to ensure timely connections with a neighboring transit system it may result in connections that are much worse with another neighboring system or route within the SMART system. SMART must consider connections with the WES, TriMet bus lines, Salem Cherrits, and Canby Area Transit (CAT) when designing route schedules and changes which can limit the amount of changes made.



# Funding Sources and Expenditures

## Revenue

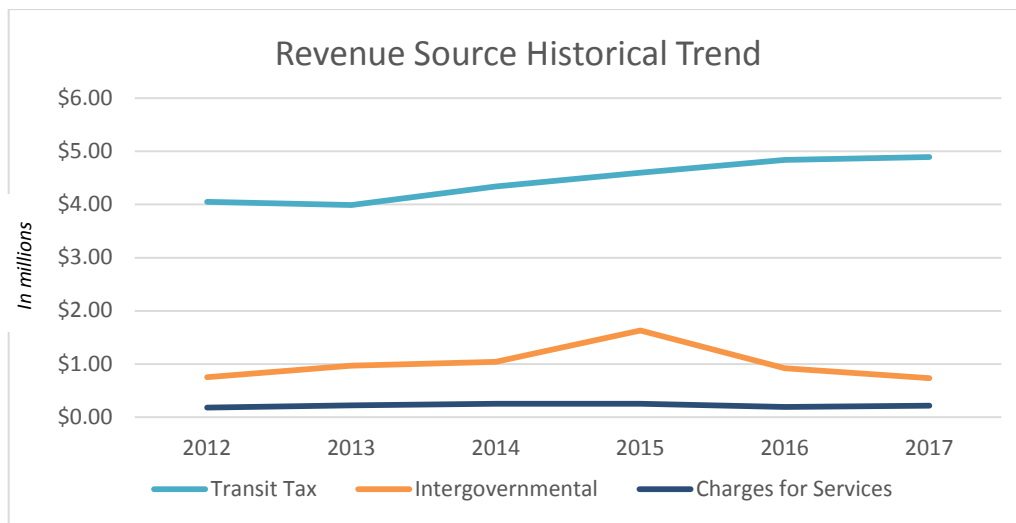
SMART has three primary sources of continuous revenue, a local payroll tax, intergovernmental revenue, and charges for services. The predominant source of ongoing funding is the local payroll tax levied on businesses performing work in Wilsonville assessed on gross payroll and/or self-employment earnings. The second largest source is intergovernmental grants and contracts. Finally, charges for service, including fares collected on out-of-town routes and transit pass sale revenue contribute to SMART’s local funding.

A historical breakout of amounts received for the past five fiscal years (City fiscal year from July 1 to June 30) plus amounts estimated for the fiscal year end 2017 are displayed in Chart A below. Chart B displays the percentage breakout of each source in relation to the total amount of revenue received.

**CHART & GRAPH A:** Revenue source historical trend, for fiscal year ending:

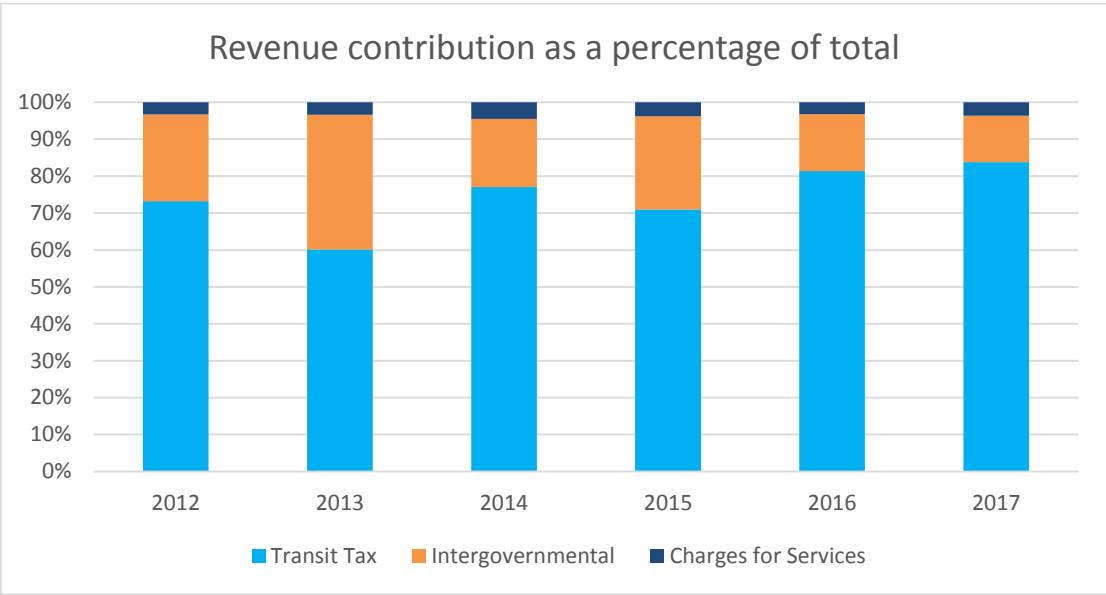
| Source                               | 2012         | 2013         | 2014         | 2015         | 2016         | 2017 (Est.)  |
|--------------------------------------|--------------|--------------|--------------|--------------|--------------|--------------|
| <b>Transit Tax</b>                   | \$ 4,048,569 | \$ 3,990,885 | \$ 4,342,885 | \$ 4,597,118 | \$ 4,838,597 | \$ 4,891,700 |
| <b>Intergovernmental<sup>1</sup></b> | 749,354      | 970,963      | 1,041,610    | 1,662,590    | 918,358      | 733,832      |
| <b>Charges for Services</b>          | 179,633      | 223,931      | 251,511      | 248,995      | 190,235      | 213,580      |
| <b>Total</b>                         | 4,977,556    | 5,185,779    | 5,635,474    | 6,508,703    | 5,947,190    | 5,839,112    |

<sup>1</sup> Includes Connect Oregon CIP funding for the construction of SMART offices and shop facilities in 2012 & 2013.



**CHART & GRAPH B:** Revenue contribution as a percentage of combined total, fiscal year ending:

| Source                      | 2012 | 2013 | 2014 | 2015 | 2016 | 2017 (Est.) |
|-----------------------------|------|------|------|------|------|-------------|
| <b>Transit Tax</b>          | 73%  | 60%  | 77%  | 71%  | 81%  | 84%         |
| <b>Intergovernmental</b>    | 24%  | 37%  | 19%  | 25%  | 16%  | 12%         |
| <b>Charges for Services</b> | 3%   | 3%   | 4%   | 4%   | 3%   | 4%          |
| <b>Total</b>                | 100% | 100% | 100% | 100% | 100% | 100%        |



**Transit Tax**

The payroll tax on local businesses covers employment within City limits. From 1990 to 2006 the tax was .3% of gross payroll expense. In 2006, the rate was raised to 0.033% and in 2008, the tax rate was raised to its current level of .5%. Transit tax funds are used to pay for SMART operations and to leverage funding from federal and state grants. Payroll tax amounts collected by the City typically increase year to year, as companies increase their payroll through wage adjustments or by adding to their payroll, and as the economy grows with new businesses relocating to the City. In Fiscal Year 2016-17, an estimated \$4.9 million in transit tax funds is expected to be received, contributing to a five year, year over year, average annual growth rate of 3.92%. Six transit agencies in Oregon levy payroll tax: Lane Transit District, TriMet, City of Sandy, South Clackamas Transportation District (SCTD), City of Canby, and City of Wilsonville. SMART’s current payroll tax rate is 48.74% less than that of TriMet and 20% less than Canby Area Transit (CAT) service. Please see table on the following page for a comparison of payroll tax rates in Oregon.

## Oregon Payroll Tax Rates

| Calendar Year | SMART      | TriMet  | Canby   | Sandy   | South Clackamas Transit District | Lane Transit District |
|---------------|------------|---------|---------|---------|----------------------------------|-----------------------|
| 1972          | Not formed | 0.3000% | N/A     | N/A     | N/A                              | N/A                   |
| 1978          | Not formed | 0.5000% | N/A     | N/A     | N/A                              | N/A                   |
| 1990          | 0.3000%    | 0.6176% | N/A     | N/A     | N/A                              | N/A                   |
| 2006          | 0.3300%    | 0.6418% | N/A     | N/A     | N/A                              | N/A                   |
| 2008          | 0.5000%    | 0.6618% | N/A     | N/A     | N/A                              | N/A                   |
| 2009          | 0.5000%    | 0.6718% | N/A     | N/A     | N/A                              | N/A                   |
| 2010          | 0.5000%    | 0.6818% | 0.6000% | 0.6000% | 0.5000%                          | 0.6500%               |
| 2011          | 0.5000%    | 0.6918% | 0.6000% | 0.6000% | 0.5000%                          | 0.6600%               |
| 2012          | 0.5000%    | 0.7018% | 0.6000% | 0.6000% | 0.5000%                          | 0.6700%               |
| 2013          | 0.5000%    | 0.7118% | 0.6000% | 0.6000% | 0.5000%                          | 0.6800%               |
| 2014          | 0.5000%    | 0.7218% | 0.6000% | 0.6000% | 0.5000%                          | 0.6900%               |
| 2015          | 0.5000%    | 0.7237% | 0.6000% | 0.6000% | 0.5000%                          | 0.7000%               |
| 2016          | 0.5000%    | 0.7337% | 0.6000% | 0.6000% | 0.5000%                          | 0.7100%               |
| 2017          | 0.5000%    | 0.7437% | 0.6000% | 0.6000% | 0.5000%                          | 0.7200%               |
| 2018          | 0.5000%    | 0.7537% | 0.6000% | 0.6000% | 0.5000%                          | 0.7300%               |
| 2019          | 0.5000%    | 0.7637% | 0.6000% | 0.6000% | 0.5000%                          | 0.7400%               |
| 2020          | 0.5000%    | 0.7737% | 0.6000% | 0.6000% | 0.5000%                          | 0.7500%               |
| 2021          | 0.5000%    | 0.7837% | 0.6000% | 0.6000% | 0.5000%                          | 0.7600%               |
| 2022          | 0.5000%    | 0.7937% | 0.6000% | 0.6000% | 0.5000%                          | 0.7700%               |
| 2023          | 0.5000%    | 0.8037% | 0.6000% | 0.6000% | 0.5000%                          | 0.7800%               |
| 2024          | 0.5000%    | 0.8137% | 0.6000% | 0.6000% | 0.5000%                          | 0.7900%               |
| 2025          | 0.5000%    | 0.8237% | 0.6000% | 0.6000% | 0.5000%                          | 0.8000%               |

*TriMet and Lane Transit District has approved their rates beyond 2017.*

*Wilsonville, Canby and Sandy require approval from City Council to change payroll tax rate.*

*South Clackamas Transit District requires a vote to change payroll tax rate.*

## Intergovernmental

In FY 2016-17, intergovernmental revenue is projected to total \$733,832. Intergovernmental grant revenue is comprised of federal, state, and county funds. Nearly all federal funds received directly by SMART are subject to the policies and regulations of the Federal Transit Administration (FTA) or the Federal Highway Administration (FHWA). Historically, SMART has relied on intergovernmental grants for its bus purchases typically funded 80% through grant and 20% through a local match. There are five federal funding programs that either directly or indirectly come to SMART to support regular operations and capital purchases.

State funding largely comes by way of the State Transportation Fund (STF) or the *ConnectOregon* program. STF is comprised of cigarette tax, non-highway use gas tax, ID card revenues, and general fund. The STF Program provides revenue in support of transportation need for people who are senior and people with disabilities of any age. The *ConnectOregon* program is a grant initiative funded by lottery-based bonds to promote stronger, more diverse and efficient transportation options throughout Oregon. Given that State lottery proceeds are now potentially over-subscribed, it is difficult to determine the future of the *ConnectOregon* program. SMART received \$2-million in *ConnectOregon* funds to help pay for the construction of SMART’s offices and shop facilities, completed in 2013.

## Charges for services

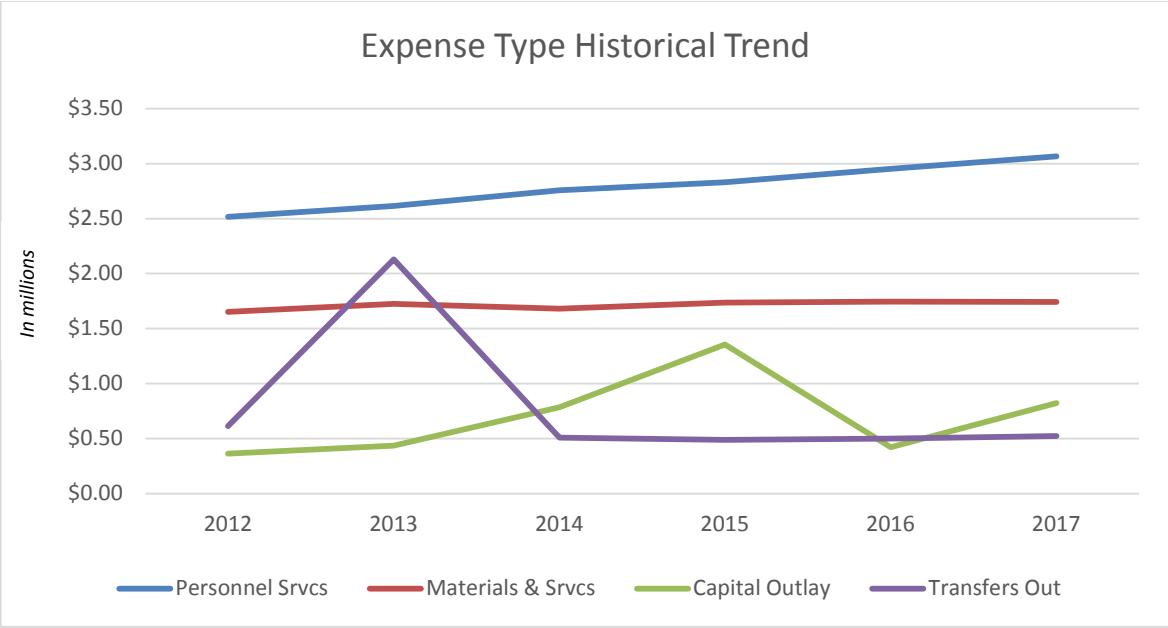
A much smaller component of local funding includes charges for services, including fare box and transit pass sale revenue. Currently, SMART charges fares for all routes that travel outside of the City of Wilsonville. Projected annual fare revenue for these routes in FY 2016-17 is approximately \$200,000 from pass sales and cash fares.

## Expenditures

As per the City’s Five-Year (FY 2016 – 2021) Financial Forecast, SMART expenses are anticipated to increase by at least an annual inflation rate of 2% per year while maintaining roughly comparable levels of service. The most volatile components of SMART’s expenses are PERS related costs, salaries, health insurance, and fuel. Salaries and wages will grow in general at roughly a 2.5% rate while benefits are projected to increase 4% to 6%. A historical breakout of expenses for the past five fiscal years plus amounts estimated for the fiscal year end 2017 are displayed in the below chart and graph.

| Expense                          | 2012         | 2013         | 2014         | 2015         | 2016         | 2017 (Est.)  |
|----------------------------------|--------------|--------------|--------------|--------------|--------------|--------------|
| <b>Personnel Services</b>        | \$ 2,516,542 | \$ 2,615,774 | \$ 2,758,428 | \$ 2,829,047 | \$ 2,952,481 | \$ 3,066,128 |
| <b>Materials and Services</b>    | 1,652,730    | 1,725,315    | 1,680,998    | 1,736,677    | 1,744,271    | 1,742,014    |
| <b>Capital Outlay</b>            | 361,330      | 436,185      | 785,703      | 1,354,869    | 419,995      | 822,000      |
| <b>Transfers out<sup>1</sup></b> | 614,385      | 2,127,838    | 507,412      | 489,028      | 500,348      | 524,060      |
| <b>Total</b>                     | 5,144,987    | 6,905,112    | 5,732,541    | 6,409,621    | 5,617,095    | 6,154,202    |

<sup>1</sup>Transfers out includes amounts for administrative overhead and capital project funding. In FYE 2013, for example, \$1.7 million was transferred out for the funding of the construction of SMART offices and shop facilities.



## Service Increases and Potential Revenue Sources

SMART aims to increase levels of service to match the City’s growing demand as it attracts more employers and residents. To provide the level of service for businesses, residents and visitors to utilize public transit, SMART will need to explore more revenue sources. Below are some options that SMART could explore further in the near future to help provide equitable and convenient service. It is important to note that the options listed are in no way meant to represent all available funding sources or set to occur; instead these options highlight some of the more common, creative and accessible funding streams.

### Public-Private Partnerships

The coordination between public and private partners can help increase knowledge and benefits of transportation options in addition to addressing barriers to productivity and development for both parties. Examples of partnerships could be with Lyft or other transportation network companies to supplement paratransit and/or Dial-a-Ride services. Another partnership could be with Enterprise Rideshare to help initiate vanpools for employers.

### Advertising

Additional revenue could be generated from advertising revenue. Revenue could be generated from advertisements on the outside of buses, within buses or in shelters. Types and content of advertising can be controversial as there are limitations on the guidelines SMART could implement to control content.

## **Commercial Services**

The citizens TMP Task Force discussed the possibility of establishing a SMART owned and operated retail shop or shops located on the property of SMART Central Transit Center. Retail could include places that are comfortable for customers to wait for their ride, such as a coffee shop. A deep dive into the feasibility and return on investment of such a project is needed. Revenue estimates and profitability would be determined through comprehensive financial analysis.

## **Grant Funding**

Funding from grants, SMART's second largest revenue source, are beginning to become fewer as monies at the federal level for transportation are being reduced. SMART has historically been successful in seeking and being awarded grants. SMART will continue to seek grants from the counties, region, state and federal sources.

## **Increase Fares**

Revenue from fares is anticipated to continue to be a small supplemental source of revenue that will serve to help offset costs associated with providing service on particular routes. Currently, fares make up about 4% of SMART's revenue. SMART currently only charges for out of town routes. Price elasticity of demand aside, a \$0.25 cent increase in fares, as currently structured, would result in an estimated \$32,300 in additional revenue.

## **Tax Revenue**

### *Payroll Tax*

Payroll tax revenue is a function of two variables, the local employment wages or earnings and the tax rate. An increase in revenues can occur by increasing either factor.

Commercial and industrial expansion is expected to continue for the next several years as existing businesses experience growth and new developments occur, provided that the economy remains strong. Once the available industrial land has been developed, the increases will come primarily through wage and business growth, resulting in a slower, but steady level of payroll tax revenue increase. In the interim, SMART staff members remain actively involved in the City's economic development activities – hoping to expand the job base in the community.

The second variable in the calculation of payroll tax revenue is the tax rate. For every .1% (.001) the transit tax goes up (for example, from the current 0.5% to 0.6%) an additional \$968,000 in revenue is generated (based on the FY 2015-16 actual local wages and earnings amount).

### *Gas Tax*

The City could impose a local gas tax and dedicate the funding to SMART operations.

### *Property Tax*

SMART could pursue property tax funding in one of three ways:

- I. Seek funding from the City's General Fund largely funded from property taxes and is used for City services such as Police, the Library, Parks, and Administration. Funding SMART from the General Fund would be a direct offset to these other services.
- II. A voter-approved local option property tax levy could be considered. These levies are limited to five years. For every dime (\$0.10) of new property tax, it means \$340,000 in revenue (based on current assessed valuation).
- III. SMART could also seek voter approval for the formation of a special property tax district with a permanent tax rate.

### *Sales Tax*

Oregon counties and cities have the right to impose a sales tax at the local level. For example, in 1990 the City of Ashland, Oregon established a voter approved five percent tax on all prepared food sold in Ashland, those proceeds were restricted for the purchase of open space for parks and to offset the costs associated with the building of a new wastewater plant. Funds could likewise be designated for transit operations.

### **Transit Operations Monthly Fee**

A monthly fee included on the City's combined utility bill. The City of Wilsonville has approximately 5,000 residential accounts and 476 multi-family utility accounts (with approximately 4,976 individual units). With a simple per account residential and per unit multi-family dwelling unit formula, for every \$1.00 per month (\$12.00 per year), an estimated extra \$119,712 could be raised.

## **Conclusion**

In order to maintain a high quality public transportation system, it is important to maintain consistent funding levels while operating efficiently. SMART management, working with City staff and City Council, can consider a range of possibilities with various considerations. The top priorities for SMART's management team are to improve operational efficiencies and seek out new funding sources, particularly intergovernmental grants. It appears to be uncertain, however, that state or federal funds will continue as they have. Through a combination of focusing on operational efficiency and resource generation, SMART can genuinely live up to its name and approach its future responsibly, effectively, and wisely.

# Recommended SMART Service

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## Proposed Routes

The following route proposals are presented as a package. The SMART network is interdependent on a variety of factors such as connecting with other transit agencies and complying with federal regulations. Note that if a proposed route is changed, it may affect another route. The services proposed in this plan are not final and are open for comment and adaptation through public input, the planning commission, and City Council.

This is a cost-neutral proposal that was developed by three main factors: public input, service efficiency, and funding. These factors combined with projections of future demand and consultant recommendation helped SMART prioritize the following fixed-route service changes with little to no financial impact to meet the transit needs of Wilsonville. For a priority list of fixed-route service changes that vary dependent on funding levels, please see Appendix B.

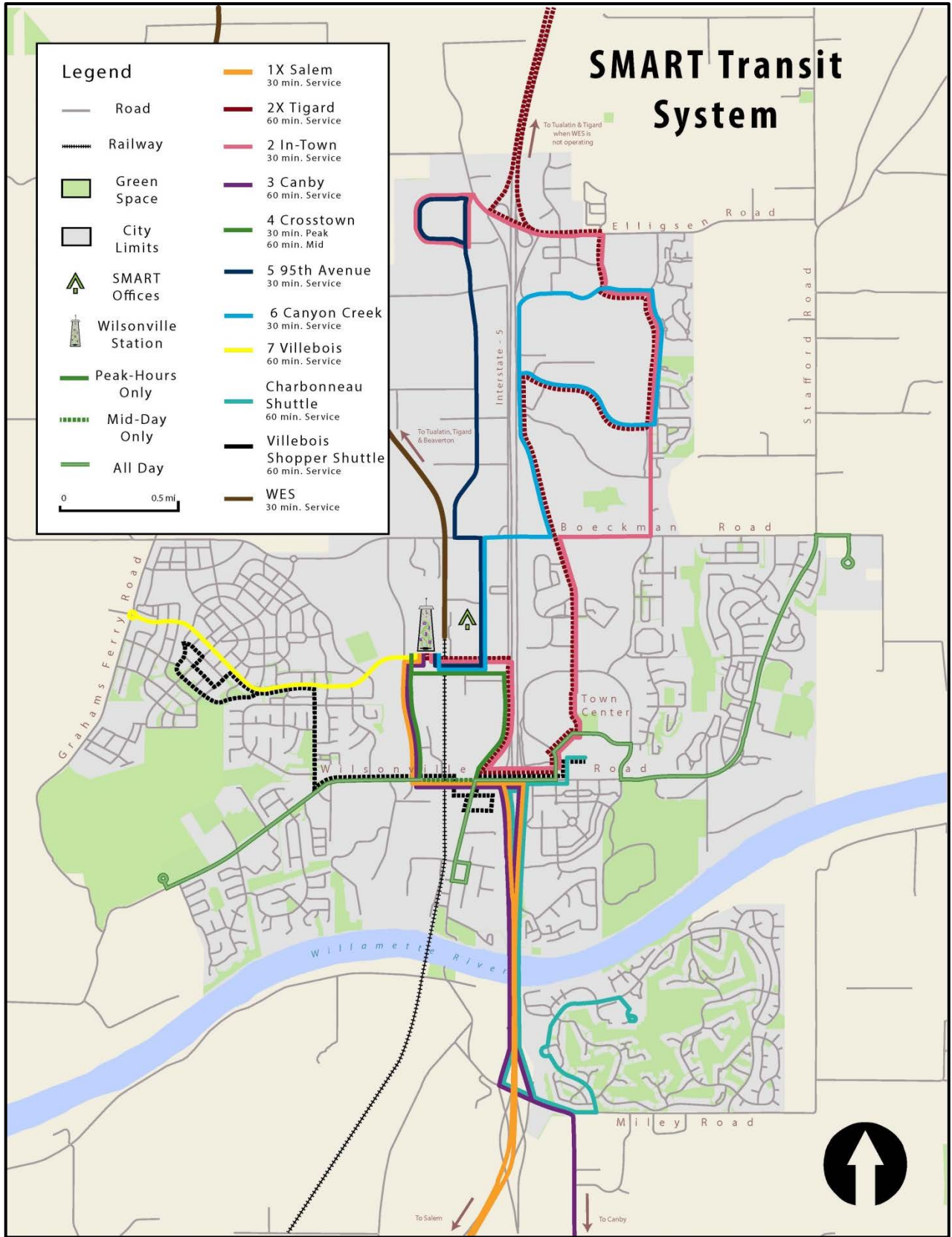
In order to balance the various divergent needs for transit service, individual routes are designed with a primary purpose. For example, one route may be designed primarily as an inter-city commuting route whereas another is designed primarily as a local service route. A mix of primary purposes ensures that there are routes to serve the widest possible range of needs. SMART supports the use of vanpools and other transportation options to provide service which is competitive with the automobile for commuters in areas where fixed-route service cannot meet all commuters' needs.





### Cost Analysis of Recommended SMART Routes

| Service Options  |   | Annual Cost |       |             |   |
|--|---|-------------|-------|-------------|---|
| Priority Level   | Description of Service  | Current     |       | Projected   | Notes   |
| 1  | Switch service from Barbur TC to Tigard TC with stop at Tualatin Park & Ride (Route 2X) | \$1,322,119 | 2     | \$383,024   | Increased connections with TriMet<br><br>Slight cost reduction  |
|  |   |             | 2X    | \$726,550   |   |
|  |   |             | Total | \$1,109,574 |   |
| 2  | a Separate Canby & Charbonneau Service (Route 3)  | \$212,424   |       | \$192,160   | Increased connections with CAT  |
|  | b Pilot Charbonneau Shuttle service   |             | \$0   | \$78,038    |   |
| 3 & 4  | Streamline Route 4 services & expand evening service                                    | \$931,259   |       | \$972,846   | Skip SMART Central during non-peak hours and streamline route along Wilsonville Rd<br><br>Expanding evening service is approximately \$182,409, which is included in this projected cost. |
| 5  | Expand Route 4 Saturday service   | \$0         |       | \$19,136    | Two extra hours (overtime wages)  |
| 6  | Villebois & Route 7 reconfiguration   | \$35,174    |       | \$47,970    | Mileage and hours essentially stay the same   |
| DAR*   | ADA Required Paratransit  | \$0         |       | \$69,968    | Paratransit service to meet ADA guidelines  |
| Total  |   | \$2,500,976 |       | \$2,489,692 | Proposed plan is cost neutral   |
| <p>*DAR is not among the 'New Service Priorities', however, ADA requires paratransit service be provided within 3/4 miles of any fixed-route service. With items 2b, 4, 5, &amp; 6 expanding fixed-route service, SMART's DAR budget reflects those additional costs of added paratransit service.</p> |   |             |       |             |   |



**Proposed System Map**

## Route 1X – Salem

Route 1X will continue to enhance regional connectivity by providing express commuter service between Wilsonville and Downtown Salem. This service is provided through a joint effort between Wilsonville SMART and Salem Cherriots, with SMART providing 16 daily trips and Cherriots providing ten. Service runs every 30 minutes, Monday through Friday, from 5:00 a.m. – 10:00 a.m. and 3:00 p.m. – 7:00 p.m.

Given the large numbers of commuters moving in both directions, especially public employees who work in Salem, the 1X is a highly utilized service. Public involvement has indicated that this service would be even more desirable if an additional morning and mid-day trip were added. At this time, service to Salem will stay the same; however, SMART will work to coordinate with Cherriots to possibly provide more service. If additional funding becomes available for SMART operations or Cherriots is willing to increase their service, adding morning or mid-day service on the 1X is one of the highest priorities.

Public input also displayed a desire for commuting service from Keizer, Woodburn, and Wilsonville. SMART recognizes the importance of this service, however, limited funding will not allow for SMART to provide this service at this point in time.

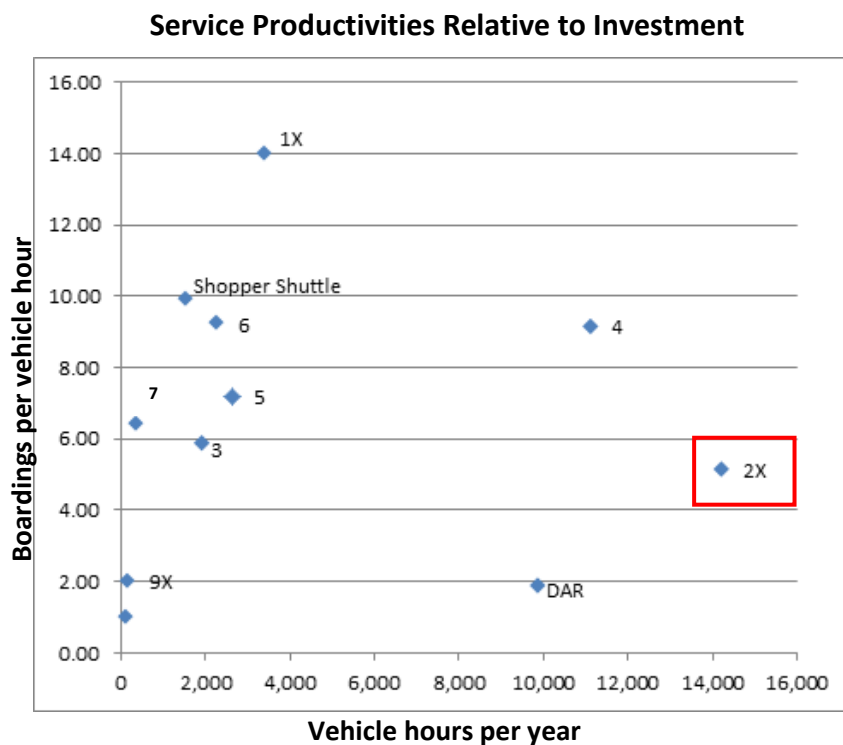


**Proposed Route 1X**

## Route 2X – Tigard/Tualatin

Route 2X is a commuter service linking Wilsonville, Tualatin, and Portland. The recommended 2X service will alter the current service in three main ways.

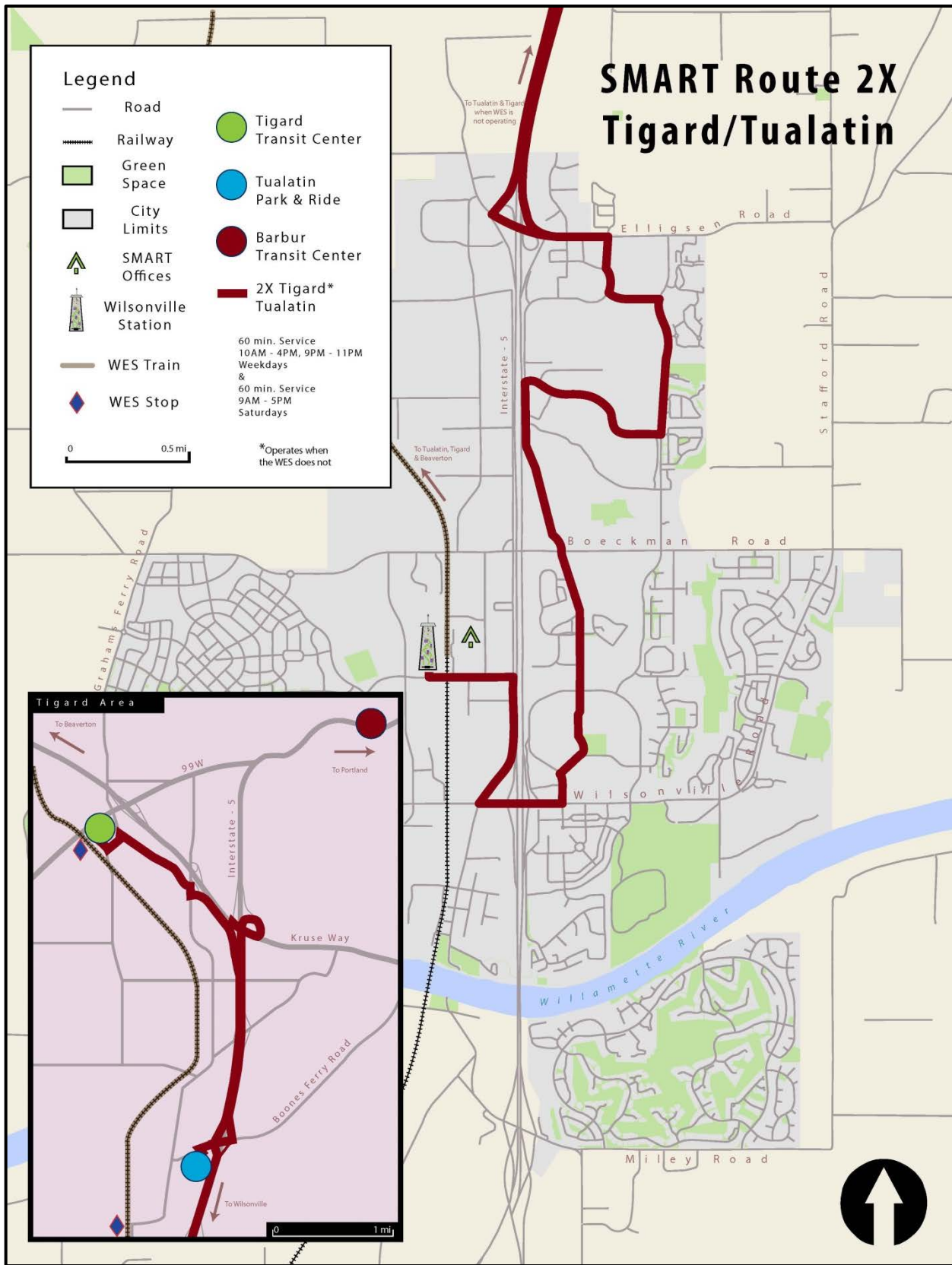
The first alteration proposed is to change service from Barbur Transit Center to Tigard Transit Center. This new service will improve regional connectivity to Washington County while still providing access to Portland. Switching from Barbur to Tigard may increase travel times for some current riders up to 20 minutes. Public feedback and consultant recommendation determined that providing service to Tigard will give more people with the option to use this service. SMART is willing to make this change as the productivity of the current service to Barbur is very low, as seen in the graph below.



The second proposed change is to provide weekday service only when the WES is not running. This would fill the current gap in mid-day and evening service for commuters who take the WES. This means that the 2X would run Monday through Friday from 10:00 a.m. - 4:00 p.m. and 9:00 p.m. – 11:00 p.m. on an hourly basis to Tigard Transit Center.

In addition to stopping at Tigard, SMART will also provide hourly mid-day and evening service on weekdays and 9:00 a.m. – 5:00 p.m. service on Saturdays to Tualatin Park & Ride.

The last proposal is to divide current service into an express route (as described above) and a local service, please see Route 2 In-Town on the next page for more details.

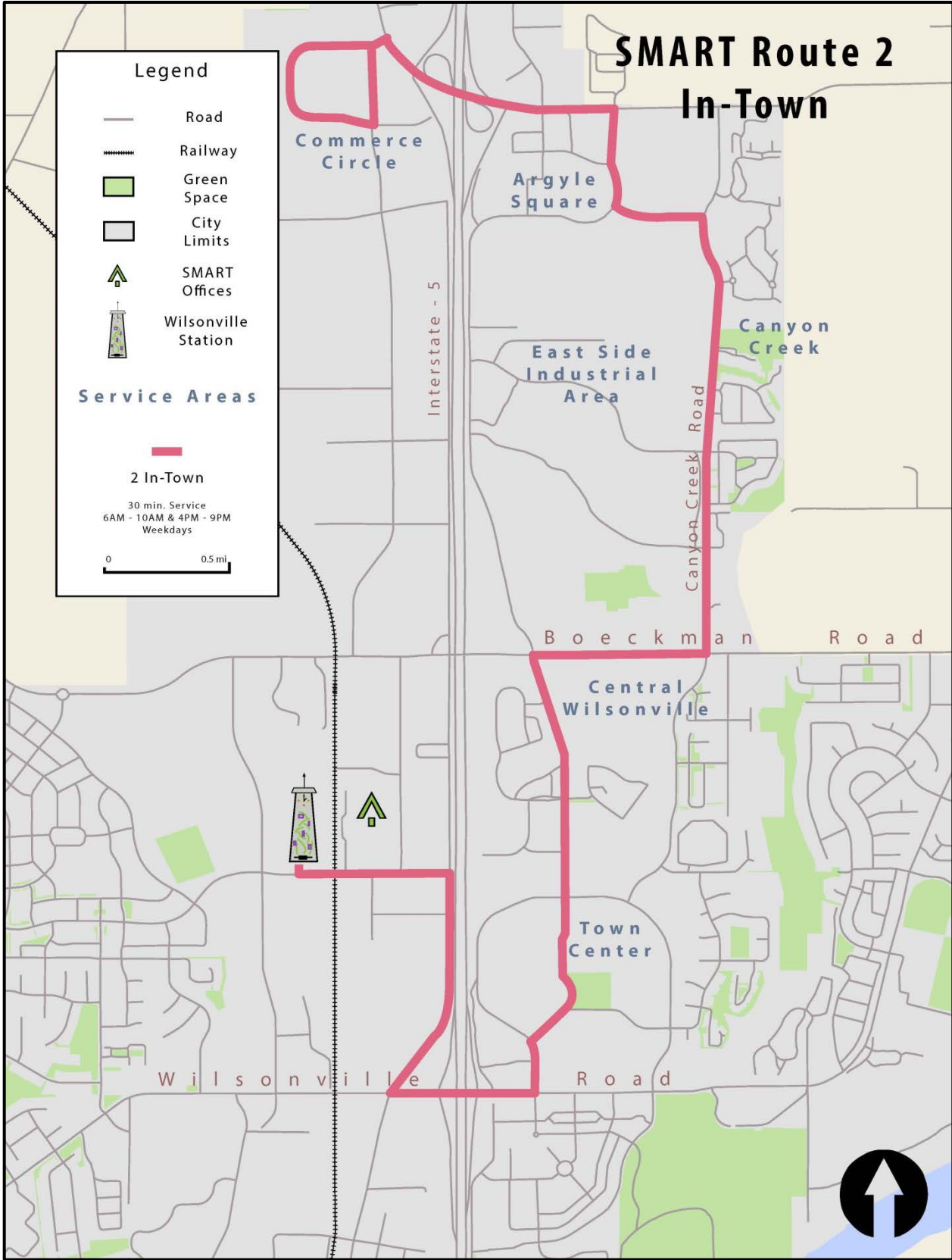


**Proposed Route 2X**

## Route 2 In-Town

To help provide better linkages between north and south Wilsonville, SMART staff recommends an additional local service, Route 2 In-Town. This service would be similar to the current in-town portion of the 2X but would increase connections to TriMet 96, commercial services, and employment. Service would run Monday through Friday every 30 minutes from 6:00 a.m. – 10:00 a.m. and from 4:00 p.m. – 9:00 p.m.

This proposed service would by-pass Printer Parkway in favor of Canyon Creek Road during peak times to help streamline the service. Service into the Parkway Woods campus would still be served by Route 6.



**Proposed Route 2 In-Town**



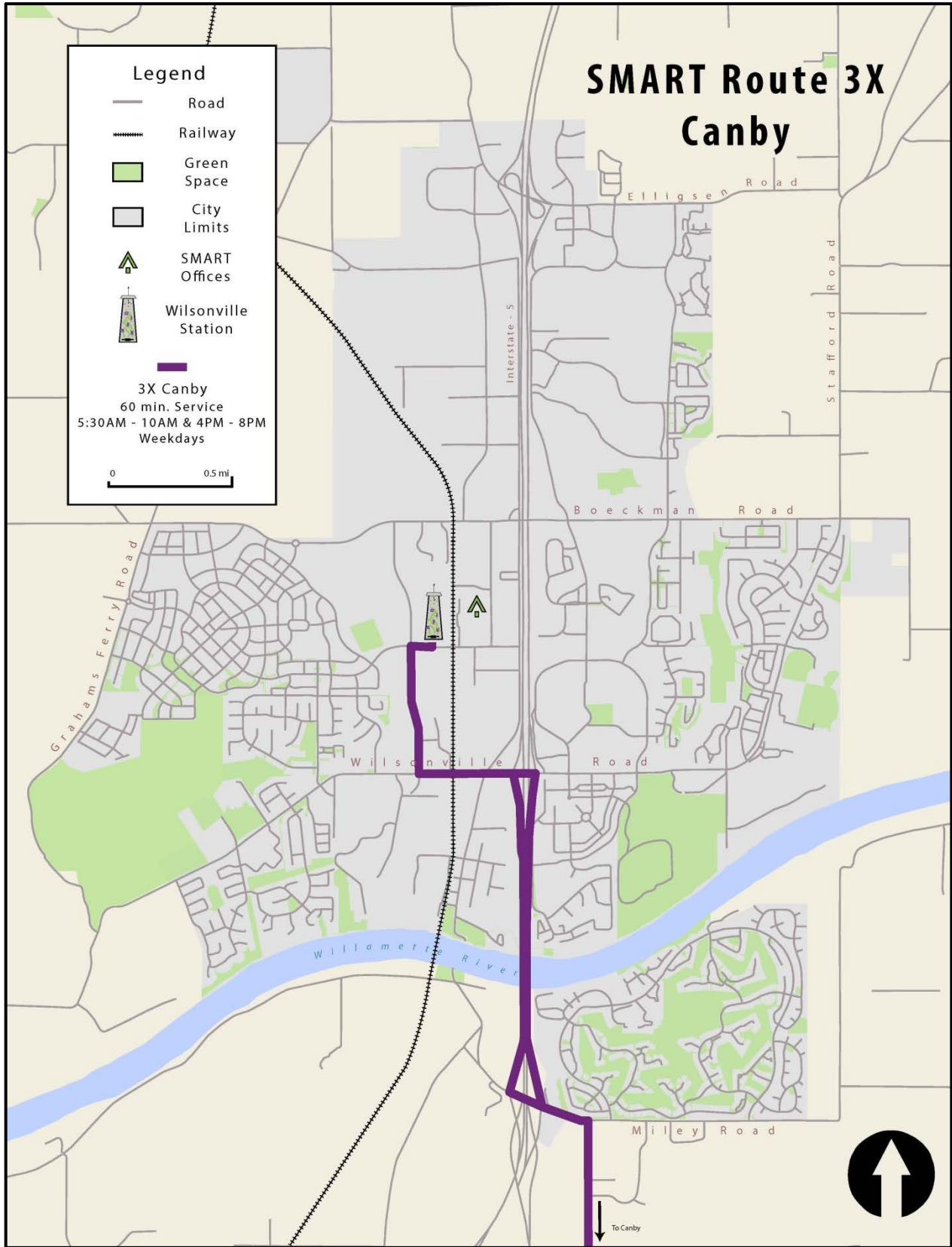
## Route 3X – Canby

SMART staff proposes that the current Route 3 be divided into an express route and local route. The proposed 3X would provide express service to Canby, bypassing the current stops in Charbonneau. To still provide service to Charbonneau, staff recommends a pilot Charbonneau shuttle as described on the following pages. The new 3X would enhance regional connectivity by connecting SMART routes and commuter rail with Canby Area Transit (CAT) and South Clackamas Transit District (SCTD) – Molalla (as well as to services and facilities in Oregon City).

Public input and ridership data found that many Charbonneau residents are not using the current Route 3 mainly because the service does not run when they want to use it. Residents of Charbonneau are more interested in local service during the day rather than commuter service to Canby.

There was also a strong demand for transit service from Wilsonville to Oregon City, primarily for access to social services and the main campus of Clackamas Community College. SMART is in discussions with CAT staff and consultants about ways to make that feasible. A possible route option for this service would be to avoid the traffic on Interstate-205 as it leads to unreliable schedules and increased costs.

SMART will provide hourly service from 5:30 a.m. – 10:00 a.m. and 4:00 p.m. – 8:00 p.m. service from SMART Central to Canby.



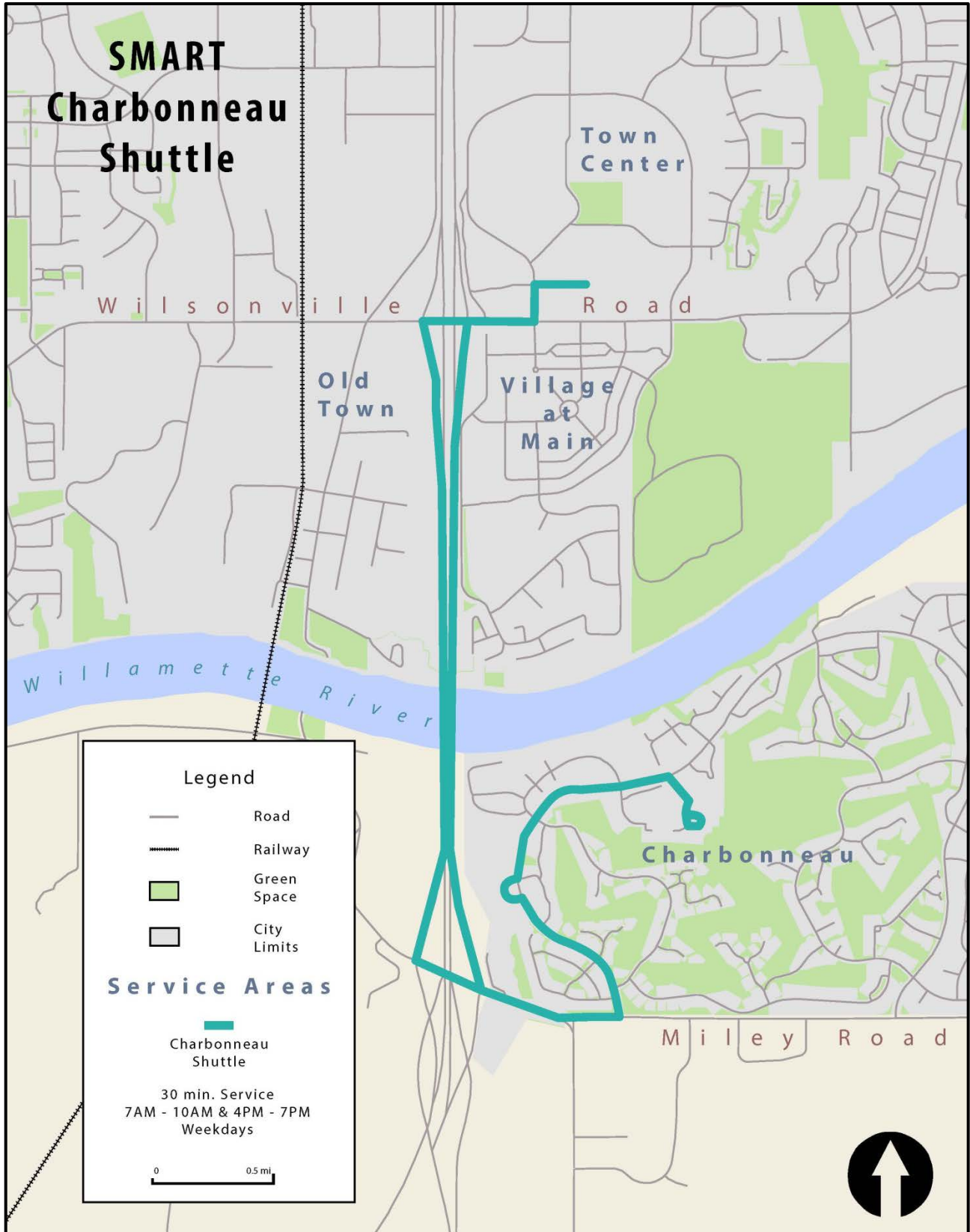
**Proposed Route 3X**

## Charbonneau Shuttle

SMART is recommending a Charbonneau shuttle service be established on a pilot basis, in lieu of the current Route 3 service to Charbonneau. This shuttle service would loop around French Prairie Drive, if requested, and bring passengers to services on the north side of the river. Shuttle service will focus initially on the commercial center of Charbonneau and on Spring Ridge during mid-day. Part of the new shuttle service to Charbonneau will include “shopper service” to the Town Center area, with deviation to the west side of town on request.

Currently, there are only two bus stops in Charbonneau. For many people who live on the east side of Charbonneau, this makes transit use impractical -- especially since there is no public parking available at the bus stop at SpringRidge. In recent years, SMART’s Route 3 has carried only a small number of people employed in Charbonneau, while residents tend to use SMART’s Dial-a-Ride service when needed.

Before implementing a Charbonneau Shuttle, SMART staff anticipates a focused outreach effort to gain input from Charbonneau residents on how the shuttle could best meet their needs.



**Proposed Charbonneau Shuttle**

## Route 4 – Crosstown

Route 4 is SMART’s primary local service that connects the east and west sides of Wilsonville so residents can access commercial services, schools, and parks.

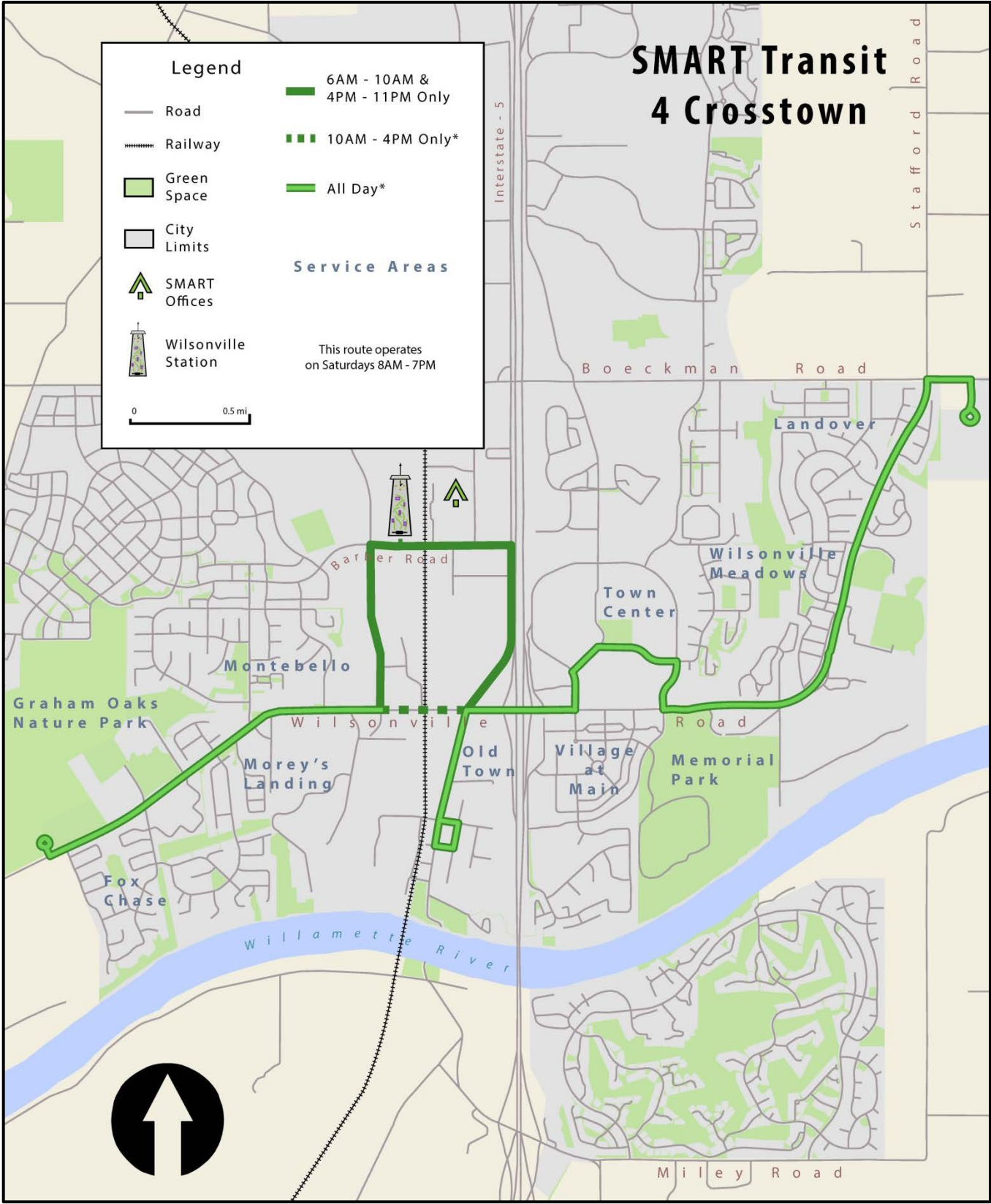
During the public outreach effort, SMART heard numerous concerns about the prolonged stops at SMART Central. From this feedback, SMART recommends streamlining Route 4 so that mid-day service from 10:00 a.m. – 4:00 p.m. would bypass SMART Central and stay on Wilsonville road. Streamlining this service will also change the route to omit the turnaround currently at Edge Fitness.

Staff also proposes to add service to this route as follows:

- 1) Extend weekday evening service from 7:42 a.m. to 9:10 p.m.
- 2) Add a half-hour of morning service starting at 8:00 a.m. instead of 8:30 a.m.
- 3) One hour of evening service from 5:52 p.m. to 6:52 p.m. on Saturdays

SMART will continue to provide crosstown service, while also serving an important role in connecting residents with commuter rail and transfers to inter-city service at the SMART Central station.

In the future, SMART may be extending Route 4 on the east side of Wilsonville to accommodate additional growth, including development in the Frog Pond area.



**Proposed Route 4**

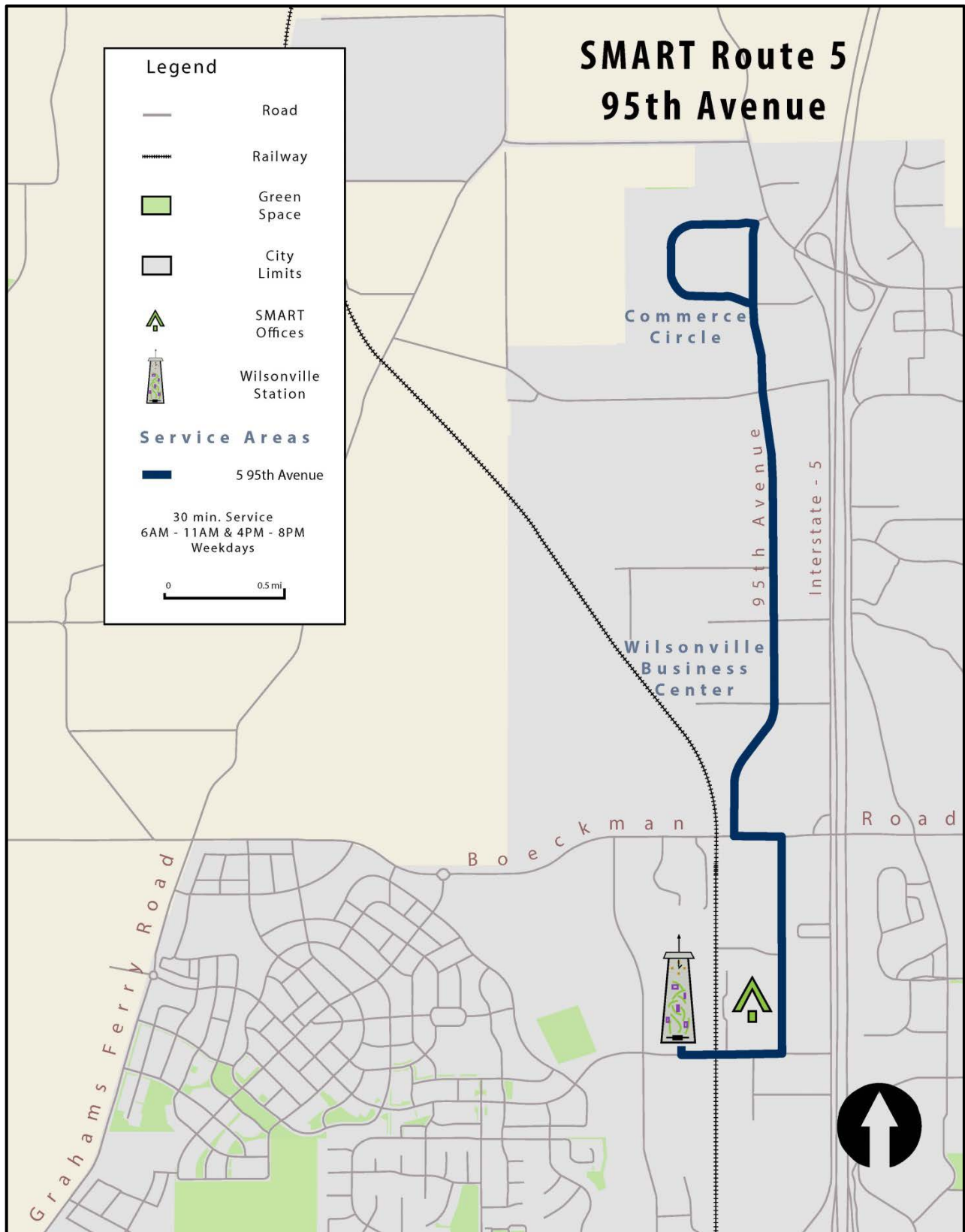
## Route 5 – 95<sup>th</sup> Avenue

The existing Route 5 provides an important link between TriMet's Route 96 and employment sites on the west side of Wilsonville. This route will remain the same with only a minor directional change at Commerce Circle to increase efficiency.

SMART has been fortunate to receive federal grant money to make sidewalk improvements where those improvements will enhance ADA access to bus stops. SMART anticipates that adequate funding will be available to make sidewalk and bus shelter improvements on Commerce Circle.

Route 5 will run Monday through Friday every 30 minutes from 6:00 a.m. – 11:00 a.m. and 4:00 p.m. – 8:00 p.m.

In the future, an extension of Route 5 will be needed to serve Coffee Creek based on the current progress of development in that area. Further development of the Basalt Creek area north of Day Road will require additional service to meet expected demand.



**Proposed Route 5**

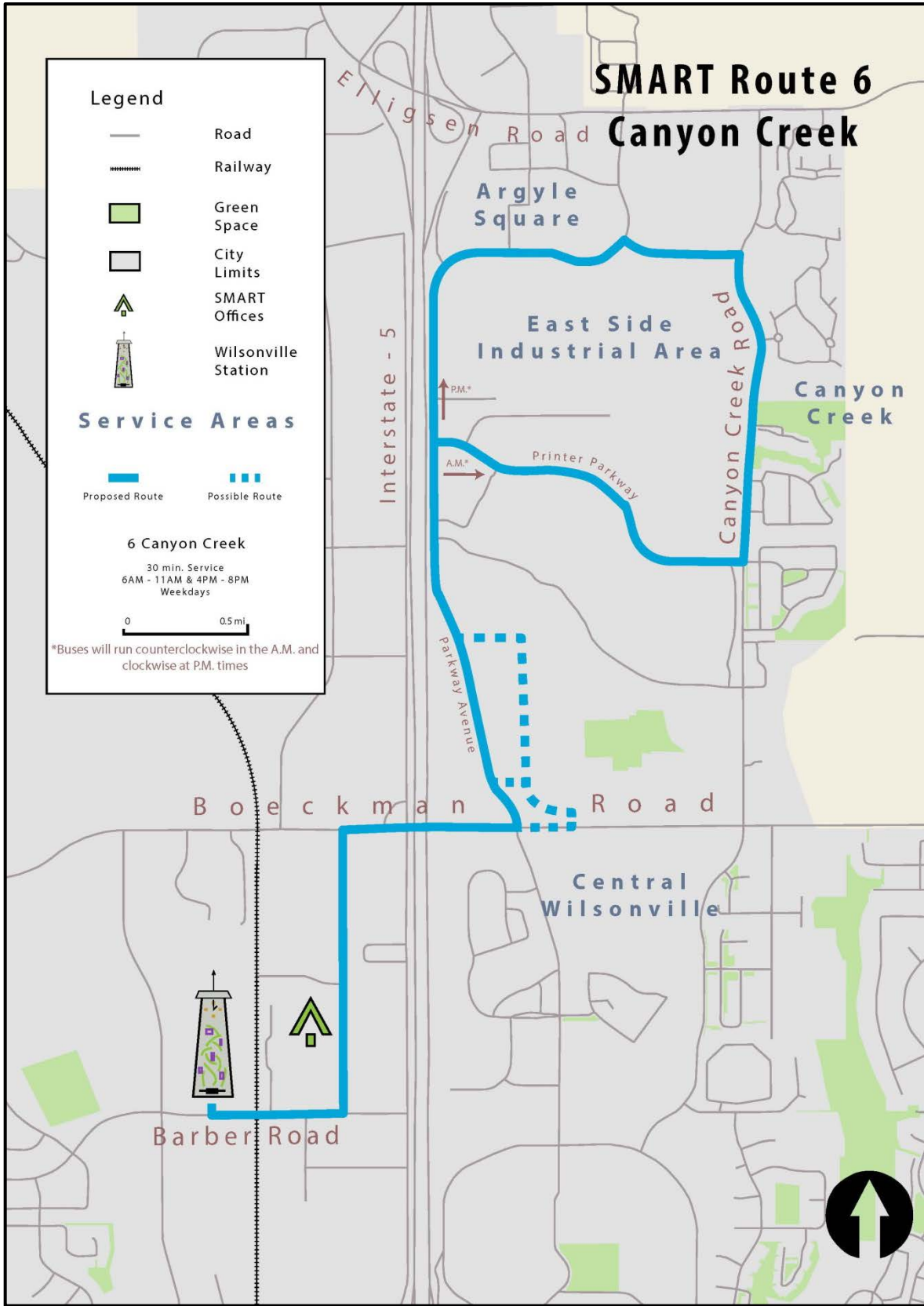


## Route 6 – Canyon Creek

Route 6 is a local service that connects commuter rail with employment, shopping, and residential areas on the east side of Wilsonville. Recommended service would run Monday through Friday every 30 minutes from 6:00 a.m. – 11:00 a.m. and 4:00 p.m. – 8:00 p.m. Service will run counterclockwise in the A.M. and clockwise at P.M. times.

Staff supports retaining most of the current configuration of Route 6 with a minor reroute such that it will by-pass Elligsen Road in favor of Parkway Avenue. This will allow SMART to provide faster service while bypassing an area that is seldom used by passengers. SMART has received public comment to continue service within the parking lots of Mentor Graphics, Oregon Tech, and FLIR and will work with businesses to accommodate their needs while ensuring it fits within the SMART network.

In the future, Route 6 may need to be extended to serve Frog Pond.



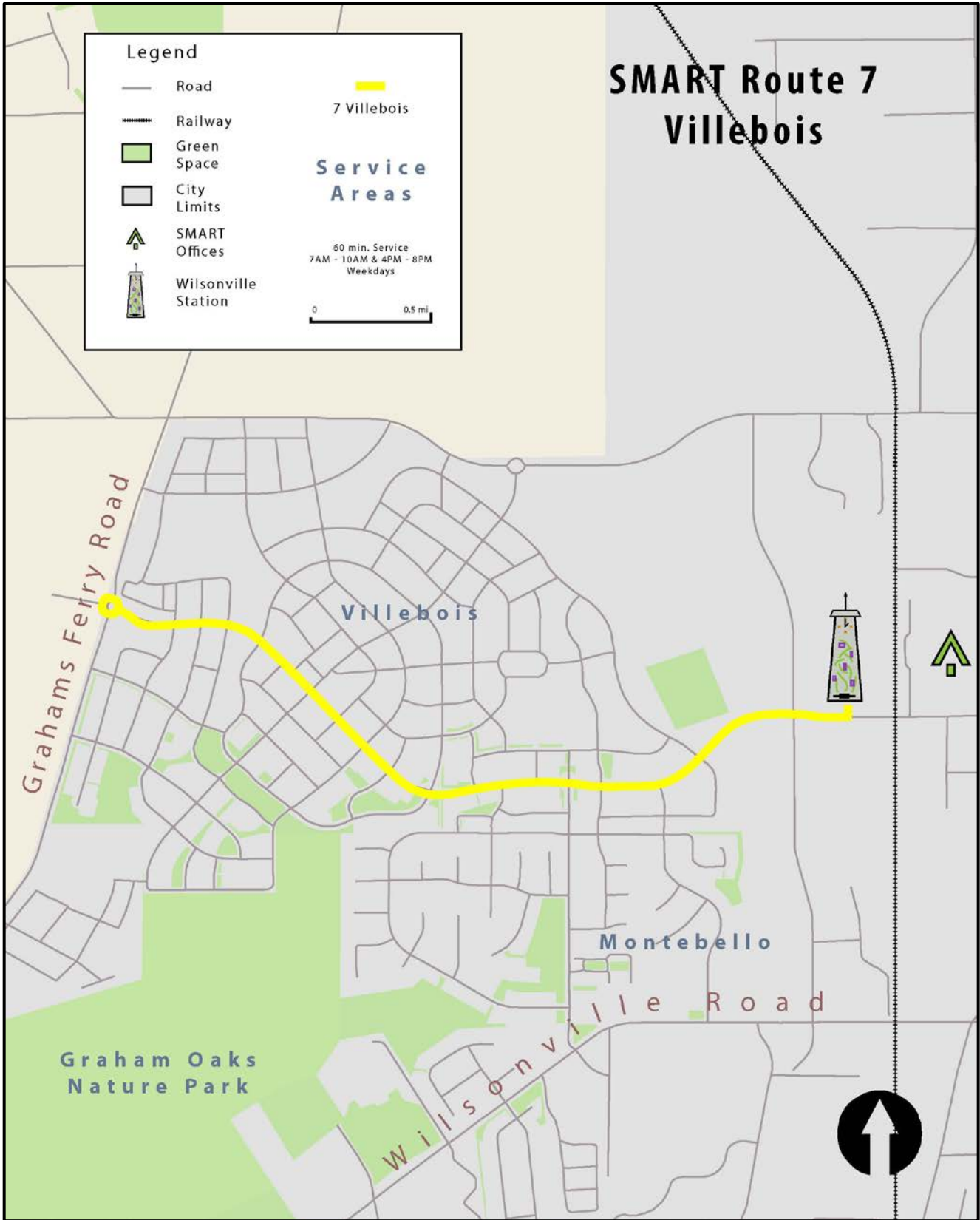
**Proposed Route 6**

## **Route 7 – Villebois and Villebois Shopper Shuttle**

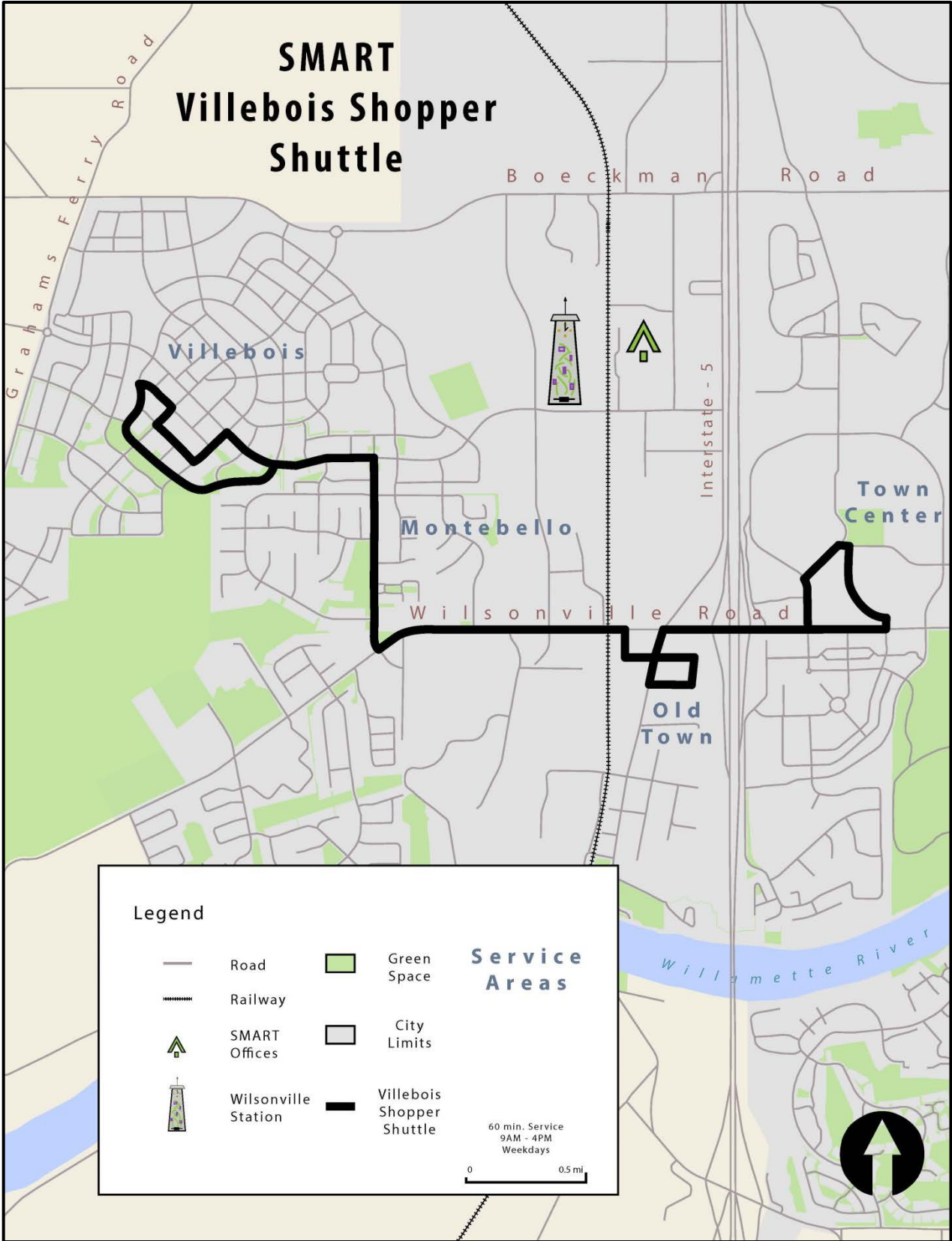
Route 7 provides local service to connect Villebois with commuter rail and other transit routes. Route 7 currently and will continue to operate hourly Monday through Friday during peak commute hours (7:00 a.m. – 10:00 a.m. and 4:00 p.m. – 8:00 p.m.). SMART recognizes that service demands are increasing in this area which is why if there is an increase in revenue, adding more fixed-route service would be a high priority.

SMART proposes streamlining the current route to increase efficiency by rerouting the service straight down Barber Road with a turnaround at Grahams Ferry Road roundabout.

In addition to retaining Route 7, SMART staff will continue to have the Villebois Shopper Shuttle. The Villebois Shopper Shuttle will continue to run hourly Monday-Friday from 9:00 a.m. to 4:00 p.m. In the future, there is potential to replace them both with a single deviated fixed-route service.



**Proposed Route 7**



**Proposed Villebois Shopper Shuttle**

# Transportation Options

Transportation options are the measures and techniques designed to encourage and provide more access to public transit, carpooling, ridesourcing, vanpooling, bicycling, walking, and telework as alternatives to driving. Implementing these options has a variety of benefits, from reducing traffic and filling in transit service gaps to increasing economic vitality and the health of people and the environment. For transit to be a viable option for people, it must be part of an integrated system that considers public/private partnerships, land use, fixed-route buses, demand-response service, taxis, carpools, vanpools, employer shuttles, bicycles, pedestrians, and innovative strategies to manage mobility.

## SMART's Role

SMART will continue to develop and assist with implementation of transportation options for employers in the City of Wilsonville by assisting with the overall administration and promotion of transportation options. The options below are currently being explored by SMART Options staff.

## Employer Vanpools

Vanpools can serve as a cost-effective alternative to providing new fixed-route service in some locations. Vanpools are typically a good choice for groups of 8 to 15 employees who share a similar commute. Riders generally meet in a specific common location, such as a Park & Ride lot, and drive to the worksite together. Vanpools are most likely to be a practical cost-saving option if the one-way commute is at least 15 miles.

A positive step towards vanpool formation would involve securing funding and setting up meetings at worksites to present vanpools as a viable alternative to single-occupancy vehicles commuting. SMART staff would assist with the process of identifying potential participants and employers, with the ultimate goal of matching them up. SMART is currently facilitating discussions between employers to help establish vanpools. In addition, SMART staff is



looking and will endeavor to apply for grants that could be used to buy down the participant's costs, which would in effect encourage participation in the program. The potential for vanpooling is very high in Wilsonville in part because of the concentration of large employment sites and the large number of people commuting into the City.

Typically, the cost of the van includes a mileage allowance, insurance, maintenance, and roadside assistance. Without subsidies, a 15-passenger vanpool from Vancouver to Wilsonville (approximately 25 miles each way) would cost approximately \$125 monthly per passenger. A subsidy could reduce the fare to approximately \$95. Employers could also subsidize their employees' vanpool expenses. Currently, tax laws allow an employer to contribute up to \$255 monthly per employee in benefits to commute by vanpool. This amount is tax-free for the employee and a business deduction for the employer. In the event that the employer does not wish to subsidize the employee's vanpool expense, the employee can still have a maximum of \$255 per month withheld from their pay as a pre-tax benefit. Direct payment and pre-tax deduction can also be combined in any proportion, up to a combined maximum of \$255. The 2015 Oregon Transportation Options Plan points out that transportation options also greatly reduce the annual cost of transportation per household; for example, a motor vehicle could cost \$10,000 a year while a vanpool costs about \$1,500.

## **Shuttles**

### *Commuter Shuttle*

Commuter shuttles between SMART Central and large employment sites could provide a practical alternative to fixed-route service that may not provide direct service. Employers could request a shuttle for their employees that could make half hourly runs to and from the transit center and employment site.

### *Corporate Lunch Shuttle*

A corporate lunch shuttle could take employees to and from their worksite to lunch. This shuttle would be by request only. A SMART shuttle could take employees to lunch at a set time into Town Center or somewhere within Wilsonville. This service can encourage using transportation options more often while also aiding in employee retention.

## **Ridesourcing**

Ridesourcing allows individuals to arrange rides using real-time online applications for pick-up and drop-off by drivers of personal cars in exchange for a fee. Two examples of ridesourcing companies that organize this system, known as Transportation Network Companies (TNCs), are Lyft and Uber. Studies have shown that ridesourcing is primarily used for late night or weekend social trips. Rather than being seen as competition with public transit service, TNCs are regarded as a complement to bus service, which typically has limited service late nights and weekends. Lyft Paratransit is an option to also help supplement service for the elderly and disabled. This service could help reduce the demand on SMART's Dial-a-Ride service. Currently Lyft boundary extends to the Willamette River, so service boundaries would need to be extended if Wilsonville sought out this option.



## Bikesharing

Bikesharing allows for individuals to use public bicycles to get between two points. Wilsonville is relatively small which makes bikesharing a realistic option for those wanting to grab lunch or bike to a meeting in town. Usually renting a bike costs very little or can be free for the first couple hours of use. Bike sharing stations could be set up at SMART Central, Town Center, and employment on 95<sup>th</sup> Ave allowing for bike commute to occur between these areas.



City of Portland BIKETOWN

## Carsharing

Carsharing provides access for people to use cars other than their own. Carsharing vehicles are either owned by carsharing companies or by individuals willing to share their own vehicle. Examples of well-known carshare companies are Avis Budget Group (owner of ZipCar) and Enterprise. This option provides short-term trips of varying types. Traditional carsharing trips have users return the vehicle at the same location they took it from. One-way trips let users drop off their vehicle at a different location from where they began. Peer-to-peer trips allow car owners to share their cars through a company. Advantages of carsharing include reduced costs of car ownership and less concern about parking space availability.

## Carpools

The area around Wilsonville is growing rapidly and an increasing number of residents from cities such as Sherwood, Newberg, West Linn, and Oregon City work in Wilsonville. Areas that may not have enough people coming from it would be a good candidate for carpools as they usually carry fewer people and those people utilize their own private vehicles. To assist in connecting people who want to carpool, Oregon provides an online ride-matching tool Drive Less. Connect.



**Drive less. Connect.**

Matching people with places.



# Goals & Implementation Measures

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This chapter highlights the measures taken and goals addressed to ensure that SMART continues to provide service that aligns with the goals created by the TMP Task Force. The policies and implementation measures are intended to ensure that transit is a viable option as the City of Wilsonville grows and the demands for transportation change.

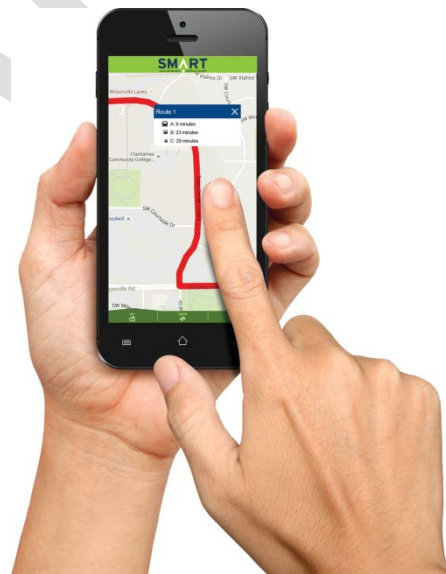
## Increasing Public Knowledge of Transit

Marketing and public information are key elements in maintaining and increasing ridership. SMART can provide service that effectively meets passengers' needs, but if people don't know it's there, they won't use it. As Wilsonville continues to grow, there are also many new residents and employees who may not have previously heard about SMART. There is great opportunity to leverage outreach efforts through coordination with other providers and existing resources. The actions that need to be taken in order to get the information to the intended audience are often very inexpensive and represent a good value in terms of increased ridership.

### Marketing of Services

#### *ETA SPOT System*

In the summer of 2016, SMART launched ETA SPOT (Spatial Positioning on Transit), a new application that provides real-time arrival predictions for all fixed-route buses. This technology will help people better navigate bus arrival times and give people options in how they want to access information. It also provides important data on ridership and other information needed by SMART staff. The app allows for users to make comments and quickly receive rider alerts. ETA SPOT is available to download for free from the Apple Store or Google Play and is also accessible on the SMART website to any computer.



#### *Printed Informational Materials*

Printed informational materials regarding SMART services allow for people without access to technology to get information they need. These materials need to be kept up-to-date while also making information available in Spanish. These materials are available to the public and are found at the library, City Hall, in welcome packets for new residents and employees at worksites with 100 or more employees, and at the community center.

### *Social Media*

Today, many people use social media outlets such as Facebook or Twitter as a means to communicate. SMART will continue to use campaigns through Facebook to reach out to those who use social media as a main source of communicating. This also allows for customers to interact with SMART through updates, 'how-to' videos and more.

### *Participation at Community Events*

Having SMART representatives at Wilsonville community events such as the Farmers Market provides an opportunity for residents and visitors to learn about the public transit system and their transportation options. These markets allow for individualized trip planning for passengers as well as information on SMART services. Summer months provide many opportunities to participate in community events such as Movies in the Park or Kiwanis Fun Run.

### *Informational Kiosks*

Informational kiosks are a useful means of providing information on a variety of transportation options in locations where there are likely to be crowds of people. Kiosks can consist of maps and brochures or can include interactive computer screens that provide information on transit routes, carsharing, bikesharing, carpools, vanpools, shuttles, bicycle routes, taxis, and local attractions, including restaurants and accommodations. In Wilsonville, the most useful location for a kiosk would be at SMART Central.

### *Website*

The SMART website serves as a valuable reference for current and prospective passengers who want to find out more about routes, schedules, and various transportation options. All programs and services can be found on the website such as the Walk Wednesday program. Employers can access the site to find information and assistance on developing transportation options for the workplace such as carpooling, ridesourcing, or shuttles.

SMART is working to find avenues to make it easier for transit riders of multiple systems for one trip (such as TriMet in Portland to SMART) to transfer between systems. This may be in the form of a website or app. SMART will need to work closely with other transit agencies to ensure that website links and information are regularly updated.

## **Specific Marketing**

Ensuring that necessary information is readily available and easy to understand will make it much easier for people to ride SMART. Marketing could be tailored to provide specialized information for employees, new residents, schools, or other specific groups. Marketing should include information on carpooling, vanpooling and other alternatives in addition to conventional transit services.

### *Employment Sites*

Employers have a particular interest in attracting the best employees available. The better the transportation options, the better their chances of attracting and retaining employees. In addition to turnover, employees less often have the traditional 9- 5 schedule. Marketing programs at employment sites should aim to provide a service or benefit in conjunction with the information. Many worksite programs that help to reduce traffic congestion and air pollution also provide a benefit for employees, including flex-time, telework, staggered work hours, parking cash-out, and preferred parking for carpools.

Providing additional information on the cost savings over driving and environmental benefits can serve as an added incentive. Low-cost incentives and promotional programs can also serve as effective tools to increase transit ridership and participation in carpools, vanpools, walking and bicycling. Many employers will not be aware of tax incentives for subsidizing vanpool and transit fares, and purchasing equipment such as bicycles, bicycle lockers, and telework hardware. SMART can ensure that they have that information. SMART can also assist in developing and implementing programs at worksites. Establishing strong relationships with employers is essential in ensuring that these programs are well supported.

### *Schools*

Marketing and information for school-age children is particularly important, because travel habits often become ingrained at an early age. The traffic impacts of children switching from car trips to other transportation modes is also higher because parents often have to make two round trips to drop off and pick up a child. Education on SMART and other transportation alternatives can often be integrated into the existing school curriculum. For instance, SMART has sponsored a SMART Art on the Bus program at Wilsonville schools. Students work together in small groups to produce art that depicts buses and other forms of travel. During the process, the costs and benefits of various transportation modes are also discussed. In years past, winning pictures were selected to be included on the exterior wrap of a SMART bus. More outreach to schools can be achieved through class demonstrations such as how to put a bike on the bus or a discussion on how the bus system works. There is also opportunity for establishing a Safe Routes to School Program at these schools.

### *Seniors and People with Disabilities*

Ensuring that seniors and people with disabilities have ready access to transit information provides them with greater freedom of mobility. Without adequate information, there is a greater likelihood that seniors and people with disabilities will forego some trips altogether or be totally reliant on friends or family to drive them around. Dial-a-Ride is available to qualified individuals by calling 24-hours ahead of time to reserve a ride. If the passengers have sufficient information on fixed-route service, they may be able to use it instead of Dial-a-Ride. This not only provides them with greater flexibility, but it also results in a lower per-trip cost than Dial-a-Ride. In addition to Dial-a-Ride, travel training is a free service provided by SMART and available for those who would like to learn how to use the fixed transit service.

## **Improving Service Quality**

Attaining a high quality of service is paramount if transit is to be competitive with the private automobile. Providing a safe, affordable, dependable, convenient, and comfortable service is the key to maintaining and increasing the current customer base. Passengers must be assured of quality service free of any discrimination based on minority status, age, gender, ethnicity, or disability. Customers must be confident that the bus will, as much as possible, arrive on time and that the bus is clean, safe, and well-maintained.

### **Schedule and Program Coordination**

The coordination of schedules between routes and transit providers is key in assuring that travel times are competitive with the automobile. SMART continues to work closely with TriMet, Cherriots, and CAT, to ensure that connections are convenient and wait times are minimized.

The SMART Options Program provides assistance to employers in setting up transportation options programs at their worksites. However, SMART can get additional leverage for the program by working cooperatively with Metro and other jurisdictions or agencies that support transportation options.

### **Customer Service Monitoring**

By maintaining a record of customer service calls, letters, and e-mail along with the resolution or action taken, SMART has been able to assess whether customer complaints are being effectively resolved.

Passenger surveys represent an opportunity to assess customer satisfaction and provision of equitable service. Passenger surveys include demographic questions in order to identify any service inequalities or unmet needs of specific demographic groups. The last demographic survey for SMART was done in 2002 and another one should be done again soon.

### **Emergency Ride Home Program**

A program that encourages employees to carpool, use transit, bike, or walk to work by the transit agency guaranteeing them a ride home in the event of an emergency. A free taxi ride is provided when an employee becomes ill at work, has to work unexpected overtime, or has a family emergency such as a sick child.

Nationwide, Emergency Ride Home programs have proven themselves to be an inexpensive insurance policy. They act as a strong incentive, yet few people actually end up needing to use the ride-home feature. The Emergency Ride Home Program in Salem costs less than \$2,500 per year to administer and covers 1,600 commuters. Programs can be structured so that participants are limited to a specified number of rides each year or quarter. SMART has had only an unofficial guaranteed ride home program for passengers. However, there are no set

guidelines and the program is not publicized. Developing guidelines, extending the program to carpoolers, and vanpoolers, and publicizing the program could provide a valuable tool in attracting and retaining passengers. The program also provides a benefit for employers and employees who participate in transportation options programs.

### **Maintenance, Security, and Vehicle Replacement**

SMART does not keep worn out or unreliable buses in service. SMART staff continues to inspect and repair all buses on a set maintenance schedule to ensure that there are no breakdowns during service hours. Thanks to successful State and Federal grant applications, SMART's fleet is relatively new by industry standards. SMART is developing a plan to work with the Transportation Security Administration to better assure system safety and security.

### **On-Board Amenities**

In order to attract passengers away from their cars, SMART must provide service which is competitive in terms of cost, comfort, and convenience. On-board amenities are a way of adding comfort and convenience. If passengers can access the internet, listen to music, watch the news, or relax in comfortable seats, transit may seem more attractive than driving. SMART should survey current and potential passengers to assess the value and cost-effectiveness of any on-board amenity being considered.

## Improving Access to Transit

Transit serves an important role in providing mobility to people who do not have access to automobiles, including the young, old, minorities, low-income residents, and people with disabilities. Transit-dependent individuals often rely on transit as a lifeline to connect them with jobs, shopping, medical appointments, and social interactions. SMART's first priority must always be to provide transportation for transit-dependent individuals. This includes measures to provide service within walking distance of all neighborhoods in Wilsonville, to provide schedule information in Spanish, and to offer demand-response service help to ensure that transit is accessible to those who need it most. Providing fare-free service within Wilsonville also ensures that cost is not a barrier. Although fares are charged for service outside of Wilsonville, these fares remain much lower than the cost of driving an automobile. Half-price fares are also available for youth, seniors, and people with disabilities.

Good access to transit means that it is safe, practical, and convenient to get to and from bus stops. Ideally, transit is located no more than walking distance of all residences and businesses. It also means that people are able to access the information they need to ride transit, such as schedules and maps.

Other considerations which can adversely affect access to transit are the lack of:

- Safe sidewalks
- ADA sidewalk ramps
- Street lighting
- Traffic signals
- Bus shelters
- Information for those with limited English proficiency
- Secure bike parking

### General Access

Factors such as transit shelters and building design and orientation affect all transit users. Transit shelters provide improved access by providing shelter from weather, an opportunity to sit down, lighting, and a display of route and schedule information.

Many buildings are designed with a priority for automobile access, making access from the sidewalk difficult or hazardous. Bus service in parking lots is generally inadvisable because of the many hazards from motorists backing out and people walking to and from their cars. However, if a person has to endure a long walk through the parking lot with bags of groceries to get to a bus stop, transit is not a practical option.

This problem highlights the importance of orienting buildings towards the sidewalk. SMART will work closely with other City departments to ensure that pedestrian and bicycle access is expanded and that new developments are designed to be transit friendly.

## Bus Shelters and Bus Stop Seating

Bus shelters and benches provide the best protection from the elements and comfort at bus stops. Some of SMART's older shelters are now in need of major repair or replacement. Bus stops with seats bring opportunities for passengers to sit down while they wait for the bus. Although they do not provide shelter from the elements, they are particularly useful for people who are not able to stand for long periods of time.

An updated bus stop priority list should be based on:

1. Number of passenger boardings per day – this criterion leverages limited resources to benefit the greatest number of customers.
2. Designation as a major transit stop - Since major transit stops are often transfer points, shelters are particularly important for these locations.
3. Type of population served - Locations that serve higher concentrations of transit-dependent and lower-income populations are given priority for shelter placement.
4. Availability of a nearby bus shelter - Shelters are distributed to obtain maximum coverage and equal access.
5. Preparation required - All other criteria being equal, level sites that provide adequate placement area with minimal impact to surrounding properties are preferred.



## Pedestrian and Bicycle Access

Providing good pedestrian and bicycle networks help to ensure that there will be good access to transit as well, particularly if pedestrian, bicycle, automobile, and transit are well interconnected. In an ideal situation, a person would be able to use any mode of transportation or combination of modes to reach a destination; this provides the greatest level of choice in mobility. Reality is more likely to present gaps in the pedestrian and bicycle networks which also create problems in accessing bus stops. A short gap in the sidewalk network or one unsafe crossing can render the trip to a transit stop impractical. Pedestrian access is therefore the key ingredient in assuring accessibility to transit.

Bicyclists have a larger “travel shed” than pedestrians but also depend on the ability to store or transport their bicycles. Bike racks are provided on all buses, so that passengers can bicycle at either end of their bus trips. SMART allows bicycles on buses when bus racks are full and there is ample room on the bus. Bicycle storage and a repair station are also provided at SMART Central.

Encouraging bicycle access to transit allows transit to attract customers from an area beyond walking distance. There are a number of reasons why people combine bicycling and transit:

- Either the origin or destination is not within walking distance of a transit stop.
- They want to get some exercise, but their trip is too long to bicycle the entire distance.
- They may want to go for a recreational ride at their destination.
- They don't drive or don't want to drive.

The Wilsonville Bicycle and Pedestrian Master Plan identifies gaps in the sidewalk network and recommends improvements, based on a number of factors including connections to bus stops. Completing these gaps will greatly improve safe access to transit. In particular, the identified improvements to Boeckman Road will provide pedestrian and bicycle connections between the commuter rail station and residential sites on the east side. Provisions for safe pedestrian crossings also need to be required of new construction, including, where applicable, curb ramps, bulb-outs, medians or pedestrian refuges, flashers or signals, and traffic calming measures, to ensure that pedestrians can safely travel to and from bus stops.

### **Park & Ride**

Automobile access to various transportation options is a solution to reducing traffic but also allowing for independence. Currently, the only Park & Ride lot in Wilsonville is at SMART Central. This lot provides more than 400 spaces and continues to operate with excess capacity. Additional land for expansion of the Park & Ride lot has been acquired and can be developed as the need arises. Having a Park & Ride lot on the east side of the freeway would also improve transit access for travelers who otherwise have to deal with getting from one side of the freeway to the other.

### **Senior and Disabled Access**

Mobility and independence are important quality-of-life issues for seniors and people with disabilities. Transit can provide an important lifeline, linking people to work, shopping, community connections, and medical appointments. Improving access to transit not only creates a greater sense of freedom and mobility for senior and disabled passengers, it can also prove to be cost-effective over the long run. Seniors and people with disabilities are also likely to be more sensitive to access issues. For instance, a senior may be mobile enough to walk to a bus stop, but unable to stand and wait for any length of time. In such a case, the availability of a shelter determines whether or not transit is a viable option. Similarly, a person in a wheelchair may be able to get to the bus stop with no problem as long as there is a continuous sidewalk with properly designed ramps.

All SMART buses are LIFT-equipped and accommodate wheelchairs. Dial-a-Ride service is available, but must be scheduled in advance. SMART also provides transportation to medical appointments in the Portland area for Wilsonville seniors and people with disabilities. A coordinated effort with TriMet and other providers would help to streamline the process for Dial-a-Ride trips between transit agencies, so that reservations for one trip could be made



with a single phone call. Dial-a-Ride customers can transfer between SMART and TriMet services, but they must schedule the two services separately.

SMART will continue to coordinate with the Wilsonville Community Center to offer senior luncheon, shopping and activity trips, providing seniors with mobility, and an opportunity to run errands and socialize.

SMART still needs to conduct an overall detailed evaluation of the transit system's accessibility for seniors and people with disabilities. The evaluation should be performed in consultation with people who represent seniors and people with disabilities, as well as the affected persons themselves. This process would provide SMART with a list of obstacles or deficiencies that need to be addressed while also dealing with ways to decrease costs and increase system efficiencies. Topics to be addressed are expected to include:

- Redefining the boundaries of SMART’s out-of-town Dial-a-Ride service;
- Creating a medical shuttle service;
- Potential public/private partnerships;
- Consideration of different types of vehicles with different accessibility features; and

*Travel Training*

Travel training consists of showing people who have never taken the bus where they need to board and how to read the schedules. A trainer goes with an individual or a group and shows them how it works. This training is extremely valuable, since "fear of the unknown" keeps many people from trying transit even though they would like to. Travel training is a particularly effective tool to introduce seniors and people with disabilities to the benefits of using fixed-route transit.

In December 2016, SMART partnered with Portland-area non-profit Ride Connection to have an employee at SMART offices to provide travel training (RideWise) to seniors and those with disabilities. This service is free to anyone in Wilsonville who qualifies. Information about this service is found in brochures and online and presentations given at the community center, retirement homes, and homes for those with disabilities.




## Access for those with Limited English Proficiency

Language barriers limit access to transit when prospective passengers are unable to get the information they need on routes and schedules. The largest concentration of non-English speakers in Wilsonville speaks Spanish. According to the 2010 U.S. Census, 12% of Wilsonville's population spoke Spanish as a native language and spoke English "less than very well". SMART continues to expand the information it provides in Spanish on the schedules and on the website.

SMART has enacted a Title VI program in compliance with federal requirements and that program now guides SMART's efforts to reach out to and accommodate people with limited English proficiency (see Appendix C). SMART also has multi-language translation available for in-coming phone calls. SMART also translates all written materials to Spanish and regularly has Spanish translators available at public meetings.

The transit schedule pages are primarily graphic and numeric and therefore understandable to most non-English speakers. However, SMART will need to continue to expand outreach efforts of all kinds to the Spanish speaking members of the community.

**¿Llevar a su bicicleta en el autobús?**



1. Espere en la acera para que llegue el autobús.
2. Cuando el autobús este detenido, haga contacto visual con el conductor para que vean que necesita cargar la bicicleta en el autobús.
3. En la parte delantera del autobús, coloque su bicicleta para que la llanta delantera este en el lado con el gancho y las ruedas se alinean con el marco.  
*Si no hay otras bicicletas ahí, es posible que tenga que apretar la palanca para bajar la rejilla hacia abajo. Si usted es el único usuario, colóque su bicicleta en el marco más cercano al autobús.*
4. Jale el gancho sobre la llanta delantera para asegurar.
5. Asegúrese de quitar todo los artículos sueltos de su bicicleta, incluyendo maletas.
6. Cuando esté listo para salir del autobús, salga por la puerta delantera y notifíqué al conductor que necesita obtener su bicicleta.
7. Después de descargar su bicicleta, levante la rejilla en la posición cerrada si está vacía.
8. No crucen la calle en frente del autobús, el tráfico de paso no los puede ver.

**SMART**  
SOUTH METRO AREA REGIONAL TRANSIT  
503-682-7790 ridesmart.com

## Service Expansion

SMART aims to provide service to new developments in Wilsonville and increase opportunities for residents, employees, and visitors to connect to jobs, shopping, parks, and community activities. By continuing to expand service, SMART can help facilitate regional connectivity and access to other cities.

### **Transportation Management Association (TMA)**

A TMA is a public/private partnership of business members, transportation providers and governmental entities that addresses important transportation issues and provides cooperative service. Members work together to organize commuting options for employees, customers, and the community. These options include carpools, ridesourcing, vanpools, transit, bicycling, walking, and telework. The goal of a TMA is to provide commuting solutions for the community, while also helping to reduce traffic congestion and improve air quality.

Wilsonville may not have a sufficient concentration of employees to justify a TMA yet, however, with development of the industrial lands near Coffee Creek and other increases in employment, a TMA will probably be viable in the near future.

### **Service Goals to Accommodate a Growing Community**

SMART believes that the lifeblood of any successful organization rests in its willingness to be introspective. Self-evaluation is necessary if SMART is to be true to its core mission of providing safe, reliable and cost-effective service. With this as its roadmap, SMART has established specific goals which were designed to speak directly to how it plans to deliver on its core mission. The list below highlights seven service goals SMART has established for accommodating a growing community.

1. Respond to public comments requesting more focus on inter-city transit service
2. Facilitate coordination of transportation options (vanpools, etc.) with bus transit
3. Provide service to new developments in Wilsonville
4. Provide increased opportunities for residents, employees, and visitors to connect to jobs, shopping, parks, and community activities
5. Facilitate regional and inter-city connectivity
6. Increase service frequency to improve convenience and coordination between routes
7. Extend service times over longer periods to accommodate travel needs

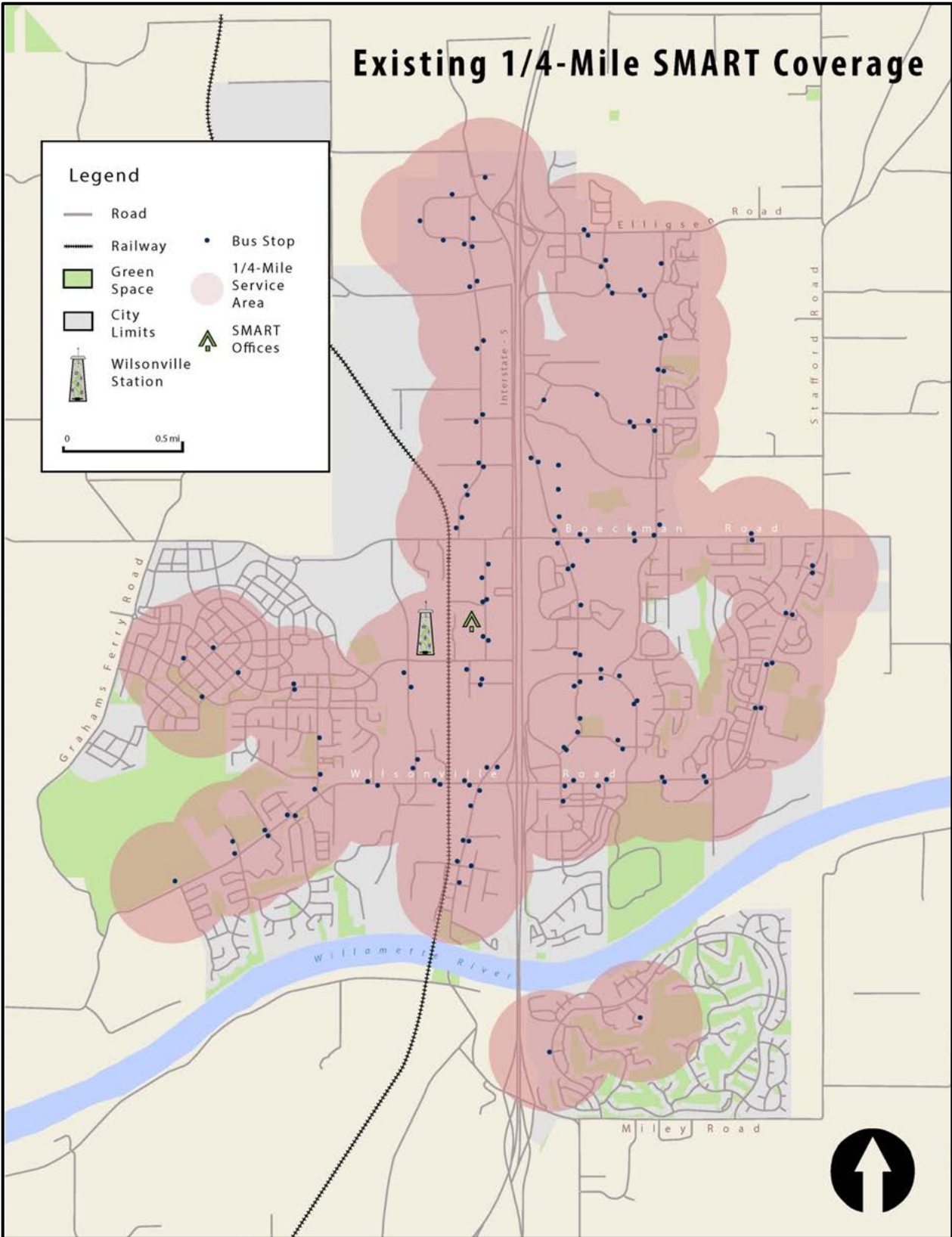
An objective of SMART's service network is to provide bus service within a fourth of a mile of all developed areas within City limits. This service coverage goal, though aggressive, is achievable and will prove to be a welcome convenience as Wilsonville continues to grow and SMART begins to focus more on inter-city service. Please refer to the map below to see SMART's existing 1/4 mile coverage.

# Existing 1/4-Mile SMART Coverage

**Legend**

- Road
- Railway
- Green Space
- City Limits
- 🏠 Wilsonville Station
- Bus Stop
- 1/4-Mile Service Area
- 🏢 SMART Offices

0 0.5mi



## Increasing Efficiency and Improving Air Quality

Transit generally provides air quality and traffic congestion benefits over automobile travel. However, there are ways to create even greater benefits. Promoting transportation options as a package can also help to reduce traffic congestion and air pollution. The use of alternative fuels provides additional opportunities for transit to reduce its impact on the environment and air quality.

### Transportation Systems Management (TSM)

TSM approaches work to increase the operational efficiency of the existing transportation infrastructure. As applied to transit, TSM measures help to reduce bus travel times, making transit more competitive with the automobile.

The two TSM measures that are most likely to work for SMART are traffic signal priority and queue bypass. They should also be carefully reviewed by traffic engineers to ensure that they do not result in negative impacts on overall traffic flow. The most effective location for implementation of either measure would be on Wilsonville Road between Town Center Loop E and Boones Ferry Road.

#### *Traffic Signal Priority*

Buses use a mechanism to alert a traffic signal of their approach. The approach of a bus either allows for the light to turn green earlier or to stay green longer. Signal priority is not the same as signal preemption, which is reserved for emergency vehicles. Preemption alters the normal operation of the signal, while priority is much less disruptive.

#### *Queue Bypass*

Buses are allowed to use a right-turn-only lane to proceed through the intersection. This allows the bus to bypass the line of through traffic at the intersection, making service more time and cost efficient.

### Transportation Options

Transportation Options programs make the transportation system more efficient and reduce pollution without adding major infrastructure. These programs are most effective when complementary elements are packaged together to fit the needs and conditions of a given site or area. Supporting infrastructure, such as functional sidewalks, bicycle racks and transit shelters, as well as transit services are all important to a successful program.

To meet the target for the Regional Transportation Plan, SMART will need reach and maintain a mode split target of 15 percent. Transportation options mode split refers to the share of trips made by a mode of transportation other than single-occupant vehicles. To ensure this target is reached SMART must work closely with employers to assist them in designing and implementing work-site programs for ECO program compliance or to benefit their employees.

In addition SMART will continue to support DEQ requirements that large employers (100 or more employees) submit transportation options plans to show how they will reduce transportation impacts.

## **Alternative Fuels and Solar Options**

SMART currently uses low-sulfur diesel with five percent biodiesel content in most buses, and would like to further reduce environmental impacts by switching to cleaner fuel sources. SMART now operates four compressed natural gas (CNG) cutaway buses and two diesel-electric hybrid buses. SMART has also attempted to secure grant funds for one or two battery-electric buses. Alternative fuels are currently being used by a number of transit providers with successful results. In many cases, initial costs are prohibitive without substantial grant revenues. As the alternatives become more available, less costly, and more reliable, these options should be pursued.

### *Clean Fuel Programs*

Both ODOT and the FTA have provided a variety of grant opportunities to support transit agencies in improving air quality and reducing greenhouse gas production. SMART has actively participated in those programs and has been the beneficiary of grants on occasion. That will continue and SMART will continue to pursue grants funds for battery-electric buses and related charging infrastructure. Two other options that may help to meet SMART's clean fuels objectives are renewable diesel or higher concentration biodiesel.

### *Solar Power and Photo-Luminescent Materials*

Solar panels are being used more widely now to power electronic parking meters, emergency phones, and a number of other uses. Solar panels can also be used to power lighting at bus shelters. This would also reduce the cost of running electrical conduit to the shelters for current lighting. The costs of photo-voltaic lighting systems continue to decline and the effectiveness of the equipment continues to improve.

Photo-luminescent materials are another means of lighting informational displays at bus shelters. The light-weight material can be applied as a decal or as an entire panel. It absorbs light during the day and continues to provide light several hours after dark.

# Creating a Sense of Community Ownership

Wilsonville residents have a strong sense of pride in their community and in the SMART transit system. SMART continues to provide service that will enhance community connectivity and help create a sense of ownership. The ideas below are currently being implemented or explored by SMART to enhance community ownership.

## Community Event Transportation

Transportation to community events not only provides a convenient option, it also provides opportunity for residents to connect to one another and participate in city events. SMART provides service to Wilsonville’s Farmers Market and Rotary Concerts in the summer. SMART also occasionally provides transportation to community functions such as sports events and the County Fair. Providing these services is an effective way to encourage people who would not normally ride the bus to try it out. Once they are familiar with the service, they may feel more comfortable using it on a regular basis.

## Naming Bus Routes

Naming bus routes after landmarks or symbols provides an easier frame of reference for some passengers than a route number. It can also provide a sense of ownership for the community. Bus routes can be named after important destinations along the route.

## Shelter Design

SMART could allow neighborhoods and businesses the opportunity to design their own bus shelters, providing them with a shelter that reflects the character of the individual neighborhood. While there are basic functional requirements for a bus shelter, there is also plenty of room for innovation and individuality. Even a basic shelter could be decorated with artwork produced by local adults or children.



The artwork is then transferred onto the shelter as decals (similar to the ones used on the buses), or the glass could be etched to create a monochrome image. This approach has been used on shelters near Wilsonville High School, where the Wildcat theme is proudly displayed.

### **Employer Recognition**

Many employers have innovative programs that encourage their employees to ride transit and use other transportation options. Recognizing these employers with an annual awards program would let them know that their efforts are appreciated. SMART could also recognize and support Wilsonville employers by displaying informational materials on environmental programs or health and wellness efforts at particular worksites.

### **Unique Vehicles**

The SMART trolley is a good example of a unique vehicle that Wilsonville residents recognize as a mobile community landmark. In fact, the trolley has been so popular that people often ask if their next ride can be on the trolley. SMART could look at additional opportunities to acquire vehicles which are distinct, aesthetically pleasing, and fun. One option could be a double-decker bus, which could provide the capacity needed for routes that have standing room only without adding the expense of another bus and driver. Adding more trolleys to the fleet could also be a popular option.

### **Art and Poetry on the Bus**

SMART has sponsored a very successful Art on the Bus program, which provides Wilsonville students with an opportunity to display their artwork on the bus. SMART could also sponsor poetry contests, with the selected poetry displayed on cards in the bus in the area above the windows that is often used for advertising cards on other transit systems. For each contest, participants would focus on a particular subject, which could range from Wilsonville, parks, transportation, seasons, or a number of other subjects. The costs to offer such a program are very low and would provide an opportunity for many residents to see their work displayed. SMART intends to revamp this program to bring more transportation education to youth.





# Appendices

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- A. Glossary*
- B. Route Priorities*
- C. Title VI Program*
- D. Planning Framework*
- E. Neighborhood Characteristics*
- F. TMP Public Outreach Summary*

DRAFT

## Appendix A – Glossary

### A

#### ***Accessibility***

The extent to which facilities, including transit vehicles, are barrier-free and can be used by people who have disabilities, including wheelchair users.

#### ***Access to Jobs***

Federal funding for programs to increase work-related transportation available to low-income individuals.

#### ***Active Transportation Plan (Regional ATP) 2014***

The Regional ATP provides a vision, policies and actions to connect active transportation, such as walking and biking, to public transit within the Metro region. An emphasis on developing safe conditions for walking and biking near transit centers will help to integrate active transportation with public transit.

#### ***ADA***

Americans with Disabilities Act: Passed by the Congress in 1990, this act mandates equal opportunities for persons with disabilities in the areas of employment, transportation, communications and public accommodations. Under this Act, most transportation providers are obliged to purchase LIFT-equipped vehicles for their fixed-route services and must assure system-wide accessibility of their demand-responsive services to persons with disabilities.

#### ***Alternative Fuels***

Vehicle engine fuels other than standard gasoline or diesel. Typically, alternative fuels burn cleaner than gasoline or diesel and produce reduced emissions. Common alternative fuels include methanol, ethanol, compressed natural gas (CNG), liquefied natural gas (LNG), biodiesel, clean diesel fuels and reformulated gasoline.

#### ***Alternative Mode***

Any type of commuting transportation other than single occupancy vehicle that results in reduction of automobile commute trips, e.g. carpooling, vanpooling, bicycling, walking, transit, and telework.

#### ***Alternative Work Schedules***

Any programs, such as compressed work weeks, flex time, telecommuting, staggered shifts, or any other program that eliminates pm peak-hour trips

## **AoA**

Administration on Aging. The agency within the U.S. Department of Health and Human Services that oversees the implementation of the Older Americans Act, including senior nutrition programs, senior centers and supportive services for elders.

## **Appropriation**

The step at which a legislative body and chief executive have agreed and signed into law an approval to spend public funds on specified programs and projects. Within the federal government, no funds may be spent unless their appropriation has been approved by Congress and signed into law by the President.

## **Automobile Dependency**

Transportation and land use patterns that result in high levels of automobile use and limited transportation alternatives. In this case, "automobile" includes cars, vans, light trucks, SUVs and motorcycles.

## **B**

### **Bus Pullout or Turnout**

A pullout is a specialized bus stop where a transit vehicle can load or unload passengers in an area separated from the traffic lanes.

### **Bus Rapid Transit**

Bus Rapid Transit - BRT is designed to be an alternative to light rail service using bus vehicles. BRT usually is made up of dedicated bus lanes with stations spaced at train-like intervals. BRT can offer more frequent, and more convenient service than regular buses, similar to a rail line, but may be cheaper to build or more flexible where buses can leave the bus-way to reach other areas.

### **Buy America**

Federal transportation law which requires that all purchases of vehicles, equipment or any other manufactured item be of US-made and assembled components, unless the purchase price is less than \$100,000 or the DOT has given the purchaser a Buy America waiver.

## **C**

### **Capital Costs**

Refers to the costs of long-term assets of a public transit system such as property, buildings and vehicles.

### ***Carpool***

A prearranged ridesharing service in which a number of people travel together on a regular basis in a car. Some carpool arrangements involve the exchange of money in exchange for driving, while others simply trade off driving.

### ***Carshare***

Carshare refers to automobile rental services intended to substitute for private vehicle ownership. It makes occasional use of a vehicle affordable, even for low-income households, while providing an incentive to minimize driving and rely on alternative travel options as much as possible. Carsharing can also be used at worksites to provide transportation from commuter rail stations and for employees' midday errands.

### ***CARTS***

The Chemeketa Area Regional Transportation System (CARTS) is a partnership between Marion, Polk, and Yamhill counties that coordinates resources to provide transportation for senior citizens, disabled, and economically disadvantaged residents access to medical services, employment, education, shopping, and recreation. CARTS is operated by Salem-Keizer Transit (Cherriots).

### ***CAT***

Canby Area Transit (CAT) provides fixed-route and dial-a-ride service in Canby and provides connections with SMART, TriMet, and SCTD.

### ***CDL***

Commercial Driver's License: The standardized driver's license required of bus and heavy truck drivers in every state. Covers drivers of any vehicle manufactured to seat 15 or more passengers (plus driver) or any vehicle over 13 tons gross vehicle weight. The CDL is mandated by the Federal government in the Commercial Motor Vehicle Safety Act of 1986. All SMART drivers, dispatcher, and mechanics have CDL's.

### ***Cherriots***

The name given to the bus service operated by Salem-Keizer Transit. In addition to Salem-area service, Cherriots also operates service between Salem and Wilsonville, coordinating with SMART's 1X service.

### ***Clackamas County Transportation System Plan 2013***

The Clackamas County Transportation Plan strongly emphasizes the link between land use and transportation. It calls for increasing transit use by encouraging land use patterns, development designs, and street and pedestrian/bikeway improvements that support transit. The plan sets a goal of transit service within 1/4 mile of most residences and businesses within the Portland Metropolitan UGB.

### ***Clean Air Act***

Federal regulations which detail acceptable levels of airborne pollution and spell out the role of state and local governments in maintaining clean air.

### ***Climate Smart Strategy 2014***

The Climate Smart Strategy was created in response to a state mandate to provide the Portland metropolitan region with a strategy for reducing greenhouse gas (GHG) emissions from cars and small trucks. The document includes nine key policy recommendations in reducing GHG from light duty vehicles, three immediate actions that focus on transportation funding, a toolbox of possible actions for 2015-2020 and regional framework plan amendments.

### ***CMAQ***

Congestion Mitigation and Air Quality: A flexible funding program administered by the Federal Highway Administration (FHWA) which funds projects and programs to reduce harmful vehicle emissions and improve traffic conditions. CMAQ funds may be used flexibly for transit projects, rideshare projects, high-occupancy vehicle lanes or other purposes.

### ***Community Transportation***

Transportation services that address the transit needs of an entire community, including the needs of both the general public and special populations.

### ***Commuter Rail***

Commuter rail is passenger train service that often shares tracks with freight or inter-city trains. Commuter rail trains are usually made up of coaches hauled by a locomotive and serve downtown travel markets. Most train service is concentrated in peak hours of travel. Train stations are usually about five miles apart to allow the heavier trains to accelerate and decelerate.

### ***Compressed Work Week***

An on-going alternative work schedule, in accordance with employer policy, that regularly allows a full-time employee to eliminate at least one (1) work day every two (2) weeks through working longer hours during the remaining days, resulting in fewer commute trips by the employee.

### ***Coordinated Transportation Plan for Seniors and Persons with Disabilities (CTP) 2016***

This document seeks to support the creation of a regionally coordinated transportation system that is efficient, effective, and founded on present and future need of elderly and disabled riders. Although the CTP addresses the provision of specific transportation services and coordination among providers, it also emphasizes land use and design which support and encourage walking and transit. An update of this plan was completed in June, 2016, with the participation of SMART staff.

## **CTAA**

Community Transportation Association of America. A national professional association of those involved in community transportation, including operators, vendors, consultants and federal, state and local officials. SMART is a member of CTAA.

## ***Curb Extension or Bulbout***

An area where the sidewalk and curb are extended into the parking lane to decrease crossing distance for pedestrians. Curb extensions can also be used to allow buses to remain in the travel lane instead of pulling over to the curb at bus stops.

## ***Curb-to-Curb Service***

A common designation for para-transit services. The transit vehicle picks up and discharges passengers at the curb or driveway in front of their home or destination. In curb-to-curb service the driver does not assist the passenger along walks or steps to the door of the home or other destination. SMART is now required to provide door-to-door service for ADA-qualified passengers.

## **D**

### ***Demand-Response Service***

The type of transit service where individual passengers can request transportation from a specific location to another specific location at a certain time. Transit vehicles providing demand-response service do not follow a fixed Route, but travel throughout the community transporting passengers according to their specific requests. SMART's dial-a-ride is demand-response. These services require advance reservations.

### ***Deviated Fixed-Route***

This type of transit is a hybrid of fixed-route and demand-response services. While a bus or van passes along fixed stops and keeps to a timetable, the bus or van can deviate its course between two stops to go to a specific location for a pre-scheduled request. Often used to provide accessibility to persons with disabilities.

### ***Disabled***

Any person who by reason of illness, injury, age, congenital malfunction or other permanent or temporary incapacity or disability is unable, without special facilities, to use local transit facilities and services as effectively as persons who are not so affected.

### ***Discretionary Grant***

Financial assistance that is awarded on the basis of competitive merits from among proposals that are submitted. Even in cases where projects are identified, or earmarked, by members of congress, grant-making agencies generally will require recipients to file applications and abide by the procedures of what was designed as a competitive grant-making process.

### ***Door-to-Door Service***

A form of paratransit service which includes passenger assistance between the vehicle and the door of his or her home or other destination. A higher level of service than curb-to-curb, yet not as specialized as door-through-door service (where the driver actually provides assistance within the origin or destination).

### ***DOT***

Department of Transportation, the federal agency that oversees how transportation money is spent and programs are conducted in the U.S.A. The DOT oversees over a dozen other agencies, including FTA and FHWA.

### ***Drug and Alcohol Testing Regulations***

DOT implemented the Omnibus Transportation Employee Testing Act in December 1992. The act requires drug and alcohol tests for all safety-sensitive employees of agencies receiving section 5307, 5309 or 5311 funding (Section 5310 agencies are not included), including drivers, maintenance workers, dispatchers and supervisors. All CDL carrying employees are subject to random drug tests.

## ***E***

### ***E&D***

An abbreviation commonly used to refer to services for the elderly and disabled.

### ***Emergency Ride Home***

A program that encourages employees to carpool, use transit, bike, or walk to work by guaranteeing them a ride home in the event of an emergency. A free taxi ride is provided when an employee becomes ill at work, has to work unexpected overtime, or has a family emergency such as a sick child. Also referred to as "Guaranteed Ride Home."

### ***Employee Commute Options (ECO) Program 1996***

In 1996, the Oregon Legislature passed a series of laws designed to protect air quality in the Portland metropolitan area that included Employee Commute Options (ECO). The Oregon Department of Environmental Quality (DEQ) then established a set of Administrative Rules to implement the law. The ECO rules set more specific goals for trip reduction than the TPR, and specifically target businesses with more than 100 employees. The ECO rules require these businesses to provide commuting options to encourage employees to reduce single-occupancy vehicle (SOV) commute trips. For instance, Wilsonville currently has more than 25 businesses with more than 100 employees, each of which must provide their employees with options that have the potential to reduce SOV trips to worksites by 10 percent within three years of the employer's plan. They are also required to maintain the trip reductions as long as the ECO rules are in effect.

### ***Employment Transportation***

Transportation specifically designed to take passengers to and from work or work-related activities.

### ***Environmental Justice***

The fair treatment and meaningful involvement of all people regardless of race, color, national origin, or income with respect to the development, implementation, and enforcement of environmental laws, regulations, and policies.

## ***F***

### ***Far-Side Bus Stop***

A bus stop that is located immediately following an intersection.

### ***Fare Box Revenue***

A public transportation term for the monies or tickets collected as payments for rides. Can be cash, tickets, tokens, transfers and pass receipts. Fare box revenues rarely cover more than a small portion of a transit system's operating expenses.

### ***FAST Act***

The current federal transportation legislation, Fixing America's Surface Transportation (FAST) Act, is a federal law signed by President Obama on December 4<sup>th</sup>, 2015. This act authorizes 305 billion dollars to go to various transportation improvements, public transportation systems, programs, research, and safety upgrades for networks across the country. This act took effect at the beginning of 2016 and will continue on until 2020. The FAST Act did not actually guarantee that funding would be provided for transit over the life of the bill.

### ***Federal Transportation Bills since 1991***

Intermodal Surface Transportation Efficiency Act (ISTEA), 1991,  
The National Highway System Designation Act (NHS), 1995,  
Transportation Equity Act for the 21st Century (TEA-21), 1998,  
Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU), 2005,  
Moving Ahead for Progress in the 21st Century Act, (MAP-21) 2012,  
Fixing America's Surface Transportation Act (FAST), 2015.

### ***FHWA***

Federal Highway Administration. A component of the U.S. Department of Transportation, provides funding to state and local governments for highway construction and improvements, including funds must be used for transit. FHWA also regulates the safety of commercial motor vehicle operations (vehicles which require a CDL to drive). FWHA is the lead agency in federal intelligent transportation activities and regulated interstate transportation.



### ***Fixed-Route***

Transit services where vehicles run on regular, pre-designated, pre-scheduled routes, with no deviation. Typically, fixed-route service is characterized by printed schedules or timetables, designated bus stops where passengers board and alight and the use of larger transit vehicles.

### ***Flex Schedules***

An employer policy allowing individual employees some flexibility in choosing work schedules. Flex schedules allow employees to start earlier or later, avoiding peak traffic times.

### ***FTA***

Federal Transit Administration (before 1991, Urban Mass Transportation Administration). A component of the U.S. Department of Transportation that regulates and helps fund public transportation. FTA provides financial assistance for capital and operating costs and also sponsors research, training, technical assistance and demonstration programs. FTA was created by the passage of the Urban Mass Transportation Act of 1964.

## ***G***

### ***Grant***

The award of government funds to an entity. Federal funds are typically awarded either as formula (or block) grants, where a predetermined legislative process establishes the level of funding available to an entity, or discretionary grants, where the funding agency is free to determine how much (if any) funding an entity will be given based on the relative merits of the proposal. Private foundations also give grants based on their own criteria.

### ***Guaranteed Ride Home***

Program that encourages employees to carpool, use transit, bike or walk to work by guaranteeing them a ride home in case they cannot take the same mode home (e.g., if they need to work late or if an emergency occurs). Also referred to as "Emergency Ride Home."

## ***H***

### ***High-Occupancy Vehicle (HOV)***

A passenger vehicle carrying more than a specified minimum number of passengers. HOVs include carpools, vanpools, and buses. HOV requirements are often indicated as 2+ (two or more passengers required) or 3+ (three or more passengers required).

### ***High Transit Service (Frequent service)***

This level of service is achieved when a location is served by multiple bus routes at intervals of 15 minutes or less during peak commuting hours.

**HOV Lane**

This is a traffic lane limited to carrying high occupancy vehicles (HOVs) and certain other qualified vehicles.

**Human Services Transportation**

Transportation related to the provision of human or social services. Includes transportation for the elderly and people with disabilities when the transportation is provided by an arrangement other than the public service available to all.

**I****Inter-city Transportation**

Transportation service between two urban areas. Under FTA's Section 5311 (f), intercity transportation service must receive no less than 15 percent of each state's total Section 5311 funding, unless a state's governor certifies that these needs are already being met.

**Interlining**

Interlining is a term used to describe a vehicle that is continuing on to another route without reporting back to the garage/depot. Often times this requires the two routes to share a common bus stop or terminus.

**L****Linked**

A linked trip is a person's entire trip between an origin and destination, which may involve transferring between vehicles (e.g., Park & ride or bus and rail transit), or multiple stops, such as stopping at a daycare center or store along a commute trip.

**M****Match**

State or local funds required by various federal or state programs to complement funds for a project. A match may also be required by states in funding projects which are joint state/local efforts. Some funding sources allow services, such as the work of volunteers, to be counted as an in-kind funding match. Federal programs normally require that match funds come from other than federal sources.

**Medicaid**

Also known as Medical Assistance, this is a health care program for low-income and other medically needy persons. It is jointly funded by state and federal governments. The Medicaid program pays for transportation to non-emergency medical appointments if the recipient has no other means to travel to the appointment.

### ***Medium Transit Service***

This level of service is achieved when at least two bus Routes serve a location with no longer than 20-minute intervals during peak commuting hours.

### ***Metro and the Regional Transportation Plan (RTP)***

Metro is empowered to coordinate the preparation of the Regional Transportation Plan and has an elected council of representatives from throughout the region. The Joint Policy Advisory Committee on Transportation (JPACT) and Metro council share decision making authority on funding and both serve as decision makers for the Metropolitan Planning Organization (MPO). Metro is a unique regional government for the Portland area and Wilsonville is the southernmost community within Metro. Besides regional planning authority, as the Metropolitan Planning Organization Metro has authority over the distribution of federal transportation funds to the region.

### ***Mode***

A method used by people or goods to get from one place to another, such as using cars and trucks, freight and passenger trains, walking, bicycling, and riding buses.

### ***Mode Split***

Travel modes include walking, biking, auto, and bus and light rail. The mode split is the percentage of total travel by each mode. For example if the mode split is 80% auto, this means that 80% of all trips are made by auto.

### ***MPO***

Metropolitan Planning Organization. The local bodies that set coordination standards and select projects in urban areas to be funded by the FAST Act. In the Portland Metropolitan area, Metro serves as the MPO.

### ***Multi-modal Transportation***

The availability of transportation options using different modes within a system or corridor, allowing for greater choice and mobility.

## ***N***

### ***National Transit Database Reports***

Annual reports formerly known as Section 15, report financial and operating data, required of almost all recipients of transportation funds under Section 5307.

### ***Near-side Bus Stop***

A bus stop that is located immediately preceding an intersection.

## **O**

### **OAA**

Older Americans Act. Federal law first passed in 1965. The act established a network of services and programs for older people. This network provides supportive services, including transportation and nutrition services, and works with public and private agencies that serve the needs of older individuals.

### ***Operating Assistance***

Funding that helps support the day-to-day costs of operating or providing services; in transportation settings, this category often includes driver salaries and operating staff expense, as well as fuel, and other routine, ongoing costs of having and operating a transportation service.

### ***Operating Costs***

Non-capital costs associated with operating and maintaining a transit system, including labor, fuel, administration and maintenance.

### ***Oregon Department of Transportation (ODOT)***

State agency that oversees and maintains the state transportation system (including public transit), under the guidance of the Oregon Transportation Commission.

### ***Oregon Public Transportation Plan (OPTP) 1997***

The OPTP provides guidance for the development of transit, rideshare, and transportation demand management services throughout Oregon. The OPTP sets first priority on service to those who are most dependent on the public transportation system (seniors, people with disabilities, low-income, and youth). The plan describes transit service as a lifeline for many people in need of transportation to medical appointments, employment, and educational services. The ODOT Policy Advisory Committee and the Technical Advisory Committee is currently updating the OPTP with the intention of completing the Plan in 2018.

### ***Oregon's Statewide Planning Goals***

The 19 goals that provide a foundation for the State's planning program. The 19 goals can be grouped into four broad categories: land use, resource management, economic development, and citizen involvement. Locally adopted comprehensive plans and regional transportation plans must be consistent with the statewide planning goals.

### ***Oregon Transportation Options (TO) Plan 2015***

The purpose of the Oregon Transportation Options Plan (TO) is to establish a vision and policy guidance that supports and advances TO program activities and suggests ways to integrate TO into transportation planning and investments. The plan also supports TO program activities and integration with capital investment planning at the local and regional level.

## **P**

### ***Para-transit***

Types of passenger transportation that are more flexible than conventional fixed-route transit but more structured than the use of private automobiles. Para-transit includes demand-response transportation services, subscription bus services, shared-ride taxis, carpooling and vanpooling, jitney services and so on. Most often refers to wheelchair-accessible, demand-response van service.

### ***Park & Ride***

A mode of travel usually associated with movements between work and home, that involves use of a private auto on one portion of the trip and a transit vehicle, carpool, or vanpool on another portion of the trip. Thus, a park-and-ride trip could consist of an auto trip from home to a parking lot, and transfer at that point to a bus, carpool, or vanpool in order to complete the trip to work.

### ***Parking Management***

Strategies aimed at making better use of available parking supply. Parking management strategies include preferential parking or price discounts for carpools and/or short-term parkers, and disincentives, prohibitions and price supplements for those contributing more to congestion.

### ***Parking Cash-Out***

This means that people (typically commuters, and sometimes residents of multi-family housing) who are offered a free parking space are also offered the cash equivalent when they use alternative transportation modes and so do not impose parking costs.

### ***Passenger Mile***

One passenger transported one mile. Total passenger miles are calculated by adding the sum of the distances ridden by each passenger.

### ***Payroll Tax***

The transit payroll tax was established by City Ordinance 340 in December, 1988. From January 1, 1990 to October 2006, the transit tax rate has remained constant at .3% (3/10 of one percent). In October 2006, the rate was raised to 0.33%, and to 0.5% in 2008. This tax is imposed for the provision of public transportation services in the local transit area.

### ***Peak Hours***

The rush hours of the day, generally 7-9 a.m. and 4-6 p.m.

### ***Pre-Award/Post-Delivery Audit Requirements***

Since 1991, FTA has required recipients of Sections 5307, 5309, 5310 and 5311 funds to carry out audits of vehicles and other rolling stock purchased with FTA money. These audits are to insure that vehicles are manufactured according to specification and comply with applicable Buy America and Federal Motor Vehicle Safety Standards.

## **R**

### ***Regional Center***

A design type designated in Metro's 2040 Growth Concept. After the Central City, regional centers have the region's highest development densities, the most diverse mix of land uses, and the greatest concentration of commerce, offices, and cultural amenities. They are very accessible by both automobile and public transportation, and have streets that are oriented to pedestrians.

### ***Regional Transportation Functional Plan (RTFP) 2014***

The Regional Transportation Functional Plan (RTFP) is a Metro document that identifies the procedures necessary in order to meet the goals that are laid out in the Regional Transportation Plan (RTP). These procedures include changes in the region's systems for streets, transit, bicycles and pedestrians that are dependent on the nearby cities and municipalities.

### ***Regional Transportation Plan (RTP)***

The guiding document developed by Metro for all federally funded transportation planning efforts in the region, with a twenty year horizon and updated every three years. The RTP is the region's transportation system plan that is required by the Transportation Planning Rule.

### ***Reverse Commute***

A rideshare program facilitates the formation of carpools and vanpools, usually for work trips. A database is maintained for the ride times, origins, destinations and driver/rider preferences of users and potential users. Those requesting to join an existing pool or looking for riders are matched by program staff with other appropriate persons. In rural areas, a rideshare program is often used to coordinate Medicaid or volunteer transportation.

### ***Ridership***

The number of rides taken by people using public transportation in a given time period.

### ***Rideshare***

A motor vehicle, carrying two or more people for any trip purpose, including work and shopping.

## **S**

### ***SCTD***

South Clackamas Transportation District (SCTD) provides bus service in the Molalla area and connections to Clackamas Community College and Canby.

### ***SDC or Systems Development Charge***

A system development charge (SDC) is a one-time fee imposed on new construction at the time of development. The fee is intended to recover a fair share of the costs of existing and planned facilities that provide capacity to serve new growth.

### ***Section 5307***

The section of the Federal Transit Act that authorizes grants to public transit systems in all urban areas. Funds authorized through Section 5307 are awarded to states to provide capital and operating assistance to transit systems in urban areas with populations between 50,000 and 200,000. Transit systems in urban areas with populations greater than 200,000 receive their funds directly from FTA.

### ***Section 5309***

The section of the Federal Transit Act that authorizes discretionary grants to public transit agencies for capital projects such as buses, bus facilities and rail projects.

### ***Section 5310***

The section of the Federal Transit Act that authorizes capital assistance to states for transportation programs that serve the elderly and people with disabilities. States distribute section 5310 funds to local operators in both rural and urban settings, who are either nonprofit organizations or the lead agencies in coordinated transportation programs.

### ***Section 5311***

The section of the Federal Transit Act that authorizes capital and operating assistance grants to public transit systems in areas with populations of less than 50,000.

### ***SMART Options***

A grant-funded program operated by South Metro Area Regional Transit (SMART). This program works with employers, schools, and individuals to help them find alternatives to driving alone. These alternatives include carpooling, vanpooling, transit, walking, bicycling, and telework. SMART Options also works with SMART Options works closely with other regional providers and services

### ***Southwest Corridor Transit Project (SWCP)***

The SWCP is on-going and works to improve a range of high capacity transit, bicycle, roadway and pedestrian access in southwest Portland and southeast Washington County. This project

included an investment strategy, transportation and land use plans, and an analysis of transit alternatives.

***State Implementation Plan (SIP)***

The statement of how the transportation, environmental, and health communities expect to meet federal air quality safety standards.

***State Transportation Improvement Program (STIP)***

A staged, multi-year, statewide, intermodal program of transportation projects, consistent with the statewide transportation plan and planning processes as well as metropolitan plans, TIPs, and processes.

***Statewide Planning Goal 12***

Oregon’s Statewide Planning Goal 12 is to provide and encourage a safe, convenient and economic transportation system. The State’s Transportation Planning Rule (TPR) is designed to implement Statewide Planning Goal 12. The TPR promotes the development of safe, convenient, and economic transportation systems that are designed to reduce reliance on the automobile so that the air pollution, traffic, and other livability problems faced by many urban areas can be avoided. The TPR permits regional planning agencies to adopt alternative standards in order to comply with the TPR.

***T***

***Telework or Telecommute***

This term refers to a transportation demand management strategy whereby an individual substitutes working at home for commuting to a work site on either a part-time or full-time basis.

***Title VI of the Civil Rights Act***

“No person in the United States shall, on the ground of race, color or national origin, be excluded from participation in, be denied benefits of, or be subjected to discrimination under any program or activity receiving federal financial assistance.”

***TOD***

Transit Oriented Development- Development in which public transportation, walking, and biking are designed to play a large role in mobility. TODs sometimes have the features often identified with New Urbanism- that is- using traditional town planning strategies to increase livability.

***Town Center***

A Metro 2040 Growth Concept design type that functions as a local activity area and provides close access to a full range of local retail and services within a few miles of most residents.



Town centers do not compete with regional centers in scale or economic diversity, but they will offer some specialty attractions of regional interest. Town centers have excellent multimodal access and connections to regional centers and other major destinations.

### ***Traffic Calming***

Various design features and strategies intended to reduce vehicle traffic speeds and volumes on roadway as a means of promoting safe and pleasant conditions for motorists, bicyclists, pedestrians, and residents.. These measures can include medians, bicycle lanes, roundabouts, curb bulb-outs, tighter curb radii, landscaping, and narrower streets.

### ***Transit***

This term refers to publicly-funded and managed transportation services and programs within the urban area, including light rail, regional rapid bus, frequent bus, primary bus, secondary bus, mini-bus, para-transit and park-and-ride.

### ***Transit-Dependent Populations***

Low-income, minority, youth, seniors, and people with disabilities tend to be more dependent on transit than the population as a whole. These groups often rely on transit as a lifeline to connect them with jobs, shopping, medical appointments, and social interaction. Since these groups do not have the mobility choices available to other groups, providing them with adequate transit service is considered first priority.

### ***Transportation Demand Management (TDM)***

Various strategies that change travel behavior (how, when and where people travel) in order to increase transport system efficiency and achieve specific objectives such as reduced traffic congestion, road and parking cost savings, increased safety, improved mobility for non-drivers, energy conservation and pollution emission reductions. Also referred to as Transportation Options or Mobility Management.

### ***Transportation Improvement Program (TIP)***

The multi-year capital program of transportation projects updated each year. SMART is subject to both the MTIP (Metropolitan Transportation Improvement Program) and STIP (State Transportation Improvement Program).

### ***Transportation Network Company (TNC)***

These companies connect paying passengers with drivers who provide the transportation on their own non-commercial vehicles, also known as ridesourcing.

### ***Transportation Options Program***

A program that seeks to promote a variety of transportation options and alternatives to the single-occupant automobile.

### ***Transportation Planning Rule (TPR)***

The implementing rule of Statewide Planning Goal 12 dealing with transportation, as adopted by the State Land Conservation and Development Commission (LCDC). Among its provisions, the TPR requires reducing vehicle miles traveled (VMT) per capita by 15 percent in the next 30 years, reducing parking spaces per capita by 10 percent in the next 20 years, and improving opportunities for alternatives to the automobile.

### ***Transportation System Management (TSM)***

Strategies and techniques for increasing the efficiency, safety, or level-of-service of a transportation facility without increasing its size. Examples include, but are not limited to, traffic signal improvements, traffic control devices (including installing medians, channelization, access management, and ramp metering), incident response, targeted traffic enforcement, preferential transit measures, and restriping for high-occupancy vehicle lanes.

### ***Transportation System Management and Operations (TSMO)***

A program designed to optimize the performance of roadway infrastructure that already exists through the implementation of specific systems and services that preserve capacity, improve reliability and safety, and improve the environment.

### ***Transportation System Plan (TSP)***

A plan for one or more transportation facilities that are planned, developed, operated, and maintained in a coordinated manner to supply continuity of movement between modes and within and between geographical and jurisdictional areas.

### ***TriMet***

Tri-County Metropolitan Transportation District, the transit agency for most of Clackamas, Multnomah, and Washington Counties.

### ***Trip***

A one-way movement of a person or vehicle between two points. Many transit statistics are based on unlinked passenger trips, which refer to individual one-way trips made by individual riders in individual vehicles. A person who leaves home on one vehicle, transfers to a second vehicle to arrive at a destination, leaves the destination on a third vehicle and has to transfer to yet another vehicle to complete the journey home has made four unlinked passenger trips.

### ***Tripper Bus***

A tripper bus is a second, often smaller bus that is used to create more rider capacity on a Route by following a regular bus and picking up passengers that otherwise would not be able to fit on the first bus due to overcrowding.

## **U**

### ***U.S. DOT***

The United States Department of Transportation goals target transportation infrastructure, congestion, reliability and access. Federal strategies are designed to produce improvements in these measures of mobility throughout the U.S. transportation network in an effort to improve commerce and air quality, reduce energy consumption, and improve quality of life.

### ***Unlinked Passenger Trips***

An unlinked trip is a passenger trip made in a single vehicle, such as a single automobile or bus ride.

## **V**

### ***Vanpool***

A prearranged ridesharing service in which a number of people travel together on a regular basis in a van. Vanpools may be publicly operated, employer operated, individually owned or leased.

### ***VMT***

Vehicle Miles of Travel - the measure of how many miles vehicle travel in a given period- use to assess the amount of roadway travel in the region.

## **W**

### ***Walk SMART***

The objective of the Walk SMART project is to increase the number of walking trips by all people in Wilsonville.

### ***Washington County Transportation System Plan 2015***

Places a strong emphasis on regional connections and on strategies to increase transit efficiency and access; including improving bicycle and pedestrian access to transit stops, road improvements, and private development in close proximity to major bus stops and commuter rail stations.

### ***WES***

WES (Westside Express Service) a commuter rail line owned by TriMet with service to Beaverton, Tigard, Tualatin and Wilsonville. WES runs every 30 minutes during the weekday morning and afternoon rush hour.

## Appendix B – Route Priorities

The content below highlights SMART’s priority if funding levels change to either (1) more available revenue and increasing service or if (2) costs increase and reducing service levels.

**If SMART has more available revenue, the first service enhancements will be:**

| Priority Level | Service Description  | Estimated Cost |
|----------------|--|----------------|
| 1              | Add hours of service on the 2X to Tualatin and/or Tigard with better connections to TriMet.        | \$60,489       |
| 2              | Add more Saturday service and hours of service on Route 4.   | \$114,650      |
| 3              | Add mid-day or late-morning service on the 1X.   | \$372,000      |
| 4              | Expand service to Villebois. Hourly service all day from Villebois to connections at SMART Central | \$168,000      |
|                | Current Villebois Shopper Shuttle would be replaced by all day Route 7                             | \$118,480      |
|                | Cost after Route 7 replaces the Villebois Shopper Shuttle  | \$49,520       |
| 5              | Add service to Coffee Creek and Frog Pond growth areas.  | \$184,967      |
| 6              | Begin service to Downtown Portland.  | \$194,513      |
| 7              | Begin service to Woodburn.   | \$35,981       |
| 8              | Acquire battery-electric buses, especially for in-town use.  | \$86,250       |
| Total          | Total cost of all projects if annual operating funding were available.                             | \$1,384,849    |

If SMART has to make cuts because of increased operating expenses, or decreased revenue, the first cuts will be:

| Priority Level | Service Description  | Estimated Cost Savings |
|----------------|--|------------------------|
| 1              | Reduce geographic area for out-of-town medical (Dial-a-Ride) trips.<br><br>Half of current revenue mile and reduced service by 1/4 | \$65,430               |
| 2              | Drop one morning trip on the 1X, returning to 30-minute headway.   | \$61,917               |
| 3              | Reduce services to Tualatin, or to Tigard, on the 2X.<br><br>Bypassing Tualatin Park & Ride  | \$46,714               |
| 4              | Reduce service to Canby on Route 3.<br><br>Decrease Route 3 to Canby by two trips  | \$53,000               |
| Total          | Total annual savings if all cuts were executed   | \$227,061              |

## Appendix C - Title VI Program

City of Wilsonville

South Metro Area Regional Transit (SMART)

Title VI Program

November 29, 2016

Stephan Lashbrook  
Transit Director

29799 SW Town Center Loop E  
Wilsonville, Oregon  
[lashbrook@ridesmart.com](mailto:lashbrook@ridesmart.com)  
(503) 570-1576

## Introduction

This program reflects the City of Wilsonville's commitment to ensuring that no person shall, on the grounds of race, color, or national origin, be excluded from participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving Federal financial assistance.

The City of Wilsonville is committed to complying with the requirements of Title VI in all of its programs and activities including the provision of transit services.

## Signed Policy Statement

A policy statement signed by Bryan Cosgrove, Wilsonville City Manager, assuring SMART's compliance with Title VI of the Civil Rights Act of 1964, can be found as Attachment A.

## Title VI Complaint Procedures

The City of Wilsonville has a standard process for investigating all complaints filed with SMART. Members of the public may file a signed, written complaint up to one hundred and eighty (180) days from the date of the alleged discrimination. Full procedures for filing a complaint and the City procedures for investigating complaints can be found as Attachment B. At a minimum, the complaint should include the following information:

- Name, mailing address, and how to contact complainant (i.e., telephone number, email address, etc.)
- How, when, where and why complainant alleges s/he was discriminated against. Include the location, names and contact information of any witnesses.
- Other significant information.

The complaint may be filed in writing with the City of Wilsonville at the following address:

Stephan Lashbrook  
Transit Director  
29799 SW Town Center Loop E  
Wilsonville, Oregon  
[lashbrook@ridessmart.com](mailto:lashbrook@ridessmart.com)  
(503) 570-1576

## Record of Title VI investigations, complaints, or lawsuits

SMART has had no Title VI complaints, investigations, or lawsuits filed against it in the past three years.

## SMART Limited English Proficiency Outreach Plan

A full copy of SMART's outreach plan for individuals with limited English proficiency can be found in Attachment C. Key elements of the plan include:

- Spanish speaking translators available upon request;
- Route and Schedule brochures available in both English and Spanish;
- Transit surveys conducted by SMART available in Spanish;
- Public meetings with translators available upon request;
- Multiple-language translators available to anyone contacting SMART by phone;
- Rider alerts and other notifications printed in both Spanish and English; and
- Information on SMART's website automatically translated into multiple languages.

## Notification of SMART's Title VI obligations

Wilsonville SMART publicizes its Title VI program by posting its commitment to providing services without regard to race, color, or national origin in all buses and in the City of Wilsonville Library, City Hall, the driver break room, and the SMART administration and maintenance facilities.

Furthermore, SMART provides information regarding Title VI obligations on the website (<http://www.ridesmart.com/274/Title-VI>) and in customer brochures.

The posters, website, and brochures provide information in English and Spanish:

- A statement that the City operates programs without regard to race, color, and national origin; and
- A statement encouraging anyone to contact the City of Wilsonville with questions or comments about SMART's non-discrimination policies or to file a complaint.

General Information/Comments/Complaints: [smart@ridesmart.com](mailto:smart@ridesmart.com)

(503) 682-7790

[www.ridesmart.com](http://www.ridesmart.com)

## Title VI – Compliance Officer & Limited English Proficiency Plan (LEP) Coordinator

SMART's Transit Director, who reports to the Wilsonville City Manager, will serve as the overall Title VI Compliance Officer. The Compliance Officer is responsible for ensuring that SMART is meeting its obligations under Title VI of the Civil Rights Act of 1964.

SMART's Program Manager, will serve as SMART's Title VI Limited English Proficiency (LEP) Plan Coordinator. The LEP Plan Coordinator will ensure that SMART satisfies the intent of the LEP Plan by making information available to LEP individuals, offering ways for them to participate in SMART's public participation efforts and ensuring the process is in place for direct input and feedback.



## Subrecipients

SMART does not pass any FTA funds through to subrecipients.

## Summary of Public Participation Efforts

Over the last three-year period, SMART conducted the following public outreach and involvement activities:

- Public Open House meetings for the Transit Master Plan (Fall 2015, Spring 2016, Summer 2016, Winter 2016).
- Onboard and community surveys for proposed service changes related to the Transit Master Plan available in English and Spanish (Spring 2016, Fall 2016).
- Information booth set up at SMART Central for community surveys of proposed service changes related to the Transit Master Plan available in English and Spanish (Spring 2016, Fall 2016).
- Online community surveys for proposed service changes related to the Transit Master Plan available in English and Spanish (Spring 2016, Fall 2016).
- FACEBOOK posts about community surveys for proposed service changes related to the Transit Master Plan, with cross posts from other departments.
- Boones Ferry Messenger articles for proposed service changes related to the Transit Master Plan.
- Separate surveys for the DEQ Employee Commute Options Rule Survey materials in English and Spanish (ongoing with Wilsonville employers)
- Had Spanish translator available at two public outreach events Summer and Fall 2016.
- Created and installed Spanish “channel cards” on the inside of SMART buses. These cards promote transit and community programs (ongoing).
- Senior and Community Center outreach workshops (ongoing)
- Wilsonville school outreach and workshops (ongoing)
- Wood Middle School Bike Safety Class materials provided in English and Spanish (Spring 2014, 2015, 2016)
- “Bike Rodeo” materials provided in English and Spanish (Summer, 2016)
- SMART website includes the Google Translator tool. This tool instantly translates all pages on the website into more than 80 languages. Phone callers are now available to connect with a third party translator as of fall 2015 (now ongoing).
- Farmer’s Market tabling – (Summer 2014, 2015, 2016).
- Employer outreach workshops (ongoing).
- Community outreach workshops (ongoing)

## Overview of SMART Service Standards and Policies

The Wilsonville City Manager adopted the updated SMART Service Standards and Policies document on November 29, 2016 - Attachment F.

## Attachment A



29799 SW Town Center Loop E  
Wilsonville, Oregon 97070  
(503) 682-1011  
(503) 682-1015 Fax Administration  
(503) 682-7025 Fax Community Development

City of Wilsonville  
SMART Transit  
Non-Discrimination Policy Statement

November 29, 2016

Title VI of the Civil Rights Act of 1964 states:

“No person in the United States shall, on the grounds of race color or national origin be excluded from participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving Federal assistance.”

The City of Wilsonville is committed to complying with the requirements of Title VI in all of its programs and activities including the provision of transit services.

Bryan Cosgrove  
City Manager  
City of Wilsonville & SMART Transit



"Serving The Community With Pride"

## Attachment B

# Discrimination Complaint Procedure

1. Title VI of the Civil Rights Act of 1964 states that no person in the United States shall, on the grounds of race, color, or national origin be excluded from participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving Federal financial assistance. Any person who believes that he or she, has been subjected to discrimination prohibited by Title VI of the Civil Rights Act of 1964, may file a complaint with the City of Wilsonville South Metro Area Regional Transit (SMART). A complaint may also be filed by a representative on behalf of such a person. All complaints will be referred to the City of Wilsonville Transit Director for review and action.
2. In order to have the complaint considered under this procedure, the complainant must file the complaint no later than 180 days after:
  - a) The date of alleged act of discrimination; or
  - b) Where there has been a continuing course of conduct, the date on which that conduct was discontinued.

In either case, SMART may extend the time for filing or waive the time limit in the interest of justice, as long SMART specifies in writing the reason for so doing.

3. Complaints shall be in writing and shall be signed by the complainant and/or the complainant's representative. Complaints shall set forth as fully as possible the facts and circumstances surrounding the alleged discrimination. In the event that a person makes a verbal complaint of discrimination to an officer or employee of SMART, the person shall be interviewed by the City of Wilsonville Transit Director. If necessary, the City's Transit Director will assist the person in reducing the complaint to writing and submit the written version of the complaint to the person for signature. The complaint shall then be handled according to the City of Wilsonville's investigative procedures.
4. Within 10 days, the City's Transit Director will acknowledge receipt of the allegation, inform the complainant of action taken or proposed action to process the allegation, and advise the complainant of other avenues of redress available, such as ODOT and USDOT.
5. The recipient will advise ODOT and/or USDOT within 10 days of receipt of the allegations. Generally, the following information will be included in every notification to ODOT and/or USDOT:
  - a) Name, address, and phone number of the complainant.
  - b) Name(s) and address(es) of alleged discriminating official(s).
  - c) Basis of complaint (i.e., race, color, or national origin)
  - d) Date of alleged discriminatory act(s).
  - e) Date of complaint received by the recipient.
  - f) A statement of the complaint.
  - g) Other agencies (state, local or Federal) where the complaint has been filed.
  - h) An explanation of the actions the City of Wilsonville has taken or proposed to resolve the issue in the complaint.

6. Within 60 days, the City's Transit Director will conduct an investigation of the allegation and based on the information obtained, will render a recommendation for action in a report of findings to the Wilsonville City Manager. The complaint will be resolved by informal means whenever possible. Such informal attempts and their results will be summarized in the report of findings.
7. Within 90 days of receipt of the complaint, the City's Transit Director will notify the complainant in writing of the final decision reached, including the proposed disposition of the matter. The notification will advise the complainant of his/her appeal rights with ODOT, or USDOT, if they are dissatisfied with the final decision rendered by SMART. The City's Transit Director will also provide ODOT and/or USDOT with a copy of this decision and summary of findings upon completion of the investigation.
8. Contacts for the different Title VI administrative jurisdictions are as follows:

Federal Transit Administration Office of Civil Rights  
Attention: Title VI Program Coordinator  
East Building, 5th Floor – TCR  
1200 New Jersey Ave., SE  
Washington, DC 20590

## Attachment C

### **CITY OF WILSONVILLE/ SMART SERVICES LIMITED ENGLISH PROFICIENT (LEP) PLAN November 28, 2016**

SMART is required to take responsible steps to ensure meaningful access to the benefits, services, information and other important portions of our programs and activities of individuals who have Limited English Proficiency (LEP). SMART consulted the USDOT's LEP Guidance and performed a four factor analysis of contact with the public to determine the appropriate mix of LEP services to offer.

#### **Four Factor Analysis:**

##### **1) The nature and importance of service provided by SMART.**

SMART provides important transit services to the City of Wilsonville through its fixed Route, paratransit, rideshare, and bicycle/pedestrian programs. SMART serves the transit needs of the City of Wilsonville and provides critical regional links to three other providers (Salem Cherriots, Portland's TriMet and Canby's CAT) through the City's Transit Center: SMART Central at Wilsonville Station.

##### **2) The number or proportion of LEP persons in the service area.**

Data was gathered from the following sources to identify information on persons who speak languages other than English at home, who speak English less than very well and are therefore classified as limited English proficient or "LEP":

- a. 2010 Census Data – See attachment E;
- b. Census Bureau's 2010-2014 American Community Survey 5-Year Estimates; \*
- c. Department of Labor LEP Special Tabulation website.

A review of the census data and 2010-14 American Community Survey Estimates on the numbers of LEP persons revealed that in Wilsonville, Oregon, the highest percentage of total population 5 years of age and over that spoke a language other than English at home is Spanish speakers. The number of Spanish speaking individuals is estimated to have risen from 9.1% to about 10.0% since the 2010 Census, while the number of identified people in the LEP population that speaks English less than "very well" is estimated to have gone down from 41.4% to 33.1% of the that group.

The second largest LEP populations in Wilsonville are Indo-European and Asian which makes up less than 5% of the total population. The most recent Census Tract (2010) information on minority populations can be found in Attachment D.

##### **3) The frequency with which LEP individuals come into contact with SMART service.**

SMART serves LEP persons daily via our buses, paratransit, demand response services, and community programs. SMART has a translator system in place for the customer service phone line. SMART receives an average of approximately one call per month that requires translation and has received no call requests for languages other than Spanish.

| Subject   | Wilsonville city, Oregon |                 |  |                 |                                     |                 |
|---|--------------------------|-----------------|--|-----------------|-------------------------------------|-----------------|
|   | Total                    |                 | Percent of specified language speakers |                 |                                     |                 |
|   | Estimate                 | Margin of Error | Speak English "very well"              |                 | Speak English less than "very well" |                 |
|   |                          |                 | Estimate                               | Margin of Error | Estimate                            | Margin of Error |
| Population 5 years and over                           | 19,133                   | +/-370          | 95.3%                                  | +/-1.7          | 4.7%                                | +/-1.7          |
| Speak only English                                    | 85.4%                    | +/-2.4          | (X)                                    | (X)             | (X)                                 | (X)             |
| Speak a language other than English                   | 14.6%                    | +/-2.4          | 67.9%                                  | +/-9.5          | 32.1%                               | +/-9.5          |
| Spanish or Spanish Creole                             | 10.0%                    | +/-2.3          | 66.9%                                  | +/-12.5         | 33.1%                               | +/-12.5         |
| Other Indo-European languages                         | 2.7%                     | +/-1.2          | 82.1%                                  | +/-12.6         | 17.9%                               | +/-12.6         |
| Asian and Pacific Island languages                    | 1.6%                     | +/-0.6          | 47.3%                                  | +/-15.8         | 52.7%                               | +/-15.8         |
| Other languages                                       | 0.2%                     | +/-0.2          | 100.0%                                 | +/-48.1         | 0.0%                                | +/-48.1         |
| <b>SPEAK A LANGUAGE OTHER THAN ENGLISH</b>            |                          |                 |  |                 |                                     |                 |
| Spanish or Spanish Creole                             | 1,919                    | +/-437          | 66.9%                                  | +/-12.5         | 33.1%                               | +/-12.5         |
| 5-17 years  | 431                      | +/-135          | 81.9%                                  | +/-12.5         | 18.1%                               | +/-12.5         |
| 18-64 years   | 1,436                    | +/-387          | 61.1%                                  | +/-16.0         | 38.9%                               | +/-16.0         |
| 65 years and over                                     | 52                       | +/-53           | 100.0%                                 | +/-41.1         | 0.0%                                | +/-41.1         |
| Other Indo-European languages                         | 514                      | +/-223          | 82.1%                                  | +/-12.6         | 17.9%                               | +/-12.6         |
| 5-17 years  | 87                       | +/-81           | 79.3%                                  | +/-33.3         | 20.7%                               | +/-33.3         |
| 18-64 years   | 324                      | +/-132          | 92.9%                                  | +/-11.0         | 7.1%                                | +/-11.0         |
| 65 years and over                                     | 103                      | +/-84           | 50.5%                                  | +/-39.3         | 49.5%                               | +/-39.3         |
| Asian and Pacific Island languages                    | 313                      | +/-108          | 47.3%                                  | +/-15.8         | 52.7%                               | +/-15.8         |
| 5-17 years  | 0                        | +/-22           | -                                      | **              | -                                   | **              |
| 18-64 years   | 311                      | +/-108          | 46.9%                                  | +/-15.9         | 53.1%                               | +/-15.9         |
| 65 years and over                                     | 2                        | +/-3            | 100.0%                                 | +/-100.0        | 0.0%                                | +/-100.0        |
| Other languages                                       | 38                       | +/-34           | 100.0%                                 | +/-48.1         | 0.0%                                | +/-48.1         |
| 5-17 years  | 0                        | +/-22           | -                                      | **              | -                                   | **              |
| 18-64 years   | 15                       | +/-21           | 100.0%                                 | +/-76.6         | 0.0%                                | +/-76.6         |
| 65 years and over                                     | 23                       | +/-27           | 100.0%                                 | +/-61.8         | 0.0%                                | +/-61.8         |
| <b>CITIZENS 18 YEARS AND OVER</b>                     |                          |                 |  |                 |                                     |                 |
| All citizens 18 years and over                        | 14,474                   | +/-527          | 99.2%                                  | +/-0.6          | 0.8%                                | +/-0.6          |
| Speak only English                                    | 91.3%                    | +/-2.1          | (X)                                    | (X)             | (X)                                 | (X)             |
| Speak a language other than English                   | 8.7%                     | +/-2.1          | 90.3%                                  | +/-6.2          | 9.7%                                | +/-6.2          |
| Spanish or Spanish Creole                             | 5.0%                     | +/-1.7          | 94.9%                                  | +/-5.4          | 5.1%                                | +/-5.4          |
| Other languages                                       | 3.7%                     | +/-1.2          | 84.1%                                  | +/-11.5         | 15.9%                               | +/-11.5         |
| <b>PERCENT IMPUTED</b>                                |                          |                 |  |                 |                                     |                 |
| Language status                                       | 5.8%                     | (X)             | (X)                                    | (X)             | (X)                                 | (X)             |
| Language status (speak a language other than English) | 9.9%                     | (X)             | (X)                                    | (X)             | (X)                                 | (X)             |
| Ability to speak English                              | 13.0%                    | (X)             | (X)                                    | (X)             | (X)                                 | (X)             |

**Figure 1: American Community Survey 2010-2014**

All SMART buses are stocked with “SMART Comment” cards in both Spanish and English. Passengers may submit a comment, question, or complaint and request that someone contact them in Spanish or English so they may have full and effective access to SMART services and programs. A copy of the “SMART Comment” card can be found as Attachment F.

**4) The resources available to the recipient of the federal funds to assure meaningful access to the service by LEP persons.**

SMART has been providing information in Spanish such as surveys, bus Routes, schedules and fares, public service announcements and general information on the buses and website. In addition, the City has provided interpreters at public meetings and has a translator system in place for the customer service information phone line. SMART estimates that over the past three years, more than 300 Spanish speaking LEP individuals have been assisted at SMART related outreach events in the City of Wilsonville.

## **5) Construction, Site or Location of Facilities**

SMART has not sited, located or constructed any facility requiring an equity analysis since the last Title VI program was approved. Minor sidewalk improvements were made at a number of locations to improve ADA access, but the locations were selected solely on the basis of the physical characteristics of the existing sidewalks.

## **6) Minority Representation on Planning or Advisory Boards**

SMART does not currently have any standing or advisory boards. However, as part of the Transit Master Plan process SMART has established a Transit Master Plan Task Force. Staff has reached out to our community partners (detailed below) to identify and encourage the participation of minorities on the Task Force. In fact, anyone who attended the early meetings of the group was automatically invited to serve on the Task Force.

### **Implementation Plan:**

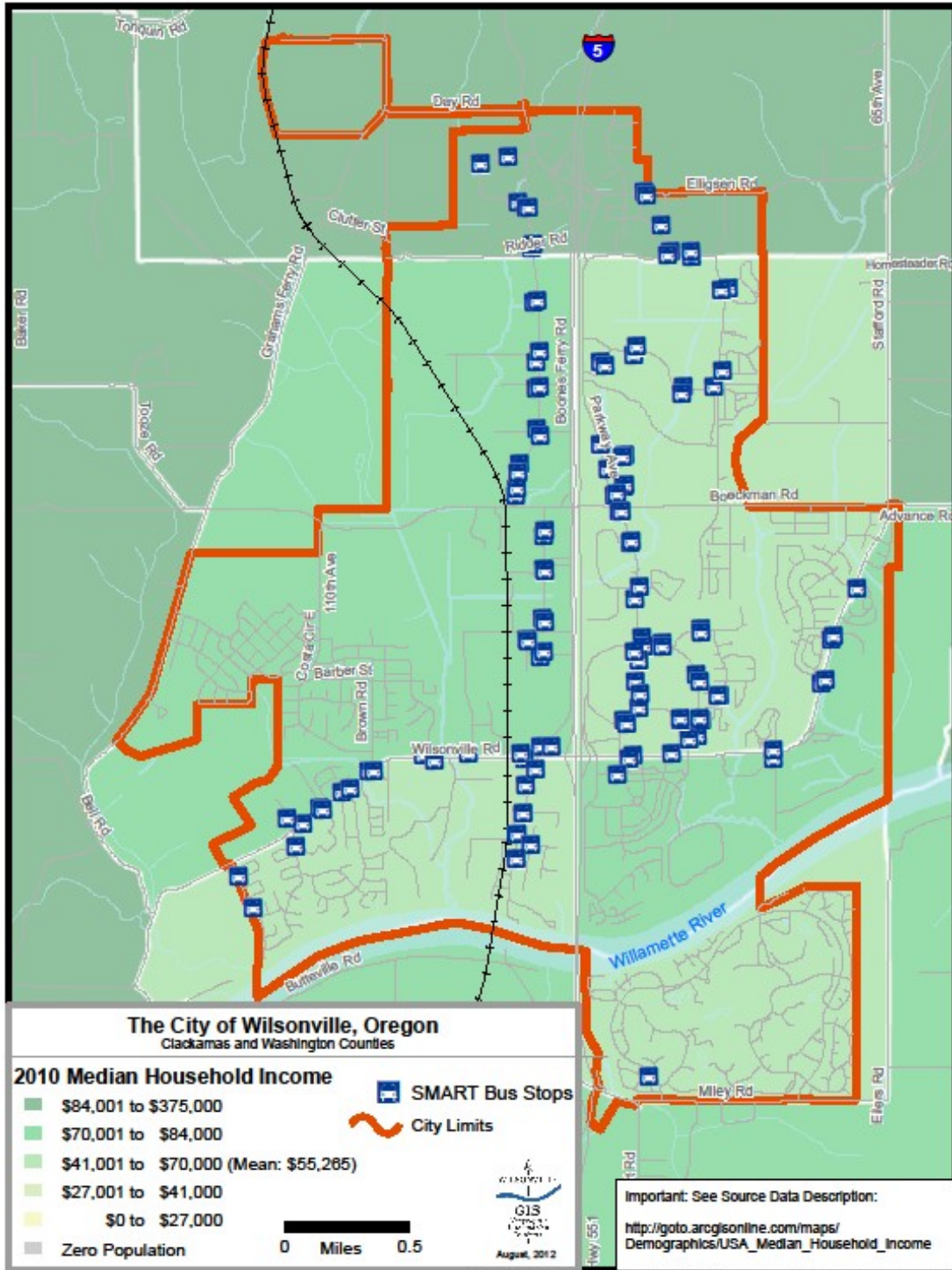
Based on the four factor analysis, SMART recognizes the need to continue providing language services in the area. A review of SMART's relevant programs, activities and services that are being offered or will be offered by the City as of November 2016 include:

- Spanish speaking representatives are available upon request.
- Route and Schedule brochures are available in English and Spanish.
- Route and schedule information are available for Google translation into Spanish or a variety of other languages on the SMART website.
- SMART Options brochures are available in Spanish with information for bicycling and pedestrian safety.
- Transit survey conducted by SMART was made available in Spanish.
- Specific public meetings related to the Transit Master Plan, other planning efforts or major fare or service changes have been held with the Spanish speaking community in Wilsonville. Information was provided in Spanish and translators were available on-site to help with questions or concerns.

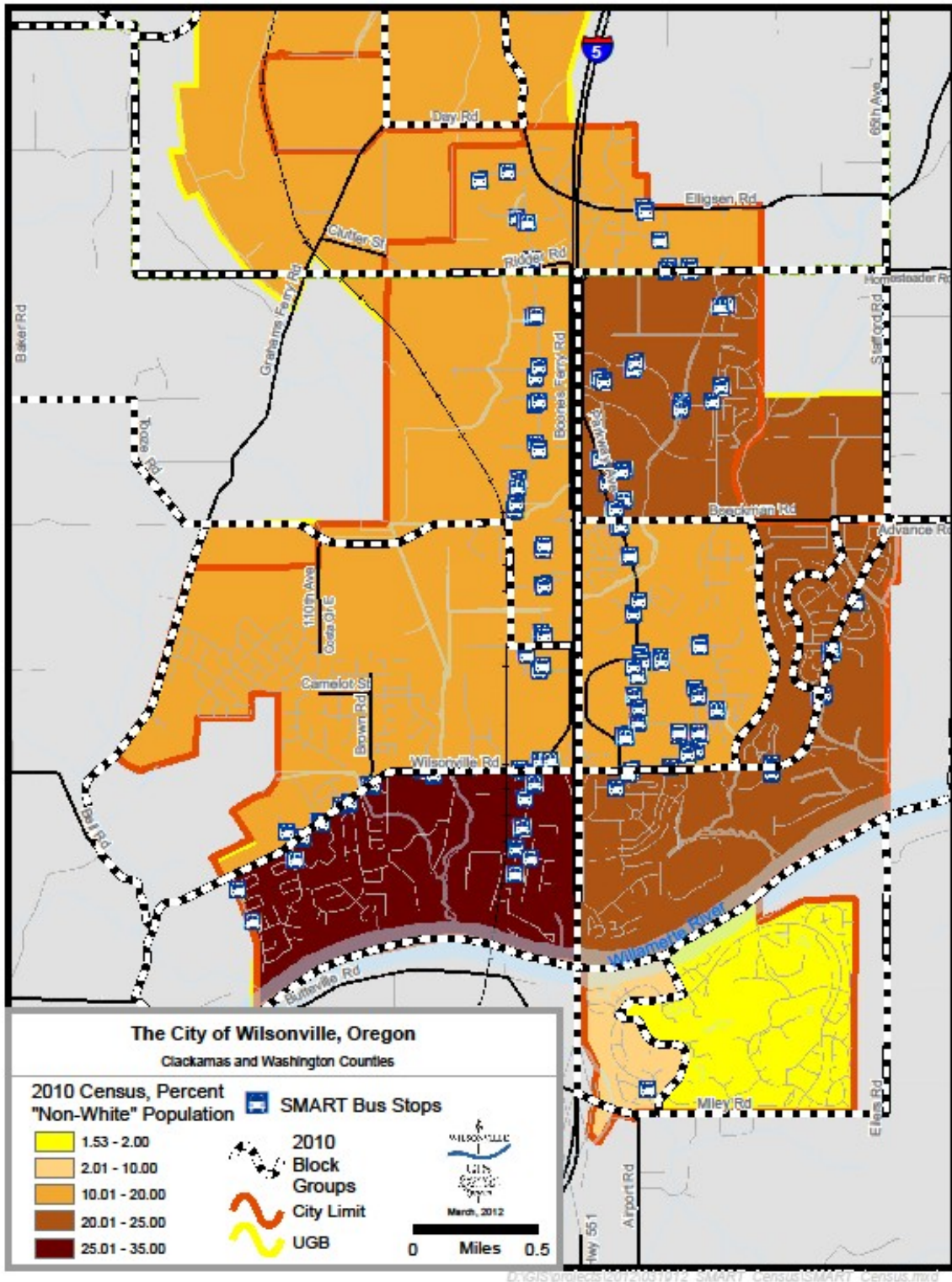
SMART's outreach and marketing initiatives have yielded a list of community organizations that provide service to populations with limited English proficiency. The following list of community organizations and schools in the area have been contacted to assist in gathering information and see what services are most frequently sought by the LEP population: Wilsonville High School, Wilsonville Art Tech High School, Wood Middle School, Boeckman Creek Primary School, Boones Ferry Primary School, Wilsonville Community Center, Wilsonville Public Library, Wilsonville businesses over 100 employees, Lowrie Primary School, and Wilsonville City Hall.

SMART will continue to contact the community organizations that serve LEP persons, as well LEP persons themselves, and perform a four factor analysis every three years to identify what, if any, additional information or activities might better improve SMART services to assure non-discriminatory service to LEP persons. SMART will then evaluate the projected financial and personnel needed to provide the requested services and assess which of these can be provided cost-effectively.

Attachment D







## Attachment E

### **TITLE VI STANDARDS AND POLICIES**

Pursuant to requirements set forth in the Federal Transit Administration's (FTA) Circular 4702.1B, SMART must establish and monitor its performance under quantitative Service Standards and qualitative Service Policies. The service standards contained herein are used to develop and maintain efficient and effective fixed-route transit service.

This Title VI 2016 Service Standards and Policies document is the second formal service standard document for SMART. While it was initially created to meet FTA Title VI requirements, SMART will be looking to make transit service standards and policies part of the Transit Master Plan through the update of that document.

#### **FTA Title VI Standards & Policies**

The FTA requires all fixed-route transit providers of public transportation to develop quantitative standards and qualitative policies for the indicators below:

- A. Vehicle Load Standard;
- B. Vehicle Headway Standard;
- C. On-time Performance Standard;
- D. Service Availability Standard;
- E. Vehicle Assignment Policy; and
- F. Transit Amenities Policy.

**(VEHICLE) PASSENGER LOAD FACTOR** - Standards for passenger capacity are used to determine if a bus is overcrowded. The chart below shows the Maximum Safe Capacity of each type of bus in revenue service, both seated and standing. The Maximum Load Factor is the ratio between seated and standing capacity.

Standard: SMART's standard for all Routes for Maximum Load Factor is 1.5, except in the case of freeway express buses traveling more than 55 mph, which then is 1.0.

| Vehicle Type       | Seated | Standing | Maximum Safe Capacity | Maximum Load Factor |
|--------------------|--------|----------|-----------------------|---------------------|
| 26 ft. Bus         | 21     | 7        | 28                    | 1.3                 |
| 30 ft. Bus         | 33     | 10       | 43                    | 1.3                 |
| 35 ft. Bus         | 35     | 11       | 46                    | 1.3                 |
| 40 ft. Bus         | 37     | 12       | 49                    | 1.3                 |
| 40 ft. freeway Bus | 45     | 0        | 45                    | 1.0                 |

Measure: Vehicle load issues will be measured through customer complaints, driver feedback and supervisor on-board reviews.

**VEHICLE HEADWAY** - Vehicle headway is the measurement of the frequency of service and is the scheduled time between two trips traveling in the same direction on the same Route at a given location. SMART provides both residential and commuter service.

Standard: The chart below shows the targeted headways for each Route.

| Route                     | Commute Period                                      | Base Period | Type of Service                                    |
|---------------------------|---|-------------|--|
| 1X - Salem                | 60 min. (30 min. in conjunction with Salem Transit) | ----        | Commuter express                                   |
| 2X - Barbur Blvd.         | 30 min.   | 60 min.     | Local in Wilsonville, then express to Barbur Blvd. |
| 3 - Canby                 | 60 min.   | ----        | Out of town commuter                               |
| 4 - Crosstown             | 30 min.   | 60 min.     | Local  |
| 5 - 95 <sup>th</sup> Ave. | 30 min.   | ----        | Local Commuter                                     |
| 6 - Argyle Sq.            | 30 min.   | ----        | Local commuter                                     |
| 7 - Villebois             | 2 trips a.m./2 trips p.m.                           | ----        | Local Commuter shuttle                             |

Measure: Ensure that schedule changes of 25% or more will go through a Title VI review process.

This will ensure that SMART meets the expectation of Title VI that the minority/disadvantaged population will be provided no less service than the non-minority/non-disadvantaged populations.

**ON-TIME PERFORMANCE** – On-time performance is a measure of trips completed as scheduled.

**Standard:** SMART has set a standard that at least 92% of all trips will run on time. SMART measures on-time performance through bus schedule adherence. A bus is considered “on time” if it is within 0-5 minutes of the schedule at any time point, “late” is defined as more than 5 minutes after the scheduled departure time, and “early” is defined as anything before the scheduled departure time.

Measure: Schedule adherence will be measured through computer software that is connected to an AVL on each vehicle. The software provides highly accurate on-time performance data regularly throughout each day. Ride checks, field checks, and trip checks will be performed periodically to ensure the computer program maintains accuracy.

**SERVICE AVAILABILITY** - Service availability (a.k.a. service access) is a general measure of the distribution of Routes within the SMART service area.

**Standard:** SMART’s goal is to ensure that 85 percent of City residents live within walking distance (i.e., no more than 1/4 mile) of a bus stop. SMART service continues to be particularly strong in neighborhoods with significant minority and low-income populations.

Measure: Transit access is determined by mapping all active bus stops within the system and then calculating the population (based on 2010 Census data) within 1/4 mile radii of those stops. This information is then compared to the City’s total population.

**VEHICLE ASSIGNMENT POLICY** - Vehicle assignment refers to the process by which transit vehicles are placed into service in on Routes throughout the SMART’s system

**Standard:** Vehicles are rotated throughout the SMART system, with newer vehicles serving all areas of the system. Specific vehicles are assigned to Routes only when required by operating conditions (e.g., in cases where a smaller bus is required to provide service on narrower streets).

Measure: Daily assignments are reviewed to ensure that the most equitable distribution is made. Generally, within the small city of Wilsonville, the same buses serve both the minority/disadvantaged neighborhoods and the non-minority/non-disadvantaged neighborhoods. As such, there is no pattern of providing poorer quality vehicles in minority or disadvantaged neighborhood.

**DISTRIBUTION OF TRANSIT AMENITIES** - Distribution of Transit Amenities is a general measure of the distribution of transit amenities (items of comfort, convenience and safety) available to the general riding public

Standard: Bus stop signs: SMART ensures that bus stops are easily identifiable, safe, and accessible places to wait for the bus. Seats can be applied to signs as needed.

Shelters: Although some shelters are provided by developers in new growth areas, SMART uses ridership as the primary criterion for determining shelter placement when public funds are used. Minimum threshold for shelter consideration is an average of 10 or more boardings per weekday. A seat bench is included with all shelters.

Trash can placements: Trash cans are provided at all shelters. They are mounted on either the shelter or bus stop sign pole so as to not block ADA pads or pedestrian walkways. At other stops, trash cans are provided on an as needed basis.

Measure: The distribution of transit amenities is determined by demand. Annually, SMART reviews ridership levels to make decisions on how limited federal resources should be spent. Currently, passenger amenities are spread equitably throughout the SMART service area.

Attachment F



**COMMENT CARD**

Please complete this card and return it to the Operator, drop off at City Hall, or mail to 29799 SW Town Center Loop E. Wilsonville, OR 97070

NAME: \_\_\_\_\_  
DATE: \_\_\_\_\_  
PHONE: \_\_\_\_\_  
EMAIL: \_\_\_\_\_  
ADDRESS: \_\_\_\_\_

WOULD YOU LIKE SOMEONE TO CONTACT YOU?  YES  NO

COMMENTS:  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

The City of Wilsonville operates services and programs without regard to race, color, and national origin. Please contact the City of Wilsonville with questions, comments, or complaints about SMART's non-discrimination policies.

**Smart@ridsmart.com 503-682-7790**



**Tarjeta de Comentario**

Por favor complete esta tarjeta y devuélvela al conductor del autobus, o dejéla en o envíela a 29799 SW Town Center Loop, E, Wilsonville, OR 97070

NOMBRE Y APELLIDO: \_\_\_\_\_  
\_\_\_\_\_  
FECHA: \_\_\_\_\_  
TELÉFONO: \_\_\_\_\_  
CORREO ELECTRÓNICO: \_\_\_\_\_  
\_\_\_\_\_  
DIRECCIÓN: \_\_\_\_\_

¿Le gustaría hablar con alguien en español sobre este comentario  SÍ  NO

COMENTARIO:  
\_\_\_\_\_  
\_\_\_\_\_  
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\_\_\_\_\_

SMART está dedicado a proveer servicios y programas sin respeto a raza, color de piel, y nacionalidad. Por favor comuníquese con el Director de Tránsito de la Ciudad de Wilsonville si tiene preguntas o comentarios sobre la política de no discriminación o para presentar una queja.

**Smart@ridsmart.com 503-682-7790**

## Appendix D – Planning Framework

The following plans, policies and documents were considered most relevant for the SMART Transit Master Plan Update:

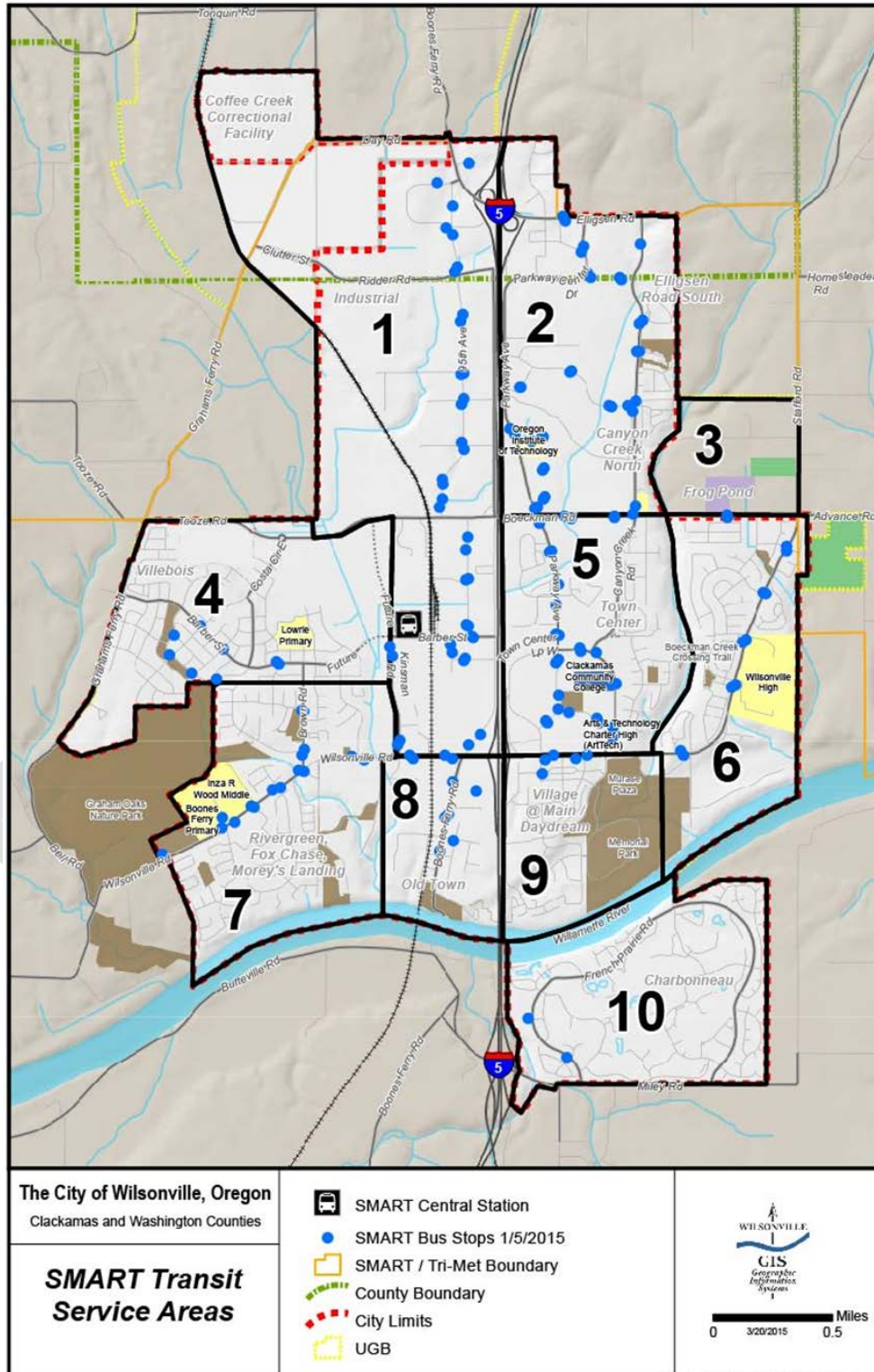
| Plan  | Agency                 | Description   |
|---|------------------------|---|
| Oregon Transportation Plan (OTP) (2006)   | ODOT                   | The OTP is the statewide multimodal transportation plan. It defines goals, policies, and strategies to guide the development of state, regional, and local transportation plans. The OTP does not recommend specific projects but provides a framework for prioritizing transportation improvements and funding.          |
| Regional Transportation Plan (RTP) (2014)   | Metro                  | The RTP proposes investing more than \$22 billion in local, regional, state and federal funds during the next 25 years to improve safety, system reliability and travel choices for everyone, revitalize downtowns and main streets, create jobs and support the region’s economy, and reduce our region’s carbon output. |
| Regional Travel Options Strategic Plan (2012)   | Metro                  | Metro’s Regional Travel Options (RTO) program supports Metro’s mission of creating a great place by increasing the awareness of non-single occupancy vehicle (SOV) travel options such as biking, walking, taking transit, and ridesharing.   |
| Transportation System Plan (TSP) (2013)   | City of Wilsonville    | The Wilsonville Transportation System Plan (TSP) is the City's long-term transportation plan and is an element of its Comprehensive Plan. It includes policies, projects, and programs that could be implemented through the City's Capital Improvement Plan, development requirements, or grant funding.                 |
| Tri-County Coordinated Transportation Plan for Seniors & Persons with Disabilities (2016) | TriMet                 | Vision: Guide transportation investments toward a full range of options for seniors and people with disabilities, foster independent and productive lives, strengthen community connections, and strive for continual improvement of services through coordination, innovation, and community involvement.                |
| Regional Active Transportation Plan (2014)  | Metro                  | Provides a vision, plan and policies for communities in our region to increase transportation options and support economic development, healthy active living and equity. An agreed upon regional strategy knits together local plans and projects efficiently, consistently and cohesively.                              |
| Salem- Keizer Transit Long-Range Regional Transit Plan (LR RTP) (2013)                    | Salem – Keizer Transit | The LR RTP provides a blueprint for Salem-Keizer Transit operations for the next 20 years and prioritizes future transit service investments amid limited resources.  |
| Climate SMART Strategy (2014)   | Metro                  | The Climate Smart Strategy is a set of policies, strategies and near-term actions to guide how the region moves forward to integrate reducing greenhouse gas emissions with ongoing efforts to create the future we want for the Portland Metro Region  |

| Plan  | Agency  | Description  |
|---|---|--|
| Oregon Transportation Options Plan (2015)           | ODOT  | The Oregon Transportation Options Plan is directed towards the education and advertisement of different transportation strategies throughout the Metro region. While most plans focus on the improvement and expansion of transit systems, the Oregon Transportation Options Plan is committed to providing investment and awareness within the current state of the system so that people can make informed decisions on how to travel in a way that has a healthy impact on our economy, environment, and society. |
| Employee Commute Options Program (1996)             | DEQ   | The Oregon Legislature passed a series of laws designed to protect air quality in the Portland metropolitan area that includes Employee Commute Options (ECO). The Oregon Department of Environmental Quality (DEQ) then established a set of Administrative Rules to implement the law. The ECO rules set more specific goals for trip reduction that specifically target businesses with more than 100 employees.  |
| Americans with Disabilities Act                     | Federal   | The Americans with Disabilities Act (ADA) requires SMART and all other public transit agencies that operate fixed-route local systems to provide paratransit or other special service to individuals with disabilities who have difficulty using fixed-route services. The law requires that SMART provide ADA service that is comparable to the level of service provided to individuals without disabilities, who can use the fixed-route system.  |
| Oregon Transportation Planning Rule (2011)          | Department of Land Conservation and Development | This division implements Statewide Planning Goal 12 (Transportation) to provide and encourage a safe, convenient and economic transportation system. This division also implements provisions of other statewide planning goals related to transportation planning in order to plan and develop transportation facilities and services in close coordination with urban and rural development. The purpose of this division is to direct transportation planning in coordination with land use planning.             |
| Clackamas County Transportation System Plan (2013)  | Clackamas County                                | Clackamas County Transportation Plan calls for increasing transit use by encouraging land use patterns, development designs, and street and pedestrian/bikeway improvements that support transit. The plan sets a goal of transit service within 1/4 mile of most residences and businesses within the Portland Metropolitan UGB.  |
| Washington County Transportation System Plan (2015) | Washington County                               | Washington County's Transportation Plan places a strong emphasis on regional connections and on strategies to increase transit efficiency and access. The Plan calls for improving bicycle and pedestrian access to transit stops. It also calls for road improvements and for private development in close proximity to major bus stops, commuter rail stations, and existing and proposed light rail stations to include appropriate features to support and complement existing and future transit services.      |



# Appendix E – Neighborhood Characteristics

The following data was collected in 2015 as part of an Existing Conditions Report. Data was collected by bus drivers within a span of a week.



## Area 1: Westside Industrial (Coffee Creek, 95<sup>th</sup> & Boberg Road areas)

### Overview

The Westside Industrial area is located just west of the I-5 Freeway. The industrial area forms a swath from Wilsonville Road north to Elligsen/Day Road and includes the Coffee Creek Wetland area to the west. The majority of development located south of Elligsen Road between SW 95<sup>th</sup> & Boberg/Boones Ferry is industrial, with some commercial uses. The largest employers currently located in the area include Convergys, Rite Aid Distribution, Microsoft, TE Connectivity Precision Interconnect, Coca Cola Bottling Company and the Coffee Creek Correctional Facility (which continues to be within TriMet's service territory in spite of being within the City of Wilsonville).

With the exception of the State Correctional Facility which is located in the northwest corner of this area just off Day Road, the land located in the Coffee Creek area is sparsely developed. The recently completed Coffee Creek Master Plan created a detailed transportation and land use plan for that area and development plans for expansion to the north into the Basalt Creek area and neighboring City of Tualatin.

The Westside Industrial area is also home to Wilsonville's SMART Central Station and the southernmost terminus of TriMet's Westside Express Service (WES) commuter rail. The SMART Central Station serves as the main transfer point for the entire SMART system including connections to neighboring regional transit providers.

Currently, the major north/south streets that serve the Westside Industrial area include 95<sup>th</sup> Avenue/Boberg, and Boones Ferry Road. Major east/west streets include Boeckman and Elligsen Roads, which offer direct connections from this area to Wilsonville's east side. In addition to these existing streets, Day/Elligsen Road, Grahams Ferry Road and the planned Kinsman Road extension will provide important access to the developing northern Coffee Creek and Basalt Creek areas.

Other key streets for SMART include Kinsman Road, Barber Street and Boones Ferry Road in the southern part of this neighborhood area, all of which provide access between Wilsonville Road and the SMART Central. The Commerce Circle Loop located near Elligsen Road is also an important transit street as it provides a transfer point between TriMet's 96 service and SMART's Route 5.

There is limited residential development within the Westside Industrial area. The one major exception is the Walnut Mobile Home Park located between Boeckman Road and Boberg Street – which includes about 57 units. Children who live within this area generally attend Lowrie Primary School (located on SW Brown Road), Wood Middle School (on Wilsonville Road West) and Wilsonville High School (on Wilsonville Road East).

## **Commuting**

As a major employment area in the City, jobs in the Westside Industrial area tend to provide more blue-collar employment with a focus on manufacturing, warehouse and wholesale trade. There are an estimated 7,000 employees who work within this area. The largest concentration of these workers commute from Portland and Wilsonville, but workers also live in other neighboring communities such as Salem, Tigard, Tualatin, Woodburn, Beaverton and Sherwood.

## **Existing Services & Amenities**

The location of the SMART Central Station in the southern portion of the Westside Industrial area means that businesses along Barber Street are served by transit throughout the day – as all seven lines provide some service to the area during the weekday; and Route 4 provides service on Saturday but only along Wilsonville Road and not directly to the SMART Central Station. Route 5, along 95<sup>th</sup> Avenue, provides weekday peak hour service to businesses located on or near Boberg Road and 95<sup>th</sup> Avenue; and TriMet Line 96 connects with SMART’s Route 5 at Commerce Circle seven times during peak service hours.

## **Fixed-Route**

As noted above, this area includes the SMART Central Transit Center. Because of this, ridership is high, with about 448 boardings per day. More than 80 percent of those boardings occur at the Transit Center. Other key stop areas include the transfer point between TriMet 96 and the SMART Route 5 at Commerce Circle and 95<sup>th</sup> Avenue. Route 5 sees about 25 boardings per day. Also in this area, the closest stop to Wilsonville Road on Boones Ferry (near Fred Meyer) sees about 30 boarding per day.

## **Dial-a-Ride**

Dial-a-Ride pickups and drops offs are also limited in this area (on average less than 10 per day). The Coffee Creek Correctional Facility sees the most boarding/alighting activity in this area as it is the current transfer point between TriMet LIFT and SMART Dial-a-Ride.

## **Major Opportunities**

- Partner with TriMet to streamline transit service between SMART Central, TriMet’s line 96 and SMART’s Route 5 to eliminate duplicative service and the extra transfer currently required for passengers traveling to and from Portland.
- Kinsman Road expansion could provide alternate routes for Route 5.
- The employment area offers opportunity for large carpool coordinating.

## **Identified Concerns**

- The nature of manufacturing and warehouse facilities that operate most hours of the day. Current limited hours of transit operation do not always match the needs of the employees in this area.
- The current transfer point between SMART and TriMet LIFT has been identified as a concern for DAR riders – they would like to see the transfer point moved to the Holiday Inn near Commerce Circle.

## Area 2: Northeast Neighborhoods

### (Elligsen Road, Eastside Industrial/Commercial and Canyon Creek areas)

#### Overview

The Northeast Neighborhood area includes both sides of Elligsen Road, south to Boeckman and east of I-5 to the City boundary. This area is made up of the East Industrial Campus neighborhood and the Canyon Creek residential neighborhood, with the major dividing line between residential and employment found east and west of Canyon Creek Road.

The Eastside Industrial area is located between I-5 and Canyon Creek Road. It is home to Wilsonville's largest employers and institutions of higher learning including Xerox, FLIR, Mentor Graphics, D.W. Fritz, Rockwell Collins, Oregon Tech and Pioneer Pacific College.

The Canyon Creek Neighborhood is located just to the east of these major employers. Largely residential, the Canyon Creek Neighborhood includes large multi-unit housing developments such as Canyon Creek Meadows, Canyon Creek Estates and the Carriage Oak Community. Sidewalks line all of the public streets but there are gaps in the bicycle lane network through this area. The one major exception is along Parkway Avenue from Xerox north to Argyle Square. Here there is no safe pedestrian or bicycle connection and access to this nearby commercial area is limited to the east side of this area along Canyon Creek Road. The one major multi-use-path starts in the southern part of this neighborhood on the Mentor Graphics Campus and provides a paved trail that meanders south through undeveloped fields to the Town Center area. This trail is signed as private property and could be developed by Mentor Graphics in the future.

Children living in the Northeast Neighborhood area generally attend Lowrie Primary and Wood Middle school, which are over two miles from the area on the west side of Wilsonville. Wilsonville High School is located less than two miles to the southeast.

#### Commuting

The Eastside Industrial area is the second largest employment area in Wilsonville. It provides jobs for over 6,000 skilled workers including high-tech and professional service industries. Of employees working in this area, close to 8 percent live in Wilsonville. The remaining employees commute from other nearby communities, including Portland (about 13%), Beaverton, Tigard, Tualatin, Hillsboro, Lake Oswego and Salem (2-4%).

In addition to workers who commute daily to this area, there are also more than 1,200 residents who live in the Canyon Creek Neighborhood and commute from their homes to jobs. Of that number, approximately seven percent (100) of the residents have jobs within Wilsonville (which might include those in the Eastside Industrial area); and another 14 percent (200) commute to jobs in Portland. Many of the others commute to jobs up and down the I-5 corridor (Salem, Eugene/Springfield, Tigard, Corvallis, etc.)

### **Existing Services & Amenities**

The Northeast Neighborhoods are served by the weekday peak hour Route 6: Canyon Creek and the Route 2X: Barbur throughout the weekday and on Saturday. Combined, these routes provide 15 minute peak-hour service to the major employers in this area.

### **Fixed-Route**

The Northeast Industrial area sees about 200 boardings per day. Key stops include the major employers (Mentor Graphics, Rockwell Collins, FLIR and Xerox) and the apartment and housing development areas along Canyon Creek Road.

### **Dial-a-Ride**

Dial-a-Ride pickups and drops offs average less than 15 per day. Argyle Square (Target/Costco) and Oregon Tech see the most boarding/alighting activity for DAR in this area.

### **Major Opportunities**

- With the majority of the workforce and a large share of the residential population in this area commuting during peak times, improved transit service could capture some of the highest ridership in the City.
- New extension of Canyon Creek Road to Town Center Loop creates opportunities for future transit service.

### **Identified Concerns**

- Lack of mid-day service may deter Oregon Tech students from using the service due to classes going throughout the day.

## Area 3: Frog Pond

### Overview

The City of Wilsonville is currently completing a master planning process that will define the future of the 500-acre Frog Pond/Advance Road area. This process will establish a Master Plan for the area and define expectations for the type of neighborhoods that will be developed there in the future. Following completion of the Plan, the project team will further refine the area within the Urban Growth Boundary (UGB) (mostly west of Stafford Rd) and develop zoning and other implementation tools needed to implement the Plan. The City of Wilsonville is expecting more than 600 single-family households and an elementary school in the West Neighborhood which is already inside the UGB. The East and South neighborhoods are expected to have similar density.

Expected to be largely residential, the Frog Pond area will include Wilsonville-West Linn School District plan to build a second middle school for Wilsonville just east of Stafford Road which is under construction and opening in Fall 2017. This new middle school will ease overcrowding at Wood Middle School (on the west side of Wilsonville) and offer a closer middle school option for many east side neighborhoods.

Key east/west access to this area will be along Boeckman/Advance Road while Stafford/Wilsonville Road will provide key north/south access. Once developed, this area should also offer improved pedestrian/bicycle access along Boeckman Road.

### Existing Services & Amenities

Currently, SMART provides very limited service to this area, as part of the current cross-town Route 4.

### Major Opportunities

- The new middle school could serve a more convenient bus turn-around point than where it currently turns around (at Mentor Graphics).
- Development with some retail could be a potential desired destination.

### Identified Concerns

- Increased demand from the new residential development without additional revenue could put a stress on SMART existing resources.

## Area 4: Villebois

### Overview

Villebois is a master planned community, located primarily on the old Dammasch State Hospital site. The Villebois Master Plan calls for 2,645 housing units and as of now, approximately two-thirds have been built. Upon completion, the developed community will offer a mix of single family homes and multifamily unit options – including town homes and apartment complexes such as the existing Renaissance Court, Rain Garden Apartments and The Charleston.

Barber Street, Brown Road, Montebello and 110<sup>th</sup> Avenues provide key connections between Villebois and other areas of town. All existing streets have sidewalks and pedestrian/bike paths that offer key connections for residents. An extension of Barber Street from Villebois east to Kinsman Road was completed in 2016 which now allows for direct connection between Villebois and the SMART Central and from Villebois to Boones Ferry Road.

Children in the area generally attend Lowrie Primary School – which is located on Lisbon Street, within the Villebois neighborhood, and Wood Middle School on Wilsonville Road. The High School is located about three miles to the east on Wilsonville Road.

### Commuting

There are 611 residents within this neighborhood that are commuting daily to jobs. Of this number, about 120 commute to jobs in Wilsonville and 150 commute to jobs in Portland. The remaining employees in this area typically commute to other nearby communities including Beaverton, Hillsboro, Tualatin and Lake Oswego.

Because this is largely a residential area, employment opportunities within Villebois are limited. As of today, the jobs that are located in Villebois are mostly due to the Lowrie Primary School and people working from home.

### Existing Services & Amenities

SMART service to this area is limited. Route 7: Villebois currently serves the neighborhood via Brown Road with two trips in the morning and two trips in the evening.

### Fixed-Route

SMART sees about eight boarding rides per day.

### Dial-a-Ride

There is an increase in demand for Dial-a-Ride pickups and drops offs in this area which currently averages 20 per day. The Charleston and Rain Garden Apartments tend to have the most active DAR usage in the area.

### **Major Opportunities**

- Close proximity to schools may offer opportunities to encourage parents/children to bike or walk to school and then take transit or other options to work.
- Adapting current Route to provide more frequent and direct service could increase ridership.

### **Identified Concerns**

- This area was identified by the community as needing additional transit service in the City's 2013 Transportation System Plan.
- Some streets are not wide enough to facilitate lay-over or turn-around opportunities for larger transit vehicles, but these streets provide no identified problems for smaller transit vehicles.

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## Area 5: Town Center

### (Renaissance, Ash Meadow, Jory Trail, Courtside, Vlahos neighborhoods and Town Center Commercial Area)

#### Overview

This area runs from Boeckman Road south to Wilsonville Road and from the Boeckman Creek Corridor on the east to the I-5 Freeway on the west. The area includes a major retail and commercial area called Town Center. Wilsonville City Hall, the US Post Office, Clackamas Community College Wilsonville Campus, Wilsonville Arts and Technology Alternative High School, the Wilsonville Community Center and other related City facilities are located here, as are major retailers, restaurants, movie theatres, commercial activities and Safeway.

Despite the commercial feel, many residents also call this area home – particularly in those areas just south of Boeckman Road and north of the Town Center Loop. Major residential areas include new mixed residential developments like Renaissance, Ash Meadows, Jory Trail and older more established neighborhoods to the east such as Courtside and Vlahos. Multifamily housing units are included throughout this area; the largest of these include Terrene at the Grove, Sundial, Woodleaf, and Jory Trail. This area also includes a significant number of retirement communities including The Springs, Portera, and Winfield Village.

The Mentor Graphics company (recently acquired by German company Siemens) holds title to approximately 60 undeveloped acres on the south side of Boeckman Road. That property is expected to become available for development in the future and will certainly create a demand for additional transit services.

Boeckman Road, Parkway Avenue, Town Center Loop, and Wilsonville Road are key connecting roadways in the area. In addition, the recent completion of Canyon Creek Road south to Town Center Loop provides an important north/south connection for the east side of Wilsonville. Most of the development in this area has an established network of sidewalks. Wilsonville Road offers some east/west bicycle access in the area. Despite this, north/south bicycle lane access is limited – though the new Canyon Creek Road extension and the restriping of Town Center Loop East to create a bike lane has recently improved safety and access in this area. Most of the children in this area attend Boones Ferry Elementary and Wood Middle School which are located west on Wilsonville Road and Wilsonville High School which is east. The communities closest to Boeckman (like Renaissance and Ash Meadows), however, attend Lowrie Primary in Villebois instead of Boones Ferry, utilizing Boeckman Road for access.

#### Commuting

There are approximately 2,000 employees who work in this area – making it the third largest employment area within Wilsonville. Of those, just over 12 percent (250) live in Wilsonville; another 11 percent (230) live in Portland, the remaining commute from other nearby communities.

There are also about 1,193 residents who live in the Town Center area and commute daily from their homes to jobs. Of that number, about 21 percent (250) commute to jobs within Wilsonville (which might include those in the Town Center area); and another 25 percent (300) commute to jobs in Portland. The rest commute to jobs in Beaverton, Hillsboro, Tualatin, Tigard, Oregon City or Lake Oswego.

### **Fixed-Route**

With about 250 boardings per day, the Town Center area sees some of the largest ridership numbers in Wilsonville. Key stops include the areas near the Community Center/City Hall and stops throughout Town Center Loop east.

### **Dial-a-Ride**

The Town Center area also sees some of the largest DAR numbers – with about 56 pickups and drop offs per day. Key pick up/drop off locations include Winfield Village and The Springs senior housing facilities as well as Safeway and the City Community Center.

### **Major Opportunities**

- The Canyon Creek Road extension offers new opportunities to serve this area.
- Town Center redevelopment provides a chance to Route buses to desired destinations.

### **Identified Concerns**

- The redevelopment may cause some detours for SMART in the upcoming years. There are difficult places to turn around.
- Route 4's stop at SMART central can become a long wait time for someone traveling from east to west Wilsonville.

## **Area 6: Southeast Neighborhoods (Wilsonville Meadows, Boulder Creek and Landover neighborhoods)**

### **Overview**

This area is located on the east side of Wilsonville. It runs from the City limits to Boeckman Creek and from Boeckman Road to the Willamette River and incorporates the Wilsonville Meadows, Boulder Creek and Landover neighborhoods. This area is largely single family residential but includes four major apartment complexes, all located along Wilsonville Road: Boulder Creek, Berkshire Court, Hathaway Court and Bridge Creek.

The largest employer in the area is the Wilsonville-West Linn school district which has teachers and support staff at Wilsonville High School and Boeckman Elementary School on Wilsonville Road.

Children living in the east side neighborhood area generally attend Boeckman Creek Primary School and Wilsonville High School, both of which are located along Wilsonville Road in this neighborhood. Wood Middle School, located on the other end of Wilsonville Road – slightly over two miles to the west, is the designated middle school for this community. Construction of a new middle school along Advance Road, northeast of Area 6, has now begun.

### **Commuting**

According to the Longitudinal Employer-Household Dynamics (LEHD) which uses Census information and 2011 State Employment Data, some 1,330 individuals within these neighborhoods are employed. Of those, 18 percent work jobs in Wilsonville, another 20 percent commute to jobs in Portland. The remaining employees in this area commute daily to other nearby communities.

Because this is largely a residential area, employment opportunities within the area are limited. The one major employer in the area is the Wilsonville-West Linn School District which employs teachers and staff at both the Boeckman Creek Primary and Wilsonville High Schools.

### **Existing Services & Amenities**

SMART provides 30 minute service to the Southeast Neighborhood Area on Wilsonville Road as far north as Wilsonville High School during an extended peak (5am - 10am; 2pm - 7:30pm) and hourly service during the midday and on Saturdays with the Route 4 – Wilsonville Road. A few peak trips throughout the day continue on past Wilsonville High School to serve Mentor Graphics on Boeckman Road.

There are stops on Wilsonville Road that serve the Southeast Neighborhoods. Most residents live within 1/3 mile walking distance of these stops.

**Fixed-Route**

This is another strong ridership area – with just under 150 boardings per day. Key stops are focused nearest the Wilsonville High School with over 70 percent of the ridership coming from the Meadows Loop and Parkway stops.

**Dial-a-Ride**

This residential area averages less than five DAR pickup and drop offs per day – the majority coming from individual homes.

**Major Opportunities**

- Close proximity to schools may offer opportunities to encourage parents/children to bike or walk to school and then take transit or other options to work.
- Opportunity for bus to turn-around.

**Identified Concerns**

- None identified.

## Area 7: Southwest Neighborhoods

### (Rivergreen, Fox Chase, Morey's Landing, Park at Merryfield, Silver Creek and Montebello neighborhood areas)

#### Overview

The Southwest Wilsonville Neighborhood area extends from the Willamette River North to Camelot Road and from the western City Boundary east to Kinsman Road. Once farm lands, the Southwest Wilsonville Neighborhood area was developed into traditional residential areas, beginning in the 1960s. The majority of housing in the area is single family homes, though there are some large multifamily developments along Wilsonville Road including Autumn Park, Wilsonville Heights, Timber Creek Village, Wiedemann Park, Carriage Estates and Montebello Apartments, among others.

Wilsonville Road, a designated minor arterial in the Wilsonville Transportation System Plan, is the only major through street to serve this neighborhood area. Located in the middle of the southwest Wilsonville neighborhood, it serves to split the area into two distinct neighborhood groups with the Rivergreen/Fox Chase/Morey's Landing communities located to the south of Wilsonville Road and the Park at Merryfield, Silver Creek and Montebello communities located to the north.

A limited number of neighborhood streets provide some north/south connection from the neighborhood areas to Wilsonville Road including Willamette Way West, Willamette Way East, Wellington Road and Orchard Drive/Guiss Way and Brown Road. A variety of other loop or cul-de-sac streets serve these neighborhood areas. These streets are specifically designed to reflect the residential nature of the community by slowing and mitigating through traffic. The majority of the streets within this southwest neighborhood area have sidewalks and several bike and pedestrian paths offer important connections between neighborhoods at dead-end streets.

The public elementary and middle school for the majority of children within the Southwest Neighborhood area is with Boones Ferry Elementary School and the Inza Wood Middle School. Both of these schools are located just across Wilsonville Road on Willamette Way east and are within ½ mile or less of most homes within this neighborhood. Wilsonville High School is located more than two miles away on the east side of town.

The only major exception is the Montebello neighborhood, which is in the Lowrie Primary School area. Lowrie School is just north of Montebello in Villebois. A pedestrian path within the Montebello community allows direct pedestrian access from the neighborhood to the Lowrie school grounds.

## **Commuting**

There are 1,252 individuals within this neighborhood that are employed. Of those, 20 percent have jobs in Wilsonville, another 27 percent commute to jobs in Portland. The remaining employees in this area commute regularly to other nearby communities.

Because this is largely a residential area, employment opportunities within the area are limited. The one major employer in the area is the Wilsonville-West Linn School District that employs teachers, and other support staff at the Boones Ferry and Wood Schools.

## **Existing Services & Amenities**

SMART provides 30 minute service to the Southwest Neighborhood Areas as far west as Willamette Way East on Wilsonville Road during an extended peak (5a.m.-10a.m.; 2p.m.-7:30p.m.) and hourly service during the midday and on Saturdays with the Route 4 – Wilsonville Road. A few trips throughout the day provide service as far east as Graham Oaks Nature Park. The Route 7 also provides limited peak hour service to the Montebello and Park at Merryfield Neighborhoods on Brown Road.

There are 16 stops on Wilsonville and Brown Road that serve the area. Over 80 percent of residents live within 1/3 mile walking distance of these stops.

## **Fixed-Route**

With about 130 boardings per day, this area sees consistent ridership. Key stops include the apartment complexes along Wilsonville Road and the Wood Middle School area.

## **Dial-a-Ride**

Another largely residential area, Dial-a-Ride use is low with about 10 riders per day. The largest concentration for Dial-a-Ride is at the Brookside Senior Living Apartment in the Montebello neighborhood.

## **Major Opportunities**

- Close proximity to school; may offer opportunities to encourage parents/children to bike or walk to school and then target transit or other options to work.

## **Identified Concerns**

- This area was identified by the community as needing additional transit service in the 2013 Transportation System Plan.
- Adequate turnaround and layover locations due to street design have made serving this area difficult.

## Area 8: Old Town

### Overview

The Old Town historic neighborhood is located on the west side of Wilsonville. It borders the I-5 freeway on the east; runs to Kinsman/Industrial Way on the west and from the Willamette River north to Wilsonville Road.

The Old Town area contains a mix of housing types and retail/commercial areas. The bulk of single family housing is found close to the river, while most of the multifamily and retail/commercial areas are located closer to Wilsonville Road. Employment in the area comes from retailers like Fred Meyer and small commercial businesses. Bell Tower Apartments and Boones Ferry Village are the major multifamily complexes within the area. Both are located on the south side of the Fred Meyer complex on Boones Ferry Road.

Wilsonville Road borders Old Town on the north and is the key arterial connecting Old Town with the rest of the City. Boones Ferry Road, a neighborhood collector, provides the only access from Wilsonville Road into the Old Town neighborhood. While all of the commercial/retail area near Wilsonville Road has sidewalks, much of the area near the river does not. A striped bicycle lane runs the full length of Boones Ferry Road in the Old Town area. This bicycle lane connects with the Boones Ferry Trail near the waterfront and provides a direct connection (under I-5) to the neighborhoods east of the freeway.

Public school options for children in this neighborhood include Boones Ferry Elementary and Inza Wood Middle School. Both are located just over a mile west of the neighborhood on Wilsonville Road. The public high school, Wilsonville High, is located on the east side of town – roughly 1 ½ miles east of the Old Town neighborhood.

### Commuting

There are about 520 employees who work in this area. Of those, just less than six percent (31) live in Wilsonville. Another 11 percent (54) live in Portland and the remainder commute from other nearby communities.

There are also about 161 residents who live in the Old Town area who are employed and commute daily from their homes to jobs. Of that number, about 21 percent (30) of the residents work in Wilsonville (which might include those working in the Old Town area); and another 26 percent (42) commute to jobs in Portland. The remaining employees mostly commute to jobs in Beaverton, Hillsboro, Tualatin, Tigard, Oregon City or Lake Oswego.

### Existing Services & Amenities

SMART provides 30-minute service along Boones Ferry Road during an extended peak (5a.m.-10a.m.; 2p.m.-7:30p.m.) and hourly service during the midday and on Saturdays with Route 4 – Wilsonville Road.

**Fixed-Route**

Modest ridership (about 80 boarding per day) can be found in the Old Town neighborhood – with close to 80 percent coming from the stops nearest Fred Meyer.

**Dial-A-Ride**

The Old Town area also sees about 20 pickups and drop offs per day – with the majority these also focused on the Fred Meyer area.

**Major Opportunities**

- The shopping area promotes a lot of ridership for those completing errands.
- Arrowhead road extension could connect Old Town to the Southwest neighborhoods.

**Identified Concerns**

- Access and turn-around options are limited.

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## Area 9: Village at Main / DayDream

### Overview

The Village at Main/Day Dream area is located just south of the Town Center Area. It runs from Wilsonville Road south to the river and from the Boeckman Creek Corridor on the west to the I-5 freeway. Restaurants, retail and commercial businesses line Wilsonville Road. The Village at Main, a collection of town homes and apartments, as well as commercial property, sits just off Wilsonville Road on Town Center Loop West, while single-family homes can be found in the Daydream and other neighborhoods closer to the river. This area is also home to Memorial Park, the City's Public Library and the Marquis Wilsonville Assisted Living Facility.

Wilsonville Road, designated a minor arterial in the Wilsonville Transportation System Plan, is the only major through street to serve this neighborhood area providing a key connection between this area and other parts of town. Memorial, Parkway, Holly and Rebekah Roads are the key connecting roadways providing access from the neighborhood to Wilsonville Road. Neighborhood children attend Boones Ferry Primary, Wood Middle School and Wilsonville High School – all are located along Wilsonville Road nearly two miles from the neighborhood.

### Commuting

There are about 630 employees who work in this area. Of those, just over 12 percent live in Wilsonville; another eight percent live in Portland, the remaining commute from other nearby communities. There are also about 650 residents who live in the Village at Main/Daydream area and are employed. Of that number, about 22 percent work within Wilsonville (which may include jobs in this area); and another 24 percent commute to jobs in Portland. The rest commute to jobs in Beaverton, Hillsboro, Tualatin, Tigard, Oregon City, Lake Oswego or other nearby locations.

### Fixed-Route

While boardings in Village at Main are relatively low, about 40 boarding per day – over 80 percent of that number is found at the pull in circle stop at Town Center Loop West and Main. This is significant, because the “loop” is out of direction and adds to the overall travel time on the Route 4 - Wilsonville Road. SMART is now planning to move this stop to Wilsonville Road in order to improve efficiency of east/west travel.

### Dial-a-Ride

Dial-a-Ride pickups and drop offs average less than two per day.

### Major Opportunities

- High density population creates lots of potential for high ridership.

### Identified Concerns

- Stop proximity to the neighborhood may not be convenient enough to be desirable for residents to walk to.

## Area 10: Charbonneau

### Overview

Charbonneau is the only Wilsonville neighborhood south of the Willamette River. Running from the river south to Miley Road and from the I-5 freeway east to Eilers Road, this golf course community is one of Oregon's earliest planned communities. Designed in 1972, Charbonneau has a small commercial district and housing types ranging from condominiums and apartments to traditional single-family homes, golf-course homes, waterfront properties and gated estates. It is home to a sizeable retirement community (Springridge Court) which offers many associated amenities.

Miley Road is the main street to serve this neighborhood area providing a key connection between Charbonneau and the rest of the City. French Prairie Road is a loop that connects with Miley Road on both ends providing a ring of access for the community around the Charbonneau Golf and Country Club.

Much of the commercial area of the community has sidewalks, though local residential streets do not. Some of the existing pedestrian paths are on private property and not designed to meet ADA standards. A marked pedestrian/bicycle path provides access to about  $\frac{3}{4}$  of French Prairie Road, though the traffic treatments along the roadway make bicycle access relatively safe throughout this neighborhood.

Children living in the Charbonneau area attend schools in the Canby School District, with the nearest primary, middle and high school about four miles away.

### Commuting

There are approximately 1,120 individuals within Charbonneau who are employed. Of those, seven percent work at jobs in Wilsonville, another 20 percent commute to jobs in Portland. The remaining employees in this area commute to other nearby communities.

Because this is largely a residential area, employment opportunities within Charbonneau are limited. The one major employer is the Charbonneau Golf and Country Club and SpringRidge at Charbonneau – Senior Living Community. Unlike other areas of Wilsonville, employees working in this area are more likely to commute from the Canby or Woodburn area than other nearby communities.

### Existing Services & Amenities

SMART bus service connects Charbonneau to the rest of Wilsonville and the City of Canby with the Route 3: Charbonneau/Canby Route. Fixed-route service on the Route 3 is limited and ridership has been low, especially since CAT stopped service to Wilsonville – with only eight peak hour trips each week day.

### Fixed-Route

Fixed-route ridership in Charbonneau is limited – averaging less than 8 boardings per day.

**Dial-a-Ride**

Dial-a-Ride averages 15 pickups and drop offs per day. The largest concentration for DAR is at the SpringRidge Senior Living Complex.

**Major Opportunities**

- A designated shuttle for the Charbonneau residents could provide more direct and mid-day service.

**Identified Concerns**

- Low ridership possibly due to the service being commute hour only as part of the Canby service.
- Neighborhood is hard to access to due congestion on I-5 and the Boone Bridge.

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# Appendix F – TMP Public Outreach Summary



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## TRANSIT MASTER PLAN

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PUBLIC  
OUTREACH  
SUMMARY

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Winter 2017

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## Public Outreach Summary

### INTRODUCTION

In January 2016, South Metro Area Regional Transit (SMART) formally began updating the 2008 Wilsonville Transit Master Plan (TMP). The TMP is a broad look ahead to the type of transit system and supportive transportation options required to meet Wilsonville's mobility needs.

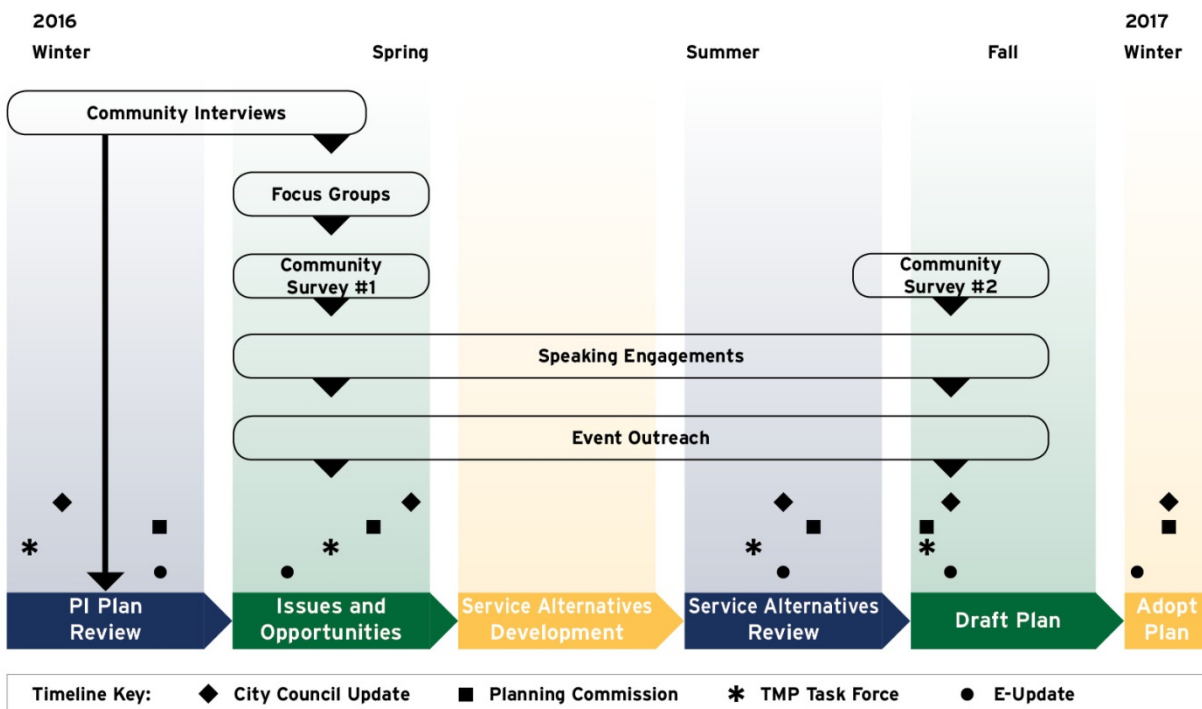
From the outset, SMART directed a process to involve a diverse and broad spectrum of existing and potential system users, and the businesses that support transit through payroll taxes.



*Outreach Photos*

### Three key phases:

- 1) **Issues and Opportunities Identification** *Winter 2016*  
 Informed community partners and stakeholders of the project purpose and framework, and listened to their perspectives on where to focus SMART resources.
  
- 2) **Service Alternatives Development** *Spring 2016*  
 Based on input from the Issues and Opportunities phase, SMART service planners and Jarrett Walker and Associates developed draft service alternatives.
  
- 3) **Service Alternatives Review** *Summer 2016*  
 Review by community partners and stakeholders via multiple and accessible outreach methods of the proposed service alternatives and potential funding strategies.



### Values guide involvement:

- Realistic:** be clear about the project constraints, objectives, and parameters
- Inclusive:** reach out to everyone, including those who don't use computers or face other barriers
- Meaningful:** provide timely information that is accurate and easily accessible
- Transparent:** make decisions public with key project materials available

To identify issues and opportunities SMART asked:

### *What are your priorities when you can't have it all?*

This frame helped SMART stakeholders to understand the trade-offs SMART faces in delivering transit services with available resources. Public responses grounded and illuminated the issues and opportunities facing Wilsonville and set the stage for transit service alternatives development. These options will be available for community consideration in summer and fall 2016.

## OUTREACH APPROACH

- Consistent, reliable, accessible information with identified SMART contact person
- Thorough stakeholder analysis
- Draft Public Involvement Plan vetted with key stakeholders
- Representative stakeholders interviewed from throughout Wilsonville
- Small issue-focused meetings held
- Non-traditional outreach targeted to those with low-income, people with disabilities, young and elderly people
- E-updates to Interested Parties List
- Timely response to all submitted comments and questions
- Electronic kiosks with information and survey placed at high visibility locations
- Speaking engagements at civic organizations
- Updates to relevant committees and City Council
- Transit Master Plan website with comment and survey functions
- Media (print, TV)

## OUTREACH METHODS

Beginning in spring 2016, staff used a series of methods to adhere to SMART's value of inclusion.

### Project Website Development

A bright, inviting, and accessible website provides community members a one-stop location to learn about the project, see upcoming events, project documents, available input opportunities, and option to sign up for project updates.



## Community Survey #1

An informative and brief survey posed questions on the key tradeoffs for SMART to consider in service alternatives development including:

- **Which types of trips do you currently use transit for?**
- **Which types of trips would you want to use transit for, but currently cannot?**
- **Are short or long transit trips more important to you?**
- **At what times should SMART primarily target services?**
- **Transit can be spread out, to get close to everyone, or it can be concentrated into frequent Routes. Which would you rather do?**

Launched on March 1, 2016 with pro-active outreach efforts, the survey received **over 800 responses** via a variety of methods including on-line, mobile kiosks, and in-person paper surveys.

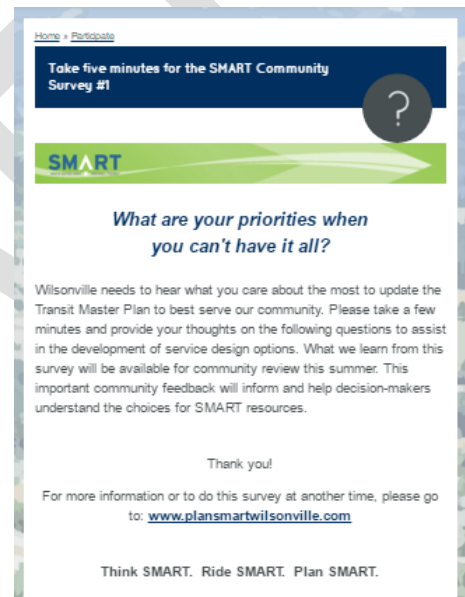
## Community Survey #2

SMART launched a second survey on August 8, 2016 to illustrate these service tradeoffs and ask about a spectrum of potential service alternatives to gauge the public's priorities:

- **How to balance ridership and coverage goals?**
- **How to balance rush-hour service with service other times of day and week?**
- **How to balance local and intercity (out of town) services?**

Over 500 respondents provided feedback.

See Appendix A for a complete list of locations and community partners instrumental in a successful survey response rate.



## Mobile Kiosks

Electronic kiosks placed at high visibility locations such as Wilsonville City Hall, Wilsonville Public Library, Oregon Institute of Technology, and Wilsonville Community Center made input easy for stakeholders on the move.





## Paper Surveys

Bi-lingual surveys accompanied the mobile kiosk stands and were distributed to large employers, schools, higher education institutions, large housing complexes, and in partnership with community organizations.

## Informational Bookmark Tear-Offs

In partnership with local businesses, community organizations, and city services, over 8,000 tear-offs were distributed throughout the community and on SMART buses. Brightly colored and easily accessible, these bookmarks connected mobile devices to the survey via code scan.

## Display Outreach

Project staff conducted survey outreach at retail outlets, WES Station, SMART Central Station, employee break rooms, educational institution lobbies, and more.



## E-Updates

With the TMP's Interested Parties List of **over 1,300**, the project sent

E-Updates announcing:

- Project Overview
- Survey Launch #1
- Survey Reminder
- Survey Launch #2
- Survey Reminder
- Draft TMP Announcement & Public Hearings Notification
- City Council Action (to be sent late March 2017)

City departments, community businesses, faith-based organizations and other partners assisted in forwarding notices to their networks of residents, employees, students, and congregants.

**Encuesta sobre la Comunidad de SMART # 1**

**¿Cuáles son sus prioridades cuando no se puede tener todo?**

Wilsonville Transit Master Plan (TMP) es un estudio de planificación de transporte para el futuro de Wilsonville. Este estudio es el resultado de un proceso de consulta de servicio comunitario para recibir de la comunidad sus ideas. Estas ideas son importantes de la comunidad para ser informadas y guiar a los encargados de tomar las decisiones para el futuro de SMART.

Para obtener más información o para tener esta encuesta en otro idioma, por favor vaya a la página web: [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

**Planeta SMART. Paseo SMART. Plan SMART.**

**P1: Por favor, contáctenos de sus hábitos:**  
**¿Qué tipo de viaje actualmente utiliza en su tránsito?**  
 Por favor, marque todo lo que se corresponde.

Viajes al trabajo  
 Viajes a la escuela  
 Viajes a la recreación  
 Servicios comerciales, reuniones o eventos  
 Otros de México  
 Otros regionales  
 Otros

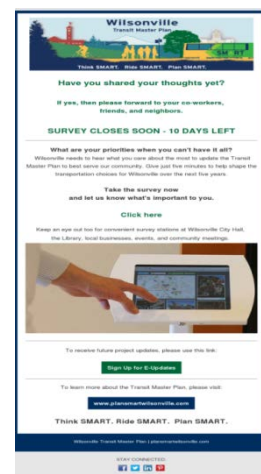
**¿Cuáles tipos de viajes desea de utilizar el tránsito de autobuses pero en la actualidad no puede?**  
 Por favor, marque todo lo que se corresponde.

Viaje al trabajo  
 Trabajo / reuniones ciudadanas  
 Compras de comida  
 Recreación  
 Servicios comerciales, reuniones  
 Otros México  
 Otros regionales  
 Otros

**Comentarios: ¿Por qué no?**

**¿Cómo se describe? Por favor, marque todo lo que se corresponde.**

Residente de Wilsonville  
 Vivo en un hogar para gente con ingresos bajos  
 Empleado en Wilsonville  
 Empleado fuera de Wilsonville  
 Sin trabajo  
 Persona viviendo con una discapacidad  
 Estudiante en Wilsonville  
 Estudiante fuera de Wilsonville



## Media

The Wilsonville Spokesman reported twice about the project launch and announced the on-line survey.

Using the city wide monthly newspaper, Boones Ferry Messenger, SMART announced and reminded residents of survey availability.

## Stakeholder Interviews

Project staff met one-on-one to interview and explore further the tradeoffs facing SMART. Participants included:

- Businesses
- Elected Officials
- City Staff
- Community Groups
- Educational Institutions
- Home Owner Associations

## Workshop and Targeted Focus Discussion Groups

### Community Workshop

Project Team and consultants held a large stakeholder workshop with community representatives to provide the necessary understanding of transit planning for well-informed decision-making and a community supportive Transit Master Plan for Wilsonville.

### Title VI/ Environmental Justice (EJ) Populations

To ensure that the impacts and benefits of the Wilsonville Transit Master Plan are equitably experienced regardless of race, national origin, gender, disabilities, English language proficiency or being low-income, project staff sought initial input on outreach methods with community organizations and conducted two targeted discussion groups:

- Title VI Focus Group (Low-income and Spanish Speaking)
- Older Adults and People with Disabilities

### Business Discussion Groups

Coordinated in partnership with the Greater Wilsonville Chamber of Commerce, two groups of business representatives met with project staff to discuss trade-offs, issues, and opportunities.

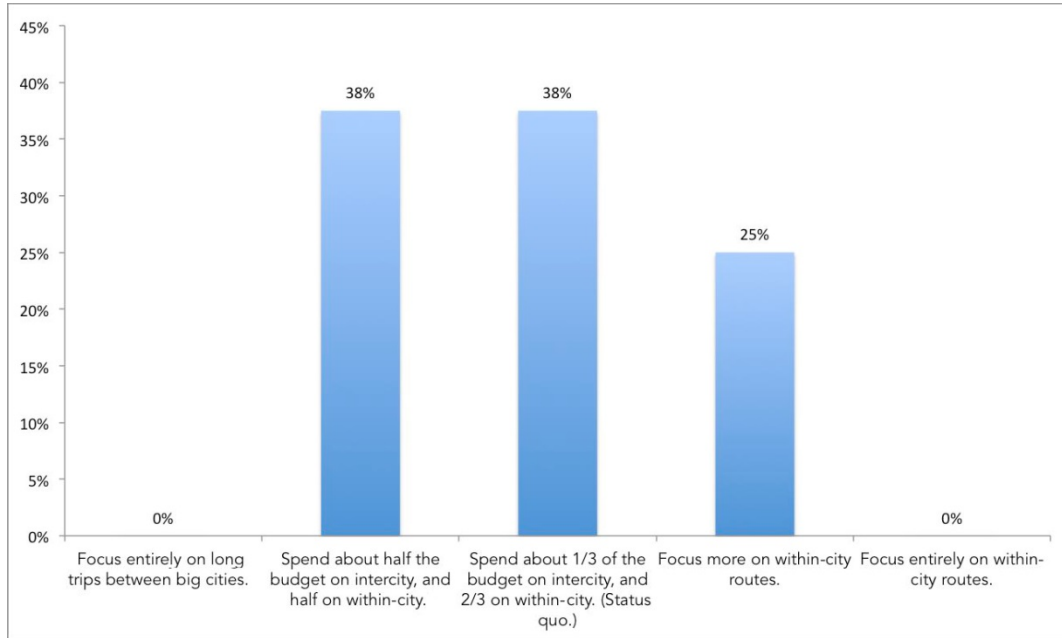


## SUMMARY OF COMMUNITY INPUT

Project Team and consultants created a variety of venues for interaction with diverse stakeholders, who were polled on some of the trade-offs addressed in the online survey. What follows is a summary of input collected from the Wilsonville TMP Community Workshop, community-wide survey, stakeholder interviews, and targeted focus groups.

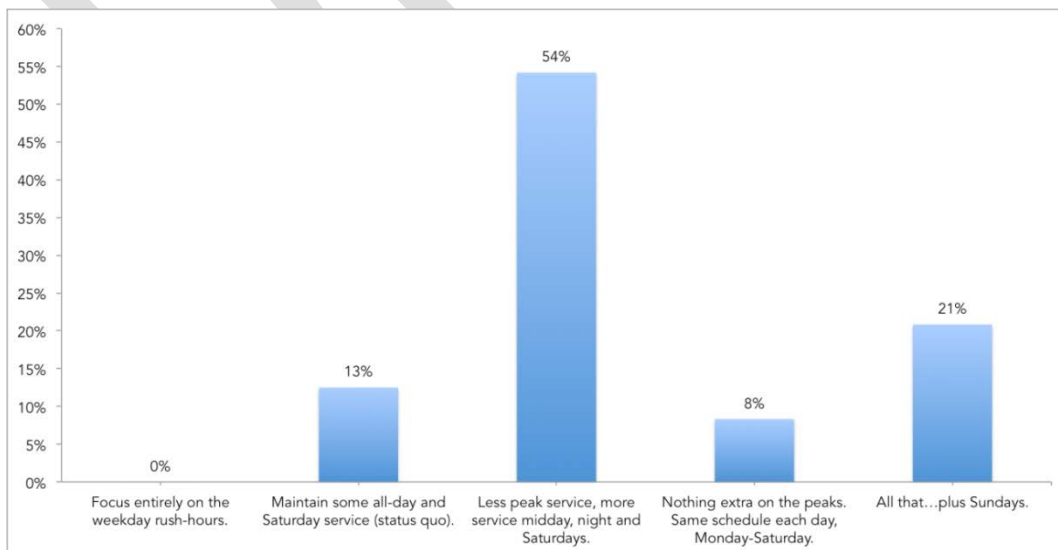
### Community Workshop Results

***Should SMART focus on inter-city services, or within-city services? (Similar to Q2 from the survey)***



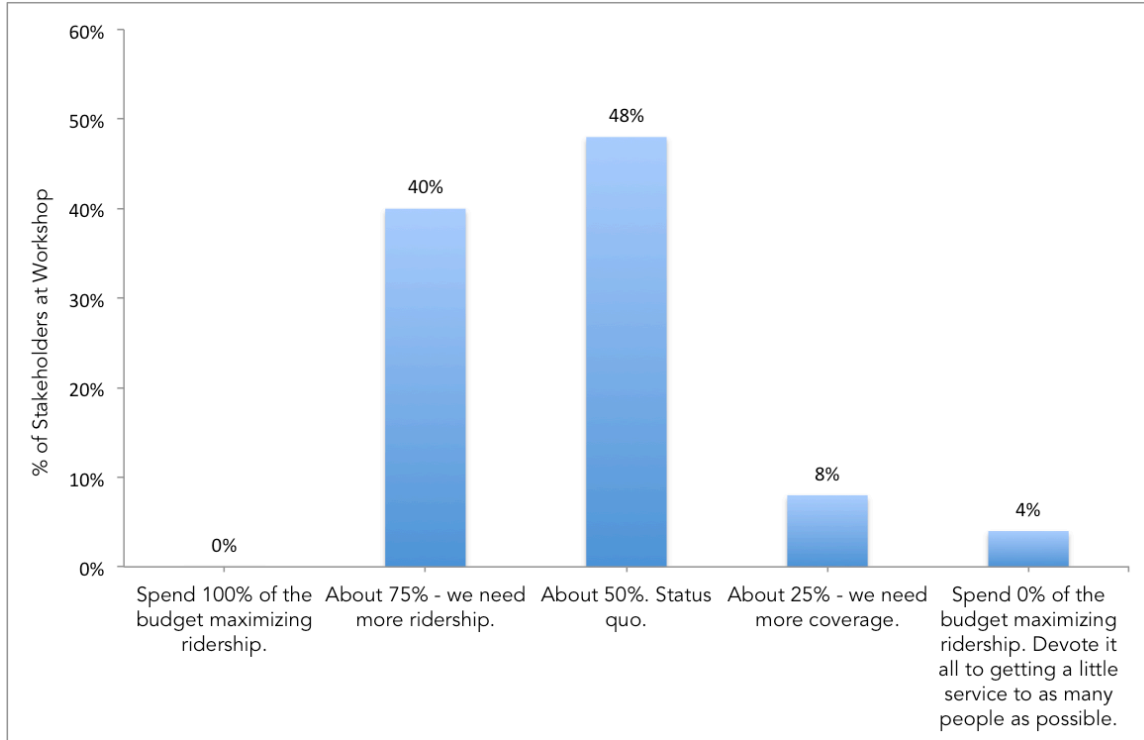
Stakeholders almost equally split on whether there should be more intercity or more local service emphasis. However, 38% were happy with the status-quo balance.

***Should SMART focus more on rush-hours or more on all-day all-week service? (Similar to Q3.)***



Stakeholders expressed fairly strong support for a move away from rush-hour service and towards flatter, all-day all-week schedules.

**How should SMART balance maximizing ridership and providing coverage?  
(In practice, this means balancing frequency with walking distance, similar to Q4.)**



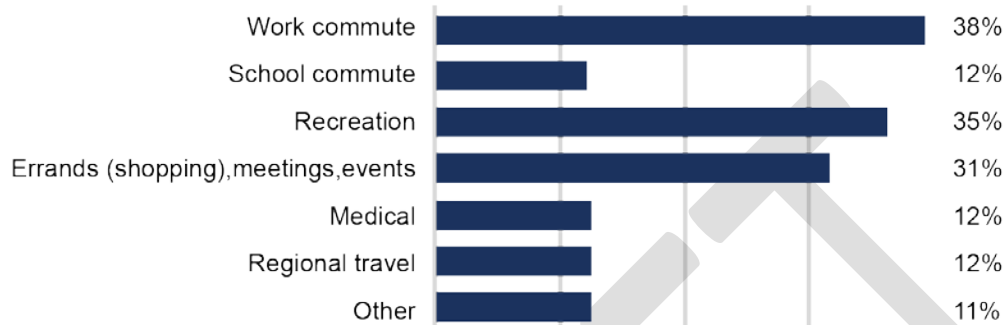
After a long conversation about what leads to high ridership and the trade-off between having few frequent Routes or many low-frequency Routes, the stakeholders were polled on how to balance these competing goals.

While nearly half of stakeholders expressed support for the current balance of maximizing ridership vs. providing coverage, 40% want to see more focus on ridership, and therefore less coverage.

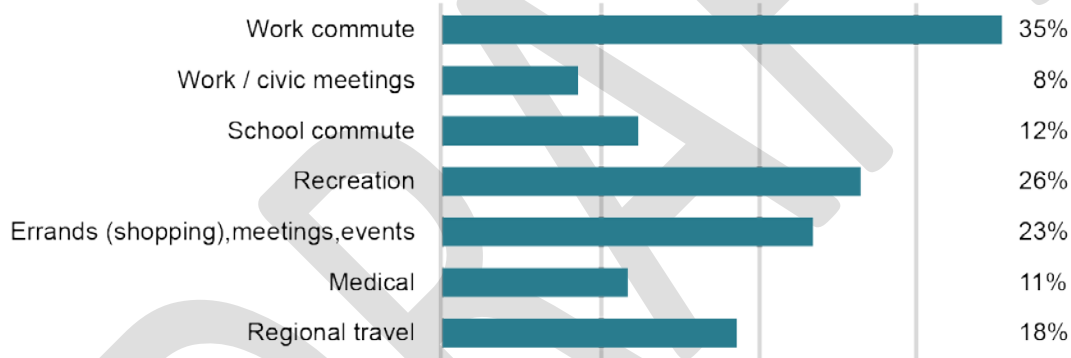
## Community Survey #1 Results

During a period of six weeks, the TMP project staff received 772 survey responses through the website and bilingual paper surveys.

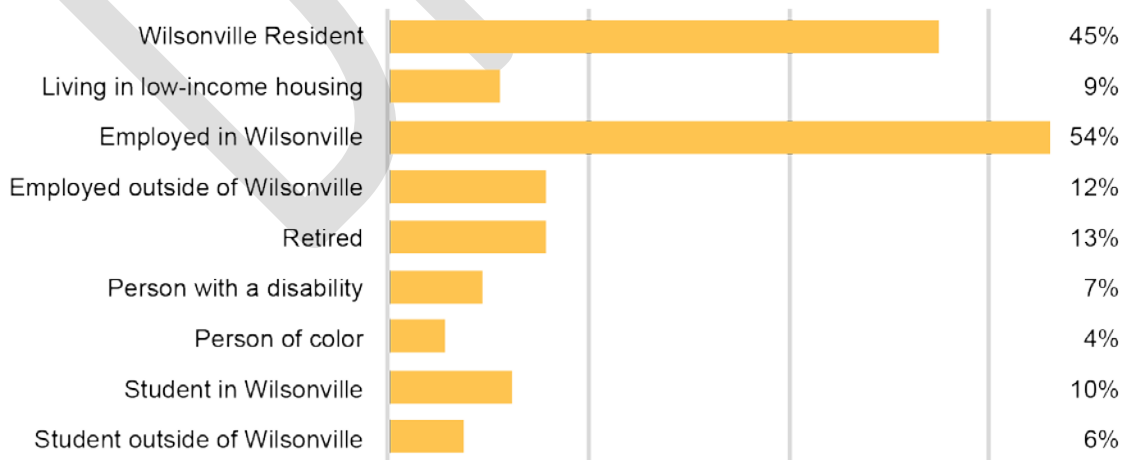
### Q1a) Which types of trips do you currently use transit for?



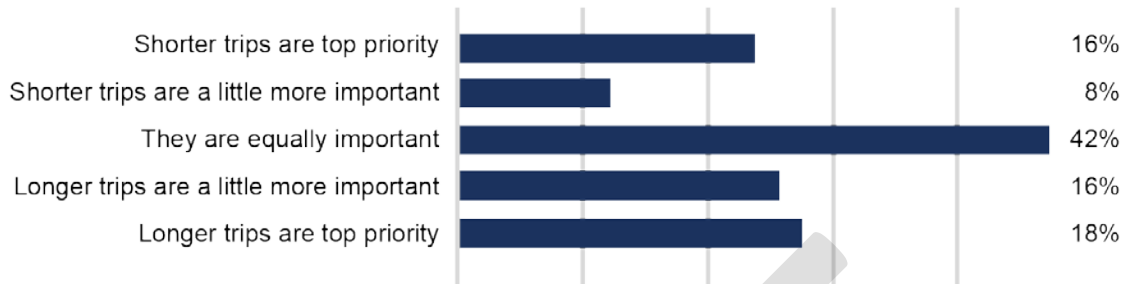
### Q1b) Which types of trips do you want to use transit for but currently cannot?



### Q1c) How would you describe yourself?



**Q2) People make both short transit trips in town and longer transit trips to other cities in the region. Which is more important to you?**



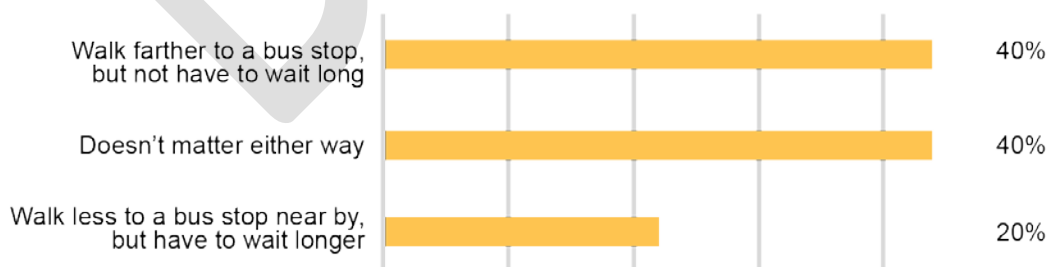
Slightly more survey respondents said that they think longer trips are more important to serve with transit than short trips.

**Q3) Some people commute to work during rush-hour for 9-5 jobs, while others work different schedules. People could use transit for other kinds of trips besides their work commute, as well as during evenings and weekends. At what times should SMART primarily target services?**



More survey respondents expressed support for a shift towards all-day and all-week service than for maintaining SMART's current focus on weekday rush hours.

**Q4) Transit can be spread out, to get a little close to everyone, or it can be concentrated into frequent Routes. Help us understand your preference.**

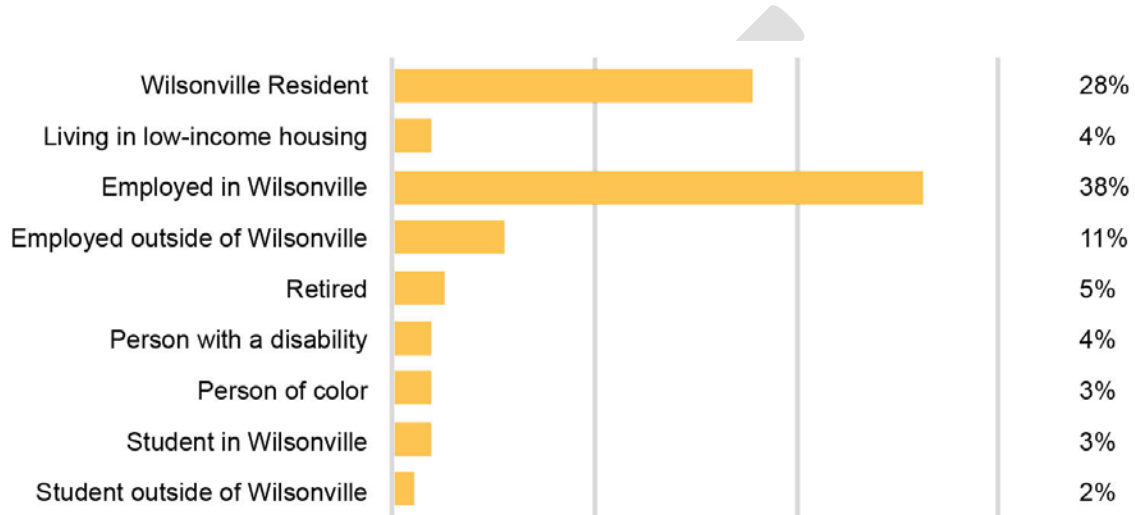


Short waits are only possible, within a fixed budget, if an agency provides less coverage and therefore more frequency. For local transit, frequency is key to high ridership. This survey question is about the *individual* experience of a high ridership transit system versus a high coverage transit system: in a high ridership system, waits are short but walks are longer.

## Community Survey #2 Results

The Service Alternatives Survey administered on line, via electronic kiosks, and at community events received 550 responses. The key question aimed to understand the community's balance between intercity focused and locally-focused service alternatives. The response data shows the majority is employed in Wilsonville and suggests a favoring towards more intercity service.

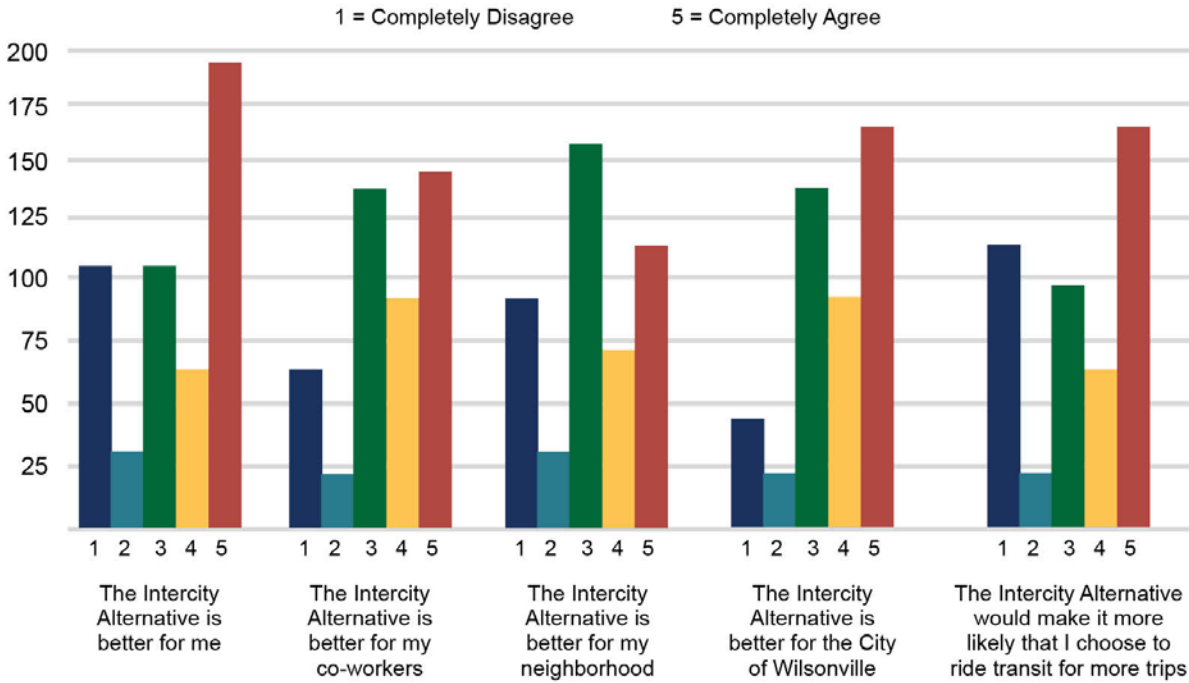
### Q1) How would you describe yourself?



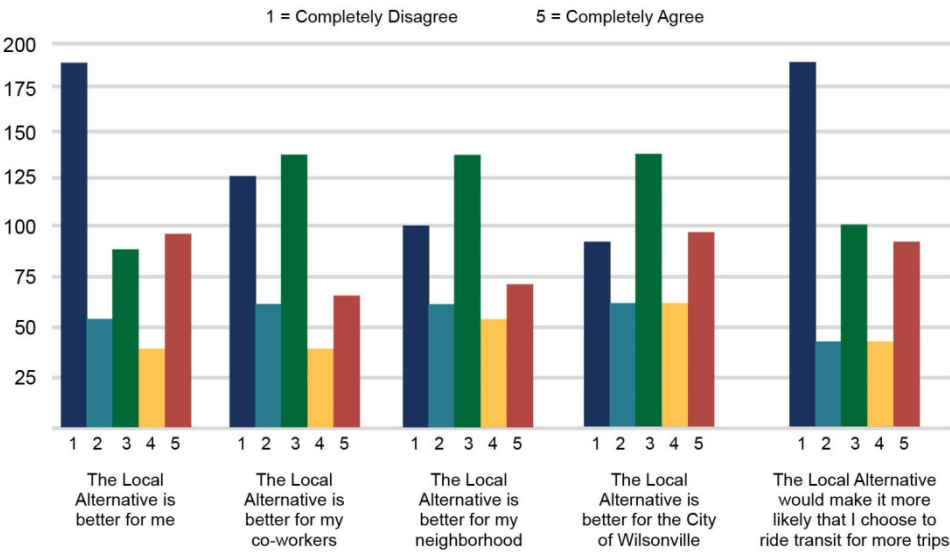
### Q2) Did you participate in the first survey?



**Q3) For the Intercity-Focused Alternative, please rank the questions below on a scale of 1-5.**

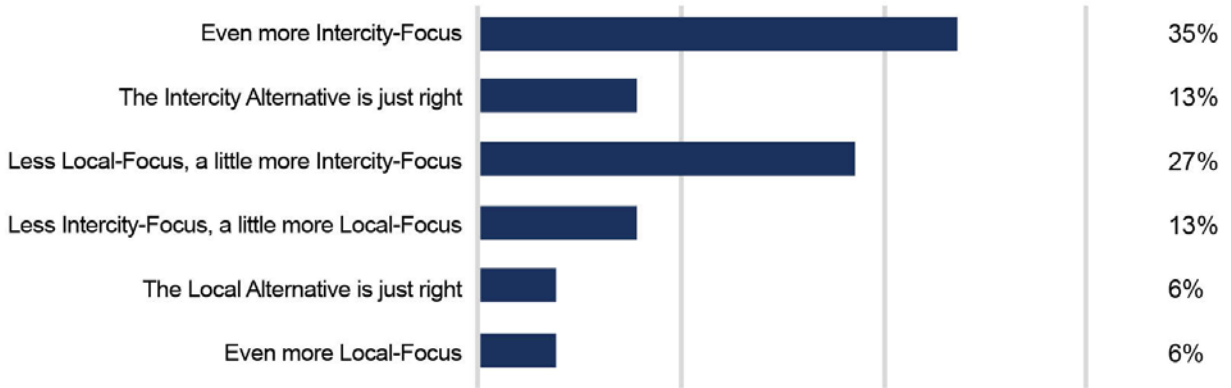


**Q5) For the Locally-Focused Alternative, please rank the questions below on a scale of 1-5.**

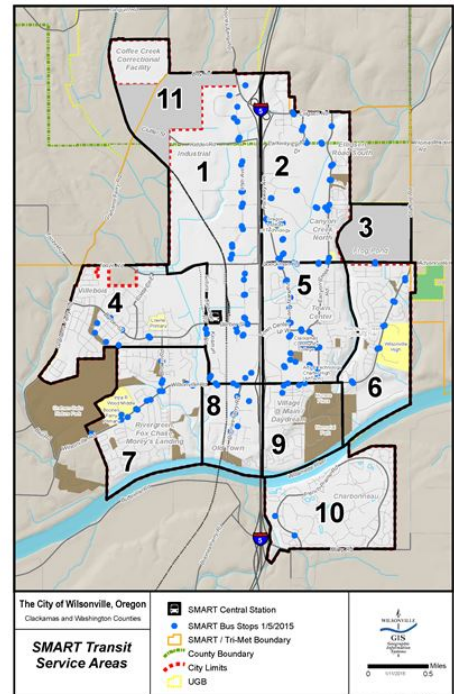
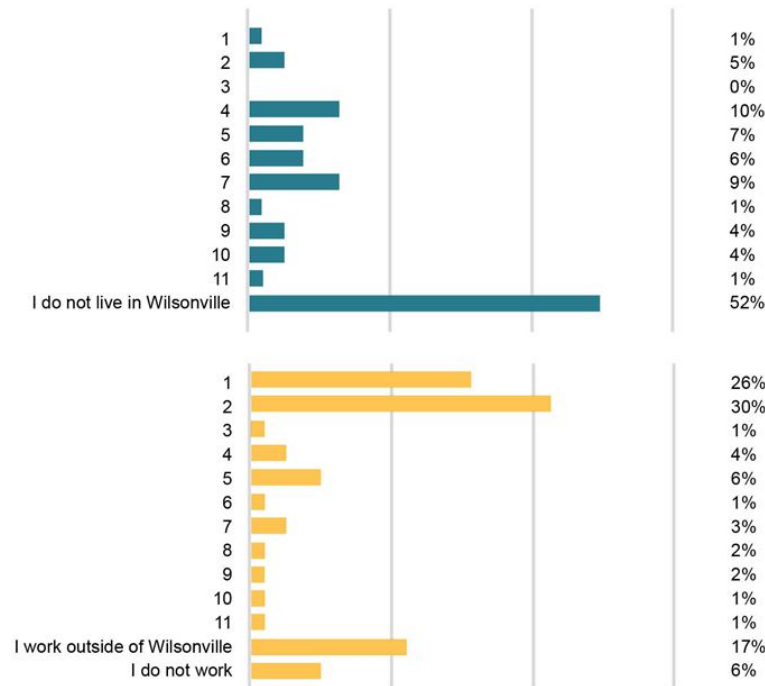




**Q7: Which of the below options comes closest to the balance between Intercity-Focus and Local-Focus that you prefer?**



**Q8: Using the service map, tell us where you live and work:**



## Stakeholder Interviews and Focused Discussion Results

### Support the economy

- Tourism could be served well with more routes on weekends to Salem and PDX
- More access from airports and other cities important to Wilsonville
- SMART'S role is to provide Wilsonville with a work force
- Important to serve workers in and out of community

### Enhance connectivity

- Proximity of stops near schools for new middle school
- Oregon City is critical location for social services
- Plan for transit in new developing areas
- Target high density neighborhood and growth in industrial areas
- Stronger connections to Portland
- More WES service
- East to west within Wilsonville take much too long

### Expand service times

- Traditional 9-5 no longer the norm for businesses with multiple shifts
- Need later service for evening shifts
- More times to address after-school needs
- Weekend service
- Quick service between transit and higher education facilities
- Priority should be given to the shift times with the highest number of workers

### Provide additional transportation options

- Promote ridesharing (vanpooling/carpooling)
- Explore additional car sharing opportunities
- Explore bike sharing

### Get the word out

- Lack of awareness that service is free within Wilsonville by English and non English speakers
- Schools, students, and families are not aware of free service
- Travel training needed
- Need to work with employment agencies to reach daily in-flux of temporary workers

### Create partnerships

- Use back-to-school information to highlight SMART services to parents and students
- Include in admission and orientation packets to higher education facilities
- Explore increases in vanpooling
- Create stronger partnerships with Tri-Met and SAMTD for more transportation options

### Explore additional funding mechanisms

- Fix the transit districts' boundaries to match location of businesses
- Businesses are dealing with minimum wage increases
- People wanting more service causes businesses concern about financing

Focus group participants named a number of transit outcomes that relate to either maximizing ridership (target high density and move large numbers of workers) or to providing coverage regardless of ridership (provide access to social services and new developing areas). In the focus group setting, we did not ask them to resolve this conflict. Insight into how people want SMART to resolve these conflicting goals may be better gleaned from the online survey and the stakeholder workshop, where respondents were asked to make a trade-off.

## Considerations for Service Alternatives Development

The input of the online survey respondents, stakeholder workshop attendees, stakeholder interviewees, and focus groups participants suggest that SMART could shift its priorities slightly, on two key trade-offs, with public support:

- **The importance of rush-hour, relative to the rest of the day.** Only 28% of online survey respondents said that service during the peaks is more important than the rest of the day and week. Similarly, of the polled stakeholders, only about 13% supported the status-quo level of emphasis on peak service; the rest wanted to see a small or major move towards flatter, all-day all-week schedules. People who participated in focus groups expressed a great deal of interest in service outside of rush hours.
- **How to balance providing frequency to maximize ridership, and providing low-frequency coverage services to more places.** A large minority of stakeholders at the workshop asked for a shift towards higher frequencies and/or higher ridership. Among survey respondents, about twice as many people prefer short waits than short walks, suggesting higher support for a more frequent network than an infrequent network that gets close to everyone.

On the matter of *local vs. intercity trips*, only a slightly larger number of survey respondents and stakeholders expressed support for longer, intercity trips than short, local trips.

### Service Alternatives Key themes:

#### Preference for more intercity service

- Connections to Oregon City remain critical
- Proposed frequencies may be challenging for higher education students
- Focus on rush hour does not apply to many of Wilsonville employer shifts

#### Transportation Options

- Explore additional options like ridesharing, car and vanpooling, bike sharing, car sharing, and ridesourcing to augment SMART services
- Involve Chamber of Commerce in this conversation

#### Considerations for local service

- Need to continue to serve low-income apartment complexes
- More service for arts and culture sites

## APPENDICES

### Outreach Overview

*Outlined are the stakeholder groups reached and by what method during Phase 1 of the Wilsonville TMP.*

#### Mobile Kiosk Placement

City Hall  
Wilsonville Public Library  
Boones Ferry Elementary School  
Clackamas Community College

#### Display Outreach

Wilsonville Community Center  
SMART Central Station  
Thriftway  
Safeway  
Oregon Technical Institute  
Town Park Center  
Wilsonville Farmers Market  
SMART Central  
Charbonneau Country Club  
Wilsonville Community Center  
Montague Park  
Wilsonville Community Sharing  
Sofia Park  
Autumn Park  
Oregon Technology  
Costco  
Creskide Apartments  
Microsoft  
Fred Meyer  
Mentor Graphics  
Pioneer Pacific College  
Vision Plastics  
Flir  
Target  
Edge Family Fitness  
Bullwinkle's Family Fun Center

#### Informational Bookmark Tear-Offs

SMART buses  
Wilsonville Public Library  
Fred Meyer  
Qdoba (stapled to all "to go" orders)  
Large and small employers

#### Paper Survey Outreach

Convergys  
Vision Plastic  
Gillespie Graphics  
Xerox  
Mentor Graphics  
Clackamas Community College  
Autumn Park (Spanish survey  
distribution)  
Valley Christian Church  
Saint Cyril Catholic Church  
SMART Central  
Shopper Shuttle

#### Community Workshop

Convergys  
Xerox  
Mentor Graphics  
Coffee Creek Correctional Facility  
City of Wilsonville Community  
Development  
City of Wilsonville Community Planning  
City of Wilsonville Planning Commission  
SMART Transit Mater Plan Task Force  
Oregon Institute of Technology  
Clackamas Community College  
Wilsonville Chamber of Commerce  
Wilsonville Public Library Foundation  
Charbonneau Home Owners  
Association  
Charbonneau Country Club  
Mayor Knapp  
Councilor Stevens

#### Issues and Opportunities Focus Groups

Business Community  
Republic Services  
Hasson Company Realtor  
Pacific Residential  
IBEW Local 48

Centrex Construction  
Columbia Bank  
Langdon Farms  
Bittner & Hans  
Crowley Landscaping Management  
Washington County Planning  
Commission

Title VI & Environmental Justice (Low  
income/Limited English Proficiency)

Lowrie Elementary School  
West Linn Wilsonville School District  
Northwest Housing Alliance  
Coordinator  
Wilsonville Public Library Spanish  
Outreach Coordinator  
SMART Bilingual Driver  
Clackamas County Social Services  
Developmental Disabilities Coordinator  
Clackamas County Social Services  
Children, Youth, & Families  
Creekside Woods

Older Adults / ADA

Clackamas County Social Services  
Volunteer Connections  
Creekside Woods Senior Housing  
SMART Paratransit  
City of Wilsonville ADA  
Wilsonville Community Center  
Wilsonville Community Sharing

**Service Alternatives Focus Groups**

Note: These groups were combined in order to  
foster understanding of competing transit  
needs.

Convergys  
Wilsonville City Councilor  
City of Wilsonville Planning Department  
Xerox Corporation  
Oregon Institute of Technology  
Langdon Farms  
Clackamas County Children and Youth  
Services  
Clackamas Community College  
Transit Master Plan Task Force  
Wilsonville Planning Commission  
Wilsonville Art and Culture Council  
Greater Wilsonville Chamber of  
Commerce  
Wilsonville Development Review Board  
Committee  
Wilsonville School District Community  
Services  
Wilsonville Library Foundation  
Wilsonville Parks and Recreation  
Wilsonville Community Sharing

**Stakeholder Interviews**

Wilsonville Youth Sports  
City of Wilsonville Councilor Lehan  
City of Wilsonville City Manager  
City of Wilsonville Public Works  
West Linn Wilsonville School District  
Pioneer Pacific College  
Microsoft  
Fry's Electronics  
Wilsonville Holiday Inn  
World of Speed  
Employment Express / Tualatin  
Chamber of Commerce  
American Family Insurance

**For a look at all public comments and meeting notes, please see the  
[www.plansmartwilsonville.com](http://www.plansmartwilsonville.com) website. The comments are categorized geographically.  
Some comments may apply to multiple categories. Comments are verbatim. No spelling or  
punctuation errors are corrected.**

## TRANSIT MASTER PLAN FINDINGS REPORT (EXHIBIT B)

### INTRODUCTION

In support of the adoption of the proposed Wilsonville Transit Master Plan 2017, the following sections present findings of compliance with the Statewide Planning Goals, the Transportation Planning Rule, Metro Regional Transportation Functional Plan and the City of Wilsonville Comprehensive Plan.

This Findings Report provides evidence supporting the City of Wilsonville's adoption of the Transit Master Plan 2017 – Case File LP17-0003 (the proposal). The proposal includes the following:

- a. Amendments to the Wilsonville Comprehensive Plan Text;
- b. Adoption of the 2017 Transit Master Plan as part of the Comprehensive Plan;

The Transit Master Plan is described in the staff report and included in the record as Exhibit A.

### COMPLIANCE WITH STATEWIDE PLANNING GOALS

ORS 197.175(2) (a) requires that cities and counties amend and revise comprehensive plans in compliance with the goals approved by the Land Conservation and Development Commission. The following findings address the proposal's compliance with the applicable statewide planning goals.

The City finds that the following Statewide Planning Goals are not applicable because the proposal will not impact the existing policy and programs relevant to the referenced goals:

- Goal 3 – Agricultural Lands;
- Goal 4 – Forest Lands;
- Goal 5 – Open Spaces, Scenic and Historic Areas and Natural Resources;
- Goal 7 – Areas Subject to Natural Disasters and Hazards;
- Goal 8 – Recreation Needs;
- Goal 10 – Housing;
- Goal 15 – Willamette River Greenway;
- Goal 16 – Estuarine Resources
- Goal 17 – Coastal Shorelands
- Goal 18 – Beaches and Dunes
- Goal 19 – Ocean Resources

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#### GOAL 1, CITIZEN INVOLVEMENT

***To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.***

**FINDINGS:** The Transit Master Plan (TMP) Public involvement process began in spring 2015 with a nine-member citizen Transit Master Plan Task Force that included residents, transit riders, and community organizations. Recruitment of the Task Force members was announced at City Council meetings, with press releases following. Participants were self-selected as anyone who chose to participate was welcomed to the task force. Citizen members of the TMP Task Force proposed a mission statement and adopted eight goals for the TMP planning process which are listed in the draft transit master plan and helped to guide the citizen involvement process.

Many opportunities were presented for citizens to be involved in developing the TMP with three key outreach phases: (1) Issues & Opportunities Identification in winter 2016, (2) Service Alternatives Development in summer 2016, and (3) Service Alternatives Review in fall 2017.

SMART directed a process to involve a diverse and broad spectrum of existing and potential system users, Wilsonville residents, and businesses that support transit through payroll taxes. To ensure effective two-way communication in this process, SMART communicated with the public through a variety of methods including:

- Two community surveys that generated over 1,300 responses (paper, mobile kiosks, online formats)
- Participated in over 50 display outreach community events and speaking engagements to employers and residents
- E-mail updates to an Interested Parties List of 1,200
- Utilize printed and social media such as articles in the Boones Ferry Messenger, Wilsonville Spokesman, TMP project website and Facebook/Twitter posts.
- Distributed 10,000 informational hand out bookmarks at events, work sites, and transit center
- Public notices of hearings and opportunity for comment on draft plan
- Seven issue-based interviews that focused on bicycle and pedestrian safety, elders, people with disabilities, Title VI populations, youth, and businesses
- One large community workshop
- Conducted five focus groups to discuss transportation options and funding realities
- Technical information regarding routes and finances were displayed in a user-friendly format in surveys, presentations, and all outreach efforts to allow citizens to provide educated feedback.

In January 2017 the City Council held a work session where SMART staff presented an update of the TMP draft that the citizen task force reviewed and approved to move forward in the adoption process.

From January through May 2017 the final draft remained open for public comment which was advertised through the communication types listed above.

The feedback mechanisms in place to assure citizens will receive a response from policy-makers are through announcements made on various media outlets. City Council will communicate decisions through verbal, recorded, and online information.

To assure proper financial support for an effective outreach approach, SMART contracted an outreach consultant who created a timeline and available resources to allow the public to make insightful comments throughout the plan's development.

Based on the forgoing, the City Council finds that the proposal satisfies Goal 1 with respect to developing a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.

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## GOAL 2, LAND USE PLANNING

***To establish a land use planning process and policy framework as a basis for all decisions and actions related to use of land and to assure an adequate factual base for such decisions and actions.***

**FINDINGS:** The development of the Transit Master Plan has followed the City's established land use planning process, and included 22 months of public meetings, outreach, committee meetings, open houses, surveys, web site information, direct mailings and opportunities for public comment.

The proposal satisfies Goal 2 because it is supported by an *adequate factual base* and its development was coordinated with all affected governmental units.

### *Adequate Factual Base*

The City has established a record that includes technical memoranda, studies, and analyses supporting each element of the Transit Master Plan. The key documents that were relied upon and form the adequate factual base for our FINDINGS are listed below:

1. 2017 Transit Master Plan
2. 2017 Transit Master Plan –Appendix A: Glossary
3. 2017 Transit Master Plan –Appendix B: Route Priorities
4. 2017 Transit Master Plan –Appendix C: Title VI Program
5. 2017 Transit Master Plan –Appendix D: Planning Framework
6. 2017 Transit Master Plan –Appendix E: Neighborhood Characteristics
7. 2017 Transit Master Plan –Appendix F: TMP Public Outreach Summary
8. 2017 Transit Master Plan Public Involvement Plan

### *Coordination with the Plans of Affected Governmental Units*

During the Master Planning processes, the following affected governmental units participated or had the opportunity to participate via notices and project information provided to them:

- Oregon Department of Transportation (ODOT)
- Metro
- TriMet
- Clackamas County



- Washington County
- City of Canby
- Canby Area Transit
- City of Sherwood
- City of Tualatin
- Salem Keizer Area Public Transit
- Wilsonville Area Chamber of Commerce

Based on the forgoing, the City Council finds that the proposal satisfies Goal 2 with respect to having an adequate factual base and being coordinated with all affected governmental units.

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#### GOAL 6, AIR, WATER, AND LAND RESOURCES QUALITY

***To maintain and improve the quality of the air, water and land resources of the state.***

**FINDINGS:** The proposal satisfies Goal 6 because it will maintain and improve the quality of the air, water, and land resources of the state.

The proposal maintains and improves **air quality** by:

- Providing efficient and reliable transit options to commuters and residents to reduce the need to travel in private motor vehicles, greenhouse gas emissions, and consumption of fossil fuel.
- Working with businesses and employers to market travel demand management and commuting alternatives that encourage workers to reduce single-occupancy vehicle (SOV) trips.
- Improving bicycle and pedestrian connections to transit stops and encouraging bicycle/pedestrian improvements that facilitate transit trips.

The proposal maintains and improves **water quality** by:

- Providing transportation options that reduce vehicle-miles traveled, consumption of fossil fuel, and demand for more roadway capacity.
- Reducing the demand for parking and the amount of land area that will become impervious surfaces.

The proposal maintains and improves **land resources** by:

- Encouraging more compact development and efficient use of land near transit stops.
- Reducing demand for more roadway space and parking, freeing up more land for housing, open space, and employment.

Based on the foregoing, the City finds that the proposal satisfies Goal 6.

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#### GOAL 9, ECONOMIC DEVELOPMENT

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***To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens.***

**FINDINGS:** The proposal satisfies Goal 9 because increasing transportation options and improving multi-modal access to employment, housing, and other opportunities help support economic development.

As the City continues to grow in commercial and industrial employment, public transit provides workers access to jobs and helps alleviate the pressure to expand roadway capacity. Considering the well-documented trend of millennial workers' preference for transit, walking, biking and lower car-ownership rate, transit service is a key factor in future economic growth.

Based on the foregoing, the City finds that the proposal satisfies Goal 9.

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## GOAL 12, TRANSPORTATION

***To provide and encourage a safe, convenient and economic transportation system.***

**FINDINGS:** The proposal satisfies Goal 12 because it has been designed to:

- Provide a safe and secure environment for transit riders, community and employees through maintenance, training, design, enforcement and the allocation of resources.
- provide mobility to people of all ages, incomes, and physical abilities with fare-free in-town fixed route and dial-a-ride services
- Offer convenient and easy way to travel within Wilsonville and other communities in the Portland region.
- Encourage residents and commuters to use transit, walking, and biking and reduce reliance on private motor vehicles.

Based on the forgoing, the City finds that the proposal satisfies Goal 12.

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## GOAL 13, ENERGY CONSERVATION

***To conserve energy.***

**FINDINGS:** The proposal satisfies Goal 13 because it has been designed to

- Encourage travel by transit, walking, and biking and reduce vehicle-miles travelled
- Enhance connectivity for pedestrians, bicyclists, and transit users.
- support more compact and energy efficient development

Based on the forgoing FINDINGS, the City finds that the proposal satisfies Goal 13.

## COMPLIANCE WITH THE TRANSPORTATION PLANNING RULE (OAR 660-012)

The State's Transportation Planning Rule (TPR) is designed to implement Statewide Planning Goal 12. The TPR promotes the development of safe, convenient, and economic transportation systems that are designed to reduce reliance on the automobile so that the air pollution, traffic, and other livability problems faced by many urban areas can be avoided.

The TPR requires that metropolitan planning organizations (MPOs, including Metro) prepare and adopt regional transportation plans consistent with the State Transportation Plan (STP). Local jurisdictions are also required to adopt Transportation Systems Plans (TSPs) consistent with the Regional Transportation Plan (RTP) for their jurisdictions. The local TSP establishes a system of transportation facilities and services, such as public transit, adequate to meet identified local transportation needs consistent with the adopted elements of the Oregon Transportation Plan (OTP). The local TSP should also be consistent with the TSPs of both Washington and Clackamas Counties.

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### 660-012-0015 PREPARATION AND COORDINATION OF TRANSPORTATION SYSTEM PLANS

*(6) Mass transit, transportation, airport and port districts shall participate in the development of TSPs for those transportation facilities and services they provide. These districts shall prepare and adopt plans for transportation facilities and services they provide. Such plans shall be consistent with and adequate to carry out relevant portions of applicable regional and local TSPs. Cooperative agreements executed under ORS 197.185(2) shall include the requirement that mass transit, transportation, airport and port districts adopt a plan consistent with the requirements of this section.*

**FINDINGS:** The proposal is an update to the current Transit Master Plan adopted in 2008. As the City of Wilsonville's transit service provider, SMART's has prepared a new transit master plan that complies with state, regional and local Transportation System Plan and Comprehensive Plan. This criterion is satisfied.

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### 660-012-0020 ELEMENTS OF TRANSPORTATION SYSTEM PLANS

*(1) A TSP shall establish a coordinated network of transportation facilities adequate to serve state, regional and local transportation needs.*

*(2) The TSP shall include the following elements:*

*(c) A public transportation plan which:*

*(A) Describes public transportation services for the transportation disadvantaged and identifies service inadequacies;*

*(B) Describes intercity bus and passenger rail service and identifies the location of terminals;*

*(C) For areas within an urban growth boundary which have public transit service, identifies existing and planned transit trunk routes, exclusive transit ways, terminals and major transfer stations, major transit stops, and park-and-ride stations. Designation of stop or station locations may allow for minor adjustments in the location of stops to provide for efficient transit or traffic operation or to provide convenient pedestrian access to adjacent or nearby uses.*

**FINDINGS:** The proposal addresses the needs of the transportation disadvantaged and proposes numerous implementation measures designed to improve access to transit. The Plan also discusses intercity bus and rail and proposes additional intercity service. Major transit routes, stops, and Park & ride locations are identified in the Plan. This criterion is satisfied.

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#### 660-012-0035 EVALUATION AND SELECTION OF TRANSPORTATION SYSTEM ALTERNATIVES

*(2) Local governments in MPO areas with a population larger than 1,000,000 shall, and other governments may also, evaluate alternative land use designations, densities, and design standards to meet local and regional transportation needs. Local governments preparing such a strategy shall consider:*

*(e) The transportation system shall avoid principal reliance on any one mode of transportation and shall reduce principal reliance on the automobile. In MPO areas this shall be accomplished by selecting transportation alternatives which meet the requirements in section (4) of this rule.*

*(4) In MPO areas, regional and local TSPs shall be designed to achieve the objectives listed in (a)-(c) below for reducing automobile vehicle miles traveled per capita for the MPO area. The VMT target and alternative standards are intended as means of measuring progress of metropolitan areas towards developing and implementing transportation systems and land use plans that reduce reliance on the automobile. It is anticipated that metropolitan areas will accomplish reduced reliance by changing land use patterns and transportation systems so that walking, cycling, and use of transit are highly convenient and so that, on balance, people need to and are likely to drive less than they do today:*

*(b) In MPO areas of more than 1 million population, 10% reduction within 20 years of adoption of a plan as required by OAR 660-012-0055(1); and (c) Through subsequent planning efforts, an additional 5 percent reduction within 30 years of adoption of a plan as required by OAR 660-012-0055(1).*

**FINDINGS:** The Transportation Planning Rule (TPR) emphasizes the strong relationship between land use and transportation planning as a means to reduce automobile trips. By providing measures to increase transit service and accessibility and promote other transportation alternatives, the Plan provides strong support for the TPR. Land use patterns and jobs-housing balance also play a key role in meeting the TPR goals and are not addressed as part of the Transit Master Plan. It should be noted that Metro's Regional Transportation Plan uses an alternate measurement standard, as allowed by the TPR, to achieve the target VMT reductions. This criterion is satisfied.

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## 660-012-0040 TRANSPORTATION FINANCING PROGRAM

(3) The determination of rough cost estimates is intended to provide an estimate of the fiscal requirements to support the land uses in the acknowledged comprehensive plan and allow jurisdictions to assess the adequacy of existing and possible alternative funding mechanisms. In addition to including rough cost estimates for each transportation facility and major improvement, the transportation financing plan shall include a discussion of the facility provider's existing funding mechanisms and the ability of these and possible new mechanisms to fund the development of each transportation facility and major improvement. These funding mechanisms may also be described in terms of general guidelines or local policies.

**FINDINGS:** The proposal's *Funding Resources and Financial Realities* chapter provides an overview of existing funding mechanisms and the future outlook for specific funding sources as well as current and projected revenues and expenditures. Costs for proposed service expansions are also addressed. This criterion is satisfied.

## COMPLIANCE WITH METRO REGIONAL TRANSPORTATION FUNCTIONAL PLAN

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### INTRODUCTION

The Regional Transportation Functional Plan (RTFP) contains policies and guidelines to help local jurisdictions implement the policies in the Regional Transportation Plan (RTP) and its modal plans, include those for active transportation, freight movement and high capacity transit.

As established in the RTFP, demonstrating compliance with the RTFP constitutes compliance with the Regional Transportation Plan.

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### COMPLIANCE WITH METRO REGIONAL TRANSPORTATION FUNCTIONAL PLAN 3.08.010 – 3.08.640

#### **TITLE 1 TRANSPORTATION SYSTEM PLAN DESIGN, SECTION 3.08.120 TRANSIT SYSTEM DESIGN**

B. City and county TSPs Include a transit plan, and implementing land use regulations, with the following elements to leverage the region's investment in transit and improve access to the transit system:

1. A transit system map consistent with the transit functional classifications shown in Figure 2.15 of the RTP that shows the locations of major transit stops, transit centers, high capacity transit stations, regional bicycle transit facilities, inter-city bus and rail passenger terminals designated in the RTP, transit-priority treatments such as signals, regional bicycle transit facilities, park-and-ride facilities, and bicycle and pedestrian routes, consistent with sections 3.08.130 and 3.08.140, between essential destinations and transit stops.

2. Include Site design standards for new retail, office, multi-family and institutional buildings located near or at major transit stops shown in Figure 2.15 in the RTP:
  - a. Provide reasonably direct pedestrian connections between transit stops and building entrances and between building entrances and streets adjoining transit stops;
  - b. Provide safe, direct and logical pedestrian crossings at all transit stops where practicable.
  - c. At major transit stops, require the following:
    - Locate buildings within 20 feet of the transit stop, a transit street or an intersection street, or a pedestrian plaza at the stop or a street intersections;
    - Transit passenger landing pads accessible to disabled persons to transit agency standards;
    - An easement or dedication for a passenger shelter and an underground utility connection to a major transit stop if requested by the public transit provider;
    - Lighting to transit agency standards at the major transit stop;
    - Intersection and mid-block traffic management improvements as needed and practicable to enable marked crossings at major transit stops.

Providers of public transit service shall consider and document the needs of youth, seniors, people with disabilities and environmental justice populations, including minorities and low-income families, when planning levels of service, transit facilities and hours of operation.

**FINDINGS:** The plan’s Future Bus Routes and Services chapter incorporates development requirements related to transit facilities. Implementation Measure 3.5 requires location of the building frontage and entrance is within 20 feet of the transit stop or transit street and; direct pedestrian connection is provided between transit stops and building entrances. Implementation Measure 3.6 requires transit amenities based on the number of peak hour trips expected to be generated by the proposed development.

The proposal includes existing and proposed transit system maps to leverage state, regional, and local investment in transit and improve access to the transit system. The Transit Master Plan identifies policies, design standards and criteria to enhance pedestrian and bicycle connections to transit stops and transit streets. Moreover, the needs of youth, seniors, people with disabilities and environmental justice populations, including minorities and low-income families must be considered when planning for transit level of service, facilities, and operations. This criterion is satisfied.

## ***TITLE 2: DEVELOPMENT AND UPDATE OF TRANSPORTATION SYSTEM PLAN***

### **SECTION 3.08.220 TRNASPORTATION SOLUTIONS**

A. Each city and county shall consider the following strategies, in the order listed, to meet the transportation needs determined pursuant to section 3.08.210 and performance targets and standards pursuant to section 3.08.230. The city or county shall explain its choice of one or more of the strategies and why other strategies were not chosen:

## 2. Transit, bicycle and pedestrian system improvements

### SECTION 3.08.230 Performance Targets and Standards:

A. Each city and county shall demonstrate that solutions adopted pursuant to section 3.08.220 will achieve progress toward the targets and standards in Tables 3.08-1, and 3.08-2 and measures in subsection D, or toward alternative targets and standards adopted by the city or county pursuant to subsections B and, C. The city or county shall include the regional targets and standards or its alternatives in its TSP.

**FINDINGS:** The Transit Master Plan proposes recommended transit improvements and service priorities to meet the transportation needs of the City of Wilsonville in the Future Bus Routes and services chapter. The chapter of Implementation Measures offers specific performance measures, compliant with the RTP, for various goals such as accessibility to transit, pedestrian and bicycle connections, service expansion, and improving air quality.

## COMPLIANCE WITH WILSONVILLE COMPREHENSIVE PLAN AMENDMENT STANDARDS

### INTRODUCTION

The Wilsonville Comprehensive Plan establishes how Plan amendments may be initiated and reviewed by the City. The guiding text is in the Introduction section, pages Intro 7-8. The standards for amendments are listed below in bold, italic type, followed by FINDINGS.

### PLAN POLICY REVIEW

#### **Standards for approval of Plan Amendments**

In order to grant a Plan amendment, the City Council shall, after considering the recommendation of the Development Review Board (quasi-judicial) or Planning Commission (legislative), find that:

- a. **The proposed amendment is in conformance with those portions of the Plan that are not being considered for amendment.**

### APPLICABLE POLICIES AND IMPLEMENTATION MEASURES

#### **Citizen involvement**

Policy 1.1.1 The City of Wilsonville shall provide opportunities for a wide range of public involvement in City planning programs and processes.

Policy 1.2.1 The City of Wilsonville shall provide user-friendly information to assist the public in participating in City planning programs and processes.

**FINDINGS:** In addition to Statewide Planning Goal 1, the TMP citizen involvement is guided by Wilsonville Comprehensive Plan Goal 1.1, “To encourage and provide means for interested parties to be involved in land use planning processes, on individual cases and City-wide programs and policies” and Goal 1.2 which states, “For Wilsonville to have an interested, informed, and involved citizenry”.

For the Transit Master Plan there was a 12 month planning process with continuing citizen involvement. Please see Statewide Planning Goal 1 findings for more detailed public involvement process description.

The Transit Master Plan process implemented Wilsonville Comprehensive Plan policies by

- Allowing for public comment of draft TMP also receive public comment during and before the development of the draft plan.
- Meetings (hearings, work sessions, etc.) regarding the TMP at Planning Commission and City Council are open to the public in accessible space and hours with public notices of meeting beforehand via press releases, media outlets, e-notifications
- TMP staff presented updates to the Planning Commission, City Council in work sessions
- Extensive outreach to residents, employers/employees, transit riders.
- Maintain and relay information through an interested parties list.
- Encouraged citizen involvement by going out to transit center, receiving feedback of draft TMP.

### **Urban Growth Management**

Policy 2.1.1. The City of Wilsonville shall support the development of all land within the City, other than designated open space lands, consistent with the land use designations of the Comprehensive Plan.

Policy 2.2.1. The City of Wilsonville shall plan for the eventual urbanization of land within the local planning area, beginning with land within the Urban Growth Boundary.

**FINDINGS:** The Transit Master Plan will set policies and identify improvements to provide efficient transportation access supporting urbanization and both existing and future development in the City

### **Public Facilities and Services**

Policy 3.1.1 The City of Wilsonville shall provide public facilities to enhance the health, safety, educational, and recreational aspects of urban living.

**FINDINGS:** The Plan proposes to provide high-quality transit service to meet the growing mobility needs of the community. The planned transit services and facilities will greatly enhance the health, safety, and recreational aspects of urban living by transporting residents and workers to their destinations.

### **Transportation**

Policy 3.2.1 To provide for safe and efficient vehicular, transit, pedestrian and bicycle access and circulation.



Policy 3.2.2 To provide for a mix of planned transportation facilities and services that are sufficient to ensure economical, sustainable and environmentally sound mobility and accessibility for all residents and employees in the city.

Policy 3.3.1 The City shall provide facilities that allow people to reduce reliance on single occupant automobile use, particularly during peak periods.

Policy 3.3.2 The City shall work to improve accessibility for all citizens to all modes of transportation.

Policy 3.5.1 Develop and maintain a transportation system that balances land use and transportation needs in a manner that enhances the livability and economic vitality of the city.

**FINDINGS:** The proposal sets new transit service priorities, recommended bus routes, and transportation options. These elements, if implemented, will improve the efficiency of transit service and safety of transit users, pedestrians, and bicyclists.

Considering growing demand and limited funding in the future, the Transit Master Plan offers recommendations and policies to cost-effectively improve accessibility for all citizens, support employment and economic development, encourage people to reduce automobile travel.

### **Land Use and Development**

Policy 4.1.4 The City of Wilsonville shall provide opportunities for a wide range of housing types, sizes, and densities at prices and rent levels to accommodate people who are employed in Wilsonville.

Policy 4.1.6 Require the development of property designated “Residential-Village” on the Comprehensive Plan Map to create livable, sustainable urban areas which provide a strong sense of place through integrated community design, while also making efficient use of land and urban services.

**FINDINGS:** The Transit Master Plan will continue to guide transit operations and infrastructure, which are critical to foster more density and compact housing. Transit also reduces the demand for parking, which may lower development costs and increase housing supply, making housing more affordable.

Transit is also much less costly than owning and operating private motor vehicles. Therefore, giving residents the option to live near transit may reduce the overall cost of living and increase overall affordability in the city.

The proposal also supports the City’s pedestrian-, bicycle-, and transit-friendly design principles in the Residential Village zone.

---

## **OTHER STANDARDS RELATED TO COMPREHENSIVE PLAN COMPLIANCE**

**b. The granting of the amendment is in the public interest.**

**FINDINGS:** The adoption of the Comprehensive Plan amendments for the Transit Master Plan is in the public interest because the proposal will:

1. Provide service to new developments in Wilsonville;
2. Respond to public comments requesting more focus on inter-city transit service;
3. Provide increased opportunities for residents, employees, and visitors to connect to jobs, shopping, parks, and community activities;
4. Facilitate coordination of transportation options (vanpool, walking, biking) with bus transit
5. Facilitate regional and inter-city connectivity;
6. Increase service frequency to improve convenience and coordination between routes; and
7. Extend service times over longer periods to accommodate travel needs.

**c. The public interest is best served by granting the amendment at this time.**

**FINDINGS:**

Transit provides mobility to people who do not have access to automobiles, including youth, senior citizens, people with disabilities, and low-income residents. Improving access to transit not only creates a greater sense of freedom and mobility for seniors, disabled passengers, and those who cannot drive, but also offers more choices and an alternative to sitting in traffic to those who can drive and own a car.

Transit reduces traffic congestion and air pollution and helps protect natural habitat by reducing the need for fossil fuel and sprawl. Transit transports more people in fewer vehicles, reducing the demands on the transportation system and making more efficient use of the system.

Public attitude surveys done in recent years have shown great support for SMART, even from people who do not ride the bus. Additional efforts should still be made to create even more of a sense of ownership within the community. The proposal will guide such efforts.

These facts support a conclusion that the public interest is well-served by adoption of the proposed amendments at this time.

**d. The following factors have been adequately addressed in the proposed amendment:**

- *The suitability of the various areas for particular land uses and improvements;*
- *The land uses and improvements in the area;*
- *Trends in land improvement;*
- *Density of development;*
- *Property values;*
- *The needs of economic enterprises in the future development of the area;*
- *Transportation access;*
- *Natural resources; and*

- *The public need for healthful, safe and aesthetic surroundings and conditions.*

**FINDINGS:** The Transit Master Plan includes extensive inventories and analysis of existing conditions, opportunities and constraints of Wilsonville’s transit network. While transit does not directly impact existing and future land use suitability and trends, the proposal supports the city’s land use policies by considering the type of future transit system and transportation options required to meet Wilsonville’s mobility needs as a growing community. For example, frequent transit services enable more compact housing or mixed-use development and reduce the demand for parking that take up valuable land.

The proposal also supports future economic development with recommendations for a reliable and convenient transit system and commute options that facilitate intra- and inter-city access to jobs. Future economic growth will depend partly on the constraints and demand on Wilsonville’s transportation system. The Transit Master Plan will promote more efficient use of existing road capacity allowing existing transportation infrastructure to accommodate more employment and economic activity.

The proposal’s recommendations for service changes and better walking, biking, and transit connections also supports Wilsonville’s natural resources and the creation of healthful, safe, and aesthetic conditions.

This criterion is met.

- a. **Proposed changes or amendments to the Comprehensive Plan do not result in conflicts with applicable Metro requirements.**

**FINDINGS:** Please see FINDINGS regarding compliance with the Regional Transportation Functional Plan

# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

March 2015

## New 'Heart of the City' Community Resource Center Opens Featuring Food Bank, Counseling and More



WILSONVILLE SPOKESMAN: JOSH KULLA

Community leaders gather on Jan. 29 while City Councilor Scott Starr (left) and Rep. John Davis hold the ribbon as Lead Pastor Mike Tatlock (holding scissors) celebrate the opening of Grace Chapel's new 'Heart of the City' community resource center. The newly-remodeled building on Boberg Road is the new home for Wilsonville Community Sharing, Young Life and Heart of the City Counseling. To learn more about the social service center see article on pg. 4.

## Program with the Help of Volunteers Delivers Meals and Community Connections for Homebound Seniors

During the "March for Meals" national campaign month, the City's Community Center Nutrition Program seeks to raise public awareness of the meal program in general and in particular the Home Delivered Meals service. Over the past year, the City has prepared nearly 12,345 meals at the Community Center, including serving 7,942 meals at the center and delivering 4,403 meals to local homebound seniors and individuals with disabilities. In addition to bringing nutritious meals to those in the community who have difficulty with mobility, the deliveries also serve as a quick safety check for those who are homebound.

The Home Delivered Meals service relies on volunteers like Wilsonville resident Marsha Herron, who recently began distributing meals three times a month. When asked "What do you get out of it?" Herron explains, "I like to stay busy and this is a good opportunity for me to get out in the community. I also receive a lot of gratitude from the people whom I deliver meals to and that is very rewarding."

Recipients of the meals are homebound seniors or disabled persons who are injured, ill, disabled or are otherwise isolated.

One such beneficiary of the program, Joyce Hensley, is recovering from medical issues and explains, "This is a real blessing for me at this time, but soon I hope to be healthy enough to have meals at the community center."

Evie Proctor, the City's Nutrition Coordinator, explains, "This program delivers a healthy meal and

personal contact as much as five times a week to some of our most vulnerable community members— both the meal and the contact improve their lives."



City volunteer Marsha Herron (right) delivering a sack lunch and soup to Joyce Hensley.

At the Community Center, the City serves lunch at noon on Mondays, Tuesdays, Wednesdays and Fridays for a suggested donation of \$3.50 for those 60 and over or \$4.25 for those under 60; however, a recipient may pay whatever he or she can afford.

The Home Delivered Meal program runs Monday through Friday and the meals are free; however, donations in support of the program are accepted.

To qualify, a person must be 60 or older and be homebound by reason of injury, illness, an incapacitating disability, or be otherwise isolated. In addition, disabled persons under 60 also qualify under certain circumstances.

The program accepts donations in the form of a check payable to: City of Wilsonville noting "Home Delivered Meals" or "Nutrition Program" in the memo line. For information about the program or other referral services, contact Sadie Wallenberg, Information & Referral Specialist, at 503-570-1526, wallenberg@ci.wilsonville.or.us.

## "Green Power Challenge" Continues City Council Seeks Supports Sustainable, Renewable Energy Sources

The Wilsonville community-wide "Green Power Challenge" continues in March with Portland General Electric (PGE) staff and consultants going door-to-door and attending events to promote the use of "green power" produced by renewable energy sources.

In January, Mayor Tim Knapp and the City Council elected to have Wilsonville participate in the U.S. Environmental Protection Agency's "Green Power Communities" nationwide program. The EPA seeks to promote increased use of electricity generated from non-fossil fuel, renewable sources such as wind, solar and geothermal power. As part of Wilsonville's participation as a Green Power Community, the City is using PGE's "Clean Wind"™ program at the Gold level, or just under seven percent of electricity used by City facilities.

Already, PGE customers in Wilsonville have demonstrated a keen interest in supporting renewable energy

sources: 14 percent of all residential customers and a dozen businesses have opted to choose a renewable-energy option. The Green Power Challenge seeks to increase those counts by an additional 200 residential and business customers.



While requiring a slight premium in cost, choosing sustainably produced energy on the regional level helps to build the new infrastructure for renewable-energy production, as well as to create local jobs, improve domestic-energy security and reduce fossil-fuel carbon emissions. Regionwide, over 110,000 PGE customers are electing to use renewable-energy. Of the 53 Green Power Communities in the country, 11 are located in Oregon.

For more information or to enroll in a Green Power program, visit [www.PortlandGeneral.com/Wilsonville](http://www.PortlandGeneral.com/Wilsonville) or call PGE at 503-228-6322.

### Mayor's Message

#### 'State of the City' Shows a Vibrant Community

Recently, I had the privilege of delivering the "2015 State of the City Address" during a February City Council meeting. As is traditional for this kind of event, the presentation focused on Wilsonville's recent accomplishments and future challenges.



However, rather than recite just a litany of 'gee-whiz' facts and figures, I wanted to use this opportunity to address larger issues that the community is facing. As one who moved to Wilsonville in 1986, I have seen the evolution of the city, a continuum of changes over a span of time.

My desire is that we help create what might be termed a "complete community" — one that offers a range of residential living options, a variety of jobs for those with different skills and abilities, an assortment of shopping opportunities, an array of ways to get around, and overall the opportunity for widespread and varied social connections.

Our community benefits when we accommodate a range of housing option, as desired by residents at different times of their lives: as singles, couples, families and retirees. At each stage of life, our individual capabilities and needs as humans change. A socially cohesive community has places for people to live, meet and interact at all of these stages.

In a similar fashion, our community benefits when we plan to accommodate needed space for jobs, for places to conduct business, and for housing choices for living at all stages. In past years Wilsonville had more jobs than residents; without sufficient housing prices tended to be higher and workers generally had no choice but to commute. Over the past few years we have seen development of a broader range of local housing options, which provides greater opportunity for people to live near their work.

Some may recall the days not too long ago when we had no choice but to leave town for many essential items — even, say, a pair of socks. Now with a variety of retailers here, we can purchase in town nearly anything we may need in our daily lives. We can even choose to get around without driving. As people have become more aware of the value of healthy habits and exercise, we see more residents seeking to use non-automobile "active transportation" methods to commute to work or shop, to access services, and to enjoy recreation and social activities.

With a wide assortment of educational and recreation programs offered by the Library and Parks & Recreation Dept., wonderful pre-K through grade 12 school activities and a host of engaged nonprofits providing activities with a "small-town" feel, Wilsonville has great opportunities for neighbors to meet, interact and enjoy their community.

Wilsonville is becoming the kind of community where one can grow up, get a good job, raise a family, and retire, all centered in a thoughtful, interconnected, desirable small city: A complete community for all phases of life.

*Tim Knapp, Mayor*

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...and more!

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## Librarian Recognized for Teaching Early Learning Skills to Children

In January, Terri Wortman, a librarian at the Wilsonville Public Library, was awarded the 2014 Volunteer of the Year Award from the Coffee Creek Correctional Facility in Wilsonville.

Wortman received the award for years of partnering with the state prison's Head Start program, where she and Youth Services Librarian Steven Englefried lead a monthly story time session for young children of the incarcerated moms.

Wortman also leads a "Mom's Discussion Group" that focuses on providing incarcerated mothers early-learning and literacy tips and techniques so they can better help their children develop reading and language skills. During the sessions the moms also create related hands-on projects that further support their learning and their children's development.

Wortman was nominated for the award by staff at the Coffee Creek Early Head Start program. In explaining the nomination, Head Start teacher Carmen Slothower states, "Terri has been providing a monthly early-literacy circle time for our participating moms and their children since 2006. During the monthly session she shares stories, sings songs and teaches the moms about the importance of expanding their child's language and reading skills early in life."

**When children are read to, an entire world opens up to them and they begin to see new possibilities**

Over the past five years, approximately 60 moms have participated in the prison-based program that allows each participant to select two books a month that she reads to her child. Funding for the free book program is provided by a grant from the Oregon State Library's "Ready to Read" program.

Terri's participation in the program is also supported by funding from the Wilsonville Library. Library Direc-



Librarian Terri Wortman is named Coffee Creek Correctional Facility's 2014 Volunteer of the Year.

tor Patrick Duke explains, "When children are read to, an entire world opens up to them and they begin to see new possibilities."

Some interesting facts and tips the Library shares about early childhood education and literacy include:

- Sharing books with children is the single most important thing you can do to help them get ready to read and learn.
- Parents and primary caregivers can be a child's first and best teachers.
- Babies are born learning. The most dramatic brain development in humans happens during the first three years of life.
- Long before children start school, they develop "early literacy" skills that will help them learn to read when they are older.
- Talking, singing and playing with a child are fun and easy ways to build his or her vocabulary and other early learning skills.
- Babies and toddlers don't always sit still for long, but any reading, singing, talking, and playing you do with them is helping them learn.

Head Start teacher Slothower said, "When a parent and child snuggle up and enjoy a book together it strengthens the attachment between them. It also lets children know that books are fun and meaningful. One mom recently shared with me that her son didn't really show an interest in books until he was in this program. As she began to read to him, little by little, he began to develop an excitement for stories and books and now he reads a lot!"

The Wilsonville Public Library offers many free opportunities to learn about and promote early childhood learning opportunities that include:

- Free weekly preschool programs for young children of all ages (see the schedule at [www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org))
- Free home delivered books for children five and under through a partnership with the Dolly Parton's Imagination Library program; sign up at [www.ImaginationLibrary.com](http://www.ImaginationLibrary.com) or at the Wilsonville Public Library.
- Nearly 40,000 books and other items for children at every stage of growing.

## Foundation Attracts Support for Library Programs and Services

The Wilsonville Public Library Foundation, a non-profit organization that fund-raises for the local library, announced that the organization received a total of five new donations of a \$1,000 or more in 2014 from first-time donors. These substantial donations qualify the contributors to have their names added to the Donor Wall Board inside the Library's vestibule. Four of the donors are private individuals and the other is Wilsonville Diamond, a local business.

Throughout the year the Foundation provides community members a variety of ways to support the Wilsonville Public Library, including:

- Honoring someone with a personalized brick in the Library plaza area.
- Making a donation of any size at [www.wplf.org](http://www.wplf.org).
- Leaving a Legacy by donating to the Library's Endowment Fund.
- Attending and supporting the Foundation's annual auction dinner in October.

Funding from the Foundation helps the Library offer a variety of programs and services that would not otherwise be possible, including:

- Science adventure programs for kids
- Support for Dolly Parton's Imagination Library (free books for children ages five and under)
- Adult education programs such as literature classes
- Community outreach events and efforts
- Heritage collections featuring local history and genealogy
- Technology center offering the public free use of 20 computers

To learn more about the Foundation and supporting the Wilsonville Public Library visit [www.wplf.org](http://www.wplf.org).



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

### Free Tax Help

A volunteer CPA will be at the library ready to answer your questions and/or help you fill out your tax forms on Saturdays from February 7 through April 11, 10 am - 6 pm. No appointment necessary!

Please bring all of your necessary documentation and your 2013 tax records.

**Saturdays through April 11, 10 am - 6 pm. Free.**



### American Extremism Class Starts March 30

This is an even-handed focus on how the *left* and *right* think and behave. The course is of special interest to those who wonder why our political system is broken. It explains the strategies of revolutionary groups (including terrorists) around the world.

Instructor Bill Meulemans has lived and worked with extremist factions in the US, Israel, and Northern Ireland.

**Monday evenings 6 - 8 pm, March 30 - May 18. \$40 for class series. Sign up at the Circulation Desk.**



### Teen "Divergent/Insurgent" Event on Friday, March 13

Abnegation, Candor, Amity, Erudite, Dauntless: which faction are you? Get ready for the film release of *Insurgent* at this special teen event.

"The truth has a way of changing people's plans."

For students in grades 6-12 on Friday, March 13, 6:30 - 9 pm. Free.



### "Lucky Day" DVDs available

Looking for the hottest movies but don't want to wait on a long holds list? Our Lucky Day Collection has high-demand books and now DVDs on a first-come, first-served basis. No holds and no renewals. Books check out for 14 days. DVDs check out for 7 days. Limit of two "Lucky Day" items at a time.

### Making the Most of Social Security Retirement on March 26

On Thursday, March 26, 6:30 - 7:45 pm the library is hosting a free class to learn strategies on how to maximize your income in retirement, based around social security.



Presenter Richard Dougall of NorthWest Wealth Advisors has 17 years experience in the financial services industry.

## Upcoming Classes and Events

### Art Wall Artist of the Month:

local painter Kirsten Sneath

### First Friday Film

Friday, March 6, 6 - 8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

### Book Club

The Daughter's Walk by Jane Kirkpatrick  
Thursday, March 12, 6 - 8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

### Teen Event: Divergent/Insurgent Night

Friday, March 13, 6:30 - 9 pm  
[www.WilsonvilleLibrary.org/teens](http://www.WilsonvilleLibrary.org/teens)

### Book Notes Concert

Beth Donnelly & Douglas Feller  
Saturday, March 14, 2 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

### Great Books Discussion Group

Origin of Government by Thomas Hobbes  
Tuesday, March 17, 6 - 8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

### K-2 Book Adventures

Thursday, March 19, 4 - 4:45 pm  
[www.WilsonvilleLibrary.org/k2](http://www.WilsonvilleLibrary.org/k2)

### Story Fiesta

Saturday, March 28, 2 - 3 pm  
[www.WilsonvilleLibrary.org/k2](http://www.WilsonvilleLibrary.org/k2)

### History Pub at McMenamins

Tuesday, March 31, 6:30 - 8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

**CHIEF'S CORNER – TOP 25 MOST COMMON VIOLATIONS CITED IN 2014**

This month, I thought *Boones Ferry Messenger* readers would be interested in learning what the most common violations for which citations were issued by the Wilsonville Police Department in 2014. This list should also serve as a reminder of the rules of the road.



Of course, this list does not include all 129 different violations for which 2,600 citations were issued, but it does give you a good idea of what the most common violations were.

Our purpose in educating you about the traffic citations we issued is to deter these infractions in the future and to increase vehicular and pedestrian safety. Please drive safely.

*Jeff Smith, Chief of Police*

**Top 25 Most Common Violations Cited in 2014 by Wilsonville Police Department**

|  |     |  |    |
|--|-----|--|----|
| 1. Violating speed limit                       | 383 | 13. Failure to renew vehicle registration  | 35 |
| 2. Illegal use of cell phone                   | 294 | 14. Illegal U-turn                         | 29 |
| 3. Failure to obey traffic control device      | 248 | 15. Failure to change name/address on lic. | 27 |
| 4. Failure to carry proof of insurance         | 221 | 16. Violation of basic rule                | 27 |
| 5. Driving uninsured                           | 190 | 17. Failure to register vehicle in Oregon  | 23 |
| 6. Driving while suspended                     | 175 | 18. Following too close                    | 21 |
| 7. Failure or improper display of sticker      | 164 | 19. Driving with suspended/revok. license  | 19 |
| 8. Operation of vehicle with no license        | 142 | 20. Parking in park after closing          | 18 |
| 9. Parking in violation of sign                | 66  | 21. Careless driving                       | 16 |
| 10. Parking-expired/no registration            | 53  | 22. Illegal window tinting                 | 16 |
| 11. Driving under the influence of intoxicants | 40  | 23. Unsafe movement from lane              | 15 |
| 12. Failure to display registration plates     | 38  | 24. Failure to drive in lane               | 14 |
|  |     | 25. Failure/improper use of seatbelt       | 12 |

**SMART Seeks Input on Transit Plan from Community Members**

The City's public transit service, South Metro Area Regional Transit (SMART), is seeking community members to serve on a year-long task force to review and advise the City on an update of the Wilsonville Transit Master Plan. Updated each decade, the plan serves as the blueprint for City transit programs, including future bus routes and other transportation options.

Participation on the task force requires a commitment of a few hours every couple of months. Task force members need not have any particular expertise — just an interest in the future of public transportation in Wilsonville and a desire to help SMART deliver the best services possible to residents and commuting workers.

Ideally, the task force features a diverse range of people, including:

- Regular or frequent transit riders
- Individuals with disabilities
- People who speak English as a second language
- Older adults
- Students
- Employees/owners/managers of local businesses
- Walkers and bicyclists

Transit commuters, those who ride SMART for shopping, fixed route riders and Dial-a-Ride clients are also encouraged to participate. Other community members who may not be transit riders today, but who want to use transit services in the future, are also invited to participate.



This is one of SMART's two new deisel-electric hybrid buses the agency purchased with funds from a federal grant.

City Councilor Julie Fitzgerald, who is serving as chair of the new task force, will lead meetings that are expected to begin in April. Known as the 'SMART Task Force,' the advisory body is expected to recommend updates to

the Transit Master Plan for potential adoption by City Council. To learn more about the history of SMART and to view a current copy of the Transit Master Plan, visit [www.RideSMART.com/about](http://www.RideSMART.com/about).

SMART Transit Director Stephan Lashbrook says, "As SMART starts our 26th year of service to the greater Wilsonville residential and business community, we are

**We seek to provide relevant and reliable transit services for the public.**

undertaking a major review of our transit programs and services. SMART provides a range of transportation services both in and outside of Wilsonville for commuting workers, residents, high school and college students, and those with disabilities. We seek to provide relevant and reliable transit services for the public."

The City of Wilsonville is committed to diversity on the task force and can make available translators and audio or visual assistance services upon request. If access to the meetings is a barrier, SMART can provide task force members transportation to and from meetings.

For more information, contact Stephan Lashbrook, Transit Director, at 503-570-1576; [Lashbrook@ridesmart.com](mailto:Lashbrook@ridesmart.com)



SOUTH METRO AREA REGIONAL TRANSIT  
 29799 SW Town Center Loop E. (mail)  
 28879 SW Boberg Road (office)  
 503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

**Passenger Appreciation Week Starts on March 16**

Public transportation would not need to exist without passengers and we would like to take a moment and thank everyone for riding on SMART. During the week of March 16, 2015, bus drivers will be handing out little tokens of our appreciation as a simple and sincere way to say thank you for riding on Wilsonville's public transit system.

If you are new to the Wilsonville area or have been thinking about how to ride the bus around town, please visit our website at [www.RideSmart.com](http://www.RideSmart.com) or give us a call at 503-682-7790 and we will help you plan your trip to get you on your way.



**Bike Performance Check-up**

March is a great time of year to give your bike an easy seven-step Do It Yourself tune-up so you're ready to roll when the nice weather hits.

- Clean your bike
- Check brake levers, cables and pads
- Lube the chain
- Tighten any loose bolts and screws
- Check your helmet for any cracks or excessive wear
- Replace your light batteries

**For more tips and area biking information visit [www.Ridesmart.com/bikesmart](http://www.Ridesmart.com/bikesmart)**



**You Asked and We Listened!**

SMART and the City's Public Works employees will be installing 30 bus stop seats along the most popular bus routes in Wilsonville. These particular stops do not quite meet the minimum ridership numbers to install a more expensive bus shelter, but these seats will help those who are waiting to be able to rest while they wait or set down a bag to help make the trip a little bit easier.

All of these seats are compliant with ADA rules and regulations and we hope that they help make your transit trip more enjoyable.

Let us know what you think. You can:

- Email [smart@ridesmart.com](mailto:smart@ridesmart.com)
- Call 503-682-7790
- Find us on Facebook or Twitter.



## 'Heart of the City' Offers Many Social Services Under One Roof

In January, civic leaders celebrated the opening of a new community resource and counseling center referred to as the 'Heart of the City,' located at the offices of Grace Chapel church on Boberg Road.

The newly remodeled office building provides several local, nonprofit organizations with office space, a meeting/class room, counseling rooms and a shared kitchen.

The resource center serves as the new home for Wilsonville Community Sharing's (WCS) food bank and

**Wilsonville Community Sharing Food Bank Hours**  
 • Tuesdays 1 - 3 pm; 6 - 8 pm  
**Heart of the City Counseling Services Hours**  
 • Tuesdays 11 am - 3 pm  
 Heart of the City Community Resource Center Building, 28925 SW Boberg Road

personal-assistance programs.

WCS, a local emergency-assistance provider, has relocated the food bank from the former location at the Meridian United Church Christ to the new site at

28925 SW Boberg Road, which offers a larger space for operations and is located along a SMART bus route. The WCS food bank is open on Tuesdays 1 - 3 pm and 6 - 8 pm.

WCS also offers utility, prescription-drug and rental assistance at the new center. WCS's programs and services are funded in part by the City of Wilsonville and are offered to anyone regardless of religious affiliation.



Leigh Crosby, WCS's Food Bank Coordinator.

The Heart of the City is also home to the Heart of the City Counseling Center and the Young Life ministry, and is to be a regular stop for the American Red Cross blood mobile. Currently the center is open on Tuesdays 11 am - 3 pm to provide direct services to patrons and also provide referrals to other social-service agencies.



Brad Peterson, Pastor of Family & Care, oversees the counseling being offered the new social service center.

Inspiration for the facility came in 2006 when community and church leaders met to discuss the question, "What are the needs in the community?" Attendees concluded the community needed a centralized, physical space that would provide care and resources for those in need. At the meeting one participant observed that, "What our community needs is a Heart of the City." This resonated with the group and ultimately became Grace Chapel's goal and the inspiration for the center.

By locating a number of nonprofits under one roof, Grace Chapel hopes to promote synergy, cooperation and networking among the various providers. Organizers also hope to make it easier for community members to get a variety of services all under one roof.

For more information about the center visit [www.TheHeartOfTheCity.org](http://www.TheHeartOfTheCity.org) or call 503-570-2988.

## City Accepting Grant Applications for Community Programs

The City of Wilsonville's Parks and Recreation Department is now accepting applications for the Community Opportunity grant program. The deadline for the grant program is Friday, March 6 at 5 pm after which the grants will be reviewed and awarded by members of the Parks and Recreation Advisory Board.

The Community Opportunity Grant program, with funds distributed twice per year for a total of \$25,000, provides funding for a wide range of

local programs and projects in Wilsonville. Funds for the Opportunity Grant program come from the City's general fund.

Successful applications for the Community Opportunity Grant program include proposals aimed at promoting education, diversity, arts or community involvement in the city. Priority is given to new projects or organizations. Eligible uses of the grant funding include:

- Projects, events, or services to encourage and foster diversity and/or increase educational opportunities in the Wilsonville community
- Services or projects to encourage advances in education, art or community leadership among Wilsonville's diverse residents
- Services, projects, or events to involve Wilsonville's youth or elderly population in community activities

For more information, go online to [www.WilsonvilleParksandRec.com/GrantPrograms](http://www.WilsonvilleParksandRec.com/GrantPrograms); or contact Brian Stevenson, Recreation Coordinator, at 503-570-1523, [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

**Save time. Pay online.**  
 Learn more at: [www.ci.Wilsonville.or.us/Pay-Online](http://www.ci.Wilsonville.or.us/Pay-Online)



29600 SW Park Place  
 Parks Programs: 503-783-7529  
 Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

## Park Rental Season Opens Soon

Memorial Park is a popular location for company gatherings, family parties, weddings and a variety of sporting events. Wilsonville Parks and Recreation offers a number of park rental options including: the historic Stein Boozier Barn, the River Shelter, the Forest Shelter, and sport fields.

Reservations for Wilsonville residents and businesses will be accepted starting on March 9 for the 2015 season.



The rental season runs from April 15 to Oct. 15.

For more information on availability and fees please visit the Parks and Recreation website at [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com) or contact Ahsamon at 503-570-1530 or [ante-marandi@ci.wilsonville.or.us](mailto:ante-marandi@ci.wilsonville.or.us).

## Antique Appraisal Day

Pack up those treasures gathering dust in the attic and let the experts appraise them! The event takes place on Saturday, April 11 at the Community Center from 10 am to 2 pm.

Appraisals will be done during set appointments, so please call to book a time. A \$10 appraisal fee per item will benefit the Senior Nutrition program. Stick around for lunch and enjoy live music by Tom Stroud. Chili and hot dogs will be available for purchase. For more information or to book an appointment, please call 503-682-3727.

## Community Garden Sign Up

The Community Garden opens for registration on Thursday, March 19. Gardeners are asked to come to the Parks and Recreation Admin Office to register and pick their garden plot. Registration opens at 8 am.



In-ground plots are \$22 for the season. Raised beds are \$25 for the season. Please visit the Parks and Recreation website at [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com) or contact Brian Stevenson at 503-570-1523 for more information.

## Spring Sports

If you are looking for opportunities to get your children out of the house and moving this spring, the Parks and Recreation Department is excited to give you three different program options.

Soccer Shots is open to 2-8 year olds with three different classes to choose from. The 13-week program starts on Sunday, March 15, and is just \$144 for the entire session.



Intro to Rugby is a brand new class offering and is open to 3rd-6th graders. This non-contact program is fast moving and guaranteed to keep every participant moving. The four-week program begins Wednesday, April 1 with a cost of \$60.

Finally, Skyhawks is back to offer a Spring Break Hoop Camp. The 1/2 day camp is open to participants aged 7-12 and is \$99 for the Monday through Thursday program.

More information on all camps and programs is available on the Parks and Recreation website.

## Upcoming Classes and Events

### Lunchtime Lecture

"No Ebola Vaccine? Create your body's own defenses"  
 Presented by, Karen McGeehan, Nutritionist  
 Tuesday, March 3  
 12 pm  
 Community Center  
 No Charge

### Monday Night Wellness

"Manage ADD, ADHD and Autism with healthy options"  
 Monday, March 9  
 6:30 - 7:30 pm  
 Wilsonville Library Oak Room  
 No charge

### Body Sculpt

Ages: 16 and older  
 Community Center  
 \$59  
 Mondays, 3/30 - 5/18  
 5:45 - 6:45 pm  
 Thursdays, 3/12 - 5/7  
 noon - 1 pm

### Wilsonville Egg Hunt

Ages: 1-11  
 Saturday, April 4  
 10 am  
 Memorial Park

### Spa Saturday - Call for Vendors!

Health, craft and art vendors are wanted for Spa Saturday at the Center on May 16. To register visit the Parks and Rec website at [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com) to complete the interest form, call 503-570-1530 or email [ante-marandi@ci.wilsonville.or.us](mailto:ante-marandi@ci.wilsonville.or.us).

## Tips for Eliminating or Reducing Pesticide Use

The most commonly applied pesticides are insecticides (to kill insects), herbicides (to kill weeds), rodenticides (to kill rodents) and fungicides (to control fungi, mold and mildew). Harmful environmental impacts of pesticide use include: loss of biodiversity and elimination of key species (e.g., pollinator bees), water pollution, soil contamination, and pest resistance that leads to an increase in the application of pesticides. Studies directly link the use of many pesticides with human health problems and chronic conditions.

In your own backyard you can do your part to eliminate or reduce the use of pesticides that might end up in streams, soil, food chains and on children's skin. In many cases, the only "active ingredient" you'll

need is some elbow grease; hand-pulling weeds, for instance, can eliminate the use of herbicides and the risks they pose to the broader environment.

Other tips for eliminating or reducing pesticide use and other unintended effects include:

- Be aware that pesticides applied in gardens can move from your intended application area by drift-



ing in the air or washing off into storm drains or streams.

- Be aware of weather patterns and do not apply pesticides just prior to rainfall or during windy conditions.
- Avoid applying pesticides to surfaces like sidewalks or driveways, where they can easily be washed off.
- Check pesticide labels for warnings regarding their appropriate use and limitations.
- Never dispose of pesticides in storm drains, sinks or toilets.
- Under no circumstances should pest control equipment be cleaned in a location where rinse water could flow into gutters, storm drains or open waterways.
- Never exceed application rates listed on a pesticide label.
- Purchase only enough pesticides for immediate needs, which eliminates pesticide storage.
- Always keep pesticides in their original containers. Unwanted pesticides, even diluted ones, should be taken to an approved waste facility, such as the Metro Household Hazardous Waste Disposal Facility, open Monday-Saturday, 9 am – 4 pm, and located at 2001 Washington St., Oregon City (503-234-3000). Unused rinse liquid is also considered a hazardous waste and must be disposed of properly at a hazardous waste facility. For more information, contact Luke Bushman, Stormwater Management Coordinator, at 503-570-1552; bushman@ci.wilsonville.or.us.

## Become a "Master Recycler" — Spring Class Registration Open Until March 5

The regional Master Recycler course is being held in Oregon City this spring. The course provides an in-depth, hands-on opportunity to learn about recycling and thoughtful consumption—and then use that knowledge to make an immediate difference in the community.

The eight-week class, offered only three times a year in the Portland region, meets once a week in the evening and twice on Saturdays for half-day field trips. The spring 2015 session begins April 1 and runs for eight consecutive Wednesdays through May 20 at the Clackamas County Development Services Building, 150 Beaver Creek Road, Oregon City. The Saturday field trips are scheduled for April 18 and May 9. The next, fall 2015 session is set for September through October in Hillsboro.

Limited to 30 participants, Master Recyclers agree to attend all classes and field trips and, after completing the course, put their skills to work to help others conserve natural resources by volunteering 30 hours of public outreach. Master Recyclers staff information booths at community events, provide presentations in the community, work on original projects, and inspire their neighbors and co-workers.

Applications are being accepted through March 5. There is a \$50 fee to cover course materials, and limited scholarships are available. For more details or

to apply, go to [www.MasterRecycler.org](http://www.MasterRecycler.org) or call 503-545-8976.

The regional Master Recycler Program is administered by the City of Portland and sponsored by Metro, the City of Portland, Clackamas County, Washington County, the Oregon Department of Environmental Quality and Recycling Advocates.



Recent participants in the Master Recycler program in front of a pile of compost.

For more information, contact Lauren Norris, Master Recycler Program Coordinator, at 503-545-8976; [masterrecycler@portlandoregon.gov](mailto:masterrecycler@portlandoregon.gov).

## Go Red Campaign Raises Awareness of Heart Attack Symptoms

During "National Wear Red Day" in February, Tualatin Valley Fire & Rescue (TVF&R) firefighter medics, training officers and fire prevention staff, exchanged their blue-duty uniform shirts for red ones in recognition that heart disease is the No. 1 killer of women in the United States, claiming more lives than all form of cancer combined.

The campaign seeks to raise awareness about the importance of knowing the signs and symptoms of a heart attack—which can be different for men and women—and when to call 9-1-1.

Identifying the symptoms of a heart attack and calling 9-1-1 triggers a potentially life-saving continuum of care for a cardiac patient. Firefighter medics can begin advanced medical care in the field and hospitals, with a catheterization lab and chest pain center, provide an expert diagnosis and fast, effective treatment.

While the message is equally important for both men and women, public awareness of heart disease

and gender-specific heart symptoms in women has lagged behind that of men. Only one in five American women believes it is her greatest health threat.



Sharon Steiger experienced a life-threatening cardiac event in 2008.

As with men, the most common heart-attack symptom in women is chest pain or discomfort. But women are more likely to experience the other common symptoms — particularly shortness of breath, nausea/vomiting, and back or jaw pain. They are also symptoms that women often brush off as the flu, stress, or simply feeling under the weather, which could put their lives in jeopardy.

"If you feel you may be having a heart attack, dial 911! Emergency responders will be able to care for you immediately upon their arrival and all the way to the hospital in case your condition worsens," explains Sharon Steiger, a resident of Wilsonville, who experienced a life-threatening cardiac event in 2008. "I tell all my friends to listen to your body and if you have concerns talk to your doctor about the warning signs and steps you can take to prevent a heart attack."

## VOLUNTEER SPOTLIGHT: CAROLINE BERRY

Caroline Berry is a relative newcomer to Wilsonville, moving here in 2010, but she did not take long before getting really involved in the community. Caroline volunteers for the City as a member of the Wilsonville Library Board, she belongs to the nonprofit Library Foundation as an associate member and she teaches Sunday school at her church.



In addition, Caroline participates in the City's new Leadership Academy that seeks to prepare community members for serving in leadership roles on various City boards and commissions. Caroline said, "I am thrilled to be a member of Wilsonville's inaugural Leadership Academy. I look forward to serving the community as Wilsonville continues to grow."

Wilsonville Library Director Patrick Duke said, "Caroline brings passion and enthusiasm to her work on the Library Board. She clearly cares deeply about making sure that the Library continues to deliver top-notch services and programs for a wide range of clients."

Caroline has lived overseas in China and in bigger cities like Bellevue and Portland, but it is Wilsonville that she has chosen to call "home." Caroline, who originally moved to Wilsonville so her youngest daughter could go to high school here, explains, "Wilsonville is family-friendly, beautiful, has wonderful schools and is well planned. I fell in love with the town and I did not want to leave after my daughter graduated from high school so I decided to stay. I can walk to everything I need including the Library, shopping and our amazing parks."

As a member of the Library Board, Caroline has been involved in the helping craft the Library's strategic plan that focuses on, among other things, early childhood literacy. She is excited about the Library's plans to hire a bilingual outreach librarian who will be responsible for promoting the Library's programs and services to a broader audience. Caroline seeks to help expand the Library's focus on early childhood literacy by getting every child five and under involved in the Library's programs and services.

Caroline, who has both a Bachelors and Master's Degree in Urban Planning from the University of Washington, has held numerous positions as a professional urban planner and has worked in project management for a variety of engineering companies throughout her career. Currently she works for an independent insurance company based in Salem that specializes in surety bonds and liability coverage for heavy construction projects and equipment.

In her free time Caroline enjoys swimming, biking, baking, laughing with her friends at book club meetings and caring for her dog and cat. She is very proud of her three grown daughters who are either in or graduated from college and live independently.

## Community Invited to Share Input on Comcast's Cable Service

Community members are invited to attend a public forum to share how they feel about Comcast's cable service and what they need from the company in the future. The City invites all community members, nonprofits, businesses and civic leaders to attend the public forum on Wednesday, March 11 at 6 pm, Wilsonville City Hall, 29799 SW Town Center Loop East. Community feedback from the forum is to be used to evaluate Comcast's performance during the current franchise term and to assess future community needs regarding Comcast cable services in the future.

**Comcast's Cable Service Public Forum**  
 • Wed, March 11, 6 pm  
 City Hall Council Chambers  
 29799 Town Center Loop East



## Reports from City Boards and Commissions

### Council Moves to Update Wastewater Collection System Master Plan and Amend Use of Public Right-of-Way Rules

At the Feb. 2 Wilsonville City Council, the Council approved an update to the Wastewater Collection System Master Plan that was last modified in 2001. The purpose of updating the plan is to provide a 20-year guide for maintaining and operating the City's wastewater-collection system. The Plan identifies needed sewer lines and pump station upgrades and replacements to ensure that the system provides adequate service for current and future residents and businesses. The updated plan includes cost estimates to be used for general budgeting purposes and for likely inclusion in the Capital Improvement Program, which lists short- and long-term major capital investments that the City plans to complete.

The Council also held a first reading and adopted an ordinance to amend City code to allow the City



Council received a report from the Office of Congressman Kurt Schrader by Whitlee Preim-Siddon, District Aide.

to adjust the terms of franchise agreements in order to take into account the impact of various proposed uses of the public right-of-way.

Under Communications, the Council heard a report from the Office of Congressman Kurt Schrader providing an update on federal affairs.

During the worksession prior to the meeting, the Council heard a report on the City's

Asset Management Program which is a system that tracks the location, condition, required maintenance and replacement schedule for all City-owned assets. Municipal assets that already are or will be covered by the plan include roadways (signs, trees, irrigation and pavement), street lights (poles and lights), facilities (buildings, shelters and pumphouses), parks (sports fields, equipment, courts, lights and irrigation), water (pipes, hydrants, valves, meters and manholes), wastewater (valves, pipes and manholes), stormwater (catch basins, pipes, culverts manholes and ponds) and the City-owned water and wastewater treatment plants.



Local Boy Scouts (left to right) Logan Schoenacker, Joshua Wilcott and Briggs Moore attended the Feb. 2 City Council meeting to earn their governance merit badge.

### Council Transfers Property to Urban Renewal Agency, Funds Study of Water Treatment Plant Expansion and Reviews Draft Plan for Frog Pond Area

On Jan. 22, the City Council approved a resolution authorizing the City Manager to transfer ownership of a 9.8-acre parcel of City-owned property located at 11650 SW Tooze Road to the City's Urban Renewal Agency. This property was purchased with Urban Renewal Agency funds in 2006, but ownership was vested with the City in order to facilitate a land exchange with the West Linn-Wilsonville School District. Subsequently, the recession made development of an elementary school at this site financially unfeasible and a new site was selected for the eventual Lowrie Primary School. The resolution approves transferring ownership of this land back to the Urban Renewal Agency so that the proceeds from any future sale of the land are returned to the Urban Renewal Agency.

City Council also approved a resolution authorizing the City to spend up to \$120,000 of the nearly \$1 million needed to study and plan for the future expansion of the Willamette River Water Treatment plant to potentially serve as a regional water treatment plant for both the City of Wilsonville and the customers of the Tualatin Valley Water District (TVWD), including the City of Hillsboro and others. An expanded plant will also help meet Wilsonville's future water demand needs and provide alternate source of water in the event of a disruption in water service from the current plant.

### Basalt Creek Concept Plan Process Focuses on Providing Services to the Area

During the past several months the cities of Tualatin and Wilsonville continue to make progress on developing the Basalt Creek Concept Plan. Once complete, the plan guides the future development of the Basalt Creek region, an 800-acre unincorporated, rural area between Wilsonville and Tualatin.

In December at a joint meeting of the Wilsonville and Tualatin City Councils, the city councilor members discussed an initial land-use, transportation and infrastructure scenario for the area. Since then, project work is focusing on determining the best way to provide sanitary-sewer service to the planning area. The project team is also gathering information about potential revenues, fees and development standards in order to



Under Mayor's Business, the Council voted to reappoint Mary Fierros Bower to the Development Review Board Panel A and also appointed Andrew Karr to the Budget Committee to fill a vacancy on the committee created by the recent resignation of Glenn Ohl.

The Council continued an ordinance that adopts the 2014 Update to the Wastewater Collection System Master Plan.

On the Consent Agenda the Council approved the City's 2015-16 state legislative agenda to guide City responses to specific legislative proposals that may arise during the upcoming 2015-16 session of the Oregon Legislative Assembly.

During the worksession well-attended by residents and property owners prior to the meeting, City Council and Planning Commission members held a special joint meeting to hear an update from staff and consultants on the draft Frog Pond area concept plan. Funded by a Metro grant, the City is planning for future residential development, parks and schools in the 500-acre study area. Major items of discussion included the type and density of residential development and the potential inclusion and location of a small-scale, neighborhood shopping area. Members of the two bodies provided staff feedback and direction on the draft plan which will be the topic of another round of community engagement this spring.

begin running and testing models of various land-use scenarios.

Meetings originally planned for this spring are being delayed in order to provide the project team more time to study how to best provide sanitary sewer service to the area. The tentative schedule for additional meetings to review and discuss the plan includes:

- A Joint City Council meeting scheduled for June 17, rescheduled from February, to be held in City of Tualatin.
- A Public Open House will occur over the summer, originally planned for March.

For the latest information on meetings and developments visit the project website at [www.BasaltCreek.com](http://www.BasaltCreek.com).

For more information or to provide comments about the plan, contact Miranda Bateschell, Long-Range Planning Manager, at 503-570-1581; Bateschell@ci.wilsonville.or.us.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration by the City Council during March. Scheduling changes can and do occur; check for updated meeting information at the City's website homepage, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, March 2:** Councilor Dirksen, Metro Update (during worksession); 2006 Metro Spaces Bond Measure; Branding/City Logo; Business License Changes.

**Monday, March 16:** Mutual use agreement Landover HOA water services; Business License and Transit Tax payment overdue policy/process; and Willamette Water Supply Update: Pipeline Routing (during worksession).

### Development Review Board Update

On Jan. 26, the DRB Panel B approved a five year Temporary Use Permit for a sales office and model homes in the Calais at Villebois subdivision along with associated parking, landscaping and other improvements. Also during the meeting Aaron Woods was elected as the 2015 Chair and Dianne Knight was elected as Vice-Chair.

On Feb. 9, the DRB Panel A approved an application for a conditional use permit for a home business of light-office duty for 10050 SW Wilsonville Road.



The home previously occupied by the Northwest Montessori School is approved for light-duty office use.

The board also approved the Final Development Plan (Architecture and Landscaping) for the Seville Row Homes (7 units) and for the Carvalho Row Homes (18 units). The Seville Row Homes are located on the northeast side of Barber Street between Villebois Drive and Ravenna Loop. The Carvalho Row Homes are located on both sides of Villebois Drive between Zurich Street and Barber Street.

### January Library Board Report

During the Library Board Meeting in January Library Director Patrick Duke and others gave reports on a variety of matters.

**Youth services:** 30 teens attended "Game Night" on Jan. 23. The March Teen event is on Friday, March 13, as a tie-in to the movie release of *Insurgent*.

**Librarian's Report:** Patrick Duke reported that a fire occurred in the outside book drop on Jan. 16. Library Operations Manager Kim Robben arrived before 7 am to smoke in the staff work area. She called 911 and Tualatin Valley Fire and Rescue and City Public Works staff. TVF&R extinguished the fire and monitored the library's carbon monoxide levels. According to the Fire Inspector, there was no evidence of cigarettes or any accelerants in the fire. Ten items were destroyed in the fire, with an approximate value of \$100. The library was closed for only part of the day of the fire and minor repairs have been made. The fire is being treated as arson by the Wilsonville Police Department.

**History Pub:** January's topic, the Willamette Locks, was very popular attracting an audience of 150 people.

**Strategic Planning:** Patrick Duke is planning to meet the chairs of the Library Board, Friends and Foundation to create an agenda for a joint meeting of the groups.

**Budget Planning:** Patrick Duke reported that he is working on the budget which is essentially a status quo budget, with a 1% increase in the Materials and Services budget and reclassifications of some staff members to better match their duties.

**Anniversary of Library (Feb. 14, 1982):** Pat Duke announced that the library turned 33 on Feb. 14.

The library is part of the Clackamas County early-learning hub to address school readiness for at-risk youth. Every library in LINCC sent a staff member to a "Reading for Healthy Families" training, which trains caregivers how to talk with and teach parents to read to their children. Librarians Shannon Belford and Steven Engelfried trained.

The next Library Board Meeting is held on Wednesday, March 25, at 6:30 pm at the Library.

# What can the City do about traffic congestion in Wilsonville?

## Q & A with the City's Community Development Director Nancy Kraushaar

The community survey conducted last summer confirmed that traffic congestion is an issue of concern to residents. While traffic congestion has been an issue of concern for the greater community, ODOT confirms that traffic counts on I-5 and other major arterials throughout the Portland metro region are increasing over the past two years. Economists indicate that the increase in traffic corresponds to the region's emergence from the recession with both general population growth (migration to Oregon) and substantial new job creation that is leading to increased commuting — which mean more cars on area roads.



Community Development Director Nancy Kraushaar

Traffic congestion in Wilsonville is manifest in two major ways: local streets and I-5. Like many issues pertaining to public infrastructure, there are a number of issues to consider.

The City has a variety of approaches to deal with traffic in town that have a wide range of costs and funding sources, some of which are restricted in how they may be used. For example, the state constitution requires that gas-tax revenues can only be used on roads.

A primary component of the City's transportation systems plan (TSP) involves providing for non-automobile mobility and convenience. This includes free SMART public-transit service in town and SMART travel-planning assistance for residents and commuting workers for local and regional trips via transit. Additionally, the City prioritizes travel via "active transportation" methods of bicycle riding and walking, which an increasing number of residents are using for local trips. The health benefits of these modes are becoming more widely proven and are important to acknowledge.

The City has a strong "concurrency" policy that requires any major new residential or commercial development that generates substantial traffic to create a funding plan to increase roadway capacity to handle increased traffic. Over the years, this policy has resulted in the rebuilding of both primary I-5 interchanges in Wilsonville at Elligsen and Wilsonville Roads. The most recent, \$21 million improvements to the I-5/Wilsonville Road interchange completed in 2012 increased the "through-put" capacity of the interchange on Wilsonville Road by 50 percent and created double turn lanes and longer "stacking" ramps for traffic merging onto I-5.

The City charges substantial systems development charges (SDCs) to homebuilders and developers of commercial or industrial sites. By law, SDCs are only to be used to increase a community's infrastructure capacity to accommodate the increased demand from new development. The City also uses urban renewal financing to build infrastructure for new developments that is then repaid by the increase in property taxes generated by new subdivisions or commercial centers.

**The City is investing in a number of new roadway connections.** In order to improve traffic circulation throughout Wilsonville both during a traffic jam and during peak travel times, the City is investing in a number of new roadway connections. These are much needed to build a more complete traffic grid in Wilsonville that better distributes traffic and reduces trip distances. These projects include:

- The Barber Street Road extension currently under-way is designed to provide a much needed east-west

link between the growing Villebois neighborhood and the rest of the city without having to use Wilsonville Road. The Barber Street project is expected to open around Thanksgiving this year. The project is funded primarily by SDCs, urban renewal and federal grants.

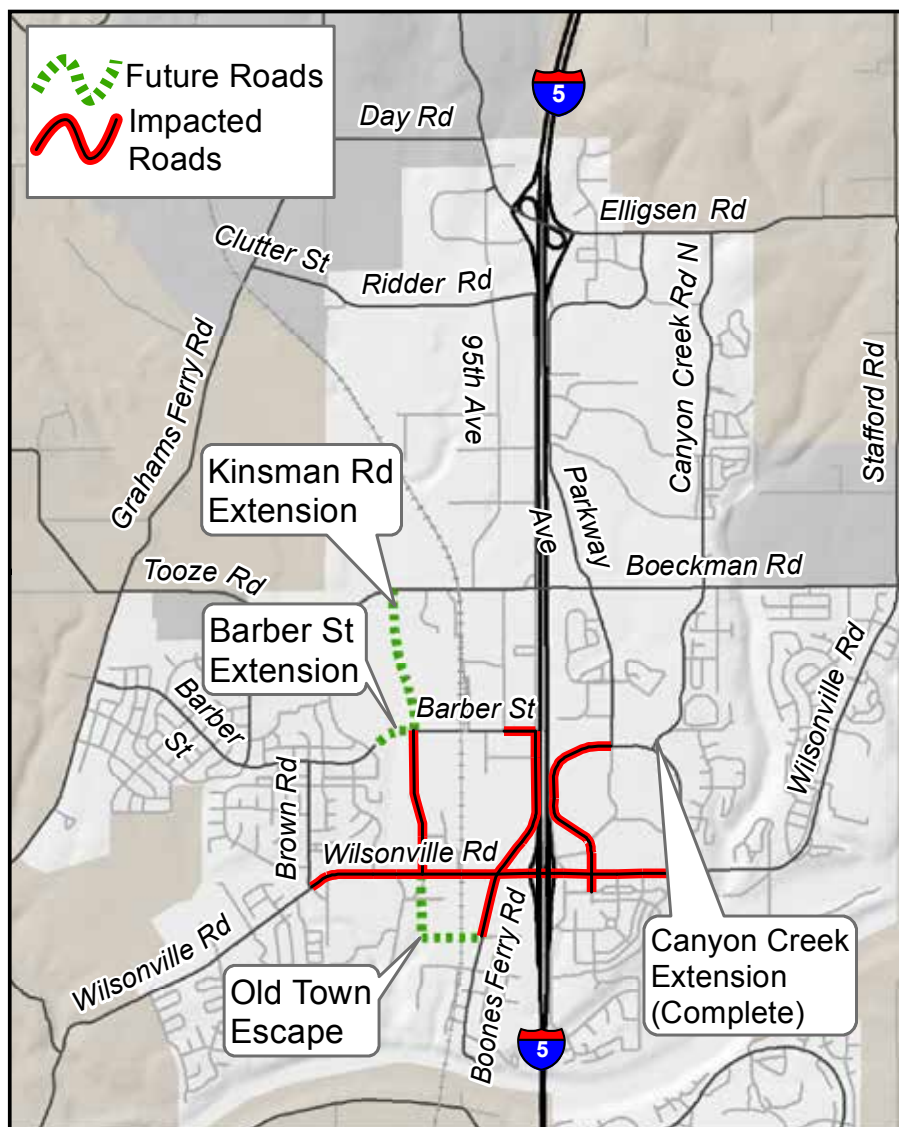
- In this same area of town we are also actively working on a complementary project that extends Kinsman Road north from Barber Street to Boeckman Road, adding a new north-south route on the west side of town. The extension of Kinsman Road is expected to begin construction in spring 2016 and to be complete in later half of 2017. This new road is designed to take some of the pressure off of SW Boberg and SW Boones Ferry Roads.
- Another transportation project that we are beginning to plan for is what we are referring to as the 'Old Town Escape.' This proposed east-west road would serve as an alternative to Wilsonville Road in and out of the Old Town shopping and business areas.

### What causes all the congestion around I-5?

Much of our worst congestion in Wilsonville occurs when there is an incident that creates a traffic jam on I-5. Because most major arterials lead to I-5, when I-5 jams-up, so do the roads leading to I-5. The traffic on I-5 leaves no room for the many vehicles wanting to get on the freeway

which then clogs the nearby local streets. Even more local congestion results from vehicles that have chosen to detour the I-5 mess by using our local roads. The

**Much of our worst congestion in Wilsonville occurs when there is an incident that creates a traffic jam on I-5.**



The red roads are commonly congested during an incident on I-5. Planned future roads shown in green will provide more alternate routes for local traffic.

resulting heavy traffic overcomes normal intersection operations and local traffic is completely tangled until I-5 resumes normal traffic flow.

Congestion starts at the I-5 on- and off-ramps and then spills over onto Wilsonville Road, which then begins to impact traffic on Boones Ferry Road, Kinsman Road, Town Center Loop West, as well as other side-streets. Once these roads are full, the intersections in these areas are overloaded. Often drivers are tempted to enter a crowded intersection without knowing if they will get through it before the light changes. Please be aware it is illegal (and very frustrating to other motorists) when a driver obstructs an intersection.

The I-5 Boone Bridge over the Willamette River poses a special problem and acts as a choke-point. As the only crossing over the Willamette for a 28-mile stretch between Oregon City (Hwy 43) and St. Paul (Hwy 219), the Boone Bridge carries nearly the same amount of traffic as the Interstate Bridge over the Columbia River, and one-third more trucks due to merger of I-5 and I-205 six miles north.

It is best to stay off of certain roads when they are impacted by I-5 incidents. Maybe you can postpone your trip or, if that is not an option, try to take another route that goes around the traffic snarls. Consider an alternate east-west route like Boeckman Road to get across town or more lightly traveled roads like SW Grahams Ferry Road and Canyon Creek Road to avoid back-ups on the busier roads closer to I-5.

### How can I avoid getting stuck in traffic?

If possible the best way to avoid getting stuck in traffic is to plan your trips during non-peak travel hours

**With one click to [www.ci.wilsonville.or.us/I-5Cams](http://www.ci.wilsonville.or.us/I-5Cams) you can quickly view traffic conditions on I-5 at Wilsonville and Elligsen Roads.**

(ODOT) traffic cams on our website. With one click to [www.ci.wilsonville.or.us/I-5Cams](http://www.ci.wilsonville.or.us/I-5Cams) you can quickly view traffic conditions on I-5 at either Wilsonville Road or Elligsen Road. You can also check many area road conditions at [www.TripCheck.com](http://www.TripCheck.com) or by calling 511.

Wilsonville's peak travel hours are from 7 - 9 am and 4 - 6 pm, which correspond to the general start and

**Peak travel hours are from 7 - 9 am and 4 - 6 pm.**

end of the workday for most employers. Generally you can expect heavy traffic on all of our roads near the I-5 interchanges during peak travel times. Other congestion points to be mindful of include the local schools during morning and afternoon pickup hours.

Community members are encouraged to consider taking alternate forms of transportation such as walking, biking or riding the SMART bus ([www.ridesmart.com](http://www.ridesmart.com)) for stress-free traveling in town. SMART bus trips within Wilsonville are free and if there is a traffic jam at least you can sit back and relax and leave the driving to someone else. Biking through heavy traffic can be faster and provides a great form of low-impact exercise. Just remember, safety is essential when biking – wear bright clothing and a helmet and use lights as needed.

### Working for Solutions

One of the larger issues that governments at all levels grapple with is how much public money to spend to deal with "peak demands" that come and go. That is, do you spend a lot of money to accommodate traffic volumes that are greatest for four hours each day (two hours in the morning and two hours in the evening), but that for the other 20 hours a given road has plenty of capacity? Some governments in the US are experimenting with "congestion pricing" (charging tolls during peak hours) or encouraging employers to go to split shifts in order to spread-out commute times.

The City Council is very aware of our traffic congestion problems and is actively working to explore solutions. While the City has very limited influence on potential solutions to Interstate 5, we do have the ability and are responsible for making improvements to

**The City Council is very aware of our traffic congestion problems and is actively working to explore solutions.**

our local transportation system. Solutions include connecting existing routes and improving the transportation grid in Wilsonville. In addition, new technology is being used for traffic signals that optimize and linking intersection operations. Another is to make sure we are providing other safe and attractive choices to get around – such as SMART and the pedestrian and bike systems. Yet another important factor is planning our land uses so that shopping, jobs, and housing are conveniently located to help reduce trip lengths throughout the community.

## Grace Chapel Rummage Sale to Benefit Community Groups

The annual Community Rummage Sale sponsored by Grace Chapel is set for Friday, March 13, 5 -9 pm, and Saturday, March 14, 9 am - noon, when all merchandise is half off. Proceeds from the sale are provided to community organizations that in the past have included school-support groups, Random Kindness, Wilsonville Community Sharing and others.

Donations of clothing, merchandise and appliances are welcome, except for mattresses, car seats and ripped or broken furniture and maybe dropped-off at Grace Chapel, 9600 SW Boeckman Road, the week of the sale.

**Community Rummage Sale**  
 • Fri, March 13, 5 - 9 pm  
 • Sat, March 14, 9 am - noon  
 Grace Chapel, 9600 SW Boeckman Rd



For more information, visit [www.WilsonvilleRummageSale.com](http://www.WilsonvilleRummageSale.com) or email [wilsonvillecrs@gmail.com](mailto:wilsonvillecrs@gmail.com).

## U.S. Bank and City Host Free Secure Shredding Event for Community

Community members are invited to bring up to 10 boxes of sensitive documents to a free and secure community shredding event sponsored by U.S. Bank and the City of Wilsonville. The event is to be held in the U.S. Bank Wilsonville Branch's parking lot located at 29112 SW Town Center Loop West on Saturday, March 14, 9 am - noon.

Community members are encouraged to shred files that contain sensitive and confidential information in order prevent identity theft and other forms of fraud. In particular documents that contain the following information should be shredded before being discarded:

- Account numbers
- Birth dates
- Passwords and PINs
- Signatures
- Social Security numbers
- Names
- Addresses
- Phone numbers
- E-mail addresses

Participants are able to watch while their documents are securely shredded by a Recall Mobile Shredding Truck. All of the shredded material is recycled. The event is free but participants are encouraged to bring new toiletry items to donate to the local food bank, Wilsonville Community Sharing.



## Wilsonville Theater Company Performing "Bus Stop"

The Wilsonville Theater Company is performing "Bus Stop" in early March at the Frog Pond Grange, 27350 SW Stafford Road in Wilsonville. The William Inge play is directed by Matt Russell.

"Bus Stop" is a drama, laced with romance and comedy, and is about how passengers, stranded by a heavy snow storm, cope with being stuck in a small rural diner in Kansas.

March performances are held at 7:30 pm on Friday March 6 and two showings on Saturday, March 7, at 2:30 pm and 7:30 pm.

General admission tickets cost \$12 and are \$10 for seniors and students. Tickets are available online at [www.WilsonvilleTheater.com](http://www.WilsonvilleTheater.com) and are also available at the door which opens 30 minutes prior to curtain.

## Get Five Free Trees from the City

The City of Wilsonville is offering up to five native tree seedlings for any community member or business within the City limits, for planting on their property. Plantings along streams and other water bodies are encouraged, but any tree planting enhances the City's 'urban forest' and contributes to reducing stream temperature and pollutants. The trees are obtained at a local nursery, which provides input on their selection, planting and care. Arrangements can be made for delivery if it's not possible to pick them up at the nursery.



For more information and to obtain a tree coupon, contact Luke Bushman, Stormwater Management Coordinator, at 503-570-1552; [bushman@ci.wilsonville.or.us](mailto:bushman@ci.wilsonville.or.us).

## Wood Middle School's Jr. Scoop Club Hosts \$1 Clothing Sale

On Saturday, April 4, 8 am – 2 pm, the Jr. Scoop Club at Wood Middle School is hosting their third annual clothing and ice cream sale in the school's gym located at 11055 SW Wilsonville Road.

Proceeds from the annual fundraising event benefit Doernbechers Children's Hospital as well as provide scholarships to send Wood students to Outdoor School. During the sale every item of clothing costs only \$1 and features clothes and styles for all ages and sizes. In addition, the club is also be selling their ice cream for \$3 a scoop on homemade waffle cones.

Community members are invited to both buy clothes at the event and also to donate clothes prior to the sale. Clothes marked as "donation for Jr. Scoop Club" are being accepted at Wood Middle School until the end of March. Community members who donate three or more bags of clothes receive a certificate for a free pint of the club's premium ice cream which they can pick up at the sale.

Jr. Scoop is both a community service club and a premium ice cream company made up of over 60 students. Students meet every Monday morning 8 - 9 am before school to work on business skills, discuss and organize upcoming events and volunteer their time. The club is dedicated to helping members learn how to be successful entrepreneurs while also helping their community.

Each month Jr. Scoop club members make and sell two flavors of high-quality, artisan ice cream at [www.JrScoop.com](http://www.JrScoop.com) and at other events in Wilsonville.



This is the club's logo.

# City Calendar

| MARCH  |  |
|--|--|
| 2 Mon  | • City Council<br>5 pm Worksession<br>7 pm Meeting<br>City Hall  |
| 3 Tue  | • Municipal/Traffic Court<br>5 pm, City Hall   |
| 9 Mon  | • DRB Panel A<br>6:30 pm, City Hall  |
| 11 Wed   | • Wilsonville Community Seniors Inc. Advisory Board<br>1 – 3 pm, Wilsonville Community Center<br>• Planning Commission<br>6 pm, City Hall<br>• Comcast's Cable Service Public Forum<br>6 pm, City Hall |
| 12 Thurs   | • Park and Recreation Advisory Board Meeting<br>6:30 pm, Park and Recreation Administrative Offices  |
| 16 Mon   | • City Council<br>5 pm Worksession<br>7 pm Meeting<br>City Hall  |
| 17 Tue   | • Municipal/Traffic Court<br>5 pm, City Hall   |
| 19 Thurs   | • Community Garden Open Registration<br>8 am, Park and Recreation Administrative Offices<br>• Wilsonville Leadership Academy<br>6 -9 pm, City Hall   |
| 23 Mon   | • DRB Panel B<br>6:30 pm, City Hall  |
| 25 Wed   | • Library Board<br>6:30 pm, Wilsonville Public Library   |
| APRIL  |  |
| 2 Thurs  | • Frog Pond Open House<br>6 - 8 pm, City Hall  |
| 4 Sat  | • Wilsonville Egg Hunt<br>10 am, Memorial Park   |
| 6 Mon  | • City Council<br>5 pm Worksession<br>7 pm Meeting, City Hall  |
| 7 Tue  | • Municipal/Traffic Court<br>5 pm, City Hall   |
| 8 Wed  | • Wilsonville Community Seniors Inc. Advisory Board<br>1 – 3 pm, Wilsonville Community Center<br>• Planning Commission<br>6 pm, City Hall  |
| 9 Thurs  | • Park and Recreation Advisory Board Meeting, 6:30 pm, Park and Recreation Administrative Offices  |
| 13 Mon   | • DRB Panel A<br>6:30 pm, City Hall  |
| 16 Thurs   | • Wilsonville Leadership Academy<br>6 -9 pm, City Hall   |
| 20 Mon   | • City Council<br>5 pm Work Session<br>7 pm Meeting, City Hall   |
| 21 Tue   | • Municipal/Traffic Court<br>5 pm, City Hall   |
| 22 Wed   | • Library Board<br>6:30 pm, Wilsonville Public Library   |
| 27 Mon   | • DRB Panel B<br>6:30 pm, City Hall  |
| All dates and times are tentative; check the City's online calendar for schedule changes at <a href="http://www.ci.wilsonville.or.us">www.ci.wilsonville.or.us</a> . |  |

## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on cable TV channels 30 (Comcast) and 32 (Frontier) and replayed periodically. Meetings are also available to stream live online on Web TV and replays in the video vault by clicking on the TV icon on the City's homepage, [www.wilsonville.or.us](http://www.wilsonville.or.us). Public comment is welcome at City Council meetings.



**Tim Knapp**  
Mayor  
[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



**Scott Starr**  
City Council President  
[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



**Julie Fitzgerald**  
City Councilor  
[fitzgerald@ci.wilsonville.or.us](mailto:fitzgerald@ci.wilsonville.or.us)



**Susie Stevens**  
City Councilor  
[stevens@ci.wilsonville.or.us](mailto:stevens@ci.wilsonville.or.us)



**Charlotte Lehan**  
City Councilor  
[lehan@ci.wilsonville.or.us](mailto:lehan@ci.wilsonville.or.us)

### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)



### Wilsonville City Hall

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)

# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

April 2016

## Help Improve Our Parks at WERK Day on May 14

The City is now recruiting volunteers for the community's 16th annual community clean-up event called Wilsonville Environmental Resources Keepers (WERK) Day on Saturday, May 14, 8 am - 1 pm. Registration and a free omelet breakfast runs 8 - 9 am in the Wilsonville Community Center located at 7965 SW Wilsonville Road. The complimentary breakfast is provided by the Wilsonville Rotary Club with free hot coffee donated by Starbucks.

During the event participants focus on improving and maintaining City parks in Wilsonville. According to Parks Supervisor Tod Blankenship, this year's focus includes a partnership with Pac-West Disc Golf Alliance. The day consists of prepping and constructing tee pads and bench sites and anchoring basket sleeves for a new Disc Golf Course in Memorial Park. Other projects for WERK day volunteers include spreading bark dust in perennial beds and tree rings, removing invasive species, painting soccer goals and resurfacing a small wooden walking bridge.

Participants should bring work gloves, water bottles, sun screen and be dressed appropriately for weeding, raking and shoveling. Participants of all ages

are welcome and encouraged to attend, but children under the age of 14 must be accompanied by an adult.

Mayor Tim Knapp said, "Each year hundreds of community members support our parks by putting on their work clothes and heading over to the Community Center to spend their Saturday morning fixing up our parks. This shows the spirit of volunteerism is alive and well in Wilsonville."

"We know how much our residents love Wilsonville parks because, not only do they frequently visit them, but also because so many residents volunteer to participate in WERK day to help us make our parks even better," said Stan Sherer,

Parks and Recreation Director.

Sponsors of the long-term event include the Wilsonville Rotary Club, Wendy's, Starbucks, the Family Fun Center, Fred Meyer and Lee's Martial Arts.

Participants in the rain-or-shine event receive coupons from Wendy's restaurants and the Family Fun Center and a free WERK Day t-shirt while supplies last.

Participants and volunteer groups are encouraged to register in advance on the City's Parks and Recreation website at [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

where they can download and sign the WERK Day volunteer liability release form.

To sign up a group, please contact Patty Brescia, Recreation Program Manager, at 503-570-1525, [brescia@ci.wilsonville.or.us](mailto:brescia@ci.wilsonville.or.us).



WERK Day volunteers fixing up Memorial Park in 2015.

## City Awards Tourism Grants to Local Events & Proposals

On March 10, the Wilsonville Parks and Recreation Advisory Board awarded \$25,000 in grants to support local tourism events with funding from the City's Tourism Grant Program.

Four local nonprofit organizations were awarded grants to produce major community events during the 2016 tourism season:

- **Wilsonville Celebration Days** – Fun in the Park festival: \$9,250
- **Wilsonville Arts & Culture Council** – Wilsonville Festival of Arts: \$5,250
- **Wilsonville Kiwanis Club** – Kids Fun Run: \$5,250
- **Wilsonville Rotary Club** – Summer Concert Series: \$5,250

The Tourism Grant Program is funded by City hotel/motel tax revenues to support events and programs that promote tourism and community activities that draw both overnight and daytime visitors, which helps to support local businesses.

On February 25, the Wilsonville Parks and Recreation Advisory Board also awarded \$20,000 in Clackamas

County Tourism "Community Partnership Program" grants to fund five local proposals. The funded proposals include:

- **Wilsonville Parks and Recreation Dept.** – Memorial Park Disk Golf Course: \$10,000
- **Willamette Falls Heritage Area Coalition** – Interactive Willamette Falls Area Map: \$4,000
- **Oregon Cascade Fast Pitch Softball Club** – Men's Fast-Pitch Softball Tournament: \$3,000
- **Willamette United Soccer Club** – Mount Hood Challenge Soccer Tournament: \$2,000
- **Farmlandia Farm Loop** – Information Kiosks: \$1,000

Funding for the Clackamas County Tourism-Community Partnership Program (CPP) grants comes from Clackamas County's 6% transient lodging tax paid by overnight lodging visitors.

For information about these grant awards and future funding opportunities, contact Brian Stevenson, Recreation Coordinator, at 503-570-1523; [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).



The City's Tourism Grant Program supports the Rotary Club's Summer Concert Series, which brings popular bands like Johnny Limbo and the Lugnuts, that draws large crowds to Town Center Park in late July and early August.

### Mayor's Message

#### 'State of the City' Shows a Vibrant Community

Recently, I had the honor of delivering the "2016 State of the City Address" during a March City Council meeting. In addition to discussing a number of Wilsonville's recent accomplishments and future challenges, I sought to address larger issues that the community is facing. As one who moved to Wilsonville in 1986, I have seen an evolution of the city through a continuum of changes over a 30-year span.

The essence of the goals of the City Council focus on creating a "complete community" — one that offers a range of residential living options, a variety of jobs for those with different skills and abilities, an assortment of shopping selections, an array of ways to get around, and overall the opportunity for widespread and varied social connections.

Our community benefits when we accommodate a range of housing options, as desired by residents at different times of their lives: as singles, couples, families and retirees. At each stage of life, our individual capabilities and needs as humans change. A socially cohesive community has places for people to live, meet and interact at all of these stages of life.

In a similar fashion, our community benefits when we plan to accommodate for jobs and sites to conduct business. City leadership has been working for several years with Tualatin, Washington County and others on planning for development of the Coffee Creek and Basalt Creek employment areas. As we now advance the Coffee Creek urban renewal area, approved by local voters in a March 2013 advisory vote, the City's infrastructure extensions can take shape, funded in large part by the increase in property value created by new employers.

Some may recall the days not too long ago when we had no choice but to leave town for many essential

*Wilsonville is becoming the kind of community where one can grow up, get a good job, raise a family and retire.*

our daily lives. An ever growing assortment of dining establishments and beverage shops provides more venues to meet or "hang out."

To an increasing degree, we can even choose to get around without driving. As people have become more aware of the value of healthy habits and exercise, we see more residents seeking to use non-automobile "active transportation" methods of walking, bike-riding and using transit to commute to work or shop, to access services, and to enjoy recreation and social activities.

With a wide assortment of educational and recreational classes offered by the Library and Parks & Recreation Dept., high-quality pre-K through grade 12 school programs and a host of engaged nonprofits providing activities with a "small-town" feel, Wilsonville has great opportunities for neighbors to meet, interact and enjoy their community.

Wilsonville is becoming the kind of community where one can grow up, get a good job, raise a family, and retire, all centered in a thoughtful, interconnected, desirable small city: A complete community for all phases of life.

*Tim Knapp, Mayor*

City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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**Our History: Jan. 10, 1969 – Wilsonville’s First City Council Sworn In**



Wilsonville’s first City Council is sworn in (left to right): City Attorney Fred Anderson, Councilman Walter Wehler, Councilman William Bruck, Mayor Phillip Balsiger, Councilman Glenn Cumberland and Councilman William Montgomery.

**Everyday Choices Improve Your Health and Your Odds**

Tualatin Valley Fire & Rescue (TVF&R) encourages residents to embrace healthy habits that improve overall quality of life. TVF&R offers the following tips to having a heart-healthy lifestyle.

**Enlist a partner:** Talk to loved ones, neighbors, and friends and invite them to participate in physical activities, share a balanced meal and have a fun adventure. Everyday activities are always more fun with a partner — someone who will keep you company, inspire you to stick with a new routine, and encourage you on your journey to a healthier you.

**Fuel your body:** Eating balanced meals helps your body better fight off sickness and recover more quickly when you are sick. Drink plenty of water. Enjoy fruits and vegetables throughout your day as snacks, toppings, side dishes or in your main meal. A healthy diet includes whole grains, fat-free or low-fat dairy products, lean meats, fish, beans, eggs and nuts. Limit the saturated fats, trans fats, cholesterol, salt and added sugars in your diet.

**Get moving:** It’s never too late to start being active every day. As little as 30 minutes of exercise a day can help keep your body fit to fight off illness and prevent

injury. Carving out time to exercise has been shown to increase your energy level, reduce health risks, lower stress, improve your mood and boost productivity. Regular physical activity is fun and an important for overall health and fitness.

**Get enough sleep:** Physical activity can improve your quality of sleep. Try to get six to eight hours of sleep each night to re-charge your body.

**Ditch bad habits:** Limit your alcohol and caffeine consumption and quit smoking for good. Set healthy goals for yourself and celebrate when you achieve them.

**Team up with your doctor:** Doctors can help with prevention, screening, and treatment. Don’t wait until you are sick to schedule an appointment. They can help prevent health problems or catch them early enough to help you beat them. Ask your doctor to check your blood pressure and cholesterol numbers.

Everyday choices can improve your odds of fending off serious illnesses. These choices also pave the way for a happier and healthier life.

For more information about adopting a heart-healthy lifestyle, visit [www.tvfr.com](http://www.tvfr.com).

**Wilsonville’s Imagination Library Turns Four Years Old**

In February Jan Rippey, a member of the local Kiwanis Club, and members of the Library Foundation celebrated the fourth Anniversary of Dolly Parton’s Imagination Library in Wilsonville. The early-literacy program, founded by Dolly Parton, currently provides about 700 local children under the age of five, free monthly books that are delivered directly to their homes.

Since the program’s local launch in February 2012, more than 1,200 local kids have received over 25,000 free books.

While kids must be younger than five to participate, there is no minimum age to enroll a child in the program. In fact, infants, who are signed up at birth, are eligible to get a total of 60 different free and age-appropriate books.

Funding for the program is provided locally by the Kiwanis Club of Wilsonville and is also supported by Joe Restic of Wilsonville Orthodontics, the Xerox Corporation, the Friends of the Wilsonville Library, the Wilsonville Public Library Foundation and local community members.

Kids who participate in the program read more and are better prepared to begin school when they enter kindergarten.

To sign up a child for Dolly Parton’s Imagination Library visit [www.ImaginationLibrary.com](http://www.ImaginationLibrary.com).



Kiwanis Club member Jan Rippey.

**More than 1,200 local kids have received over 25,000 free books.**



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

**April is National Poetry Month**

This year marks the 20th anniversary of National Poetry Month, which was inaugurated by the Academy of American Poets in 1996. Since then, National Poetry Month has become the largest literary celebration in the world celebrating poetry’s vital place in our culture.

Discover your inner poet with activities and interactive displays throughout the library, and join us for an inspiring performance by Chris Lee and Colleen O’Brien of PoetryMusic on April 30 at 1 pm.  
[www.WilsonvilleLibrary.org/poetry](http://www.WilsonvilleLibrary.org/poetry)

**Personal Spring Cleaning Talk Saturday, April 16**

Join local scientist and acupuncturist PK Melethil for tea and conversation about stress and its effects on the digestive system.

Topics may include stress and food sensitivities, water the universal cleaner, and managing stress without drugs.

**Saturday, April 16, 1 - 2 pm. in the Oak Room. Free.**



**Creating and Sticking to a Budget Workshop April 28**

Gain financial peace of mind and confidence in your ability to save money. Local retired CPA Alan Steiger will take the mystery and confusion out of budgeting and share tips for creating and maintaining a budget that works for you.



**Thursday, April 28, 6 - 7 pm, in the Rose Room. Free.**

**Food for Fines During National Library Week**

Come celebrate National Library Week! During April 10 - 16, bring in non-perishable food and get \$1 off your library overdue fines (per item). All food will be donated to local food banks.

For more information, contact the Reference Desk at 503-682-2744.

**Poetry & Music Event Saturday, April 30**

Local chamber jazz duo Chris Lee and Colleen O’Brien of PoetryMusic will perform poems set to live music in a free multi-media performance on **Saturday, April 30, 1 - 2 pm, in the Oak Room.**



Sponsored by the Wilsonville Friends of the Library.  
[www.WilsonvilleLibrary.org/poetry](http://www.WilsonvilleLibrary.org/poetry)

**Upcoming Classes and Events**

**Local Artist of the Month: Richard Armstrong**

**First Friday Film**

Friday, April 1, 6 - 8  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

**Book Notes Concert**

*I-5 Connection Choir*  
Saturday, April 9, 2 - 3 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

**K-2 Book Adventures**

Thursday, April 14, 4 - 4:45 pm  
[www.WilsonvilleLibrary.org/k2](http://www.WilsonvilleLibrary.org/k2)

**Book Club**

Thursday, April 14, 6 - 8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

**Genealogy Club**

Monday, April 18, 1 - 2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

**Great Books Discussion Group**

Thucydides’ *History of the Peloponnesian War*  
Tuesday, April 19, 6 - 8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

**History Pub at Wilsonville McMenamins**

Tuesday, April 26 6:30 - 8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

**Game Night**

Wednesday, April 27, 6 - 8 pm  
[www.WilsonvilleLibrary.org/games](http://www.WilsonvilleLibrary.org/games)

## Chamber Hosts Annual Wilsonville Business EXPO

Area businesses are showcasing their local goods and services at the Wilsonville Chamber of Commerce '2016 Business Expo', set for Wednesday, April 27, 10 am - 2 pm at the Al Kader Shrine Center, 25100 SW Parkway Ave. in Wilsonville.

**3rd Annual Business Expo**  
 • Weds, April 27, 10 am - 2 pm  
 Al Kader Shrine Center  
 25100 SW Parkway Ave

The EXPO is free, open to the public and features products and services from more than 50 local businesses including: health care, fitness, financial services, insurance, banking, printing and restaurants. If you are looking to do business with local vendors and find services to meet the needs of your business or home, you're sure to find a wealth of information and great networking opportunities.

## County Seeks Volunteers for Victim Assistance Program

The Clackamas County District Attorney's Office is looking for Volunteer Victim Advocates who are interested in helping people who are victims of crime. The criteria for becoming an advocate include the ability to care about others, possess good listening skills, be at least 18 years of age, have reliable transportation and no criminal background. Victim Advocates provide crisis intervention, follow-up support, keep victims informed of their case status, accompany victims to court (if applicable) and encourage them to exercise their rights. Applications are currently being accepted for the spring 2016 training session. All accepted applicants receive training to prepare them to be a Volunteer Victim Advocate.

For more information or to get an application, visit [www.clackamas.us/da/victimassist.html](http://www.clackamas.us/da/victimassist.html) or contact Krysti Bellmore, 503-655-8616; [krystibel@co.clackamas.or.us](mailto:krystibel@co.clackamas.or.us).

## CHIEF'S CORNER - BURGLARY PREVENTION TIPS

Recently there has been an uptick in burglary activity in Wilsonville. In fact, 2015 saw a 23% increase in these incidents over the prior year. So this month I wanted to share some tips from National Crime Prevention Council that will reduce your chances of being burglarized. The tips include:



- **Keep your home well-lit** and always lock doors and windows.
- Install a **security alarm system** and **motion-activated flood lights**. If you have an alarm, set it! If you have motion detector lights, turn them on!
- **Never leave valuables in plain sight**. If you have garage windows, keep them covered.
- Keep your **bushes and lawn neatly trimmed**.
- Be sure your kids **know important phone numbers** and tell them never to answer the door or phone if you are not home.
- **Trust your instincts**. If it seems suspicious, report it to the 24-hour non-emergency number, 503-655-8211 or 911 in the event of an emergency or a crime in progress.
- Organize or participate in a **Neighborhood Watch** program (see contact information on right.)
- Install **quality locks on doors and windows** and use them. Install track-type locks, drop bars, **dowels or pinning devices on sliding glass doors**.
- Close and **lock your doors and windows even when you're home**.
- **Install deadbolt locks** and peepholes on exterior



- doors and any interior door **between the garage and house**.
  - **Do not leave your garage door open** or unlocked if you are not in the immediate area.
  - **Beware of solicitors**. Door-to-door salespeople must have a business license or permit issued by the City, so always ask to see it for verification.
  - **Don't let strangers, workers and others, into your home** if you are not with them.
  - **Never give keys to people making improvements** to your home.
  - If you are going to be out of town **have family, friends or neighbors check on your home**.
  - Do not hide spare house keys outside your home but **leave them with a trusted neighbor instead**.
  - **Have locks changed or re-keyed** when moving into a new home or apartment.
  - **Store ladders in the garage** or padlock them to fixed objects so burglars can't use them to reach upper floor windows.
- Please take these common sense steps to reduce your risk of being burglarized and for more information visit the National Crime Prevention Council website at [www.ncpc.org](http://www.ncpc.org).



For more information on forming a Neighborhood Watch group, contact Officer Sara McClurg of the Clackamas County Sheriff's Office at 503-785-5077; email [crimeprevention@clackamas.us](mailto:crimeprevention@clackamas.us) to arrange a free 90-minute Neighborhood Watch training session.

*Jeff Smith, Chief of Police*

## SMART SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
 28879 SW Boberg Road (office)  
 503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

## Walk @ Lunch - April 29, 2016

Join Walk SMART at noon on Wednesday, April 29, 2016 for a 30-minute walk during lunch and take the first steps towards a healthier you. Walk SMART will be coordinating the walk on National Walk @ Lunch Day and it will take place at City Hall in Wilsonville - rain or shine. Join us at the picnic tables on the south end of the building. Meet new friends and do some lunchtime networking. Remember to bring your walking shoes to work with you!



## Please Take Our Survey and Give us Your Feedback!



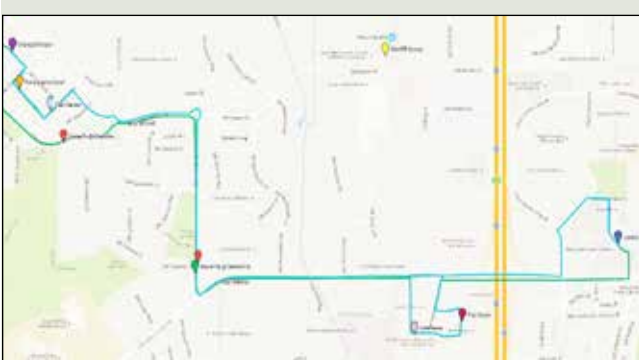
Wilsonville needs to hear what you care about the most to update the Transit Master Plan to best serve our community. We hope to hear from residents, employers, employees and visitors to help guide SMART resources over the next five years. With quick and easy-to-answer survey questions, this important community feedback will inform and help decision-makers understand the choices for SMART resources. Help shape the future of transit in and around Wilsonville. Encourage your friends and neighbors to take the survey as well. The survey only takes a couple minutes and can be found online and at mobile kiosks around town: [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com).



## Improved Villebois Shuttle Breaking Ridership Records

Improved shuttle service from Villebois neighborhood to area shopping destinations of Albertsons, Fred Meyer and Lamb's Market has shown steady growth since day one. The service began on February 8 and has as many as 60 passengers per day. The new service is runs on weekdays, 9 am - 3:30 pm.

Check out the schedule online at [www.ridesmart.com/Villebois](http://www.ridesmart.com/Villebois)



## Fun Times at the 2016 Daddy Daughter Western Buckaroo Dance



29600 SW Park Place  
Administrative Office: 503-783-7529  
Community Center: 503-682-3727  
www.WilsonvilleParksandRec.com

### Save 10% on Spring and Summer Registration

Enroll by May 1st and receive 10% off program tuition costs. Registration for classes and camps opens on April 18 and all registrations completed by May 1 receive 10% off the cost of the program or camp (excluding Skyhawks Sports Academy). All programs can be viewed and registered for online.



### Park Reservations Now Being Accepted

Applications for Memorial Park shelters and fields are now being accepted. Memorial Park is a popular location for company gatherings, family parties, weddings and a variety of sporting events. Wilsonville Parks and Recreation offers a number of park rental options including: the historic Stein Boozier Barn, River Shelter (SW corner of Memorial Park), Forest Shelter (SE corner of Memorial Park), Splash Shelter (adjacent to the Murase Plaza water feature), Grove Shelter (near the new playground at Murase Plaza) and sport fields.

The 2016 rental season runs from April 9 - October 9. For more information on availability and fees please visit [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com) or contact Ahsamon at 503-570-1530.

### Senior Fitness Testing

Fitness Specialist Brad Moore and Recreation Program Manager Patty Brescia will be offering free fitness testing to the community on Friday, April 8 from 11 am to 1 pm at Portera at the Grove in Wilsonville as part of Portera's Fitness Fair. The Senior Fitness Test is a nationally recognized, evidence based test, and participants will receive detailed testing results. Please call the Community Center at 503-682-3727 to register. Portera is a senior independent living facility and is located at 8945 Ash Meadows Lane, Wilsonville.



### Nutrition Lecture - Toxins in our Food

The Community Center is hosting Certified Nutritionist Karen McGeehan for a lecture on toxins in our food, what is real and what is altered, and how to tell the difference on Wednesday, April 27 at noon. A complimentary salad bar lunch is being provided, through a sponsorship provided by United Healthcare. Please call the Community Center to register.

### Upcoming Classes and Events

#### Beginner Learn to Ice Skate

Saturdays, April 2 - May 28  
Programs available for ages 2 to 17  
Times vary based on age  
Sherwood Ice Arena  
\$90 (plus \$15 annual fee to ice arena)

#### Alzheimer's Disease Lecture

Tuesday, April 10  
10 am  
Community Center  
No Charge  
Call the Community Center to register

#### Early Memory Loss Program

Tuesdays, April 19 - May 10  
10 - 11:30 am  
Community Center  
No Charge  
Call the Community Center to register

#### Creative Movement/Ballet

Ages: 3 - 4  
Thursdays April 21 - May 26  
3:30 - 4:15 pm  
Tauchman House  
\$65

## Spa Saturday at the Center

Saturday, April 16  
10 am - 1 pm at the Community Center

enjoy free samplings\* of:

Meditation    Acupuncture    Tai Chi    Reiki  
Massage        Yoga                    And More!

For more information: [WilsonvilleParksandRec.com/SpaSaturday](http://WilsonvilleParksandRec.com/SpaSaturday)



\* Appointments required for some offerings

## Celebrate Arbor Day with Friends of Trees and the City

On Saturday, April 2, 9 am – 1 pm, join Friends of Trees and the City of Wilsonville in celebrating Arbor Day by planting native trees and shrubs and removing invasive English ivy at Tranquil Park, 10580 SW Brown

**Volunteer Tree Care Event**  
• Sat, April 2, 9 am - 1 pm  
Tranquil Park, 10580 SW Brown Road

Road. Please arrive around 8:45 am to be registered and assigned to a crew. Breakfast snacks and hot chocolate/coffee, as well as gloves, tools and guidance will be provided. Dress for field conditions, all ages are welcome!

RSVP's are requested for groups of five or more. Register online at [FriendsofTrees.org/volunteering/group-registration](http://FriendsofTrees.org/volunteering/group-registration). Contact Jenny or Randi with Friends of Trees at 503-595-0213 if you have any questions or need more information.

## Wilsonville Celebrates 18th Year with Tree City USA Program

The City of Wilsonville has been recognized for the 18<sup>th</sup> consecutive year as a Tree City USA, and has received its 8<sup>th</sup> Growth Award by the National Arbor Day Foundation. This recognition reflects Wilsonville's continued commitment to maintaining and promoting the urban forest, one of the community's greatest assets. The urban forest contains all trees in the City, whether they are located in parks, landscape islands, natural areas, or along your street.

Trees help clean the air, conserve the soil and water, reduce heating and cooling costs and bring nature close to where we live – ultimately contributing to a sense of place and qual-

## Get Five Trees for Free

The City of Wilsonville is offering up to five native tree seedlings for any community member or business within the City limits, for planting on their property. Plantings along streams and other water bodies are encouraged, but any tree planting enhances the City's 'urban forest' and contributes to reducing stream temperature and pollutants. The trees are obtained at a local nursery, which provides input on their selection, planting and care. Delivery arrangements can be made for those who are not able to pick up at the nursery. For more information and to obtain a tree coupon, contact Luke Bushman, Stormwater Management Coordinator, at 503-570-1552; [bushman@ci.wilsonville.or.us](mailto:bushman@ci.wilsonville.or.us).



ity of life in Wilsonville.

The Tree City USA designation requires a comprehensive urban forestry program, which consists of a tree ordinance, a city department responsible for trees, an annual community forestry budget of at least \$2 per capita and annual Arbor Day observance and proclamation.

For 2016, an Arbor Day proclamation will be made during the City Council meeting on April 4, 2016. The celebration, however, will be held in conjunction with Friends of Trees on

April 2, 2016, and will include a tree planting event at Tranquil Park.



## Coffee Creek Urban Renewal District Begins to Take Shape

During last November's election Wilsonville voters expressed support for creating a new urban renewal district in the Coffee Creek Industrial area — an area that is located near the Coffee Creek Correctional Facility in northwest Wilsonville, south of Day Road, west of Interstate 5, north of Ridder Road and east of Grahams Ferry Road.

With support from voters in a March 2013 election, City Council directed staff to begin developing an urban renewal plan for the Coffee Creek Industrial Area for potential City Council adoption late this summer.

The Coffee Creek urban renewal district was first envisioned in a master plan for the area that was completed in 2007. The original plan includes development of a new employment center in north Wilsonville with approximately 1,800 jobs and an estimated annual payroll of \$55 million at build-out. The plan predicts the area will attract general industrial, warehouse, flex, and research and development related businesses.

The recommendation to create the new urban renewal district was also advanced in the Wilsonville Urban Renewal Strategic Plan that Council adopted in 2014 based upon the input from a community task force that included representatives from local taxing districts, as well as community and business leaders. The strategic plan concludes that in order to attract private-sector industrial development, the Coffee Creek industrial area requires a substantial investment in public infrastructure improvements such as new roads, sidewalks and utility lines. Once in place these public improvements are expected to then attract private investment and development that is estimated to increase the assessed value of the proposed district from a tax base of \$62 million to about \$790 million over the life of the district.

The projected increase in overall assessed value and increased tax revenues in the district will then generate future urban renewal funds that are planned to be used

to repay urban renewal loans used to fund the needed infrastructure investments. In this way funding the cost to build the infrastructure in the Coffee Creek area will be paid for by new tax revenues from the private-sector industries that are located in the district and benefit the most from the investments.

Prior to potential adoption late this summer, the public and other stakeholders are going to have many opportunities to provide input on the proposed Coffee Creek urban renewal plan.

On Monday, April 25, the City is holding an open house, 5:30 – 6:30 pm, followed by an Urban Renewal Task Force meeting, 6:30 – 8 pm, at City Hall. The open house is an informal setting where the public has the opportunity to learn about and provide input on the overall proposal. The Task Force meeting, immediately following the open house, is expected to be more formal and technical with a presentation by the consultants and staff who are developing the draft plan. The public is welcome to attend the task force meeting, but public testimony is not planned for the initial meeting.



This summer the urban renewal district plan is to be reviewed and discussed as follows: by the Wilsonville Urban Renewal Agency on June 20; the Planning Commission on July 13; and finally by the City Council at a public hearing that is presently scheduled for August 1.

Watch for more information and ongoing updates about the formation of the Coffee Creek Urban Renewal District in upcoming issues of the *The Boones Ferry Messenger* throughout the spring and summer. For more information contact Nancy Kraushaar, Community Development Director, at [kraushaar@ci.wilsonville.or.us](mailto:kraushaar@ci.wilsonville.or.us) or 503-570-1562.

## VOLUNTEER SPOTLIGHT: PAUL KELLER

In the fall of 2014 Paul Keller, a resident of Wilsonville for more than a decade, made a big decision. Paul explains, "I felt like I could do more to help the community that has provided so much for me." So Paul applied for and began participating in the Wilsonville Leadership Academy, a City-sponsored program that helped him learn about the City and also various volunteer opportunities in the community where he could become more involved.



Paul and Gaedin Keller

As the six-month Wilsonville Leadership Academy program came to an end, right away Paul decided to join the board of the Wilsonville Community Seniors Inc. (WCSI), an independent, not-for-profit organization that looks out for the needs of seniors in Wilsonville and raises funds to support various programs and services at the Wilsonville Community Center. Paul explains, "I volunteered for WCSI because I saw a need that I felt I can fill with my background in business and process improvement and also because I care deeply about helping seniors."

At only 52 years old, Paul is not a senior citizen, but that did not stop him from not only joining the WCSI board, but also becoming the board's interim chair in less than a year.

In this new leadership role Paul explains that he seeks to, "Raise awareness about WCSI so that the organization can attract more funds and to help more seniors. WCSI does a lot of great things like offer low-income seniors class scholarships and emergency assistance, but few people know about us and therefore are not aware of how to support us."

In addition to fundraising Paul is also eager to recruit a few more board members. In particular the organization needs a Treasurer and also someone who has expertise in website development to help the organization create and launch a new website.

Until February this year Paul worked for Xerox in Wilsonville, but just recently he took a new position as an Operation and Policy Analyst focused on process improvement with the Oregon State Hospital in Salem.

*Without Gaedin's support I would not have come to be at a place in my life where community service is a top priority for me.*

While working for Xerox Paul also earned a Bachelor of Science from Marylhurst University with a degree in Business Management. During this time he also became a Lean Six Sigma-Certified

Green Belt which is a professional process and project improvement program he completed.

When not volunteering for the WCSI, Paul and his wife Gaedin, whom he has known since she was 16, enjoy hiking, playing cribbage and dominoes, having a picnic near the river in Memorial Park and volunteering with his church.

In explaining his decision to give back more to his community Paul explains, "Without Gaedin's support I would not have come to be at a place in my life where community service is a top priority for me."

Did you know...

You can ride the

**SMART**  
SOUTH METRO AREA REGIONAL TRANSIT

bus for free anywhere in  
**Wilsonville!**

For more info, [RideSmart.com](http://RideSmart.com)



## Reports from City Boards and Commissions

### Mayor Delivers State of the City Address, Council Approves Annexation and Zone Change for Villebois and Coffee Creek

On March 7, Mayor Tim Knapp presented the “2016 State of the City Address,” providing an overview of the City’s current activities and future plans. During the presentation, the Mayor shared a vision for creating a complete community where residents can have all of their basic needs met locally and are able to enjoy an exceptional standard of living at any stage of life. The address included a review of notable public and private investments in Wilsonville, plans for future developments, and major transportation projects completed or underway. Mayor Knapp also encouraged active public engagement that creates a stronger sense of community and collaboration among the City, community members, businesses and service organizations. A replay of the State of the City Address is available on the City’s website at [www.ci.wilsonville.or.us/StateoftheCity](http://www.ci.wilsonville.or.us/StateoftheCity).

The Wilsonville City Council held a first reading, public hearing and then approved a pair of related ordinances for an annexation and a Zone Map amendment of a 1.0-acre parcel, located at 11700 SW Tooze Road. The Council also rezoned the parcel from Rural



Mayor Tim Knapp delivering the 2016 ‘State of the City’ Address at the March 7 City Council meeting.

Residential Farm Forest 5 to Village (V) in order to allow the property to be used for a residential use consistent with the adjacent parcels of land that already have the Village zoning designation. The parcel is the last piece of land needed to complete the Villebois neighborhood and is part of a larger previously-approved residential subdivision known as Calais East at Villebois that when developed includes a total of 63-lot residential subdivision to be developed by applicant/homebuilder Polygon Northwest.

During the meeting the Wilsonville City Council held a second reading and gave final approval for a set of related ordinances for an annexation, comprehensive plan amendment and zone change for an 8.7-acre parcel, located on the southwest corner of SW Boones Ferry Road and SW Day Road (see article below for details).

Under the consent agenda the Council authorized the City Manager to enter into a Cooperative Utility Agreement with Oregon Department of Transportation, Tualatin Valley Water District, City of Hillsboro and the Willamette Water Supply Program for the inclusion of a 66-inch water-transmission pipeline in the Kinsman Road project. Also on the consent agenda the City Council approved the City to enter into an agreement with the City’s Urban Renewal Agency and Polygon Northwest that sets forth the terms and conditions applicable to the construction of Regional Park 5, also known as Trocadero Park, in the north section of the Villebois neighborhood.

Community members who desire to watch a replay of all or a portion of the City Council meeting may do on Comcast/Xfinity Ch. 30, Frontier Ch. 32 or on the City’s video-on-demand service at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV).

### City Council Approves North Wilsonville Medical Facility and Solid-Waste Rate Increase

On Feb. 18, the Wilsonville City Council conducted first readings and public hearings on a set of related ordinances for an annexation, comprehensive plan amendment and zone change for an 8.7-acre parcel, located on the southwest corner of SW Boones Ferry Road and SW Day Road. Previously approved by the City’s Development Review Board Panel B in January, applicant Universal Health Service, Inc. plans to construct a 62,000-square-foot behavioral-health treatment facility for a wide range of patients, including veterans. The proposed approximately \$32 million Willamette Valley Behavioral Health Facility, which projects total employment with 190 new jobs, kicks-off development of the long-planned Coffee Creek Industrial Area.

The Council approved a system-wide 4.4 percent aggregate rate-increase for the City’s solid-waste and recycling franchise hauler, Republic Services, Inc., effective March 1. The applicant testified that the rate-change request was prompted by a number of factors, including an 8 percent increase in disposal costs brought about by the collapse in recycled-products market that had subsidized garbage-dumping costs; disposal costs account for 44 percent of all operating expenses by Republic Services. The applicant also cited depreciation costs related to construction of a new maintenance facility and conversion of the collection-truck fleet from diesel-gas fuel to CNG (compressed natural gas) increased. Acting on public comments from fixed-income and senior residents, the City Council modified the proposed rate request to reduce a proposed price increase of the smallest-size residential and commercial carts, lowering the rate to \$20 per month. Council members indicated that the pricing structure also encourages a policy favoring a reduction in the production of household garbage. City staff research found that the solid-waste rates proposed for Wilsonville were in alignment with rates charged throughout the greater metro area.

The Council approved on a 4:0 vote with Mayor Tim Knapp abstaining a resolution modifying the boundary of the Year 2000 Urban Renewal District and moving the proposed \$7 million “Old Town Escape” road project from the Westside Urban Renewal District to the Year 2000 Urban Renewal District. The boundary change reduces the total acreage in the district in order

to free-up acreage needed to create an urban renewal district in the Coffee Creek Industrial Area. The changes in urban renewal plans are consistent with the recommendations from Wilsonville Urban Renewal Strategic Plan adopted by City Council in October 2014 as recommended by a community task force.

The Council also authorized the Mayor to sign an Agreement of Understanding on behalf of the City Council regarding design changes to the Newberg-Dundee Bypass project designed to reduce excess traffic on Wilsonville Road. The agreement includes the Oregon Department of Transportation, the City of Wilsonville, the City of Newberg, Clackamas County, Yamhill County, and the Ladd Hill Neighborhood Association.

Also during the meeting the Council approved City Attorney Barbara Jacobson’s employment contract with a standard one-year term. Mayor Knapp shared with the Council a gift the City received from the Consular Office of Japan that recognizes the Wilsonville-Kitakata Sister City Relationship with Kitakata, Japan.

Mayor Knapp issued a “March for Meals” proclamation in order to recognize the City’s Home-Delivered Meals program, which the City has run for the past 31 years.

Under the consent agenda the Council authorized the Mayor to enter into a Local Agency Agreement with Oregon Department of Transportation that is related to the construction of the Kinsman Road extension project. The Council also approved a resolution that clarifies cost sharing between the City and homebuilder, Polygon NW for certain infrastructure at the Villebois Grande Pointe residential subdivision.



In March Councilor Susie Stevens (left) helped Evie Proctor (right) prepare meals for home delivery to local seniors.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during April. Scheduling changes can and do occur; check for updated meeting information at the City’s website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, April 4:** refunding water bonds; Tourism Promotion Committee: Annual Business Plan; LED lighting; volunteer appreciation proclamation; Arbor Day proclamation; Clackamas 800 Radio Group; Proposed; and a recreation aquatic center presentation.

**Monday, April 18:** Transit Master Plan update; sidewalk repair grant program; sewer lateral repair grant program; road maintenance fee update; Megan McKibben, Office of Congressman Kurt Schrader; Oregon Tech Wilsonville CEO Laura McKinney; Tourism Promotion Committee: Annual Business Plan; Zone Change for 15-lot subdivision; IT Strategic Plan; Refunding water bonds; condemnation on Tooze Road.

### Planning Commission Update

At the Planning Commission meeting on March 9, the Commission reviewed, discussed and provided input on amendments to the Transportation Systems Plan, Phase II of the Frog Pond Area Plan; Basalt Creek Concept Plan; and the 2015 Annual Housing Report.

### Community Enhancement Committee

The Wilsonville-Metro Community Enhancement Committee plans two meetings in April to review process and project nominations for FY2016-17 Community Enhancement Program. Meetings are being held on Tuesday, April 19, and Tuesday, April 26, 6 - 7:30 pm, in the Wilsonville City Hall Council Chambers, 29799 SW Town Center Loop East.

### Development Review Board Update

The regularly scheduled DRB Panel B meeting for Monday February 22, was cancelled. The regularly scheduled DRB Panel A meeting for Monday, March 14, was also cancelled. The next DRB Panel A meeting is scheduled for Monday, April 11.

### City Accepting Applications for a New Library Board Member

The City of Wilsonville is now accepting applications for a new Library Board member for a term that begins on July 1.

The Board supports and assists the Library Director in all library planning, and advises the City of budgetary and policy considerations relating to the Library’s operation and development. Board members assist in monitoring library operations and service to the public; encouraging cost effectiveness; and periodically reviewing the Library’s policies, procedures, and the Library’s goals and objectives.

Persons interested in serving on the Library Board may pick up an application from Sandra King, City Recorder, at City Hall, 29799 SW Town Center Loop in Wilsonville or may obtain an application online at [www.ci.wilsonville.or.us/apply](http://www.ci.wilsonville.or.us/apply). The application deadline is Friday, April 29, 2016. For more information contact Sandra King, City Recorder, at 503-570-1506; [king@ci.Wilsonville.or.us](mailto:king@ci.Wilsonville.or.us).

### February Library Board Report

During the Library Board meeting in February various reports included:

- A video game workshop was held and taught teens the tools and basics of video game design.
  - The Book Notes concert series drew 60 - 70 people to the February performance by the Portland Opera.
  - In March a medieval music group performed using period instruments.
  - Alan Steiger and Carol Hanna provided free tax help to 12 - 14 patrons a week in February and March.
  - In February, the History Pub hosted an internment camp survivor.
  - In February, Food for Fines generated substantial donations for Wilsonville Community Sharing.
  - In September Radio Frequency Identification (RFID) is coming to the library speed up check outs and improved security for important items.
- The next regular meeting is scheduled for Wednesday, April 27, 6:30 pm, at the Library.

## Five-Year Forecast Looks at the City's Operating Accounts

In February, the Budget Committee heard a report from the City's Finance Director regarding the five-year financial forecast for the City's nine main operating funds. These funds include: the General Fund, Community Development, Building, Transit, Water Operating, Sewer Operating, Stormwater, Street Lighting and the Road Operating Fund.

In general, the City's operating funds are in a strong position and are maintaining healthy financial reserves. The presentation to the Budget Committee in February however, identified particular challenges and opportunities for the City's General Fund, the Transit Fund and the Community Development Fund. These three funds may have to rely on reserves in the future to keep up with service demands.

The General Fund is comprised of property taxes and pays for services such as the Library, Parks and Police. Over the next five years, the City faces challenges in providing services and implementing technological tools to keep up with the City's growth. The forecast demonstrates that the City's reserves may have to be tapped in order to pay for on-going costs.

The City's South Metro Area Regional Transit (SMART) is actively exploring options to avoid using reserves for operations over the next five years. SMART relies on a payroll tax to fund approximately 70% of the transit agency's operation, the rest is fund-

ed by grants and fare revenue. Because overall payroll growth in the region is projected to be relatively flat in the near future, the payroll tax revenue is not keeping up with the cost to operate the system. Even with the addition of new jobs, an adjustment to the payroll tax may be necessary in order to ensure continued operation at current levels.

The Community Development Fund is responsible for land use, planning, engineering and administering the City's capital infrastructure, and code enforcement. This fund is struggling to avoid a deficit, and the City has engaged consulting services to review the fund's activities, fees and charges, with the goal of placing the fund on a firm financial foundation into the future.

The Street Lighting Fund is responsible for the electricity and maintenance of the City's street lights. Over the next five-years, the City is exploring retro-fitting fixtures with light-emitting diode (LED) bulbs using resources from the fund and in coordination with Portland General Electric.

The Road Operating Fund is funded by the gas tax and is used for street maintenance and repair, maintenance of sidewalks, pavement markings, signs and signals. Over the next five years, gas tax receipts are shown to grow very slowly, as people drive less and purchase more fuel efficient vehicles.

The Five-Year Forecast document is available online at [www.ci.wilsonville.or.us/173/Financial-Forecasting](http://www.ci.wilsonville.or.us/173/Financial-Forecasting).

The Budget Committee is scheduled to hear the City Manager's proposed budget for Fiscal Year 2016-17 on May 18, June 1 and if necessary, on June 8. These meetings are open to the public and public comment is encouraged. Proposed budget documents are expected to be available to the public on May 9 and will be posted online at [www.ci.wilsonville.or.us/budget](http://www.ci.wilsonville.or.us/budget).

### Budget Committee Meetings

- Weds, May 18, 6 - 10 pm
  - Weds, June 1, 6 - 10 pm
  - Weds, June 8, 6 - 10 pm
- City Hall, 29799 SW Town Center Loop East

*In general, the City's operating funds are in a strong position and are maintaining healthy financial reserves.*

## WHS Hosts Public University and Community College Night

The Wilsonville High School Counseling Department is hosting a Oregon Public University and Community College Night on Wednesday, April 6, 6:30 pm in the WHS Library and Pointe. Eight Oregon public uni-

versities and several local community colleges are planning to attend the college fair event. The evening begins with an overview and introduction from the universities, followed by an open house where attendees can talk to representatives of any or all of the schools. High school juniors who are just starting their search or planning on attending an Oregon university, as well as seniors looking at community colleges, should plan on attending this event. For more information contact Lyndi Tucker, WHS College & Career Center Coordinator, [tuckerl@wlwv.k12.or.us](mailto:tuckerl@wlwv.k12.or.us).

## Long-Range Planning Projects

### Basalt Creek Concept Plan

The Basalt Creek Concept Plan guides future development of the Basalt Creek area, an 800-acre unincorporated, rural area between Wilsonville and Tualatin. Both the Wilsonville and Tualatin City Councils' are considering resolutions at upcoming meetings to acknowledge the discussion and



agreements from the December 16, 2015, joint Council Meeting. The Wilsonville City Council is scheduled to hear the matter on March 21.

In order to provide the public another opportunity to learn about and provide input on the preferred alternative for the Basalt Creek Con-

cept Plan, the public is invited to attend an Open House on Thursday, April 28, 5:30 pm. The open house is being held in Tualatin at the Juanita Pohl Center, 8513 SW Tualatin Road.

For more information visit [www.BasaltCreek.com](http://www.BasaltCreek.com) or contact Miranda Bateschell, Long-Range Planning Manager, at 503-570-1581; [Bateschell@ci.wilsonville.or.us](mailto:Bateschell@ci.wilsonville.or.us).

The Planning Department has created a Town Center project web page which will continue to be updated with project information at [www.ci.wilsonville.or.us/TownCenter](http://www.ci.wilsonville.or.us/TownCenter).

### Town Center Master Plan

Last fall, the Metro Council awarded the City of Wilsonville a \$320,000 grant to fund the majority of costs for Wilsonville's Town Center Master Plan proposal. The City is matching the grant with \$100,000 contribution to cover the long-range plan's total anticipated cost of \$420,000.

This project seeks to create a long-range plan and near-term actions for how the City's Town Center area can better serve the interests and needs of residents, workers and visitors. The plan will develop strategies for how the Town Center area can evolve into a more walkable, attractive and commercially vibrant, mixed-use district capable of supporting a range of businesses.

The Planning Department has created a Town Center project web page which will continue to be updated with project information at [www.ci.wilsonville.or.us/TownCenter](http://www.ci.wilsonville.or.us/TownCenter).

For more information Miranda Bateschell, Long-Range Planning Manager, at 503-570-1581; [bateschell@ci.wilsonville.or.us](mailto:bateschell@ci.wilsonville.or.us).

## Garden Club Holds Annual Plant Sale on Saturday, May 7

The Wilsonville Garden Club is holding an annual plant sale on Saturday, May 7, at the Wilsonville Public Library, 8200 SW Wilsonville Road.

**Wilsonville Garden Club Annual Plant Sale**  
• Sat, May 7, 9 am  
Wilsonville Public Library  
8200 SW Wilsonville Road

The club's sale offers large selection of the very popular sun and shade hanging baskets, blooming annuals, perennials plus cuttings and divisions from club member's gardens and an assortment of Garden

Gallery yard art.

All of the proceeds from the event go to support local horticulture education and scholarships.



## New Car Dealership in North Wilsonville Launches Community Art Exhibition to Support Local Artists

In April, a new car dealership in north Wilsonville, Findlay Chrysler Jeep Dodge Ram of Wilsonville, is debuting the Findlay Community Art Exhibition on Friday, April 1.

"Findlay of Wilsonville embraces the art and culture that Portland and its surrounding suburbs have to offer, and we plan to share it with our customers," stated Laurie Tarter, Findlay Marketing Director.

The Findlay Community Art Exhibition hosts talented local artists by offering exhibition space in the dealership's main lobby to showcase local artists. Each month a new artist is selected to feature their art for approximately three to four weeks. Exhibitions begin with a gallery reception that includes refreshments and a chance to meet the artist.

"We are pleased to bring this dynamic new art event to the community," said Bruce Schneider, Managing Partner. The goal of the exhibition is to celebrate and

showcase the many talented artists who live here and to offer community members a place to enjoy great local art with their families. We encourage people of all ages to come enjoy this unique event and support area artists."

For the inaugural exhibition, Findlay of Wilsonville is partnering with the "Wandering Arts" series which is a program coordinated by the Tualatin Chamber Arts Committee to select the first artist to be featured at the monthly exhibition.

April's featured artist is photographer, Brian Belefanti of Portland. Brian photographs landscapes and environmental portraits utilizing a camera filtration system that involves shooting through liquids that are relevant to the subject being photographed. His work has taken him to Cuba, Montreal, the American South, Mexico, Spain, Nova Scotia, Portugal and Turkey. In 2006, he was named Photographer of the Year by the International Color Awards and has received numerous honors at the 2004 International Photography Awards.

The exhibition runs April 1 - April 30 and is open Monday - Saturday, 10 am - 7 pm, and on Sundays, 11 am - 5 pm. The general public is invited to attend the free Meet the Artist Reception on Thursday, April 14, 7 pm, at the new indoor car dealership located at 25600 SW Parkway Center Drive in Wilsonville.

Artists interested in learning more about the featured artists program should contact Laurie Tarter, Findlay Marketing Director, at 503-563-7222; [ltarter@findalysauto.com](mailto:ltarter@findalysauto.com).



In April the Findlay car dealership is featuring art from photographer, Brian Belefanti of Portland shown here.

## US Bank Hosts Document Shredding Day on Saturday, April 16

Community members are invited to bring up to 10 boxes of documents to a free and secure community shredding event sponsored by U.S. Bank, the City and the Wood Middle School. The event is being held at the U.S. Bank Wilsonville Branch's parking lot, 29112 SW Town Center Loop West on Saturday, April 16, 9 am - noon.

**US Bank Shredding Event**  
 • Sat, April 16, 9 am - 12 pm  
 US Bank Wilsonville Branch,  
 29112 SW Town Center Loop W

Community members should bring and shred documents that contain sensitive and confidential information in order prevent identity theft and other fraud.

Student volunteers from Wood Middle School will help unload and prepare documents to be shredded by a Recall Mobile Shredding Truck. The event is free, but

participating community members are encouraged to make a financial contribution to a Wood Middle School program that teaches seventh graders about leadership and community involvement.



US Bank staff greet customers and help unload documents.

## Rite Aid Distribution Center Hosts 10th Annual Plant Sale

On Friday, April 22 and Saturday, April 23, 9 am - 5 pm, the Rite Aid Distribution Center in Wilsonville is hosting the 10th Annual Plant Sale and

**Rite Aid Plant Sale/Fundraiser**  
 • Fri - Sat, April 22 - 23, 9 am - 5 pm  
 Rite Aid Distribution Center  
 29555 SW Boones Ferry Road

Fundraiser to benefit the Children's Miracle Network Hospitals —

a non-profit organization that supports Doernbecher's Children's Hospital.

The sale takes place at Rite Aid's Distribution Center's parking lot located at 29555 SW Boones Ferry and is open to the public.

The plant sale includes most major gardening categories, including trees, potted plants, shrubs, annuals, bulbs, perennials, ground cover and garden art do-

nated by local nurseries. Proceeds from the sales and rentals are donated to the Children's Miracle Network Hospitals.



Shoppers at last year's Rite Aid Annual Plant Sale.

Local vendors are also invited to participate in the event by renting a booth. For more information contact Ricki Hackworth, event organizer, at (503) 685-6069; rhackworth@riteaid.com.

## Jr. Scoop Hosts \$1 Clothing Sale on Saturday, April 23

On Saturday, April 23, 8 am - 2 pm, the Jr. Scoop Club at Wood Middle School is hosting the third annual \$1 Clothing Sale in the school's gym located at 11055 SW Wilsonville Road. Proceeds

**Jr Scoop \$1 Clothing Sale**  
 • Sat, April 23, 8 am - 2 pm  
 Wood Middle School Gym  
 11055 SW Wilsonville Road

from the annual fundraising event benefit Doernbecher's Hospital as well as provide scholar-

ships to send Wood students to Outdoor School. During the sale every item of clothing costs only \$1 and

features clothes and styles for all ages and sizes. In addition, the club is also selling their premium ice cream for \$3 a scoop on homemade waffle cones. To learn more about Jr. Scoop visit www.JrScoop.com



## Bulky Waste Day Set for May 7

In partnership with Metro and Republic Services, the City of Wilsonville is holding a Bulky Waste Day on Saturday, May 7, 10 am - 2 pm, at 10295 SW Ridder Road in Wilsonville.

**Bulky Waste Day**  
 • Sat, May 7, 10 am - 2 pm  
 Republic Services  
 10295 SW Ridder Road

Community members, with proof of Wilsonville residency and toiletries to donate, are invited to drop off large, bulky items that don't fit into standard-sized garbage cans.

The types of waste that **will be accepted** include: dishwashers, televisions, refrigerators, computers, monitors, stoves, dryers, water heaters, couches, mattresses, scrap metal, tables and chairs

The following are types of items **will not be accepted**: propane bottles/canisters; paint; batteries; solvents;

thinners; household garbage; tires; and other related items.

The event is free, but participating households will be asked to donate toiletries such as shampoo, soap and deodorant to benefit Wilsonville Community Sharing's Food Bank and the low-income households they serve.



For more information contact Matt Baker at mbaker@ci.wilsonville.or.us; 503-570-1548.

# City Calendar

| APRIL    |   |
|----------|---|
| 2 Sat    | • Volunteer Tree Planting Event 9 am - 1 pm, Tranquil Park  |
| 4 Mon    | • City Council, 5 pm, Work Session 7 pm Meeting, City Hall  |
| 5 Tue    | • Municipal/Traffic Court, 5:30 pm City Hall  |
| 11 Mon   | • DRB Panel A, 6:30 pm, City Hall   |
| 13 Wed   | • Planning Commission 6 pm, City Hall<br>• Wilsonville Community Seniors Inc. Advisory Board, 6 pm, Wilsonville Community Center                  |
| 14 Thurs | • Parks and Recreation Advisory Board Meeting, 4:00 pm, Parks and Recreation Admin Building   |
| 16 Sat   | • US Bank Document Shredding 9 am - 12 pm, US Bank, 29122 SW Town Center Loop West<br>• Spa Day, 10 am - 1 pm, Wilsonville Community Center       |
| 18 Mon   | • City Council, 5 pm, Work Session 7 pm Meeting, City Hall  |
| 19 Tue   | • Municipal/Traffic Court 5:30 pm, City Hall<br>• Community Enhancement Committee 6 - 7:30 pm, City Hall  |
| 21 Thurs | • Wilsonville Leadership Academy 6 - 9 pm, City Hall  |
| 25 Mon   | • Coffee Creek Urban Renewal District Open House, 5:30 - 6:30 pm, Task Force Meeting, 6:30 - 8 pm, City Hall<br>• DRB Panel B, 6:30 pm, City Hall |
| 26 Tue   | • Community Enhancement Committee 6- 7:30 pm, City Hall   |
| 27 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library  |
| 28 Thurs | • Basalt Creek, Joint Tualatin-Wilsonville Open House, 5:30 pm, Juanita Pohl Center, 8513 SW Tualatin Rd, Tualatin                                |
| MAY      |   |
| 2 Mon    | • City Council, 5 pm - Work Session 7 pm - Meeting, City Hall   |
| 3 Tue    | • Municipal/Traffic Court, 5 pm, City Hall  |
| 7 Sat    | • Bulky Waste Day 10 am- 2 pm, Republic Services  |
| 9 Mon    | • DRB Panel A, 6:30 pm, City Hall   |
| 11 Wed   | • Planning Commission 6 pm, City Hall<br>• Wilsonville Community Seniors Inc. 6 pm, Community Center  |
| 14 Sat   | • WERK Day, 8 am - 1 pm, Wilsonville Community Center   |
| 16 Mon   | • City Council, 5 pm - Work Session 7 pm - Meeting, City Hall   |
| 17 Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |
| 18 Wed   | • Budget Community Meeting 6 - 9 pm City Hall   |
| 19 Thurs | • Wilsonville Leadership Academy 6 - 9 pm, City Hall  |
| 21 Sat   | • Queen of the High Road Half Marathon and 10K, 8 am - 3 pm, Wilsonville Memorial Park River Shelter  |
| 23 Mon   | • DRB Panel B, 6:30 pm, City Hall   |
| 25 Wed   | • Library Board 6:30 pm, Wilsonville Public Library   |
| 30 Mon   | • Memorial Day Holiday- All City Offices CLOSED   |

All dates and times are tentative; check the City's online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and replayed periodically. Meetings are also available to stream live and by video-on-demand online at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at all City Council meetings.



**Tim Knapp**  
Mayor  
[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



**Scott Starr**  
City Council President  
[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



**Julie Fitzgerald**  
City Councilor  
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**Susie Stevens**  
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**Charlotte Lehan**  
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### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)



### Wilsonville City Hall

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)

# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

February 2016

## 2015 New Single-Family Home Construction Shatters Record-High For Third Year in a Row

The City of Wilsonville experienced another record-high level of single-family new home construction in 2015. Both the number of single-family building permits issued and the value of residential construction are Wilsonville records for the third consecutive year.

In 2015 the City issued a total of 316 new single-family home building permits with a valuation of \$79.0 million, representing more than a threefold increase over the average number of building permits issued annually over the past 10 years.

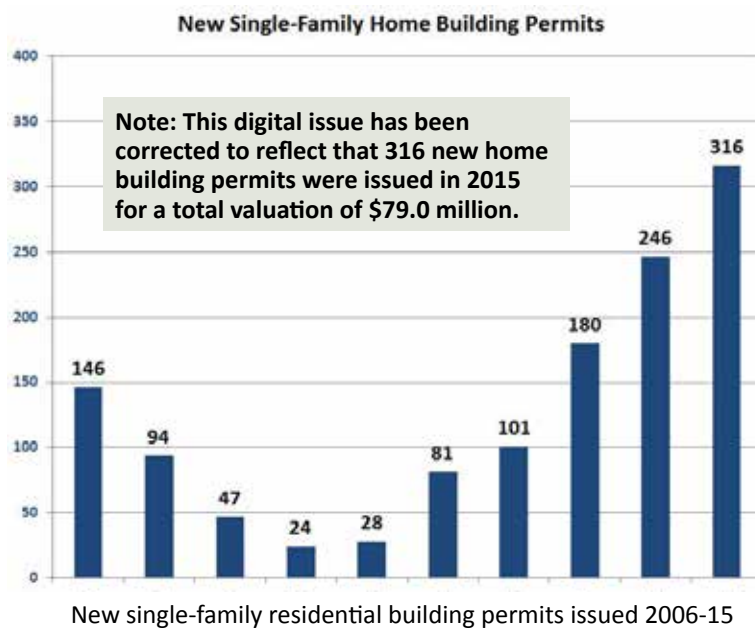
Mayor Tim Knapp observed that, "In just the past two years, 562 new single-family homes with a value of over \$131 million are coming online. These figures show that Wilsonville continues to be an attractive city to buy a new home and raise a family."

Community Development Director Nancy Kraushaar said, "Our building division staff are keeping up with the record number of plan reviews and building inspections, while maintaining our rigorous building standards."

Other 2015 residential building-permit data shows that four new multi-family residential permits were issued for 15 residential units valued at \$2.0 million.

A majority of new home-building is located in the Villebois urban village development where home build-

ers Polygon Northwest, Lennar, Legend Homes and Renaissance Homes are most active.



In 2015, 124 permits for commercial/industrial/multi-family alterations or improvements to existing buildings with a valuation of \$8.2 million were also issued. One new commercial structure is a \$8.3 million Subaru dealership, located south of the Fred Meyer in the Old Town neighborhood, that is now under construction.

### Mayor's Message

#### Strategic Infrastructure Improvements Concurrent with Growth Are Crucial for Livability

For the third consecutive year Wilsonville experienced a record level of single-family home construction. Concurrent with new development, the City continues to advance a range of infrastructure projects—mainly around transportation—designed to accommodate our growing community.



The City's "concurrency" policy permits major new developments only when a funding plan is in place to increase the capacity of key public infrastructure such as roads and sidewalks, water utility services, storm-water systems, etc. Developers construct some public improvements and also pay System Development Charges (SDCs) to offset new impacts on infrastructure by contributing funds to build new roads, utility lines and other infrastructure components. In this way, new construction helps to pay its own way, rather than rely on existing rate-payers, by underwriting improvements to public infrastructure.

Access to SDC fees, coupled with local urban-renewal proceeds, provide "match monies" that facilitate the City's ability to leverage funds from state and federal governments. State/federal government grant programs are increasingly requiring larger "up-front" local matching funds to qualify for a grant.

Some City road projects that improve mobility and transportation options include:

- **Barber St. extension and bridge** that connects Villebois to SMART/WES Transit Center and provides an alternate route used \$100,000 SDCs, \$2.5 million in local urban renewal dollars and \$3.8 million in federal and state grants.
- **Grahams Ferry Road improvements** just south of Villebois includes \$67,000 in SDCs and \$448,000 in direct developer construction. Improvements near and north of Villebois completed in July utilized \$215,000 of SDC funds.
- **Kinsman Road extension** from Barber Street to Boeckman Road starts this spring and is funded in part with \$2.6 million in SDCs that helped to leverage \$3.6 million of federal and state grants.
- **Tooze Road widening** between Villebois Drive and Grahams Ferry Road uses \$2 million in SDCs and \$800,000 in federal funds.
- The new **Advance Road Middle School**, which requires \$5 million in street and utility improvements to be built by the School District, will benefit with a City contribution of \$2.8 million in SDC funds.

Working together with the private-sector and state and federal partners, Wilsonville continues to carefully coordinate construction of needed infrastructure in pace with growth demands.

*Tim Knapp, Mayor*

## Garbage and Recycling Hauler Requests Rate Increase

On Thursday, Feb. 18, the City Council plans to hold a public hearing on a rate increase request by solid-waste hauler, Republic Services, Inc. The company is requesting a system-wide average rate increase of 4.4 percent effective Feb. 1.

Community members are invited to attend the public hearing and to provide feedback on the proposed rate increase either at, or prior to, the hearing. After the hearing and public testimony, the City Council is expected to discuss and decide what level of rate increase is appropriate and also when the rate change is to be effective. The accompanying chart shows the current and proposed monthly rates for Republic Services' most popular types

of service. The fees listed below include current and proposed rates for all three carts for garbage, recycling and yard debris for both residential and commercial customers. Community members who are not able to attend the meeting may provide feedback and or learn more about the requested rate increase at [www.ci.wilsonville.or.us/RateRequest](http://www.ci.wilsonville.or.us/RateRequest).

| Monthly Residential Rates | Current Rates | Proposed Rates | \$ Amount Difference | Percent Difference |
|---------------------------|---------------|----------------|----------------------|--------------------|
| 20 gallon                 | \$21.84       | \$23.00        | \$1.16               | 5.3%               |
| 35 gallon                 | \$24.54       | \$25.89        | \$1.35               | 5.5%               |
| 65 gallon                 | \$32.34       | \$34.12        | \$1.78               | 5.5%               |

| Monthly Commercial Rates | Current Rates | Proposed Rates | \$ Amount Difference | Percent Difference |
|--------------------------|---------------|----------------|----------------------|--------------------|
| 35 gallon                | \$16.48       | \$25.89        | \$9.41               | 57.1%              |
| 90 gallon                | \$32.97       | \$39.25        | \$6.28               | 19.0%              |

## Xerox and Wilsonville Holiday Inn Donate Generously to Community

The start of the new year has been great for corporate giving in Wilsonville. Xerox, through the company's Community Involvement Program (CIP), donated \$5,000 to replace nine-year old publicly-used computers in the Computer Lab at the Wilsonville Community Center (see article on pg. 4).

On New Year's Eve the Wilsonville Holiday Inn held a Monte Carlo Night which raised \$4,215 for the Wilsonville Library Foundation (WLF) to support programs at the public library.

This is the second year the hotel held the fundraiser. Last year's event raised \$1,700 to support Wilsonville Community Sharing, the local food bank and emergency assistance provider.

The hotel is planning another New Year's Eve fundraiser in 2016.



Xerox donates \$5,000 to replace public computers at the Community Center (left to right): Recreation Program Manager Patty Brescia, Chair of Wilsonville Community Seniors, Inc. Wes Morris, Xerox CIP Coordinator Diane Adams, Xerox CIP Coordinator Garry Jones and retired Xerox employee Dick Spence.



Wilsonville Holiday Inn donated \$4,215 to the Wilsonville Library Foundation (left to right): Wilsonville Library Foundation (WLF) board member Dick Spence, WLF Vice Chair Hilly Alexander, Holiday Inn Director of Sales Nichole Cooper, Wilsonville Holiday Inn General Manager Jeff Brown and WLF board member Joe Briouillette.

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City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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## Art Community Seeks Volunteers to Plan June 2016 Festival

Local art advocates are seeking volunteers to help plan the 17<sup>th</sup> Annual Wilsonville Festival of Arts that is scheduled to be held June 4 - 5, 2016, in Wilsonville's Town Center Park.

The long-running festival is organized and planned by the Wilsonville Arts & Culture Council, local high school art teachers and other community volunteers who attend monthly planning meetings. Interested community members and art supporters are encouraged to get involved by contacting Theonie Gilmore, Wilsonville Arts & Culture Council Executive Director, at 503-638-6933.

Local art advocate Theonie Gilmore states, "Our community's long tradition of hosting an annual art festival demonstrates how much Wilsonville appreciates art and with the help of a few more passionate people, this year's festival is going to be another great event for our community."

The annual event is a free art festival where community members and visitors tour art tents, meet

authors, listen to live music and enjoy wine, beer and food from local vendors. In 2015, the festival included



Wilsonville High School artist Jacob Dyer's collage shown at last year's art festival.

original art from 45 professional artists who showcased paintings, drawings, jewelry, literature and more. Last year's festival also included art from 200 local students. The Authors Pavilion featured 11 local authors, who represented a wide variety of genres, discussing writing and selling their books. Other annual highlights also include both bike and historic trolley tours of art installations around Wilsonville.

Last year's festival was supported by local grants from the City of Wilsonville and Clackamas County, as well as, generous donations from Fred Meyer, Argyle Square, The Laue Team, US Bank, and Columbia Bank. Event organizers are currently seeking new sponsors for this year's upcoming event.

## Clackamas County Libraries Announce Teen Art Contest

The Libraries in Clackamas County (LINCC) are inviting interested teens to submit original artwork for a Teen Summer Reading Artwork contest with the theme of "Wellness, Fitness and Sports."

The contest submissions are welcome until Feb. 13. Artists must be between 12-18 years old at the time of submission, create artwork representing the theme, and include the slogan of "Get in the Game: Read." Entries should be in color with no text other than the slogan.

"We're thrilled to offer a teen in Clackamas County the opportunity to have their artwork showcased during our Summer Reading Program in 2016," said Anna Bruce, Librarian at the Happy Valley Public Library. "I know we have many talented artists out in our communities and we look forward to seeing their creativity and designs related to our theme."

A prize will be awarded for first place. Honorable mentions may also be recognized. The winning design will be used by LINCC throughout the county for all summer reading program materials such as posters, forms and flyers.

For more information, contest rules and an entry form, go to [www.lincc.org](http://www.lincc.org) and click on "Library Events." You can also obtain an entry form by visiting any of the 13 libraries in the county.

For more information, contact LINCC Member Services Librarian Kathryn Kohl at 503-723-4915 or [kkohl@lincc.org](mailto:kkohl@lincc.org).

### "Oregon Promise" Provides Grants for Community College

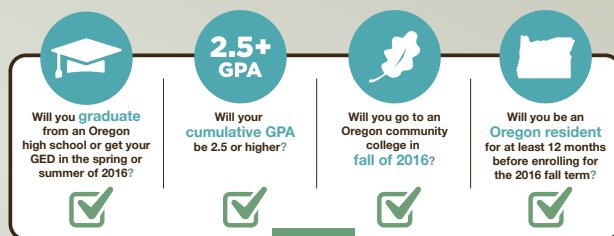
The "Oregon Promise", passed by the Oregon Legislature and signed by Governor Kate Brown in July 2015, provides a \$10M appropriation targeted toward tuition grants for students who graduated (or the equivalent) from an Oregon high school no more than six months prior to attending and pursuing a certificate or degree at one of Oregon's 17 community colleges.

The grant program provides a minimum of \$1,000 for each full-time community college student.

Grants are to be available for the fall term of the 2016-17 academic year. **The deadline to apply is March 1.** For more information and to apply, go to

[www.OregonPromise.org](http://www.OregonPromise.org).

### Can you qualify for Oregon's new community college grant program?



**Oregon Promise**

New Oregon program could help you with tuition or costs. Apply between **Nov. 1, 2015 and March 1, 2016.** [www.OregonPromise.org](http://www.OregonPromise.org)

Students that don't qualify are encouraged to visit [www.OregonStudentAid.gov](http://www.OregonStudentAid.gov) to explore other financial aid options.

Administered by the HECC Office of Student Access and Completion.

HIGHER EDUCATION COORDINATING COMMISSION

## Be the First to Know!

Sign up for City eNotify (e-news):

[www.Ci.Wilsonville.or.us/enews](http://www.Ci.Wilsonville.or.us/enews)

Follow the City on Twitter:

@WilsonvilleOR

Like the City's Facebook page:

[www.Facebook.com/CityofWilsonville](http://www.Facebook.com/CityofWilsonville)



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

## Food for Fines in February

Reduce or eliminate library overdue fines by bringing in cans or boxes of food during the month of February.

Each can or box of unexpired food will be worth a one dollar credit on overdue fines, subject to the approval of library staff.

All food will be donated to Wilsonville Community Sharing for the assistance of needy area families.

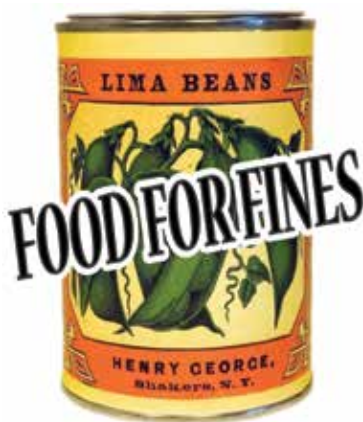
Please check expiration dates carefully! The food bank cannot accept or use expired food.

The food bank prefers donations of:

- Breakfast cereals
- Canned fruit
- No ramen

For more information about Wilsonville Community Sharing, please visit their website:

[www.WilsonvilleCommunitySharing.org](http://www.WilsonvilleCommunitySharing.org)



## New Children's Program: The Zone

In February the library debuts a new after school event: **The Zone** for Grades 3, 4, and 5.

The new event is an hour full of fun, creative, hands-on activities for kids in the upper half of the primary school grades. The event features various "Zones" that have different engaging activities to explore.

In the Lego Zone, we'll photograph cool creations and post them in the library. Test your skill and accuracy with target practice in the Nerf Zone. We'll also have a Game Zone and a Craft Zone, as well as a display of great new books for grades 3 through 5, hand-picked by the children's librarians.

Try as many Zones as you want, and don't forget to enter the prize drawing for a new hardcover book!

The Zone will be held in the Oak Room on Wednesday, Feb. 23, from 4 - 5 pm.



## Adult Winter Reading Program Ends Feb. 29

Just four weeks left in our 5th annual Adult Winter Reading Program "In Cold Blood."

Read three books and qualify for entry into the Grand Prize Drawing for a Kindle Fire HD8 Tablet or a "Dinner and a Movie" (both prizes valued at \$150). Choose your books to fill the Book Log or the categories of the Tic-Tac-Toe, and choose the prize.

Pick up forms at the library, or download them from our website:

[www.WilsonvilleLibrary.org/AWRP](http://www.WilsonvilleLibrary.org/AWRP)

## Upcoming Classes and Events

### \*Library Closed\*

Presidents Day  
Monday, Feb. 15

### First Friday Film

Friday, Feb. 5, 6 - 8  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

### Book Club

*Behind the Beautiful Forevers*  
by Katherine Boo  
Thursday, Feb. 11, 6 - 8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

### Great Books Discussion Group

*Hamlet* by William Shakespeare  
Tuesday, Feb. 16, 6 - 8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

### K-2 Book Adventures

*Dr. Seuss Celebration*  
Thursday, Feb. 18, 4 - 4:45 pm  
[www.WilsonvilleLibrary.org/k2](http://www.WilsonvilleLibrary.org/k2)

### Genealogy Club

Monday, Feb. 22, 1 - 2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

### Game Night

Wednesday, Feb. 24, 6 - 8 pm  
[www.WilsonvilleLibrary.org/games](http://www.WilsonvilleLibrary.org/games)

### History Pub at Wilsonville McMenamins

Tuesday, Feb. 23, 6:30 - 8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)



Did you know...  
You can ride the  
**SMART**  
SOUTH METRO AREA REGIONAL TRANSIT  
bus for free anywhere in  
**Wilsonville!**  
For more info, see [RideSmart.com](http://RideSmart.com)

**SMART Launches Transit Master Plan Update**

The City of Wilsonville's update to the Transit Master Plan is underway. The purpose of the plan is to guide the City's public transit agency, South Metro Area Regional Transit (SMART), over the next five years.

Much has changed in Wilsonville since the Transit Master Plan was last updated in 2007:

- SMART now serves more of Wilsonville with a higher level of service.
- Commuters have enhanced transit connections to rail for improved regional travel.
- A new administrative and fleet facility on Boberg Road opened.
- SMART Transit Center and WES Rail Station on Barber Road also opened providing riders better connectivity.
- A fleet of alternative fuel buses is in service.
- A new Compressed Natural Gas (CNG) station opened for more environmentally-sound service.

This update to the Transit Master Plan addresses the following two key questions:

**How should the City prioritize available transit resources?**

**What improvements are needed in the next five years?**

Wilsonville is projected to grow over the next 20 years. This means SMART and the City Council need to make some strategic choices about future transit investments.

SMART seeks community member input on how SMART adapts to:

- A growing community.
- An aging population.
- An increase in commuters who are traveling daily to jobs throughout the region.
- Addressing the state and region's Climate Smart goals.

Beginning in March, the first of two community-wide surveys is to be available to inform the planning process. In the meantime, please visit the project's website to learn more about the process and sign up for updates on upcoming opportunities to help shape Wilsonville's SMART services.

[www.PlanSMARTWilsonville.com](http://www.PlanSMARTWilsonville.com)



SMART offers commuters bus service to and from Salem.



SMART offers pre-scheduled Dial-a-Ride services to help seniors get to and from medical trips.

Think SMART. Ride SMART. Plan SMART.



SOUTH METRO AREA REGIONAL TRANSIT  
29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

**Improved Service in Villebois**

On Monday, Feb. 1, SMART begins offering hourly bus service from Charleston and Rain Garden locations in Villebois to three shopping centers along Wilsonville Road. The new service, which is free and open to the public, is offered weekdays, Monday - Friday, 9 am - 4 pm.



**Passenger Appreciation Week**

Public transportation would not need to exist without passengers and we would like to take a moment and thank everyone for riding on SMART. During the week of March 14, bus drivers will be handing out little tokens of our appreciation as a simple and sincere way to say 'thank you' for riding on Wilsonville's public transit system.

If you are new to the Wilsonville area or have been thinking about how ride the bus around town, please visit our website, [www.RideSMART.com](http://www.RideSMART.com), or give us a call and we will help you plan your trip to get you on your way.



**Bike Performance Check-up**

March is a great time of year to give your bike an easy 7-step Do-It-Yourself tune-up so you're ready to roll when the nice weather hits.

- 1) Clean your bike.
- 2) Check brake levers, cables and pads.
- 3) Lube the chain.
- 4) Tighten any loose bolts and screws.
- 5) Check your helmet for any cracks or excessive wear.
- 6) Replace your light batteries.

For more tips and area biking information visit

[Ridesmart.com/BikeSmart](http://Ridesmart.com/BikeSmart)



## City Accepting Tourism and Opportunity Grant Applications

The City of Wilsonville's Parks and Recreation Department is now accepting applications for the Community Tourism and the Community Opportunity Grant Programs. There are separate deadlines for the two grant programs, both of which are awarded by the Parks and Recreation Advisory Board.

The **Community Tourism Grant Program** awards grants to organizations that produce projects, programs or events that promote local business and tourism, and for festivals and special events for the benefit of the Wilsonville community. The deadline for the

Tourism Grant is Friday, Feb. 5. Funding for the program comes from the City's transient room taxes that are assessed on overnight lodging visitors.

The **Community Opportunity Grant Program**, with funds distributed twice per year for a total of \$25,000, provides funding for a wide range of local programs and projects in Wilsonville. Funds for the Op-

portunity Grant program come from the City's general fund.

Successful applications for the Community Opportunity Grant program include proposals aimed at promoting education, diversity, arts or community involvement in the city. Applications are due Friday, March 4.

For more information, go online to [www.WilsonvilleParksandRec.com/GrantPrograms](http://www.WilsonvilleParksandRec.com/GrantPrograms); or contact Brian Stevenson, Recreation Coordinator, at 503-570-1523, [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

**Community Tourism Grant Due Date**  
 • Fri, Feb 5, 5 pm  
 Parks and Recreation Office,  
 29600 SW Park Place

**Community Opportunity Grant Due Date**  
 • Fri, March 4, 5 pm  
 Parks and Recreation Office,  
 29600 SW Park Place

## In Case of an Emergency — Resolve to be Ready in 2016

Roughly half of all Americans make New Year's resolutions and commit to improving their lifestyles or reaching a long-term goal. This year, why not make a resolution that is easy to keep – and can save lives and protect property? For 2016, Resolve to be Ready for emergencies by taking simple steps to prepare your family, your home, your business and your community in the face of potential disaster.

**Resolve to be Ready**



[www.Ready.gov](http://www.Ready.gov)

- **Be informed.** Know the hazards and risks in your area. And learn what you need to do to get ready for them.
- **Make a family emergency plan**, so you know how you would communicate with and find your loved ones if a disaster hits. For example, think about how you would reach your kids at school or your spouse

- at work. If you had to evacuate, where would you go. Think this through in advance.
- **Build an emergency supply kit** – both at home and in the car – that includes water, food and first aid supplies to help you survive if you lose power or get stranded in your car. This is especially important for dealing with icy roads and snowstorms this winter.
- **Get Involved** - Be an advocate and educator for safety and emergency preparedness within your community. Now is the time to think about the basic needs you, your family and your pets will have in advance of an emergency; how will you communicate, what supplies you should keep in your home, car or office. The more you know about what to do in an emergency, the more confident and secure you will feel in your abilities to manage through a disaster. Spread the word to your loved ones and encourage them to "Resolve to be Ready" in 2016.

## County Tourism Grants Due to Parks and Rec Office by Feb. 12

The City is accepting grant applications for local tourism projects that aim to increase the numbers of tourist visitors and increase the economic impact of tourism. In partnership with Clackamas County Tourism, up to \$20,000 is available for projects that can:

- **Increase overnight lodging** stays in the County.
- **Bring visitors** into communities from **greater than 50 miles away** to shop, dine, recreate, take in arts or cultural offerings, or visit heritage sites.
- Entice visitors to **"linger longer."**

The maximum grant for a single project is \$20,000; the minimum request is \$500. Funding for the grants comes from the County's 6 percent transient room tax paid by overnight lodging visitors. Eligible projects, which must take place within Clackamas County, must be completed by Dec. 31, 2016.

Applications are due at the City's Parks and Recreation administrative office by 5 pm on Friday, Feb. 12. The City-administered County tourism grant pro-

gram is separate and distinct from the City's Community Tourism and Community Opportunity grant programs. Application to the County's program does not preclude application to the City's program. Copies of all program materials are located on the Wilsonville Parks and Recreation website at [WilsonvilleParksandRec.com/GrantPrograms](http://WilsonvilleParksandRec.com/GrantPrograms).

Proposals are evaluated by the Parks and Recreation Advisory Board based on: likelihood of attracting visitors; ability to generate overnight stays; partnerships; use of additional/leveraged funds; self-sustaining growth and capacity-building potential; and measurable return of investment.

For more information, contact Brian Stevenson, Recreation Coordinator, at 503-570-1523; [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

**Clackamas County Tourism Grant Due Date**  
 • Fri, Feb 12, 5 pm  
 Parks and Recreation Office,  
 29600 SW Park Place



29600 SW Park Place  
 Administrative Office: 503-783-7529  
 Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

## Presidents Day - Science Time!

The Parks and Recreation Department has partnered with Mad Science to offer two programs for children ages 6 to 12 when school is out on Presidents Day. The first program, "Science Saves the Planet," runs 9 am - noon. The second program, "Fun-Tastic Physics," is to be offered 1-4 pm. The cost is \$40 per class program and complimentary supervision is to be provided over the lunch hour for children enrolling in both programs. Register now before the program sells out!



## Save the Date - Spa Saturday

Mark your calendar for the third annual Spa Saturday at the Center. This event is scheduled for Saturday, April 16, 10 am - 1 pm, at the Community Center and this year's theme is "Island Paradise." Let your cares melt away as you sample free offerings of healthy and relaxing services like massage and meditation, sip on coconut water and snack on tropical fruit, enjoy the sweet tunes of Hawaiian music as you peruse the health and craft vendor area, and return home wearing your free lei and feeling rejuvenated. Practitioners and vendors interested in participating may call 503-570-1530 for more info. Deadline for submissions is March 1. Appointments, highly recommended for some offerings, are accepted starting April 1. For more information visit [WilsonvilleParksandRec.com/SpaSaturday](http://WilsonvilleParksandRec.com/SpaSaturday).

## New Class Spotlight - Nia

Nia is the art of movement; incorporating 52 simple moves with a combination of dance arts, martial arts, and healing arts. The class is designed so anyone can comfortably move to his or her own body's way. Nia tones your mind and tunes your body so you can walk away feeling energized, mentally clear, and emotionally balanced. Nia is practiced barefoot, non-impact, adaptable to individual abilities and choreographed to music. The Friday class starts on February 5 and runs for eight weeks 9 - 10 am at the Community Center. The fee for the class is \$47.

## Xerox Donates New Computers

The Parks and Recreation Department would like to thank Xerox and their employees for their generous donation of \$5,000 to purchase six new computers and monitors for the computer lab at the Community Center. The lab is open Monday - Friday, 8 am - 5 pm, for drop-in use and is also used for teaching classes. Volunteer Dick McPartland is also available in the computer lab by appointment every Wednesday, 10:30 - 11:30 am, for assistance with computers, laptops or tablets.



## Upcoming Classes and Events

### Everyone is an Artist - Watercolor

Mondays, Feb 1 - Mar 21  
 1 - 3 pm  
 Community Center  
 \$55 (plus \$20 material fee)

### Beginner Learn to Ice Skate

Ages 6 to 10  
 Saturdays, Feb 6 to Mar 26  
 12:15 - 12:45 pm  
 Sherwood Ice Arena  
 \$70 (plus \$15 annual fee to ice arena)

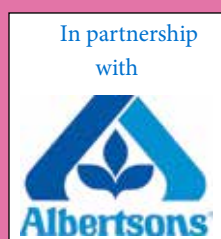
### Swing Dance

Tuesdays, Feb 16 - Mar 15  
 Beginner: 6:30 - 7:20 pm  
 Intermediate: 7:30 - 8:20 pm  
 Community Center  
 \$47

### Manners Matter

Saturdays, Feb 20 and 27  
 Tauchman House  
 Ages 6 - 8: 9:15 - 10:45 am  
 Ages 8 - 10: 11 am - 12:30 pm  
 \$60

## Community Egg Hunt



Saturday March 26  
 Memorial Park - 10 am  
 Ages 1 to 11  
 No Charge!



## Six Scammer Secrets They Don't Want You to Know

### #1 Want You To Wire Money

You may be asked to wire money or purchase pre-paid debit cards. This is the easiest way for scam artists to get their hands on your money, and it's almost impossible to get it back once it has been sent. Don't do it!

### #2 Tell You To Keep It "Secret"

By asking you to keep a transaction secret, scammers know you won't have to respond to questions from family and friends who might see through the scam. Check with someone you trust before acting.

### #3 Sounds Too Good To Be True

If it seems too good to be true, it probably is! Above all, use this simple mantra to help you detect and avoid scams. It's always better to be cautious than to be a victim.

### #4 They Contact You "Out Of The Blue"

It could be a knock on the door, a phone call, or a piece of mail you weren't expecting. For example, you didn't think you owed the IRS or a debt collection agency money, but they called claiming you could be in trouble if you don't pay.

### #5 There Is An "Emergency"

A scam might warn that if you don't respond immediately your prize winnings will be lost, or that a relative or friend is in trouble in a foreign country. If something prompts immediate action, be cautious.

### #6 Need Your Personal Information

Scammers often pose as banks, health care providers and government officials asking for identifying personal or financial information. Anytime someone asks you for this information, be suspicious!

For more information on scams, or to file a complaint, contact the **Oregon Department of Justice** at 1-877-877-9392 or online at:

[www.OregonConsumer.gov](http://www.OregonConsumer.gov)

## CHIEF'S CORNER - PROTECT YOURSELF FROM IDENTITY THEFT

Identify theft occurs when someone steals your mail, runs off with your wallet, takes your phone, hacks a computer that holds your personal information or takes advantage of your sensitive personal information in a variety of other ways.



Personal information (such as your name, date of birth, social security number, credit card numbers and security codes) is all a crook needs to charge up your credit cards, steal your money and potentially ruin your credit.

Stolen mail and wallets are the most common way thieves steal your identity, but online and mobile theft are also common and growing concerns.

### Tips to Protect Your Identity

Here are some tips I found from the American Association of Retired Persons, the Federal Trade Commission and the State of Oregon's Division of Finance and Corporate Securities to help protect you from being a victim of identity theft:

- **Remove especially risky items** that shouldn't be in your wallet such as your social security card, "cheat sheets" noting PINs or passwords for bank cards or online accounts, blank checks and spare keys for your home or car.
- **Make photocopies** of the front and back of every card you keep in your wallet and keep the list safely at home as a record of all your account numbers, security codes and contact information so that if your wallet is stolen you can quickly contact all your creditors.
- Before disposal, **shred all sensitive financial documents**, credit card offers and other related paperwork that include sensitive information about you.
- **Do not give out any personal information** over the phone, internet or through the mail unless you are certain of the requestor.
- **Check your credit report** regularly to ensure you have not unknowingly had your identity stolen. To get a free report, visit [AnnualCreditReport.com](http://AnnualCreditReport.com) or

call 877-322-8228 toll-free.

- **The best deterrent is vigilance.** Keep your personal information, including your social security card, in a locked, secure place in your home.
- **Protect your phone from theft!** The number of attacks on mobile devices is growing. The Oregon Bankers' Association recommends that you password protect your mobile devices.
- **Don't post or share sensitive information online**—the more you post, the more your security and privacy are at risk.
- **Take mail out of your mailbox as soon as you can.** Or, better yet, get a locking mailbox.
- **Only give your social security number if you must**, but first ask if you can use another kind of identification.
- **Use passwords that are not easy to guess** and use both numbers and symbols when creating passwords.
- **Do not respond to emails or other messages that ask for personal information.**

I hope you find the above tips to be helpful and I strongly encourage you to take these steps and start better protecting your identity today.

*Jeff Smith, Chief of Police*

### Learn more about preventing identity theft and other scams at:

- **Association of American Retired Persons**  
[www.AARP.org/money/](http://www.AARP.org/money/)
- **Federal Trade Commission**  
[www.consumer.ftc.gov/topics/privacy-identity](http://www.consumer.ftc.gov/topics/privacy-identity)
- **Div. of Finance and Corporate Securities**  
[www.dfcs.oregon.gov/id\\_theft](http://www.dfcs.oregon.gov/id_theft)
- **Oregon Department of Justice—Scam Alert**  
[www.doj.state.or.us/consumer/](http://www.doj.state.or.us/consumer/)

## Most Falls Happen in the Home and Can Be Avoided

### TVF&R offers safety tips to limit hazards, avoid falls

As people grow older, the consequences of a fall grow more serious.

Unintentional falls are the leading cause of injury deaths for adults ages 65 and older, according to the Centers for Disease Control and Prevention. They are the most common cause of traumatic brain injuries and account for more than 95 percent of hip fractures. In addition, fractures from falls are a leading cause of disability and often result in entering a nursing home.



Tualatin Valley Fire & Rescue crews responded to 4,304 reports of falls within the district's service area in 2015. That number is up from 4,034 fall reports in 2014.

"Ground-level falls are one of the most concerning type of medical calls our firefighter paramedics respond to because we know many of our elders struggle to recover from them," states Cassandra Ulven with TVF&R. "Most falls happen in the home and can be avoided.

"Factors such as poor lighting, narrow stairs, and slippery surfaces can increase the chances of a fall. In addition, problems with glare, depth perception, tiredness or dizziness from prescription medications contribute to falls."

Research conducted by Yale University showed that older adults are also most at risk of falling when they fail to use handrails on stairs, reach for objects overhead, and climb on chairs or ladders.

Tualatin Valley Fire & Rescue reminds seniors and children of older adults to take the following precau-

tions to help prevent a fall injury:

### Take Care of Yourself

Get regular medical check-ups and talk to your doctor to ensure appropriate levels and types of medication. Have your hearing and vision checked. Exercise regularly, and eat nutritious meals.

When you have a medical emergency, it is beneficial for emergency responders to know about any pre-existing medical conditions and current medications you are taking. Having this information helps ensure that you receive the best medical treatment possible in the shortest amount of time. Help us help you by completing a TVF&R Medical Emergency Card and posting it on the refrigerator in your home.

A completed card will provide our firefighters and paramedics with critical information such as current medications, physician information, significant surgeries, and emergency contacts.

### Remove Existing Hazards

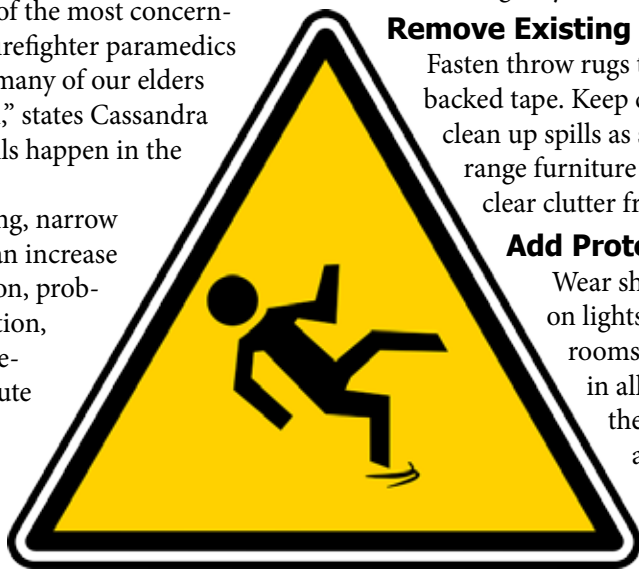
Fasten throw rugs to the floor with double-backed tape. Keep cords out of pathways, and clean up spills as soon as they happen. Also, arrange furniture to minimize obstructions, and clear clutter from your living areas.

### Add Protection

Wear shoes with non-skid soles. Turn on lights when walking through dark rooms or hallways. Use nightlights in all rooms. Install grab bars in the bathtub, shower, and toilet areas, and add handrails on stairwells. Place phones in multiple rooms or keep a cellphone with you in case you have to call for emer-

gency assistance.

For more information about fall prevention and safety tips, visit [www.tvfr.com](http://www.tvfr.com).



## Wilsonville Stage Performs A View From the Bridge

Wilsonville Stage, also known as Wilsonville Theater Company, continues the 2015-16 season with a spellbinding American drama by one of America's most famous playwrights, Arthur Miller. Play director Terry Kester explains, "There is no more powerful, painful, dramatic play, than *A View From the Bridge*. It is an emotionally raw drama about a low-middle-class immigrant family who lives in fear of deportation." The play opens Thursday, Feb. 18, and runs until Saturday, March 5. For ticket prices and performance dates/times visit [www.WilsonvilleTheater.com](http://www.WilsonvilleTheater.com).

WILSONVILLE STAGE PRESENTS ARTHUR MILLER'S

# A VIEW FROM THE BRIDGE

Directed by Terry Kester

**FEBRUARY 18 THRU MARCH 3** WILSONVILLE FROG POND GRANGE & SPECIAL PERFORMANCE VENUES

PURCHASE TICKETS ONLINE AT [WWW.WILSONVILLETHEATER.COM](http://WWW.WILSONVILLETHEATER.COM) OR AT THE DOOR

WILSONVILLE FROG POND GRANGE  
27350 SW STAFFORD ROAD  
WILSONVILLE OR 97070

\* VISIT OUR WEBSITE FOR SPECIAL VENUE LOCATIONS AND TIMES



## Reports from City Boards and Commissions

### City Council Implements Marijuana Ban, Approves Zone Changes, Annexes Property and Updates Public Works Standards

On Monday, Jan. 4, the Wilsonville City Council made effective an ordinance that continues an existing ban within city limits on the sale and production of both medical- and recreational-marijuana products, including medical marijuana-processing sites, medical marijuana dispensaries, recreational marijuana producers, recreational marijuana processors, recreational marijuana wholesalers, and recreational marijuana retailers. The ban is permitted under state law by House Bill 3400 (2015). The law further requires that the ban be referred to the voters at the next statewide general election in November 2016, if the state-sanctioned ban is to remain in effect thereafter.

The City Council also held two public hearings and approved ordinances that amend the City's Zone Map from a "Public Facility" to "Village" zoning designation for two parcels of land located in the central area of the Villebois neighborhood. The zone change by home-builder applicant Polygon Homes Northwest seeks to permit construction of 132 row houses. The first planned development, Brookside Terrace, includes 50 row houses on approximately 3.2 acres located northwest of SW Villebois Drive North between SW

Costa Circle West and SW Berlin Avenue. The second development, Camden Square and Royal Crescent at Villebois, includes 82 row houses located in the Villebois Village Center between Costa Circle and SW Villebois Drive. Both developments were previously reviewed and approved by the Development Review Board Panel A on December 14.

The City Council also held a public hearing and approved an ordinance that annexes into the City limits specific segments of SW Grahams Ferry Road and SW Tooze Road and also a 22-acre undeveloped property located within the Villebois Master Plan Area on the south side of Tooze Road. The City already has authority for the road and is required to maintain it. The approved annexation is needed in part to allow the investment of urban renewal funds in the Tooze Road improvement project, which is currently being designed to widen and improve the western section of Tooze Road just east of SW Grahams Ferry Road. A portion of the other property to be annexed is to be used, in part, for a public park.

The City Council also held a public hearing and approved an ordinance that amends the City's Public Work Standards to require: the use of warm mix asphalt for road improvements; changes to the storm

water design standards; the use of iron pipes and fittings that are "made in the USA"; sanitary design changes to comply with the City's odor code; and changes to the colors used to paint public and private fire hydrants to make identification easier and faster.

Under Communications, the Council heard a report from the City's Auditor that found the City's financial statements are "free from material misstatement" and conform to Generally Accepted Accounting Principles, applicable state standards and also federal requirements. The Council also heard from 2015 Leadership Academy graduates, Lorie Loen and Kathryn Martinez-Gilbertson, who, along with others, hosted a Spaghetti dinner fundraiser in December raising over \$1,100 in funds that they presented to Wilsonville Community Sharing board members.



Wilsonville Community Sharing board members accept donation check from Leadership Academy participants of funds raised at December's Spaghetti benefit dinner (left to right) Dick Spence, Taft Mitchell, Michelle Ripple, Mayor Tim Knapp, Lori Loen and Kathryn Martinez-Gilbertson.

Under Mayor's Business, the Council recognized the City's Public Works Delora Kerber in being elected as the 2016 President of the Oregon Chapter of the American Public Works Association, also known as APWA, which represents 800 professional engineers, technicians, operations staff, public works directors, business people, contractors and consultants.



Delora Kerber

At the meeting the City Council also decided to continue a resolution regarding the adjustments to the sales price of a 10-acre surplus property that the City is selling to Polygon WLH LLC to the next City Council meeting that is planned to be held on Thursday, Jan. 21, 7 pm.

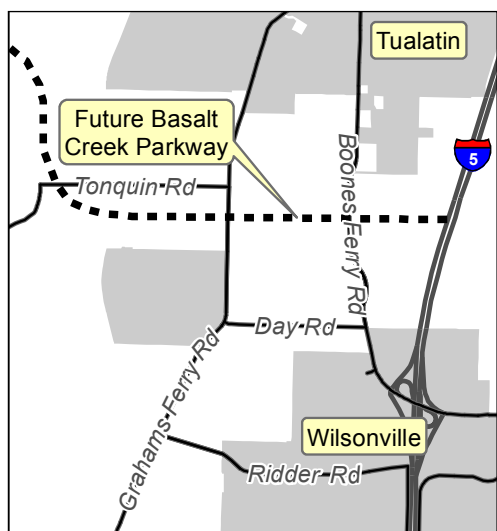
At work session prior to the City Council meeting, the Council heard updates and discussed plans to expand SMART's Compressed Natural Gas station, a proposal to study the City's role in bringing broadband fiber to the community and a proposal to terrace the hill in Murase plaza adjacent to the plaza's water feature.

### Wilsonville and Tualatin Identify Preferred Jurisdictional Boundary for Basalt Creek Concept Planning Area

On Dec. 16, the Tualatin and Wilsonville City Councils held a joint meeting to discuss the Basalt Creek Concept Plan and reached an agreement that the "Future Basalt Creek Parkway," a proposed road that is to run south of, and parallel, to Tonquin Road, is the preferred jurisdictional boundary between the two cities. Acceptance of the jurisdictional boundary as the preferred alternative allows project planners to move forward with finalizing the Basalt Creek Concept Plan for further consideration.

At the meeting the Councils discussed and provided staff guidance on "essential agreements" that need to be covered and resolved in the concept plan to make the proposed

boundary and associated land use plan successful. The areas to be further addressed in the plan include: land uses, transportation (capacity, funding and planning), sanitary sewer, stormwater management, transit service and protections for the Basalt Creek Canyon that runs through the area.



Additional public feedback on the draft plan is to be sought early next year prior to presenting the draft plan to the two Councils for further discussion and potential adoption. For more information about future meetings and feedback opportunities visit [www.BasaltCreek.com](http://www.BasaltCreek.com), or contact Miranda Bateschell, Long-Range Planning Manager, at 503-570-1581; [bateschell@ci.wilsonville.or.us](mailto:bateschell@ci.wilsonville.or.us).

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during February. Scheduling changes can and do occur; check for updated meeting information at the City's website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, February 1:** SMART/Tri-Met Service boundary Basalt Creek area; Tooze Road project; Energy Trust results ; Clackamas Community College bond project Update; substantial amendment to West Side URA; sidewalk repair grant program; minor amendment to the Year 2000 URA boundary.

**Thursday, February 18:** Universal Health Services – annexation, zoning; cooperative utility agreement with WWSP/ODOT - Kinsman project; Republic Services rate increase request; Year 2000 minor URA amendment.

### Planning Commission Update

At the Planning Commission meeting on January 13, the Commission elected Jerry Greenfield as 2016 Chair and Eric Postma as 2016 Vice Chair. The Planning Commission received an update and provided feedback on the Basalt Creek Concept Plan. The Commission also heard a report and provided suggestions for engaging the public and receiving input on the Town Center Master Plan project which is in the initial stages of implementation.

### Development Review Board Update

On December 14, the Development Review Board (DRB) Panel A approved a Zone Map Amendment and various plans proposed by home builder Polygon Northwest for a development of 50 row houses, known as Brookside Terrace, located northwest of SW Villebois Drive North between SW Costa Circle West and SW Berlin Avenue. In addition the DRB Panel A also approved a second Zone Map Amendment and various other plans also proposed by home builder Polygon Northwest for a development of 82 row houses, known as Camden Square and Royal Crescent at Villebois, to be located in the Villebois Village Center between Costa Circle and SW Villebois Drive. At the same meeting the DRB also approved the annexation into the City limits of specific segments of SW Grahams Ferry Road and SW Tooze Road and also a 22-acre undeveloped property located within the Villebois Master Plan Area on the south side of Tooze Road. Finally the DRB approved design review for two single-family dwellings in Old Town to be located at 30580 SW Boones Ferry Road.

### Donate Food for Library Fines

Reduce or eliminate your library overdue fines by bringing in cans or boxes of food during the month of February. Each can or box is worth a one dollar credit toward overdue fines, subject to the approval of library staff.

The food bank prefers donations of:

- Breakfast cereals
- Canned fruit

Please do not donate Ramen noodles.

For more information about Wilsonville Community Sharing, please visit their website: [www.WilsonvilleCommunitySharing.org](http://www.WilsonvilleCommunitySharing.org).

All food is to be donated to Wilsonville Community Sharing for the assistance of needy area families. Please check expiration dates carefully! The food bank cannot accept or use expired food.



## High School Students Invited to Enter Safe Driving Media Contest

High school students in Clackamas County are invited to participate in the “Drive to Zero” Posters and Coasters safe driving media contest. The goal of the contest is for students to create artwork that promotes safe driving to others in the community.

Student artwork or written messages should be based on risk factors such as speed, distracted driving or impaired driving, or on prevention strategies such as seat belt use, parent support or eliminating distractions. Students are encouraged to use positive, uplifting messages that reflect the need to drive with respect and courtesy.

Entries are to be judged on creativity, message impact and promotional appeal, and are all eligible to be used for outreach. The students who create the top three entries each win a gift certificate from Clackamas Town Center (\$300 for first place, \$200 for second place, and \$100 for third place).

Drive to Zero Program Coordinator Patty McMillan states, “Traditionally driver education has come from

parents, educators and safety professionals. Now, students in high schools in Clackamas County are invited to create their own safe driving media to share with their peers and the community.”

She added, “When we last ran the contest in 2014, posters were displayed at Clackamas Town Center and on local billboards.”

Entries must be submitted by March 11 to McMillan, either:

- By mail: Drive to Zero – Engineering, Development Services Building, 150 Beaver Creek Road, Oregon City, OR 97045
- By email: PMcMillan@clackamas.us



All entries must be accompanied by a signed release form. Contest details are available at [www.Clackamas-SafeCommunities.org](http://www.Clackamas-SafeCommunities.org). The contest is sponsored by Clackamas County Drive to Zero Program, Clackamas Town Center and State Farm Insurance.

For more information, members of the media and public may contact McMillan at PMcMillan@clackamas.us or 503-742-4661.

## Oregon Tech Invites Local Companies to Participate in Career Fair

On Wednesday, Feb. 17, the Oregon Tech campus in Wilsonville is hosting a Career Fair and invites local businesses to participate in the free event in order to recruit interns and full-time employees. The first-year event includes:

- Employer Campus Tour and Open House: 2:30 pm – 3:30 pm
- Registration and set up: 3:30 pm – 4 pm
- Fair open to students: 4 – 7 pm

The Wilsonville campus serves about 800 students who are often uniquely prepared to make immediate contributions when hired because the university uses an applied technology education model which fosters



experiential hands-on learning. In addition, the Oregon Tech faculty and staff is comprised of industry-savvy experts who have a deep understanding of workforce demands and emerging technological advances and applications.

Space is limited so priority to participate in the free career fair is being offered first to company's and other organizations who are recruiting for interns, in addition to full-time jobs. To register a company for the career fair visit [www.myinterfase.com/oit/employer](http://www.myinterfase.com/oit/employer).

For more information contact Jennifer Kass at 503-821-1155 or Jennifer.Kass@oit.edu.

## Clackamas Community College Offers Classes on Bees and Trees in February

### Feb. 6 and 13 — Beekeeping School

This Saturday workshop provides new beekeepers with information on basic bee biology, proper equipment, handling of bees and more. Co-sponsored by the Portland Metro Beekeepers Association. The workshop is being held 9 am - 3 pm in Clairmont Hall, room 118. Fee is \$45.

### Feb. 6 — Fruit Tree Pruning Course

This Saturday class features demonstrations and practice of pruning methods appropriate for a variety of fruit trees. Class is 9 - 11:50 am at the Home Orchard Society Arboretum



at CCC, 19600 Molalla Ave. Co-sponsored by the Home Orchard Society. Fee is \$10.

### Feb. 27 — Fruit Tree Grafting

Two Saturday workshops (one beginning and one intermediate) provide hands-on experience for grafting fruit trees. Co-sponsored by the Home Orchard Society, the beginning workshop is 9 - 10:30 am and the intermediate workshop is 12:45 - 2:15 pm, held in Clairmont Hall, room 118. Fee is \$15 per workshop.

For more information about any of these classes, call 503-594-3292 or email [lorettam@clackamas.edu](mailto:lorettam@clackamas.edu).

## Students Invited to Enter 'If I Were Mayor' Contest

Wilsonville-area students are invited to enter the 2015-2016 “If I Were Mayor I Would...” Contest.

The statewide contest is open to all Wilsonville residents in grades 4-12 and is sponsored by the Oregon Mayors Association. Locally there are prizes for the winning entries in each age category, and the local winners from each category advance to the state-wide competition for a chance to win an iPad Air tablet.



Council President Scott Starr gives Mikala Hoffman an award for her 2014 “If I Were Mayor” entry.

of the three grade levels described as follows:

- **Elementary** students (grades 4 and 5) enter a poster with a minimum size of 17 inches and a maximum of 28 inches in height or width. Students are encouraged to be creative and may use any art medium (e.g., poster paints, felt pens, color pencils, pastels, crayons, etc.)
- **Middle school** (grades 6-8) an essay of 500-1,000 words in length and must be typed.
- **High school** students create and present their recommendations using digital media (video, PowerPoint, Prezi, etc.). The presentation must be one to three minutes in length and submitted on disk or emailed.

Contest entries must be e-mailed or dropped off at the City Hall front desk no later than 5 pm on April 1 and local winners are to be announced on April 8 and featured at the April 18 City Council meeting. For official rules visit [www.ci.wilsonville.or.us/IfIWereMayor](http://www.ci.wilsonville.or.us/IfIWereMayor) and for more information, contact Angela Handran, Community Relations Specialist, at 503-570-1503; [Handran@ci.wilsonville.or.us](mailto:Handran@ci.wilsonville.or.us).

## COUNCILOR CORNER: SUSIE STEVENS

As a City Councilor I serve as the liaison for both the Library Board and the Wilsonville Community Seniors, Inc. Both of these all-volunteer boards are doing great work for our community and I am pleased to have the opportunity to help community members become more acquainted with the organizations.



### Library Board

The Library Board, comprised of community members Reggie Gaines, Caroline Berry, Alan Steiger, Megan Chuinard and Mallory Nelson

*Both of these all-volunteer boards are doing great work for our community.*

Both of these all-volunteer boards are doing great work for our community. This year the board, in close partnership with the Library Director Pat Duke, is working to advance a number of important broad initiatives outlined in the Library's Strategic Plan, which include:

- Igniting a passion for reading in young children
- Enhancing access to information using technology
- Advancing the library as a community focal point
- Fostering the joy of reading in all ages
- Cultivating relationships with local groups
- Supporting a Library team that is rooted in service

One recent accomplishment that I am proud to see completed is the hiring an Outreach Librarian, Deborah Gitlitz, who is now focused on many aspects

### Library Board Meetings

- **Fourth Wednesday 6:30 pm**, Library, 8200 SW Wilsonville Road

of the Strategic plan including community outreach, early childhood literacy, community partnerships and promoting

the library as a community focal point. For interested community members, the Library Board meetings are open to the public and are held on the fourth Wednesday of every month, 6:30 pm, at the Wilsonville Public Library.

### Wilsonville Community Seniors Inc.

The Wilsonville Community Seniors Inc. (WCSI) is another great group of local volunteers who are focused on supporting our local seniors and the related programs in our community. Specifically WCSI is actively involved and responsible for supporting the following programs and services:

- Partial and full scholarships for seniors to participate in various classes and programs that they otherwise are not able to afford
- Bakery Project provides low- or no-cost bakery goods on Mondays and Fridays at the Community Center through donations provided by Fred Meyer and Albertsons
- Fundraising for the Wilsonville Community Center's Computer Room
- Planning and organizing special lunch-time events and themes at the Community Center
- Fundraising and support for the City's energy/medical assistance for low-income seniors
- Planning and organizing out-of-town trips and special tours for local seniors
- Publishing the *WCSI Gazette* a monthly newsletter

### WCSI Board Meetings

- **Second Wednesday, 1 pm**, Community Center 7965 SW Wilsonville Road

Members of WCSI include, Wes Morris, Chair, John McDonald, Barbara Ricker, Jean Tsokos, Debbie Dahlquist, Robert Thompson, Donna Atkinson and Paul Keller.

WCSI meets monthly at the Wilsonville Community Center on the second Wednesday of every month at 1 pm. The group welcomes members of the public to attend the meetings and become more involved in supporting our local seniors.

Watch your mail for a fundraising letter from WCSI for more information on how you can support WCSI and the seniors in our community. I encourage you to support these groups as they work to improve two important aspects of our community— maintaining a top-notch library and caring for seniors in our community. Thank you!



**Live Music**  
**ROCK THROUGH THE DECADES**


**Saturday, Feb. 13, 7 - 10 pm**  
**Wilsonville Community Center**

**Two Bands (Blues and Rock)**  
**Big Plans & The Lashbrook Brothers**  
**\$10 at the door**

At the event The Beer Station will be selling beer and wine and pizza will also be for sale. All proceeds benefit Wilsonville Community Sharing. For more information contact Angela Handran 503-570-1503, handran@ci.wilsonville.or.us.

**Become a Certified Master Recycler Volunteer**

Take an eight-week course and learn from experts on the latest information on consumption and recycling. The Clackamas County course and volunteer program is being held at the Clackamas County Bldg., 150 Beaver Creek Rd., Oregon City, and costs \$50 per participant. Partial and full scholarships are available without extra application requirements.



The course is held on eight consecutive Wednesdays, 6:30 - 9:30 pm, starting on March 30, and two Saturdays, 8 am - 2 pm, on April 9 and May 7. The deadline to submit an application is Tuesday, Mar. 1, 2016, at 12 pm. Upon completion volunteers are asked to spend 30 hours sharing what they learned with neighbors, coworkers and the community. **Get details and apply at: [www.MasterRecycler.org](http://www.MasterRecycler.org)**

**Tree and Shrub Care Volunteers Needed on Feb. 6 and April 2**

On Saturday, Feb. 6, 9 am - 1 pm, join Friends of Trees and the City of Wilsonville in planting native trees and shrubs and removing invasive English ivy at the Memorial Park Forest Shelter, 8100 SW Wilsonville Road.

Please arrive around 8:45 to be registered and assigned to a crew. Breakfast snacks and hot chocolate/coffee, as well as gloves, tools and guidance will be provided. Dress for field



conditions. All ages welcome. RSVPs are requested for groups of five or more people. Register online at [FriendsOfTrees.org/volunteering/group-registration](http://FriendsOfTrees.org/volunteering/group-registration). Contact Jenny or Randi 503-595-0213 at Friends of Trees if you have any questions or need more information.

A second tree and shrub care event is on Sat., April 2, 9 am- 1 pm, at Tranquil Park, 10580 SW Brown Road in Wilsonville.

**Volunteer Tree Care Event**  
• Sat, Feb 6, 9 am - 1 pm  
Memorial Park Forest Shelter,  
8100 SW Wilsonville Road

**Rotary Celebrates Service at 2016 Heart of Gold Dinner & Auction**

Tickets to “Celebrating Service: The 2016 Heart of Gold Dinner & Auction” are now on sale at [www.WilsonvilleRotary.com/heart](http://www.WilsonvilleRotary.com/heart).

The annual Rotary Club of Wilsonville event is being held Saturday, Feb. 6, at Holiday Inn Wilsonville, 25425 SW 95th Ave. Doors open and the silent auction begins at 5 pm with the awards dinner and live auction following at 7 pm. Tickets are \$50 per individual, \$100 per couple or

**Heart of Gold Dinner and Auction**  
• Sat, Feb 6, 5 pm Silent Auction, 7 pm Dinner, Holiday Inn Wilsonville, 25425 SW 95th Avenue  
[www.WilsonvilleRotary.com/heart](http://www.WilsonvilleRotary.com/heart).

\$400 per table of eight.

The annual Heart of Gold event recognizes local volunteers, including one individual or couple who is chosen for Wilsonville’s highest volunteer honor — the First Citizen award. The nomination process was held in December and finalists are to be announced in early January. This year’s theme is “Celebrate Service” set in a Mardi Gras/masquerade ambiance.

“Our Heart of Gold Dinner and Auction exists so we can celebrate Wilsonville’s great volunteers and support Rotary’s great service projects,” Rotary Club president Kyle Bunch said.

At the 2015 Heart of Gold Celebration, Donna Bane was honored with the First Citizen Award. For more details visit [www.WilsonvilleHeart.com](http://www.WilsonvilleHeart.com)

**Campus Preview Day at Oregon Tech in Wilsonville, Feb. 15**

Ready to check out your college options? Oregon Tech – located right here in Wilsonville – is inviting the public to learn about the university’s degree programs, on-campus student support services, and tour the campus’ labs and classrooms (including the cool 3-D printing lab, Optical engineering lab, Paramedic lab, and Clinical Lab Science facilities.) Meet with current students and faculty to learn about the student experience inside the classroom and beyond, Oregon Tech’s connections with local industry and the University’s high student success rates for employment and starting salaries.

Come check out the exciting things going on at the

**Oregon Tech Campus Preview Day**  
• Mon, Feb 15, 9 am - 1 pm  
Oregon Tech - Wilsonville Campus  
27500 SW Parkway Ave, Wilsonville

#1 top rated, affordable public institution in the west (as ranked by US News & World Report Best Colleges). As a public polytechnic university, Oregon Tech offers in-demand bachelor’s and master’s degrees in engineering, technology, healthcare, management, and applied psychology, among others.

Lunch is provided at this free event. For more information go online to [www.OIT.edu/Wilsonville](http://www.OIT.edu/Wilsonville); or call Oregon Tech at 503-821-1250.



**City Calendar**

| FEBRUARY |   |
|----------|---|
| 1 Mon    | • City Council<br>5 pm Work Session<br>7 pm Meeting<br>City Hall  |
| 2 Tue    | • Municipal/Traffic Court<br>5:30 pm, City Hall   |
| 5 Fri    | • Community Tourism Grant Due Date<br>5 pm, Park and Rec Admin Building   |
| 10 Wed   | • Wilsonville Community Seniors Inc. Advisory Board, 1 – 3 pm<br>Wilsonville Community Center<br>• Planning Commission 6 pm, City Hall    |
| 12 Fri   | • County Tourism Grant Due Date<br>5 pm, Park and Rec Admin Building  |
| 13 Sat   | • Live Music Fundraiser for Wilsonville Community Sharing<br>7 - 10 pm, Community Center  |
| 14 Mon   | • DRB Panel A, 6:30 pm, City Hall   |
| 15 Mon   | • President’s Day- City Office Closed   |
| 16 Tue   | • Tourism Promotion Committee Meeting, 10 am - 12 pm, City Hall Council Chambers<br>• Municipal/Traffic Court<br>5:30 pm, City Hall       |
| 17 Wed   | • Wilsonville Leadership Academy<br>6 - 9 pm, City Hall   |
| 18 Thurs | • City Council<br>5 pm Work Session<br>7 pm Meeting<br>City Hall  |
| 22 Mon   | • DRB Panel B, 6:30 pm, City Hall   |
| 24 Wed   | • Library Board, 6:30 pm<br>Wilsonville Public Library  |
| 25 Thurs | • Parks and Recreation Advisory Board Meeting, 4:30 pm, City Hall   |
| MARCH    |   |
| 1 Tue    | • Tourism Promotion Committee Meeting, 10 am - 12 pm, City Hall Council Chambers<br>• Municipal/Traffic Court<br>5 pm, City Hall          |
| 5 Fri    | • Community Opportunity Grant Due Date, 5 pm, Park and Rec Admin Bldg.  |
| 7 Mon    | • City Council<br>5 pm - Work Session<br>7 pm - Meeting<br>City Hall  |
| 9 Wed    | • Wilsonville Community Seniors Inc. Advisory Board, 1 – 3 pm<br>Wilsonville Community Center<br>• Planning Commission<br>6 pm, City Hall |
| 10 Thurs | • Parks and Recreation Advisory Board Meeting, 4:30 pm, City Hall   |
| 14 Mon   | • DRB Panel A, 6:30 pm, City Hall   |
| 15 Tue   | • Municipal/Traffic Court<br>5 pm, City Hall  |
| 17 Thurs | • Wilsonville Leadership Academy<br>6 - 9 pm, City Hall   |
| 21 Mon   | • City Council<br>5 pm - Work Session<br>7 pm - Meeting<br>City Hall  |
| 23 Wed   | • Library Board, 6:30 pm<br>Wilsonville Public Library  |
| 28 Mon   | • DRB Panel B, 6:30 pm, City Hall   |

All dates and times are tentative; check the City’s online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Wilsonville City Council**

The City Council usually convenes on the first and third Monday of the February at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on cable TV channels 30 (Comcast/Xfinity) and 32 (Frontier) and replayed periodically. Meetings are also available to stream live online on Wilsonville TV and replays in the video vault by clicking on [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.

|   |  |   |  |  |
|---|--|---|--|--|
|                       |                                       |    |                                       |                                   |
| <b>Tim Knapp</b><br>Mayor<br><a href="mailto:knapp@ci.wilsonville.or.us">knapp@ci.wilsonville.or.us</a> | <b>Scott Starr</b><br>City Council President<br><a href="mailto:scottstarr97070@gmail.com">scottstarr97070@gmail.com</a> | <b>Julie Fitzgerald</b><br>City Councilor<br><a href="mailto:fitzgerald@ci.wilsonville.or.us">fitzgerald@ci.wilsonville.or.us</a> | <b>Susie Stevens</b><br>City Councilor<br><a href="mailto:stevens@ci.wilsonville.or.us">stevens@ci.wilsonville.or.us</a> | <b>Charlotte Lehan</b><br>City Councilor<br><a href="mailto:lehan@ci.wilsonville.or.us">lehan@ci.wilsonville.or.us</a> |

**City Manager**  
Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)



**Wilsonville City Hall**  
29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)  
**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)

# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

July-August 2016

## Wilsonville Re-Named a Walk Friendly Community

The Pedestrian and Bicycle Information Center (PBIC) based in Chapel Hill, North Carolina, announced that the City of Wilsonville has been re-designated as a bronze level Walk Friendly Community (WFC). Wilsonville is being recognized for working to improve a wide range of conditions related to walking, including safety, mobility, access, and comfort.

Wilsonville is one of only 58 Walk Friendly Communities across the nation. In Oregon other WFC communities include Corvallis, Eugene, Bend, Sisters and Tigard. The WFC program is funded by FedEx and the U.S. Department of Transportation Federal Highway Administration. For more information visit [www.WalkFriendly.org](http://www.WalkFriendly.org).



Mayor Tim Knapp (center) flanked by Planning Director Chris Neamtzu and City Councilor Charlotte Lehan (left) and City Councilors Scott Starr and Susie Stevens (right) with the City's recent Walk Friendly Community award.

## Report Shows Wilsonville's Drinking Water Surpasses all Federal and State Water Quality Standards

The City of Wilsonville has released the 2016 Annual Water Quality Report, which documents results of the City's 2015 water-quality monitoring and testing. This report lists the regulated contaminants that may be detected in treated water and the sample results from Wilsonville's water system.

The report shows that in every category of chemical measurement, including lead, that the City's water quality surpasses all federal and state standards for drinking water. Distribution of the Water Quality Report, also known as the Consumer Confidence Report (CCR), is required of all community water systems under the 1996 Safe Drinking Water Act



The 2016 Annual Water Quality Report is available online at [www.ci.wilsonville.or.us/WaterQualityReport](http://www.ci.wilsonville.or.us/WaterQualityReport).

amendments by July 1 of each year.

Delora Kerber, Public Works Director, said that, "Wilsonville is proud of the safe, high-quality drinking water we provide to our citizens as is validated in the Annual Water Quality Report."

The Water Quality Report includes lead-testing results that were conducted last year on older, potentially higher-risk homes in Wilsonville. The testing found that no corrective action is necessary and that none of the water tested exceeds federal action limits. During the most recent testing conducted in August 2015 by an independent third-party laboratory, water samples from 33 older homes indicated that none had tap water above the federal action levels. Four of the homes had very low levels of lead at 0.002 micrograms per liter, which is nearly 10 times lower than the EPA Action Limit (AL) of 0.015 micrograms per liter. Every three years the City conducts lead and copper tests on tap water from a sample of older homes as required by the EPA. (For tips on lead in drinking water see pg. 5.)

Copies of the 2016 Annual Water Quality Report are available at City Hall and the Library, and online at the City's website, [www.ci.wilsonville.or.us/WaterQualityReport](http://www.ci.wilsonville.or.us/WaterQualityReport). Paper copies of the report are available at City Hall and the Public Library and upon request.

## Nine-Hole Disc Golf Course Now Open in Memorial Park

The new nine-hole disc golf course in Memorial Park is now open for play. The free, family-friendly, course is located on the east side of the lower portion of the park, just east of the Dog Park. Two-disc sets can be rented at the Parks and Rec Admin office Mon-Fri for \$5 per day (credit card required for deposit.) Through the summer, a special \$5 weekend rate is also available.



The course was recommended as a new amenity in the Memorial Park

Master Plan adopted by City Council in 2015. Funding for the course was made possible in part with a \$10,000 grant from Clackamas County Community Partnership Program and also volunteer labor from

the Pac-West Disc Golf Alliance.

Park and Rec Director Stan Sherer explains, "The new disc golf course offers community members a new, fun low-cost activity for people of all ages and is a great new amenity in the east side of Memorial Park."



A volunteer from Pac-West Disc Golf Alliance digs a hole for a disc basket at the now open nine-hole disc golf course.

### Mayor's Message

## New Community Enhancement Program to Offer Tangible Improvements

This past year Wilsonville began to receive benefits from Metro's Community Enhancement Program that helps cities which host solid-waste transfer stations, such as that operated here by Republic Services. Funded by a tonnage fee Metro collects on biodegradable solid-waste, the new Community Enhancement Program can be used by cities to advance a wide range of projects that can provide community benefit.



The new Wilsonville Community Enhancement citizen committee considered a variety of applications and recommended the following for funding:

**\$25,000 — Memorial Park "Dog Park" Relocation Project** to relocate the enclosed, off-leash dog-run area, including creating 37 parking spaces and installing new fencing, drinking-water fountains, two covered shelters with benches and dog-play elements.

**\$21,433 — "Bee Stewards" Wilsonville Pollinator Improvement Project** to create productive pollinator habitats on City and School District property, utilizing volunteers and youth organizations to help plant and maintain plantings; develop an Integrated Pest Management Plan for the City; and provide public educational opportunity and materials about pollinator habitats.

**\$16,000 — Multifamily Waste-Reduction and Recycling Project** to increase recycling opportunities at multifamily communities, including conducting a public-education campaign with signage, working with residents and property managers, distributing light-weight recycling materials tote bags and looking at possible modification of central collection/enclosure areas.

**\$10,000 — Frog Pond Church Streetscape Restoration Project** to undertake extensive landscaping and safety improvements on public street frontage of this historical church property.

**\$7,110 — Fluorescent Mercury-Lamp Business Recycling Program** for two fluorescent-lamp collection events, targeted to businesses known to use fluorescent lamps, with proper disposal and recycling.

*Tim Knapp, Mayor*

### City Council Notice:

A Public Hearing for the **Coffee Creek Urban Renewal Plan** is to be held on **Monday, August 1, 7:00 pm at City Hall, 29799 SW Town Center Loop East, Wilsonville.** See pg. 7 for details.

### In This Issue...

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- Chiefs Corner — Summer Safety Tips . 5
- More Summer Safety Tips . . . . . 5
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...and more!

City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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### Our History: Locals Harvest Hops and Get Paid in Gold Coins



This Boones Ferry Historical Society photo taken in 1900 shows a group of local French Prairie hop pickers at harvest time. Throughout the area there were numerous hop yards that supplied local brewers with one of the key ingredients to make beer. On pay day the hop pickers are reported to have been paid with gold coins by the town banker J.W. Thornton.

### Películas para la Familia - ¡en español!

Disfruten de unas películas divertidas en la biblioteca este verano. Traigan su almuerzo o unas meriendas y disfruten de una variedad de películas los viernes al medio día en la Biblioteca Pública de Wilsonville, en el Salón Oak, del 1 de julio al 29 de julio. Las películas **En inglés:** 12–1:30 pm, **En español:** 2–3:30 pm.

#### Las películas son:

- **1 julio**–Intensa-Mente (Inside Out)
- **8 julio**–Zootrópolis (Zootopia)
- **15 julio**–Un Gran Dinosaurio (The Good Dinosaur)
- **22 julio**–Carlitos y Snoopy: la película de Peanuts
- **29 julio**–Los Minions (Minions)

#### ¿Hay que inscribirse?

No, nada más venir. El programa es gratis. ¡Llegue temprano para conseguir un asiento bueno!

#### ¿Las películas son para niños de qué edad?

Todas las películas tienen la clasificación PG. Son

para la pública en general, pero se recomienda la guía/orientación de los padres.

#### ¿Se puede traer un bocado o almuerzo?

Sí, como no. ¡Queremos que sean cómodos y que se disfruten de la película!

Patrocinado por Los Amigos de la Biblioteca de Wilsonville.



Todos los viernes de julio habrá películas para la familia en español en la Biblioteca de Wilsonville, 2-3:30 p.m.

### National Night Out on August 2

Tuesday, August 2, is National Night out when many Wilsonville residents and neighborhood associations host an event in their neighborhood.

National Night Out provides an opportunity for neighbors to get to know one another, heighten crime-prevention awareness and strengthen neighborhood spirit and police-community partnerships.

**National Night Out**  
• Tuesday, Aug 2,  
Various neighborhoods  
in Wilsonville

While not a City-sponsored program, the city can assist neighborhoods in requesting a visit by Wilsonville Police and Tualatin Valley Fire and

Rescue (TVF&R) if they are available. To arrange a visit please contact Jon Gail, Community Relations Coordinator, at 503-570-1502 or gail@ci.wilsonville.or.us.

No city permit is required if events are held on private property and do not block street traffic. If a neighborhood wishes to close a street to traffic or hold a gathering on public property or in a park, a permit may be required; please contact Brian Stevenson, Recreation Coordinator, at 503-570-1523 or stevenson@ci.wilsonville.or.us.

### Neighbors Helping Neighbors Walk Set for Saturday, July 16

Wilsonville Community Sharing, a non-profit food bank and social service agency in Wilsonville, is holding a second annual benefit walk, called Neighbors Helping Neighbors, on Saturday, July 16, beginning at 8:30 am at the Charbonneau Country Club, 32000 SW Charbonneau Drive, in Wilsonville.

In the first year of the walk, the event raised \$5,000 in cash donations and another \$5,000 in in-kind donations. This year event organizers hope to beat that goal by raising an additional \$1,000 in each category.

City-wide recruitment is underway for walkers (individually or as teams), sponsors, volunteers and in-kind donations. To register a team visit [www.counta.cc/1SYXJBD](http://www.counta.cc/1SYXJBD).

For more information contact Leigh Crosby, Food Bank Coordinator, Monday – Thursday, 503-682-6939; [leigh@wilsonvillecommunitysharing.org](mailto:leigh@wilsonvillecommunitysharing.org).



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

### Thursday Summer Fun Shows Continue through July 28

Thursday Summer Fun Shows for children continue through July with performances filled with stories, songs, reptiles and more! These free shows are Thursdays at 11 am and 12:30 pm in the Oak Room, unless otherwise stated.

- **July 7:** Mz. Pearl's Variety Show
- **July 14:** Teatro Calamari
- **July 21:** Music and more with Brad Clark
- **July 28:** The Reptile Man (11 am at Murase Plaza)



Find out more at [www.WilsonvilleLibrary.org/SRP](http://www.WilsonvilleLibrary.org/SRP).

### Adult Summer Reading Program

Complete a Tic-Tac-Toe or a Reading Log for entry into the Grand Prize Drawing at the end of summer.

Prizes include a Kindle Paperwhite e-reader, Portland Art Museum annual pass for two, Dinner and a Movie (\$100 value), movie tickets, and more.

Find out more at: [www.WilsonvilleLibrary.org/ASRP](http://www.WilsonvilleLibrary.org/ASRP)

### Teen Summer Programs continue through July 25

Join us on Mondays in July for special events just for teens:

- **July 11:** Library After Dark (8:30–11 pm)
- **July 18:** Game Day (4–6 pm)
- **July 25:** Party in the Park & Grand Prize Drawing (4–6 pm in Murase Plaza at Memorial Park)

Find out more at [www.WilsonvilleLibrary.org/TSRP](http://www.WilsonvilleLibrary.org/TSRP).



### Science Classes for Kids in August

Free one-hour hands-on science classes by Mad Science for kids entering grades 1–5.

- **Super Structures,** Monday, Aug. 1, 5–6 pm and 6:30–7:30 pm
- **Watts Up,** Tuesday, August 2, 10–11 am and 11:30 am–12:30 pm
- **Slime Time,** Wednesday, August 3, 1–2 pm and 2:30–3:30 pm
- **Stunt Planes & Gliders,** Thursday, August 4, 10–11 am and 11:30 am–12:30 pm



Sign-up starting July 11 at the Children's Desk or by phone (503-570-1599). Space is limited, so register early. Limit of two sessions per child.

### Upcoming Classes and Events

**Library closed Monday, July 4**

**Local Artist of the Month: Clarice Benz**

**Mama Bear Moms Group**

Networking and support for mothers of young children.  
Fridays, 4–5:30 pm  
[www.WilsonvilleLibrary.org/moms](http://www.WilsonvilleLibrary.org/moms)

**First Friday Film**

British comedy-drama about a lady in a van.  
Friday, July 1, 6–8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

**Book Club**

*All the Light We Can Not See*  
by Anthony Doerr  
Thursday, July 14, 6–8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

**Genealogy Club**

Monday, July 18, 1–2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

**Great Books Discussion Group**

*On Evil* by Maimonides  
Tuesday, July 19, 6–8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

**History Pub at Wilsonville McMenamins**

Tuesday, July 26, 6:30–8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

**Game Night**

Wednesday, July 27, 6–8 pm  
[www.WilsonvilleLibrary.org/games](http://www.WilsonvilleLibrary.org/games)

### SMART Expands Compressed Natural Gas Fueling Station

South Metro Area Regional Transit (SMART), the City's employer-supported public transportation service, has expanded the agency's Compressed Natural Gas (CNG) fueling station at the SMART Central Office and bus yard on Boberg Road in Wilsonville.

The expanded CNG station provides SMART the ability to increase the number of buses that run on the lower-cost and lower-emission fuel. Currently SMART has four CNG-powered buses, but has plans to grow the fleet now that the station has been expanded.

SMART originally designed the station to be incrementally expanded and upgraded over time. In addition to increasing the fuel storage capability by roughly 40%, the new fuel tanks installed are much

more robust and safer even under higher pressure.

To fund the \$160,643 project, SMART secured a \$100,000 grant from the Oregon Department of Transportation (ODOT) and the City matched the grant by covering the remaining \$64,643 of project costs.



### Tips for Saving Water—Spruce Up Your Sprinklers

The Environmental Protection Agency (EPA) wants you to know, "When it comes to a home's irrigation system, a little maintenance goes a long way."

Before you ramp up your watering efforts, spruce up your irrigation system by remembering four simple steps—inspect, connect, direct, and select. Homes with clock timer-controlled irrigation systems use about 50 percent more water outdoors than homes without irrigation systems.

Your system can waste even more if programmed incorrectly, a sprinkler head is pointed in the wrong direction, or you have a leak.

**Inspect.** Check your system for clogged, broken or missing sprinkler heads. Better yet, go with a pro—find an irrigation professional certified by a WaterSense labeled program to do the work for you.

**Connect.** Examine points where the sprinkler heads connect to pipes or hoses. If water pools in your landscape or you have large wet areas, you could have a leak in your system. A leak about as small as the tip of a ballpoint pen (or 1/32nd of an inch) can waste about 6,300 gallons of water per month!

**Direct.** Are you watering the driveway, house, or sidewalk instead of your yard? Redirect sprinklers to apply water only to the landscape.

**Select.** An improperly scheduled irrigation controller can waste water and money. Update your system's watering schedule to align with the seasons, or select a WaterSense labeled weather based irrigation controller to take the guesswork out of scheduling.

To learn more water conservation tips and to lower your water bill visit: [www.epa.gov/watersense/](http://www.epa.gov/watersense/).

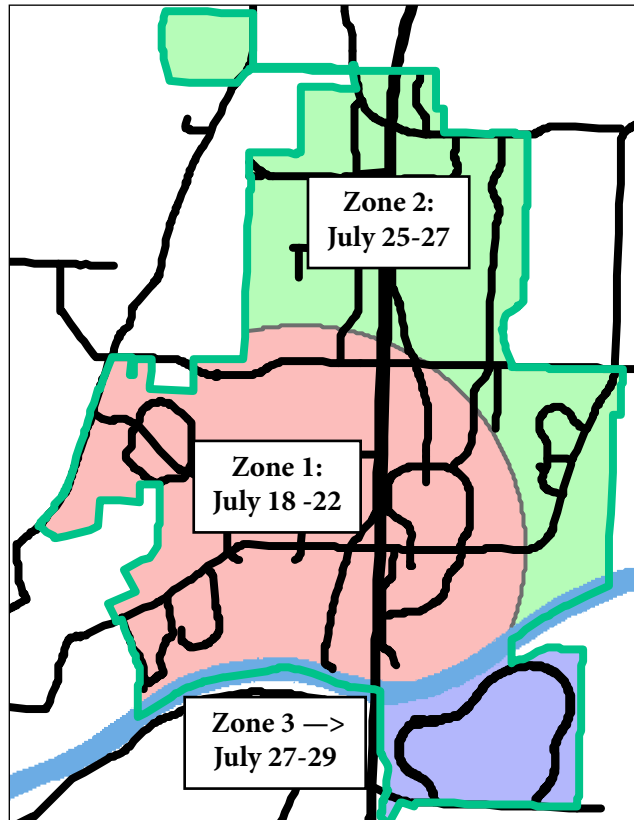


### Annual Water Line Flushing to Take Place in Mid- to Late-July

Every year Wilsonville's utility workers flush the City's water mains to remove naturally occurring mineral deposits from the pipes. Annual flushing ensures optimal water quality for Wilsonville's residents and helps maintain the infrastructure by exercising hydrants, valves and blow-off assemblies during the process.

When the utility workers is flushing in your area, you may also observe that the water in your home or the water coming out of a nearby hydrant appears to have an orange or rust-colored tint. This is a normal occurrence and the water is completely safe. The discoloration is caused by iron deposits in the pipes which get scoured out during flushing. If you observe discolored water in your home, simply run the COLD water for several minutes to clear the lines and flush the toilets once or twice. The water should run clear again within a few minutes.

If you observe a utility crew flushing in your area, give them plenty of room to work safely while sharing the roadway. Also, be aware that some flushing activities take place at night to minimize traffic congestion. If you have any questions or concerns regarding the City's annual water-main flushing program, call the Public Works Department at 503-570-4092.



This map shows when the water lines are to be flushed.

## SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
smart@ridesmart.com  
www.RideSMART.com

### Walk @ Lunch – July 27, 2016

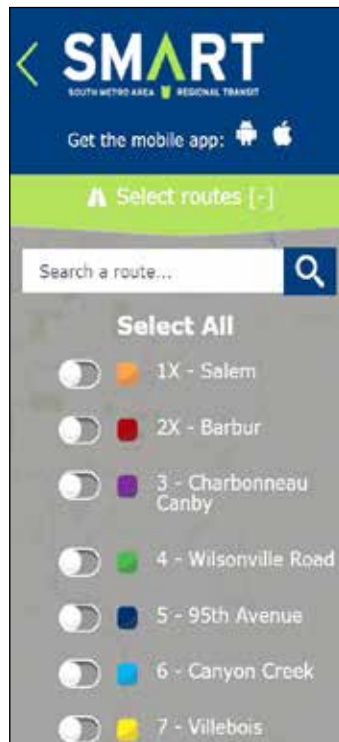
Join SMART at noon on Wednesday, July 27, for a 50-minute walk during lunch and take steps towards a healthier you. Meet at Graham Oaks Nature Park, 11825 SW Wilsonville Road in Wilsonville – rain or shine. Join us at the parking lot for a walk down to Subway, Qdoba and Wilsonville Smiles who are the partners of this group walk.



Meet new friends and do some lunchtime networking. Remember to bring your walking shoes to work with you!



### New App Gets You Bus Routes on the Go!



Not long ago the idea of being able to use your computer or smart phone to locate a bus or see when the next bus arrives at a given stop would have seemed futuristic. Thanks to SMART's new SPOT App the future is now!

Visit [smartpublic.etaspot.net](http://smartpublic.etaspot.net) to view our live system map. This system is brand new, so we may experience glitches along the way. Please let us know at [smart@ridesmart.com](mailto:smart@ridesmart.com) if you have problems or would like to share feedback about the new App.

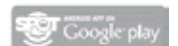
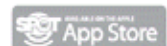


VIEW ROUTES • SYSTEM ALERTS  
REAL-TIME STATUS UPDATES • 100% FREE

VISIT OUR MOBILE SITE



DOWNLOAD THE SPOT APP FOR YOUR MOBILE DEVICE



### Ride the Trolley Shuttle to Kiwanis Fun Run Saturday, July 30

SMART is providing a free trolley ride on Saturday, July 30, 8 am–2 pm, for the Kiwanis Kids Fun Run.

On the day of the event the trolley runs between

**Kiwanis Kids Fun Run & 5K**  
• Sat, July 30, 8 am–1 pm  
Wood Middle School  
11055 SW Wilsonville Road

Boones Ferry Primary and SMART Central. Attendees are also welcome to just hop on the trolley to enjoy

a fun and relaxing activity to do between while at the event. The round-trip takes about 20 minutes.





29600 SW Park Place  
Administrative Office: 503-783-7529  
Community Center: 503-682-3727  
www.WilsonvilleParksandRec.com

**Parks and Rec and WCSI Host Chamber Spark Event**



Chamber Ambassador, Gladys Boutwell (right) of Pacific Benefit Planners talks with Diane Mead (left) of Pets First, at the Wilsonville Community Center during the Chamber of Commerce Morning Spark event. The event was jointly hosted by Wilsonville Parks and Recreation Dept. and Wilsonville Community Seniors Inc.

**Inter-Generational Partnership**

Donna Atkinson and the Wilsonville Community Seniors, Inc. formed a new partnership with Art Tech High School. Art Tech students Diana Campos, Austin McFarland, Jacob Humphrey and Carolyn Remington, under the watchful eye of teacher Ben Whitcomb, produced an order of shirts for the senior Board of Directors. As part of a printing class, the group saw the project through from design of the artwork, to printing, as well as sending an invoice for payment. According to Jacob Humphrey, “Producing the shirts was not easy, this was our first time filling a print order.” According to teacher Ben Whitcomb, “This has been a real-life business experience for the students from beginning to end. The school has already invested in equipment to improve production for next year’s school year.” The WCSI Board and Art Tech plan to continue working together on future print jobs to provide learning opportunities for future Art Tech students.



**Upcoming Classes and Events**

**Womens Personal Safety and Self Defense**

Saturday, July 16  
2–5 pm  
Lee’s Martial Arts  
\$89  
Open to females ages 13 and older

**July Picnic Lunch**

Friday, July 1  
Noon  
Community Center  
Enjoy a burger bar as part of the Senior Nutrition Program  
\$3.50 for those 60 and older  
\$4.25 for those under 60

**Crafty Card Creations**

Tuesday, August 8  
9 am–Noon  
Community Center  
\$20 (includes all supplies)

**Camps, Camps, and more Camps!**

Wilsonville Parks & Recreation offers a variety of program options for children of all ages. In July and August the camps continue!  
Full list of camp offerings and registration available at: [WilsonvilleParksandRec.com/SummerCamps](http://WilsonvilleParksandRec.com/SummerCamps)

**Skyhawks Soccer Camp**

July 11–15  
Memorial Park  
5–7 yrs: 9 am–noon  
7–12 yrs: 9 am–3 pm  
\$109/\$145

**Cartoon Zoo Camp**

Ages 6–11  
July 18–21  
9–11:30 am  
Tauchman House  
\$140 (+\$25 materials)



**Lego Engineering Brixology**

Grades K–5  
July 25–29  
9 am–noon  
Tauchman House  
\$179

**Skyhawks Basketball Camp**

Ages 7–12  
July 25–29  
9 am–3 pm  
Boones Ferry Primary  
\$145

**Acting Experience Theater Workshop**

Ages 13–19  
Aug. 1–6  
10 am–5 pm  
Tauchman House  
\$75

**Junior Science Explorers**

Grades K–2  
Aug. 8–12  
9 am–noon  
Tauchman House  
\$179

**Pastel Drawing Ocean Life**

Ages 9–13  
Aug. 15–18  
9 am to noon  
Tauchman House  
\$160 (+\$35 materials)

**Skyhawks Mini Hawks**

Ages 4–7  
Aug. 15–19  
9 am–noon  
Memorial Park  
\$109



**Challenger Mini Soccer**

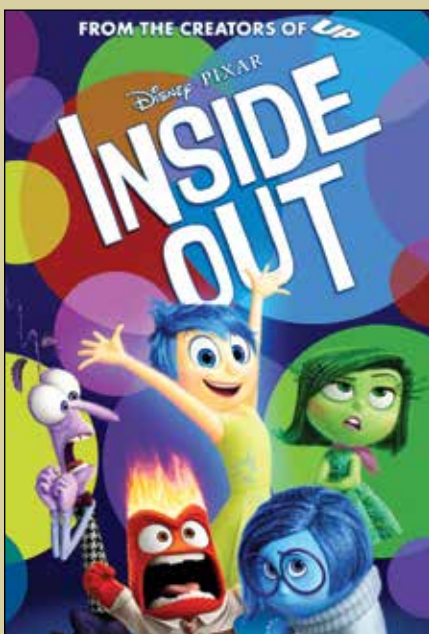
Aug. 22 –26  
Memorial Park  
2–3 yrs: 5–6 pm  
4–6 yrs: 6–7:30 pm  
\$89/\$105

**Jr. Golf**

Ages 7–17  
Sat. Aug. 6–27  
3–4:30 pm  
Charbonneau Golf Club  
\$120

**MOVIES IN THE PARK**

*Fridays this summer!*



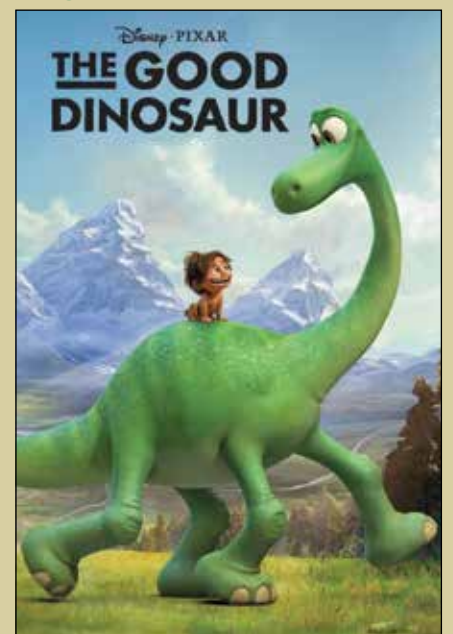
July 15



July 29



August 5



August 19

All movies are free and will be shown on an inflatable screen at the River Shelter in Memorial Park. Lawn opens at 8:15 pm, movie starts at dusk. Be sure to bring a blanket to sit on.

## Fireworks Safety Tips

### Got Fireworks? Keep it Legal. Keep it safe!

Oregon law bans fireworks that fly, explode, or travel on the ground more than six feet — this includes bottle rockets, roman candles, firecrackers and M80s.

To be legal, purchase all fireworks at a licensed Oregon fireworks stand. Fireworks purchased by mail order or in the state of Washington or at Native American reservations may be illegal in our state.

Under Oregon law, officials can seize illegal fireworks, and you can be fined up to \$500 per violation and/or arrested. You can be held civilly liable for damages resulting from improper use of any fireworks — legal or illegal. Oregon law also makes parents liable for damage caused by their children and al-

lows fire departments to charge for the cost of suppressing fires caused by fireworks. Additionally, the use of illegal fireworks constitutes criminal activity, and your insurance policy may not cover you if you engage in a criminal act that results in damage.

Even legal fireworks are dangerous and have caused injuries and burns to users, especially unsupervised children. Also, during dry summer conditions, an errant firework can ignite vegetation, vehicles and even your roof.

Tualatin Valley Fire & Rescue advises you to celebrate safely this year by following the following safety tips:

- Only adults should light or handle fireworks.
- Supervise children at all times when fireworks are being used.
- Store fireworks, matches, and lighters out of the reach of children.
- Use fireworks outdoors on a paved surface — away from buildings, vehicles and vegetation.
- Never point or throw fireworks at people, pets or buildings.
- Never alter fireworks or make your own. Homemade explosives can be deadly!
- Have a hose or bucket of water nearby to douse misfired and spent fireworks and to extinguish potential fires.

For more safety tips, visit Tualatin Valley Fire & Rescue's website at [www.tvfr.com](http://www.tvfr.com).



## CHIEF'S CORNER – SUMMER SAFETY TIPS

Summer is a great time of year for activities with the longer days and the warmer weather. Your officers with the Wilsonville Police Department want to help you have a safe and enjoyable summer by passing along some summertime tips and reminders.

### Visiting our Parks

Again this year, all Wilsonville Parks are open, 5 am–10 pm, which is a change from two years ago when the parks were only open from dawn to dusk, so you'll continue to have more time to enjoy them. To maintain the quality of life in the parks, alcoholic beverages are not allowed, other than for pre-approved events and reservations. Additionally, smoking is prohibited in all parks, as well as dogs off leash except in approved off-leash areas.

### Child Safety

Proper supervision of children in the summer can be a challenge but it is especially important. Summer activities such as swimming, cycling and other out-

***In cases of non-emergency situations call the Police Department's dispatch line at 503-655-8211.***

ings can present dangers that our children might not recognize. Regardless of their age, I encourage parents to have conversations with their children about what their expectations are for safe behavior and specific risks which should be avoided. Additionally, they should be supervised in a manner appropriate to their age, maturity and the risks inherent in the activity. Many activities have protective equipment that is required by law. For example, boating with children requires that they wear an approved flotation device. Cycling requires a helmet. There are other activities where helmets and other protective



Chief Jeff Smith

gear are not mandated but can definitely reduce the dangers of an activity.

### Curfew

While many other cities have more restrictive curfew hours, the City of Wilsonville follows state statute which includes the hours between midnight and 4 am. If a child is found out in public during curfew hours without a parent or guardian in their company and the minor is not engaged in a lawful activity that requires that they be in the public place at that time, the minor is in violation of ORS 419C.680. For the safety and security of that child, he or she must be taken into protective custody.

### Contacting the Police

Any time you are in need of police, fire or medical response, you should do so in the way that will produce the best possible response. If you are experiencing or witnessing an emergency or a crime in progress, call 911. In cases of non-emergency situations call the Police Department's dispatch line at 503-655-8211. Our officers are on the street, responding to calls and looking for issues of public concern, so calling police dispatch is the best tool for getting the response or answer you need rather than calling our business office.



I hope these tips and reminders are helpful. However, if something does arise, please know the Wilsonville Police Department is always here to help any time of the day or night. I hope you and your family have a safe and fun summer.

*Jeff Smith, Chief of Police*

## More Summer Safety Tips

### Know What's Below Call 8-1-1 Before You Dig

Summer is a popular time of the year to tackle projects around the house or yard, but don't forget to call before you dig. Many utilities, including natural gas lines, are often located just a foot or so under the surface and may be located in places one would not expect.

Careless digging is not only against Oregon law, doing so can also damage underground utilities, causing disruption of vital services, property damage and even possible injury. To avoid problems,



**Know what's below.  
Call before you dig.**

residents are urged to call 8-1-1 to obtain a "Locate Request" at least two days before you begin digging.

You can submit a Locate Request any time of the day or night, however, locate requests received outside normal business hours will be transmitted and start of the next business day. After the request is received, the utility companies have two business days to locate and mark underground lines and facilities using color-coded paint.

## Window Fall Prevention Tips

- **Keep windows closed and locked** when not in use.
- **Keep play areas, furniture,** and anything a child can climb on **away from windows.**
- **Do not open windows more than 4 inches** and install window stops to prevent windows from opening wide enough to allow a child to tumble out.
- Window stops should be easily removed by an adult in the event of a fire or other emergency.
- For a **double-hung window, open the top portion** near the ceiling for ventilation while keeping the bottom portion closed.
- Actively **watch children near windows.**

## Water Rescue Safety Tips

Trained rescuers teach "Talk, Reach, Throw, Row":

1. **Talk.** First, call 911 — then talk to the person and see if you can coach them to swim to you on shore or to a boat.
2. **Reach.** Next you can try to reach to them by extending a pole, branch, or inflatable boat. Do not allow them to grab hold of your body, as they may accidentally also pull you in.
3. **THROW.** You can throw a distressed person a life jacket, life ring, rope or any other object that will help them stay afloat.
4. **ROW.** If needed, use a boat to get to the person.

## Tips to Limit Exposure to Lead from Older In-Home Water Pipes

Water treated at the Willamette River Water Treatment Plant in Wilsonville does not contain lead, however lead may still leach into the tap water if a home or building's plumbing system was constructed with lead fittings or lead solder.

In order to help limit corrosion and leaching of lead into a home's drinking water, Wilsonville's water supply is treated at the plant, but this does not prevent all potential leaching that can occur.

According to the Center for Disease Control (CDC), "Lead found in tap water usually comes from the corrosion of older fixtures or from the solder that connects pipes. When water sits in leaded pipes for several hours, lead can leach into the water supply."

Prior to the 1986 amendments to the Safe Drinking Water Act federal law allowed a home's water pipe/plumbing system with fixtures and solder that were made with high concentrations of lead.

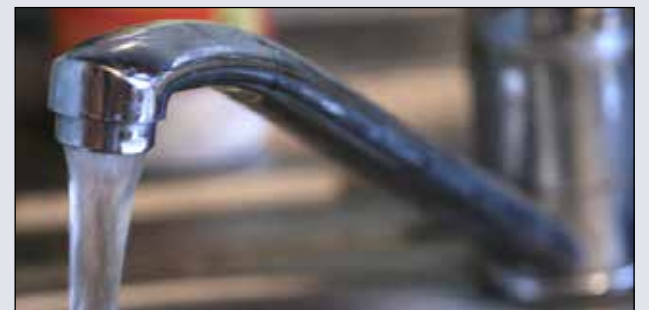
In older homes or buildings constructed with lead fittings and solder the CDC recommends:

- Using only cold water for drinking and cooking— Hot water is more likely to contain lead and other contaminants.
- Before using any tap water for drinking or cooking, flush the water system by running the tap on cold for one-two minutes; After flushing the system this water is suitable for drinking, cooking, preparation

of baby formula, or other consumption. To conserve water, collect multiple containers at once.

- Boiling water does not remove lead from water.
- Children and pregnant women are especially vulnerable to the effects of lead exposure so they should use bottled water or water from a certified filtration system.
- Testing your drinking water for lead by contacting your state lead program.
- Inspecting the aerator on the end of the tap's faucet and remove any debris.
- Skin does not absorb lead in water so bathing and showering poses not risk of exposure to lead.

For more information about safe drinking water visit [www.epa.gov/safewater](http://www.epa.gov/safewater).



To remove lead from drinking water in older homes that have lead plumbing fixtures, run cold water for one-two minutes before using the water for cooking and drinking.



## Reports from City Boards and Commissions

### Council Approves 10-Acre Annexation and Rezoning for Republic Service's SORT Bio-Energy Facility and Campus Expansion

At the June 6, City Council meeting, the Council held public hearings and approved on first reading an annexation, a Comprehensive Plan amendment and a Zone Map amendment for a 10.2-acre site on SW Garden Acres Road that is adjacent to and north of Republic Services' campus located at 10295 SW Ridder Road in Wilsonville. The parcel is to be the future site of Republic Services' proposed SORT Bio-Energy Facility that is designed to collect and convert commercial food waste into electricity and Compressed Natural Gas (CNG). The approval advances the City's waste-hauling and disposal provider's plans to expand the campus located in northwest Wilsonville. The Council action changed the zoning of the parcel from Washington County Future Development-20 to a Planned Development Industrial-Regionally Significant Industrial Area Zone and changed the Comprehensive Plan designation from Washington County Future Development-20 to the Industrial designation. The development proposal was approved by the Development Review Board (DRB) Panel A on May 9 and a second reading of the land use applications is to be held at the City Council meeting on June 20. Construction of the bio-energy facility may begin later this year, but still also requires Republic Services to obtain a permit from the Department of Environmental Quality (DEQ) and a franchise agreement from Metro to begin the innovative new method of converting commercial food waste from the Portland-metro area into energy and compost instead of being disposed of in landfills.

The City Council also held a second reading and adopted a Comprehensive Plan amendment and a Zone Map amendment for a proposed 14-lot residential subdivision located on 4.37 acres at 28500 and 28530 SW Canyon Creek Road South. The Council's approval changed the zoning from Residential Agriculture-Holding to the Planned Development Residential-3 Zone and changed the Comprehensive Plan designation from 0-1 to 4-5 dwelling units per acre. In addition to the 14 residential lots, the subdivi-

sion includes a public street, preservation of over two acres of Significant Resource Overlay Zone (SROZ) and a private park for the neighborhood. The development proposal was first approved by the Development Review Board (DRB) Panel B on April 25.

Under the Consent Agenda the Council approved a resolution authorizing the City Manager to execute an addendum to the Frog Pond Area Plan Contract with Angelo Planning Group to continue the long-range planning work the consultant group is completing for the Frog Pond area.

During the Mayor's Business portion of the meeting, the City Council approved the reappointment of two current committee members Kathleen Mason and Elaine Swyt to three-year terms on the Tourism Promotion Committee ending June 30, 2019.

The City Council also approved a set of proposed amendments to Wilsonville's 2013 Transportation Systems Plan known collectively as the "2016 TSP Amendment." The TSP is the City's long-term policy and planning document for all types of transportation improvements, including road, bicycle, pedestrian and transit-related projects. The Plan also features higher priority projects that are planned to be implemented over a 20-year timeframe through the City's Capital Improvement Program (CIP), development review processes and occasionally by other agencies.

During the Communications portion of the meeting, the City Council heard separate reports from the Washington County Visitors Association by CEO Carolyn McCormick and also a report on Wilsonville's recent designation as a Bronze Level Walk Friendly Community by the national Pedestrian and Bicycle Information Center (PBIC) based at the University of North Carolina in Chapel Hill (see article on page 1).

Prior to the regular meeting at the work session City Council heard reports and discussed the Declaration of Emergency Exercise; Water Treatment Plant Master Plan Update; Wilsonville Road / Kinsman Road truck turning radius results; Ozone Coalition Letter; and an update on the Basalt Creek Concept Plan.



City staff along with City Councilors Susie Stevens and Julie Fitzgerald (not shown) participated in the Tonkin Challenge in June. The annual fun run and walk hosted at Audi Wilsonville raised money for schools in Wilsonville and other areas.

### Council Approves 14-Lot Subdivision on Canyon Creek Road South and Construction Contract for Utility Repairs in Charbonneau

On May 16, the City Council held public hearings and approved both a Comprehensive Plan amendment and a Zone Map amendment for a proposed 14-lot residential subdivision located on 4.37 acres at 28500 and 28530 SW Canyon Creek Road South. In addition to the 14 residential lots, the subdivision includes a public street, preservation of over two acres of Significant Resource Overlay Zone (SROZ) and a private park for the neighborhood. The development proposal was first approved by the Development Review Board (DRB) Panel B on April 25.

Under the Consent Agenda the Council approved a resolution authorizing the City to award a construction contract to Canby Excavating, Inc., for the construction of the Charbonneau High Priority Utility Repair Phase I which is the first construction project from the Charbonneau Consolidated Improvement Plan.

During the Mayor's Business portion of the meeting, Mayor Tim Knapp issued a proclamation for May 15-21 to be Public Works Week recognizing the consistent contributions of the City's Public Works staff to provide a high-quality service to the community.

During the Communications portion of the meeting, the City Council heard separate reports from Metro Councilor Craig Dirksen and Oregon Institute

of Technology Vice President Laura McKinney. The Council also received an update on the Rotary Club of Wilsonville's Through A Child's Eyes (TACE) program and Alan Kirk of OrePac Building Products presented Clackamas County Chair John Ludlow a check in the amount of \$5,000 to support the annual inmate-child bonding event at Coffee Creek Correctional Facility. Republic Services Manager Frank Lonergan presented a donation check of \$550 in support of the food-pantry and social services programs to Wilsonville Community Sharing board member Michelle Ripple.

Prior to the regular meeting at the work session City Council heard reports and discussed the Boones Ferry Road to Brown Road Connector Corridor Plan, Wilsonville priority transportation projects, Transportation Systems Plan minor amendments, LED street-lighting project, Metro-Wilsonville Community Enhancement Program and Metro's Equitable Housing Planning and Development grant.

After the meeting the City Council convened as the City's Urban Renewal Agency and awarded an engineering contract to OTAK for the Boones Ferry Road to Brown Road Connector Corridor Plan that seeks to design an alternate route into and out of the Old Town commercial district.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during July. Scheduling changes can and do occur; check for updated meeting information at the City's website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Thursday, July 7:** Planning fees and Community Development fund review; discussion of Marijuana Ballot Measure Title and regulations; Frog Pond Funding and Parks implementation; recreation and aquatic center operator award; recreation and aquatic center Ballot Measure Title; Town Center Redevelopment Plan consultant contract; regional park development agreement; Interfund Loan; and property purchase.

**Monday, July 18:** Transit Master Plan update; IT Strategic Plan; Wilsonville Road traffic management/red light camera; Metro Equitable Housing Grant Application; franchise agreement.

### Planning Commission Update

At the Planning Commission meeting on June 8, the Commission heard a report and discussed the proposed recreation and aquatic center and also heard an update and discussed the Basalt Creek Concept Plan. For more information on the two projects visit [www.WilsonvilleParksandRec.com/WRAC](http://www.WilsonvilleParksandRec.com/WRAC) and [www.BasaltCreek.com](http://www.BasaltCreek.com).

On July 13 the Planning Commission is holding a public hearing on the formation of the proposed Coffee Creek Urban Renewal District when public comment is welcomed.

### Development Review Board Update

At the Development Review Board (DRB) Panel A meeting on June 13, the board held a Public Hearing and approved a sign to be installed at 8855 SW Holly Lane Monument Sign for the applicant Two G's Real Estate.

Due to a lack of agenda items the Development Review Board Panel B meeting on May 23 and June 27 were canceled.

### Library Board Update

During the Library Board meeting in May various reports included:

- A Disney themed teen event was held in May and local volunteers of all ages held a work party to prepare Summer Reading Program (SRP) packets.
- SRP began in June along with Teen programs and children's programs.
- Approximately 2500 kids are expected to sign up for the SRP.
- The Friday Family Films shown in the summer is expanding to include Spanish films at 2 pm following the English language film. Outreach librarian Deborah Gitlitz will be in attendance to make connections with any Spanish speaking families that attend.
- Librarians toured other libraries to see other layouts and programs and gain insights to consider for Wilsonville.
- Library statistics show print material is shifting from paper to e-books, away from practical non-fiction categories to online video. The Library strives for maintaining a collection that reflects what people are looking for in the building.
- Improvement projects have been identified by an energy services contractor for building upgrades which include a new HVAC system in the west side of the building and in for the conference rooms.
- The Library Board is completing a final review of the Library's Policies and Procedures which are expected to be finalized at the June meeting.
- The Library Foundation held a Spring Luncheon fundraiser and is developing a plan for continuing to work with donors throughout the year.
- After eight years (two terms) on the Library Board member Alan Steiger's term ends June 30. The City has received numerous applications to serve on the Library Board and expects to make an appointment later this summer.

The next regular meeting is scheduled for Wednesday, July 27, 6:30 pm, at the Library.



Alan Steiger served eight years on the Library Board.

## Charbonneau High-Priority Utility Repair and Replacement Project Now Underway

The first phase of the Charbonneau High-Priority Repair project is now underway to repair and replace failing utility lines that are broken, partially collapsed or sagging. The project is part of the Charbonneau Consolidated Improvement Plan, a 25-year plan to upgrade and improve deficient Charbonneau infrastructure approved by City Council in 2014.

The project involves digging open trenches to repair and replace approximately 2400 feet of sanitary, storm, and water pipeline in Charbonneau. In many cases due to the size and condition of some of the failing pipes the contractor must dig an open trench to repair and install new utility lines. Using this technique the contractor expects to be able to repair about a block per week.

The project also involves repairing approximately 850 feet of sanitary and storm pipe that is cracked or corroded using a technique called Cured-In-Place Pipe (CIPP). This method of construction consists of inserting a resin filled liner through the old pipe that is inflated and steam cured to create a new pipe inside of the old one. This construction method moves along much quickly, about a couple of blocks per day, because the technique does not require as much excavation. CIPP, however, generates a lot of noise and steam during installation and can cause a styrene odor during the curing process that is noticeable, but is not

harmful.

In addition to the pipeline work, the project also includes replacement of manholes, catch basins, and water valves to meet current design standards. Service pipes servicing individual properties are also to be replaced within the roadway.

Also, beginning in late July through October, the Charbonneau pathway next to French Prairie Drive is to be closed for repairs in order to remove major trip

### The Charbonneau High-Priority Repair project involves eight different construction zones this year:

1. Estates Post Road: Sanitary & Storm Open Trench & CIPP
2. Molalla Bend Road: Sanitary & Storm Open Trench
3. Armitage Road (South): Storm, Open Trench & CIPP
4. Armitage Road (North): Storm, Open Trench & CIPP
5. Boones Bend Road: Storm, Open Trench
6. Winchester Way: Storm, Open Trench
7. Boones Bend – Mariners Village: Water, Open Trench
8. French Prairie Drive Pathway: Repair trip hazards

hazards.

Construction, which is anticipated to last through October, is to be performed on weekdays, 7 am–5 pm, with no work to be completed on weekends.

Due to the number of job sites involved in this large-scale project there may be periods of time where the

work appears to have stopped. The contractor is likely performing similar types of work at one time, such as catch basin installation and final paving, and is moving from site to site.

Community members should expect daytime road closures in the areas of active construction, but normal traffic operations are to be restored by 5 pm each day. Access for garbage service, deliveries and emergency vehicles are to be maintained throughout construction zones. Residents are to be provided access to their driveways during construction, but delays should be expected. During active construction the contractor is planning to store materials, such as rock and pipe and equipment, nearby, along the side of the roadway and/or in nearby parking areas.

During active construction residents should plan for additional travel time and noise and dust from construction. Interruption of utility services is not anticipated, however, there may be instances where residents will be asked not to run appliances with heavy water use (dish or clothes washer), flush your toilet, or take a shower for a couple of hours at a time.

To follow construction progress and get weekly updates check out the project board located in the entrance to the Charbonneau Country Club, visit [www.ci.wilsonville.or.us/CharbonneauRepairs](http://www.ci.wilsonville.or.us/CharbonneauRepairs) or the “Charbonneau Country Club” Facebook page. For more information contact the City’s project manager, Zach Weigel, at 503 570-1565 or by email at [weigel@ci.wilsonville.or.us](mailto:weigel@ci.wilsonville.or.us).

## Sample New Types of Berries

OSU’s North Willamette Research & Extension Center, 15210 NE Miley Road, Aurora is holding a Blueberry Field Day on July 6, 1–5 pm, to introduce new varieties of blueberries bred and grown at the nearby research farm. Local farmers and community members are invited to attend the free event.



## Community Health Fair Planned for Saturday, August 20

Members of the 2016 Wilsonville Leadership Academy are holding a free Community Health Fair at the, Lamb’s Wilsonville Market Place, 8255 SW Wilsonville Road, on Saturday, August 20, 9 am–1 pm.

The purpose of the free event is to explore ways to really keep your family fit and healthy. The event includes local health and wellness vendors, cooking demonstrations, fun activities, free kids bike helmets from Legacy Health (while supplies last) and much more!



## Calling All Explorers: Become a Wilsonville Pathfinder!

There is a new way to explore and get to know Wilsonville—become a Wilsonville Pathfinder.

The brand new activity, developed by the 2016 Wilsonville Leadership Academy, invites community members of all ages to create a passport, solve clues, explore City parks, attend events and win prizes.



Each page of the passport takes you on a new and different adventure, including fun events/activities that you may not know about and places you may have never been.

To play you first need to download and create a passport at [www.WilsonvilleParksandRec.com/Pathfinders](http://www.WilsonvilleParksandRec.com/Pathfinders) and then pick your first destination and head out for a fun adventure!

After you have visited ten destinations and four events bring your passport into the Parks & Rec office at 29600 SW Park Place Court by August 31 to get a fun prize and be entered in the grand prize drawing.

Get started today at [www.WilsonvilleParksandRec.com/Pathfinders](http://www.WilsonvilleParksandRec.com/Pathfinders).

## Road Construction Report

### Kinsman Road Extension

In July, construction of the Kinsman Road extension between SW Barber Street and SW Boeckman Road begins. This project involves construction of a new section of Kinsman Road and includes up-sizing and relocation of a 30-inch sanitary sewer pipe and installation of a 66-inch water line for the Willamette Water Supply Program. The contractor for the project is Emery & Sons Construction and the road is expected to be complete by spring 2018.

### Wilsonville Road Railroad Crossing

On Friday, Aug. 5, until Monday, Aug. 8, repair work on the Wilsonville Road railroad crossing is planned to occur. In order to resolve ongoing grade issues, the railroad tracks and the bed they rest on must be rebuilt. **The project requires the full closure of Wilsonville Road**



**from Friday, Aug. 5, 8:00 p.m. until Monday, Aug. 8, 5:00 a.m.** Work will also continue to take place in the surrounding area for the following two weeks with periodic single lane closures during non-peak hours. **A detour route using SW Barber Street will be in effect during the first weekend in August.**

## City Council Notice: Coffee Creek Urban Renewal Plan

The Wilsonville City Council is holding a public hearing to consider an ordinance for the adoption of the proposed Coffee Creek Urban Renewal Plan. The hearing is to be held as follows:

**Monday, August 1, 2016  
at 7:00 p.m.**

**City Hall, 29799 SW Town Center  
Loop E, Wilsonville**

A copy of the ordinance, the proposed Coffee Creek Urban Renewal Plan and accompanying report may be obtained by contacting:

Nancy Kraushaar, Community Development Director at 29799 SW Town Center Loop E, Wilsonville 97070 (503) 570-1562 [kraushaar@ci.wilsonville.or.us](mailto:kraushaar@ci.wilsonville.or.us).

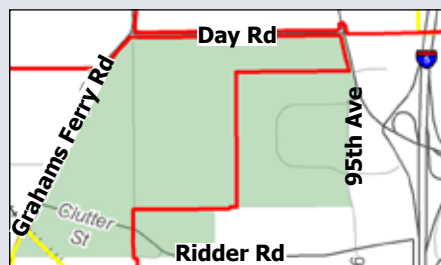
The proposed maximum indebtedness for the Coffee Creek Urban Renewal Plan is \$67,000,000. The ordinance, if approved, is subject to referendum. The adoption of the Plan may impact property tax rates for general obligation bonds approved by voters prior to October 6, 2001. This impact is estimated to last two years and is calculated at a total of .97 per \$100,000 of assessed value.

## Council Considers Coffee Creek Urban Renewal District

A plan to implement the voter-supported Coffee Creek Urban Renewal District in northwest Wilsonville is expected to be considered by the City Council on Monday, August 1, where public comment is welcome.

Prior to the City Council meeting the plan is to be reviewed by the Planning Commission on July 13.

The plan establishes how the City’s Urban Renewal Agency proposes to fund and build the public infrastructure that is needed to transform the now largely undeveloped area south of Day Road and west of 95th Avenue into a new employment center with approximately 1,800 jobs and an estimated annual payroll of \$55 million. The plans shows how urban renewal funding is to be generated and invested in building public infrastructure such as new modern roads, sidewalks, utility lines, bike lanes and paths.



When complete the tax assessed value of the district is estimated to rise from a tax base of \$62 million to about \$790 million, which then generates additional urban renewal funding to repay the initial infrastructure investment.

For more information contact Nancy Kraushaar, Community Development Director, at [kraushaar@ci.wilsonville.or.us](mailto:kraushaar@ci.wilsonville.or.us) or 503-570-1562.

## Rotary Concert Series Offers Four Concerts in Four Weeks

The Wilsonville Rotary Concert Series is set to begin a 16th year at Town Center Park on July 21. This year entertainers include:

- **July 21 – Johnny Limbo & the Lugnuts** (Classic Hits)
- **July 28 – The Junebugs** (Folk Pop)
- **August 4 – Ben Rice Band** (Blues)
- **August 11 – QuarterFlash** (Rock)

All the concerts are held on Thursday nights and include an opening act at 5:30 and the headliner at 6:30 pm in Town Center Park, 29800 SW Park Place in Wilsonville.

The Concert Series is free to attend, but donations of any amount are encouraged to support the Rotary International's End Polio Now fund-raiser which seeks

to eradicate polio throughout the world.

For more information about the concerts and opening acts visit [www.WilsonvilleRotary.com/concerts](http://www.WilsonvilleRotary.com/concerts).



Johnny Limbo and the Lugnuts perform on Thursday, July 21.

## Kiwanis Kids Fun Run and 5K Run/Walk Set for Saturday, July 30

This year the Wilsonville Kiwanis Kids Fun Run on Saturday, July 30, 8 am–1 pm, at Wood Middle School, 11055 SW Wilsonville Road, also includes a 5K run and walk for longer-distance runners and

**Kiwanis Kids Fun Run & 5K**  
• Sat, July 30, 8 am–1 pm  
Wood Middle School  
11055 SW Wilsonville Road

walkers. Normally the annual event is limited to children, 3–12 years old, but this year older kids and

adults are invited to participate in a 5K fun run/walk that begins at 8 am and costs \$30. Just like in years past the kids fun run is a non-competitive event where age-grouped heats complete appropriate distances that begin at 9 am and last until approximately 1 pm. A heat for special needs children is run as well.

While not running, participants visit various booths where kids can play fun games, learn about healthy living and take part in other exciting activities. The event also includes a free lunch from Albertsons.

To avoid long lines on the day of the event advanced registration is strongly encouraged and now open at

[www.KidsFunRun.org](http://www.KidsFunRun.org). The kids fun run is free, but a \$10 per child donation to the Wilsonville Kiwanis Club is encouraged.

All proceeds from the event are used to support local programs for kids and the community such as the Imagination Library for toddlers, scout troops, youth arts, sports programs, the Wood Reads program, and Junior Scoop, which is a club at Wood Middle School. The club also grants scholarships, supports high school clubs and sponsors Wilsonville High School's Key Club.



Kids get exercise and raise funds for local educational programs at the Wilsonville Kiwanis Kids Fun Run.

## Time to Have Fun in the Park on Saturday, August 6

Town Center Park is the place to be on the first Saturday in August because that is the day Wilsonville celebrates the 16th annual Fun in the Park.

**Fun in the Park Festival**  
• Sat, Aug 6, 10 am–5 pm  
Town Center Park,  
29800 SW Park Place

Kids of all ages have a blast playing on colorful inflatables, listening to live music, taking part in activities, watching live entertainers and touching live animals at the free family festival that begins at 10 am and lasts until 5 pm.

The entertaining event includes jugglers, magicians, clowns, balloon artists, face painters, and mimes. The main stage features a wide variety of musicians, singers, dancers and gymnasts.



The festival is made possible in part by a grant from the City of Wilsonville, along with Fred Meyer and Xerox who are premier sponsors. For more details, see [www.FunInThePark.com](http://www.FunInThePark.com).

## Wilsonville Brewfest Planned for Saturday, August 13

The second Annual Wilsonville Brewfest is taking place on Saturday, August 13, in the Piazza at Villebois, 28900 SW Villebois Drive in Wilsonville.

**Wilsonville Brewfest**  
• Sat, Aug 13, 12–8 pm  
Piazza at the Villebois  
28900 SW Villebois Drive

The event features tastings of a wide variety of craft beers and cider from over 20 different local breweries. The festival also

features live music performances, local food vendors and a root beer garden for kids. Sponsored by Green Group Real Estate and The Beer Station, all proceeds from the event are being donated to Wilsonville Community Sharing.

Organizers are providing tables and chairs, but participants are also encouraged to bring their own chairs

and are not allowed to bring pets except for service dogs. Parking is limited and so the participants are encouraged to walk or take alternative modes of transit including the SMART Transit that is providing on the hours bus service to the festival from Town Center Park, 12–8 pm.



Community members who are 21 years and older are encouraged to purchase tickets, which include eight tastings and a commemorative glass, for \$20 from The Beer Station or online at [www.WilsonvilleBrewfest.com](http://www.WilsonvilleBrewfest.com).

# City Calendar

| JULY   |       |   |
|--|-------|---|
| 4  | Mon   | • July 4 <sup>th</sup> Holiday – City Offices Closed  |
| 5  | Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |
| 7  | Thurs | • City Council, 5 pm Work Session<br>7 pm Meeting, City Hall  |
| 9  | Sat   | • Hazardous Waste Collection Day<br>9 am–2 pm, City Hall Parking Lot  |
| 11   | Mon   | • DRB Panel A, 6:30 pm, City Hall   |
| 13   | Wed   | • Planning Commission, 6 pm, City Hall<br>• Wilsonville Community Seniors Inc.,<br>5 pm, Wilsonville Community Center                     |
| 14   | Thurs | • Village at Main Neighborhood Picnic<br>5–7 pm, Village at Main Park   |
| 15   | Fri   | • Movie In The Park – Inside Out, Movie<br>starts at dusk, Memorial Park River<br>Shelter   |
| 18   | Mon   | • City Council, 5 pm Work Session<br>7 pm Meeting, City Hall  |
| 19   | Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |
| 21   | Thurs | • Rotary Concert Series – Johnny Limbo<br>and the Lugnuts, 6:30 pm,<br>Town Center Park   |
| 25   | Mon   | • DRB Panel B, 6:30 pm, City Hall   |
| 27   | Wed   | • Library Board, 6:30 pm, Wilsonville<br>Public Library   |
| 28   | Thurs | • Rotary Concert Series – The June Bugs<br>6:30 pm, Town Center Park  |
| 29   | Fri   | • Movie In The Park – Shaun the Sheep,<br>Movies starts at dusk, Memorial Park<br>River Shelter   |
| AUGUST   |       |   |
| 1  | Mon   | • City Council, 5 pm Work Session<br>7 pm Meeting, City Hall  |
| 2  | Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |
| 4  | Thurs | • Rotary Concert Series – Ben Rice Band<br>6:30 pm, Town Center Park  |
| 5  | Fri   | • Movie In The Park – Avengers Age of<br>Ultron, start at dusk, Memorial Park<br>River Shelter  |
| 6  | Sat   | • Fun in the Park, 10 am – 4 pm, Town<br>Center Park  |
| 8  | Mon   | • DRB Panel A, 6:30 pm, City Hall   |
| 10   | Wed   | • Planning Commission, 6 pm, City Hall<br>• Wilsonville Community Seniors Inc.,<br>5 pm, Wilsonville Community Center                     |
| 11   | Thurs | • Park and Rec Board Meeting<br>4:30 pm, Park and Rec Admin Building<br>• Rotary Concert Series Quarterflash<br>6:30 pm, Town Center Park |
| 13   | Sat   | • 2016 Wilsonville Brewfest, 12–8 pm,<br>Piazza at Villebois, 28900 Villebois Dr  |
| 15   | Mon   | • City Council, 5 pm, Work Session<br>7 pm Meeting, City Hall   |
| 16   | Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |
| 18   | Thurs | • Morey's Landing Neighborhood Picnic<br>5–7 pm, Walt Morey Park  |
| 19   | Fri   | • Movie In The Park – The Good<br>Dinosaur, Movie starts at dusk,<br>Memorial Park River Shelter  |
| 20   | Sat   | • Community Health Fair, 9 am–1 pm,<br>Lamb's Wilsonville Market Place,<br>8255 SW Wilsonville Road                                       |
| 22   | Mon   | • DRB Panel B, 6:30 pm, City Hall   |
| 24   | Wed   | • Library Board, 6:30 pm, Wilsonville<br>Public Library   |
| All dates and times are tentative; check the City's online calendar for schedule changes at <a href="http://www.ci.wilsonville.or.us">www.ci.wilsonville.or.us</a> . |       |   |

## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and the meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and replayed periodically. Meetings are also available to stream live and by video-on-demand online at [www.ci.wilsonville.or.us/wilsonvilleTV](http://www.ci.wilsonville.or.us/wilsonvilleTV). Public comment is welcome at all City Council meetings.



**Tim Knapp**  
Mayor  
[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



**Scott Starr**  
City Council President  
[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



**Julie Fitzgerald**  
City Councilor  
[fitzgerald@ci.wilsonville.or.us](mailto:fitzgerald@ci.wilsonville.or.us)



**Susie Stevens**  
City Councilor  
[stevens@ci.wilsonville.or.us](mailto:stevens@ci.wilsonville.or.us)



**Charlotte Lehan**  
City Councilor  
[lehan@ci.wilsonville.or.us](mailto:lehan@ci.wilsonville.or.us)

### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)



### Wilsonville City Hall

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)

# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

June 2016

## US Bank, the City and Wood Middle School Teamed Up to Shred



In April, US Bank, the City, and Wood Middle School teamed up to offer a free document shredding event. Hundreds of community members benefited from the service and then generously donated over \$2,600 to support the school's Our Community and Beyond Program that promotes community involvement and civic engagement to Wood's 7th grade students.

## Don't Miss this Weekend's Wilsonville Festival of Arts

The Wilsonville Arts & Culture Council presents the 17th annual Wilsonville Festival of Arts. This free community event is held on Saturday, June 4, and Sunday, June 5, at Town Center Park. The festival begins at 10 am both days and lasts until 6 pm on Saturday and 5 pm on Sunday.

The festival includes original art from over 50 professional artists and 200 West Linn-Wilsonville School District students. The Authors Pavilion features over 10 local authors,

who represent a wide variety of genres, discussing writing and selling their books. There are also interactive activities, a henna artist, face painting and much more.

Listen to live music and watch performances from the Minidoka Swing Band, Los Lobos Dancers, Dina y los Rumberos, Michele Van Kleef, Sweet Adeline quartet, B. Joy Blues Jazz, Mary Flower and the BBQ

Boys and Unit Souza. Enjoy delicious food from Dogzilla, Dos Gringos Grill, Lupita's, The Local Grind, Steal Your Plate, Slow Squeeze Juice and Jr. Scoop ice cream.

On Saturday, South Metro Area Regional Transit (SMART) sponsors historic trolley tours of art installations around Wilsonville.

On Saturday and Sunday, the Drummer's Corner, features a drum circle, 3-5 pm, and on Saturday, 10 am-1 pm, there is a drum-making workshop which requires pre-registration.

The Wilsonville Festival of Arts is supported by grants from the City of Wilsonville Community Tourism Grant Program, as well as dona-

tions from Fred Meyer, Pamplin Media Group, Argyle Square, The Laue Team, Frontier, Findlay of Wilsonville, NW Natural, Republic Services and Walmart. Visit [www.WilsonvilleArts.org](http://www.WilsonvilleArts.org) for more information.



**Wilsonville Festival of Arts**  
• June 4 & 5, opens 10 am  
Town Center Park, 29600  
Park Place, Wilsonville

## Long-Term City Employees Martin Brown and Blaise Edmonds Retire

Two long-term City employees—Building Official Martin Brown and Current Planning Manager Blaise Edmonds—are retiring from the City.

Martin Brown, who is retiring at the end of June, began his 36-year career with the City in 1979 as a Building Inspector. City Manager Bryan Cosgrove states, "As the City's Head Building Official for more than three-and-a-half-decades, Martin has played a key role in our town by making sure the city grows responsibly and according to our high construction standards. Beyond his many years of exceptional service to the City of Wilsonville, Martin was also heavily involved in the Oregon Building Officials Association. He was twice honored by his peers by being



Martin Brown

*We are very grateful for Martin's many meaningful contributions to the safe and orderly development of our community.*

named Oregon Building Official of the Year. What I appreciate most about Martin aside from his technical abilities is the outstanding attitude and customer-service focus he demonstrates every day on the job. We are very grateful for Martin's many meaningful contributions to the safe and orderly development of our community."

At the end of May another long-term City employee,

Blaise Edmonds, who has served as the City's Current Planning Manager since 2000, is also retiring. Blaise began working for the City in 1986 as an Associate Planner. During his impressive 30-year-long career,



Blaise Edmonds

*Blaise brought integrity, vision, and passion to his work planning Wilsonville*

Blaise oversaw hundreds of land-use applications and proposed projects that largely make Wilsonville what it is today. Planning Director Chris Neamtzu remarks, "Blaise brought integrity, vision and passion to his work planning Wilsonville. His calm demeanor and professionalism also allowed him to work well with internal staff and external partners. Blaise's contributions to Wilsonville growth are too extensive to list, but you can see the benefits of his insightful work all throughout town."

City Manager Bryan Cosgrove added that, "Blaise's background in architecture greatly improved the hundreds of design-review projects he oversaw during the course of his impressive career. He has left a remarkable legacy of high-quality projects that will continue to add value into the future."

In retirement Martin looks forward to more hiking, traveling and volunteering. On the other hand Blaise is eager to spend more time restoring bicycles, painting, watching the Ducks and spending time with family.

## Mayor's Message

### Summer Events Bring our Community Together

Care are a key part of the traditions that make a great place to live. In Wilsonville the summer months are a fun time when residents and visitors of all ages come together to participate in events celebrating arts and recreation, watch movies in the park, listen to live music and splash with friends in our fountains.



To help you plan to participate in this summer's offerings, we have produced a double-sided poster highlighting major community and library events in the greater Wilsonville area. The poster is included in this issue of *The Boones Ferry Messenger* and copies are also available at City Hall, Community Center, Library and online on the City's website, [www.ci.Wilsonville.or.us](http://www.ci.Wilsonville.or.us).

### Major community events in Wilsonville include:

- **Wilsonville Festival of the Arts:** Sat, June 4–Sun, June 5
- **Wilsonville Kiwanis Kids Fun Run:** Sat, July 30
- **Movies in the Park:** Fri, starting at dusk, on July 15 and 29, Aug 5 and Aug 19
- **Wilsonville Rotary Concerts in the Park:** Thur, 6:30–8:30 pm, on July 21 and 28, and Aug 4 and 11
- **Library Party in the Park:** Mon, July 25, 4–6 pm
- **Fun in the Park:** Sat, Aug 6, 10 am–5 pm
- **Wilsonville Brewfest:** Sat, Aug 13, 12–8 pm

### On-going events include:

- **Water features open:** Wed, June 1–Sun, Sept 11
  - **Wilsonville Farmers Market:** every Thur, 4–8 pm, until Sept 29
- So check out and save the enclosed calendar and I hope to see you at one of the many special summertime traditions that makes our town special.

*Tim Knapp, Mayor*

## Summer Events Posters Inside!



City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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**Library's Summer Reading Program Starts Monday, June 6**

Get ready for a summer filled with fitness and reading fun with the 2016 Summer Reading Program "On Your Mark, Get Set, READ" at the Wilsonville Public Library.



Public Library.

Everyone is invited to join us starting Monday, June 6, as we celebrate health and sports with reading challenges.

**KIDS!** Children ages birth to sixth grade can sign up between Monday, June 6, and Friday, August 12, at the library. Children who read for at least 20 minutes a day for 20 days receive a free

book and other prizes. Parents are welcome to read to their pre-reading child to complete the goal. All readers qualify for a chance to win an annual pass to the Oregon Zoo.

But the Summer Reading Program isn't just about reading. The popular program is also about science! Children who complete 10 science explorations receive a science prize. All science finishers qualify for a chance to win an annual pass to OMSI.

**TEENS!** Students (6th-12th grades) are invited to sign up for the Teen Summer Reading Program. Teens who read at least 20 minutes a day for 20 days win a free book and qualify for fun prize drawings. Special programs on Mondays just for teens start June 13. Programs range from movies and games, to melting beads and an epic water fight.

**ADULTS!** ages 18 and over qualify for the grand prize drawing that include a Kindle e-reader and museum memberships. Pick up a Tic-Tac-Toe card and read books in the different genres to make three squares in a row or read 20 minutes a day for 20 days to complete a reading log and you qualify for the grand prize drawing.

Sign up at the library starting Monday, June 6. For more information, call the library at 503-682-2744 or visit the library website: [www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org).

**Our History: Inza R. Wood at 1915's 8th Grade Graduation Event**



Wilsonville's 8th grade graduating class in 1915. In the center of the photo is teacher Inza R. Wood. Mrs. Wood taught in Wilsonville for 35 years, retiring as teacher-principal in 1949. Inza R. Wood Middle School, which opened in 1980, is named after the long-time educator.

**Neighbors Helping Neighbors Walk Set for Saturday, July 16**

Wilsonville Community Sharing, a non-profit food bank and social service agency in Wilsonville, is holding a second annual benefit walk, called Neighbors Helping Neighbors, on Saturday, July 16, beginning at 8:30 am at the Charbonneau Country Club, 32000 SW Charbonneau Drive, in Wilsonville.

In the first year of the walk, the event raised \$5,000 in cash donations and another \$5,000 in in-kind donations. This year event organizers hope to beat that goal by raising an additional \$1,000 in each category.

City-wide recruitment is underway for walkers (individually or as teams), sponsors, volunteers and in-kind donations. To register a team visit [www.contact/1SYXJBD](http://www.contact/1SYXJBD).

For more information contact Leigh Crosby, Food Bank Coordinator, Monday - Thursday, 503-682-6939; [leigh@wilsonvillecommunitysharing.org](mailto:leigh@wilsonvillecommunitysharing.org).

**Flag Retirement Ceremony to be held on Flag Day, June 14**

Local Boy Scout and Cub Scout troops from Wilsonville are holding a Flag Retirement Ceremony on Tuesday, June 14, 10 am, at Town Center Park, 29600 Park Place Court, in Wilsonville. At the ceremony U.S. flags are to be properly retired from use. Community members with flags that need to be retired can be turn the flags into any Wilsonville Boy Scout, family member, Scout Masters, or to the UPS Store, 29030 Town Center Loop W #202, Wilsonville.



**Korean War Remembrance Ceremony to be Held on June 18**

Community members, veterans and family of veterans are gathering on Saturday, June 18, to honor living and fallen soldiers who served their country. The annual Korean War Observance Ceremony, free and open to the public, is to be held 10-11:30 am at the Oregon Korean War Memorial, in Town Center Park. The event is sponsored by the Korean War Memorial Foundation of Oregon.



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

**Children's Summer Programs Start June 21**

Summer gets into high gear with these fun programs for kids:

**Family Stories & Science** is our summer Storytime program. Enjoy a story and a science demonstration, then experience follow-up crafts and hands-on science activities. For children ages three and up. Tuesday evenings at 6:30 pm, and Wednesdays at 10:30 am and 1:30 pm.



**Baby Time and Toddler Time** return for the summer. Enjoy books, rhymes, and songs for ages 0-12 months in Baby Time on Tuesday mornings, 11-11:45 am. Toddler Time features stories, rhymes and activities for ages one and two on Tuesday mornings, 10-10:30 am.

**Thursday Fun Shows** are back. The Oregon Bird Man performs on Thursday, June 23, at 11 am, 12:30 pm, and 2 pm. Presto the Magician performs on Thursday, June 30, at 11 am, 12:30 pm, and 2 pm.

**Friday Family Films** start Friday, June 24, featuring recent releases of popular family-friendly movies. Films in English start at noon, and films in Spanish start at 2 pm.

For more details, visit the Summer Reading Program page at [www.WilsonvilleLibrary.org/SRP](http://www.WilsonvilleLibrary.org/SRP).

**Teen Summer Monday Programs Start June 13**

Students entering grades 6-12 are invited to the Teen Summer Reading Program kick-off event "Movie Sundae Monday" on Monday, June 13, at 4 pm.

Other June events include Nerf Day (June 20) and Melting Beads (June 27). [www.WilsonvilleLibrary.org/TSRP](http://www.WilsonvilleLibrary.org/TSRP)



**Introduction to Meditation on June 2**

Meditation reduces stress, improves concentration, and increases happiness, but just how exactly do you do it? Instructor Ahsamon Ante-Marandi provides an overview of meditation, and then lead a variety of meditation exercises.



Thursday, June 2, 6-8 pm. Free. [www.WilsonvilleLibrary.org/classes](http://www.WilsonvilleLibrary.org/classes)

**Talking to Aging Parents About Money on June 7**

Are you worried about your parents' ability to manage their money? Would you be able to help them with their finances and care if they were suddenly ill? Local retired CPA Alan Steiger shares conversation strategies and questions to consider when talking with your parents about their money.



Tuesday, June 7, 6-7 pm. Free. [www.WilsonvilleLibrary.org/classes](http://www.WilsonvilleLibrary.org/classes)

**Upcoming Classes and Events**

**Local Artist of the Month: Brad Isom**

**First Friday Film**

Friday, June 3, 6-8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

**Basics of Going Solar Workshop**

Saturday, June 11, 10-11:30 am  
RSVP at [www.SolarOregon.com](http://www.SolarOregon.com)

**Red Cross Blood Drive**

Saturday, June 11, 11 am-4 pm  
[www.RedCrossBlood.org](http://www.RedCrossBlood.org)

**E-reader Basics**

Wednesday, June 15, 1-2 pm  
[www.WilsonvilleLibrary.org/classes](http://www.WilsonvilleLibrary.org/classes)

**Genealogy Club**

Monday, June 20, 1-2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

**Great Books Discussion Group**

*Uncle Vanya* by Chekhov  
Tuesday, June 21, 6-8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

**History Pub at Wilsonville McMenamins**

Tuesday, June 28, 6:30-8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

**Game Night**

Wednesday, June 29, 6-8 pm  
[www.WilsonvilleLibrary.org/games](http://www.WilsonvilleLibrary.org/games)

## Wilsonville Volunteer Fair Set for Thursday, June 16

The City of Wilsonville is hosting a free Volunteer Fair on Thursday, June 16, 6:30–8 pm, at City Hall, 29799 SW Town Center Loop East. The fair seeks to connect community members with volunteer opportunities with various local non-profit organizations. Currently participating

**Volunteer Fair**  
• June 16, 6:30-8 pm  
City Hall, 29799 SW Town Center Loop East

organizations include:

- City of Wilsonville–Boards and Commissions
- Clackamas County Victim Assistance Program
- Clackamas Women’s Services
- Court Appointed Special Advocates (CASA)
- First Responders Assistance Organization (FRAO)
- Kiwanis Club of Wilsonville
- Rotary Club of Wilsonville
- Wilsonville Arts and Culture Council



Potential volunteers chat with a non-profit representative.

- Wilsonville Community Seniors Inc. (WCSI)
- Wilsonville Community Sharing
- Wilsonville Relay for Life
- Wilsonville Sister City Association
- WilsonvilleSTAGE (aka Wilsonville Theater Co.)

For more information, contact Angela Handran, Community Outreach Specialist, at 503-570-1503; Handran@ci.wilsonville.or.us.

## CHIEF’S CORNER—DO NOT BLOCK INTERSECTIONS

Regularly in Wilsonville during rush hour and other peak travel times, our police can sometimes be found at major intersections writing citations for drivers who are blocking intersections and impeding traffic.



Chief Jeff Smith

During heavy congestion drivers often enter an intersection without knowing they are able to pass through before the light changes. Blocking an intersection can result in one or more of the following moving violations:

- **Failure to Obey a Traffic Control Device** — \$260 fine
- **Illegal standing or stopping of a motor vehicle** — \$110 fine
- **Obstructing Cross Traffic** — \$110 fine

Blocking or obstructing traffic is a serious offense and has negative consequences. Doing so results in worsening congestion, interrupting the flow of cross traffic, slowing emergency response times and causing driver frustration to escalate.

The 2016-17 Oregon Driver Manual states, “Even

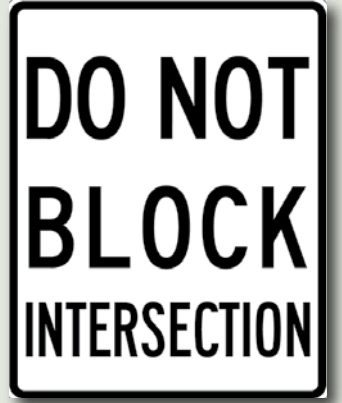
if you have a green light, do not enter an intersection unless there is room for your vehicle on the other side.” In other words, before you enter an intersection you must be able to see that you will be able to clear the intersection before the light changes colors. If you are not sure you should wait to enter the intersection.

As the weather warms up and more people hit the road for vacation, our streets will become more congested. If possible, try and avoid driving during peak travel times and in well-known congested areas.

If you find yourself blocking an intersection, don’t be surprised to see one of our bike patrol or other police officers ready to enforce these important rules of the road.

I wish you all a great summer.

*Jeff Smith, Chief of Police*



## Business License Renewals and Fees Due by June 30

The City of Wilsonville’s Business License renewal period occurs during the month of June as all business licenses issued by the City expire on June 30 each year. To expedite business license renewals the City mails renewal application forms to all currently licensed businesses starting the first week of June.

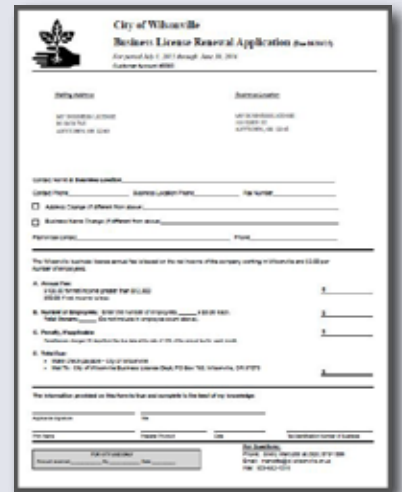
Renewing a business license is an easy three-step process:

1. **Complete** the information at the top of the form.
2. **Calculate the amount due** according to the fee schedule.
3. Insert the **signed and dated form with the fee** in the **return** envelope and mail back or place in the drop box at City Hall.

When the renewal application is processed a new

business license is issued and is mailed to the business to be displayed.

If you have questions regarding a business or renewal application please contact Shelly Marcotte at 503-570-1586.



## SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)

503-682-7790

smart@ridesmart.com

www.RideSMART.com

## Walk @ Lunch—June 29, 2016

Join SMART at noon on Wednesday, June 29, 2016 for a 30-minute walk during lunch

and take steps towards a healthier you. Meet at Convergys work site at 9275 SW Peyton Lane, in Wilsonville—rain or shine.

Join us out front of the building, for a 15-minute walk down to Subway, Qdoba and Wilsonville Smiles who are the partners of this group walk. Meet new friends and do some lunchtime networking. Remember to bring your walking shoes to work with you!



## Join SMART at the Wilsonville Festival of Arts on Saturday, June 4

SMART is hosting a transportation information booth at the festival and is also providing free Art Tours by trolley at the beginning of each hour on Saturday, June 4. Art Tours begin at noon and conclude at 5 pm. Come take a test ride with Cynergy E-Bikes, just try one you will love the power.

urday, June 4. Art Tours begin at noon and conclude at 5 pm. Come take a test ride with Cynergy E-Bikes, just try one you will love the power.



## Farmers Market Shuttle, Now Serving Charbonneau District

Service is now available from Charbonneau County Club each Thursday night to the Wilsonville Farmers Market in Villebois. Here are the bus schedules:

| Charbonneau Country Club | Farmers Market |
|--------------------------|----------------|
| 4:35 pm                  | 5:00 pm        |
| 5:35 pm                  | 6:00 pm        |
| 6:35 pm                  | 7:00 pm        |



| Farmers Market | Charbonneau Country Club |
|----------------|--------------------------|
| 5:02 pm        | 5:24 pm                  |
| 6:02 pm        | 6:24 pm                  |
| 7:00 pm        | 7:24 pm                  |
| 8:00 pm        | 8:22 pm                  |

## WERK Day Volunteers Freshen Up Memorial Park

In May, hundreds of volunteers participated in the City's annual park clean-up event known as Wilsonville Environmental Resources Keepers (WERK) Day.

WERK Day kicked off at the Community Center with a free hot breakfast and coffee provided by the Wilsonville Rotary Club and Starbucks. Volunteers, wearing their free WERK Day '16 t-shirts, were grouped into crews and dispatched to work all throughout Memorial Park.

In the forested portion of the park, teams pulled out non-native, invasive plants. Other groups hauled and spread fresh bark dust around trees and other areas, while another group worked on the tee boxes for Memorial Park's new nine-hole disk golf course. A group also conducted a Bio Blitz Survey in which volunteers with the help of a local biologist from Portland State University sought to identify and record all living things in the park.

WERK Day participants included community groups such as: Pioneer Baptist Church, Convergys, Toastmasters, Wilsonville Latter Day Saints, First Robotics Team, Wilsonville Parks and Recreation Board, Wilsonville National Honor Society, Jr. Scoop and Pac-West Disk Golf Alliance.

After volunteering, participants enjoyed a free lunch which included a burger, fries and a drink from the Wilsonville Wendy's restaurant. According to Wendy's owner Vickie Kibler, "It's important to us as a business to give back to the community. Last year, I went out to some of the sites, and I appreciate the work the volunteers do to help maintain the parks. We've been a sponsor since the first WERK Day in 2000, and we are committed to continuing to support this event." Volunteers also received a coupon for a free round of miniature golf at the Wilsonville Family Fun Center and Bullwinkle's Restaurant. Republic Services also donated two 20-yard debris boxes for use at the event.

## Photos from WERK Day Supported by Wendy's, Starbucks, Bullwinkle's Restaurant and Wilsonville Rotary Club



29600 SW Park Place  
 Administrative Office: 503-783-7529  
 Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

### Summer Fun with Zumba Gold!

Zumba Gold in the Park is back! Instructor Heidi Baum is ready to take your workout to the next level. Zumba Gold is a great way to combine fitness, dance and fun, and now you can enjoy the summer sun as well! The first eight-week session begins on Monday, June 6. The class runs 9-9:45 am, costs \$55 and takes place at Town Center Park.



### Elder Financial Abuse

Over the past few years there has been an increase in the number of reported cases of elder financial abuse within the State of Oregon and our community. Wilsonville Attorney, Shawn M. O'Neil, represents victims of elder financial abuse and is speaking on the subject and offering tips on how you can protect your assets and finances from financial abuse. The lecture takes place at the Wilsonville Community Center on Tuesday, June 28, at 10:30 am. The seminar is free, but please call the Community Center to register.

**Water Features at Town Center Park and Murase Plaza open for the season on June 1.**

### Mental Health First Aid

Did you know that you are more likely to encounter someone having a mental health crisis than a heart attack? Sign up for this free, evidence based public education program, and learn how to identify warning signs of mental health and substance abuse disorders. The program is taught by Clackamas County Mental Health Professionals and is offered on Monday, June 13, 8 am-5 pm, at the Tauchman House. There is no fee, but pre-registration is required.

### Upcoming Classes and Events

**Womens Personal Safety and Self Defense**  
 Saturday, June 11  
 2-5 pm  
 Lee's Martial Arts  
 \$89  
 Open to females ages 13 and older

**Bunco Meetup**  
 Wednesday, June 15  
 1 pm  
 Community Center  
 No Charge  
 If interest exists, Bunco meets on an ongoing basis on the 3rd Wednesday of each month

**Yoga**  
 Tuesdays and Thursdays, June 28 to August 4  
 5:30-6:30 pm  
 Community Center  
 \$88 (\$7.33 per class)  
 \$10 drop-in per class also available

## Camps, Camps, Camps!

Wilsonville Parks & Recreation offers a variety of summer camps for children of all ages. In June, the program offerings get started with a number of camp options. Full list of camp offerings and registration available at: [WilsonvilleParksandRec.com/SummerCamps](http://WilsonvilleParksandRec.com/SummerCamps).

Babysitters Bootcamp  
 Ages 11-15  
 June 15 and June 16  
 9 am-5 pm  
 Tauchman House  
 \$210

Skyhawks Tennis Camp  
 Ages 7-12  
 June 20-24  
 9 am-noon  
 Memorial Park  
 \$109

Skyhawks Mini Hawk Camp  
 Ages 4-7  
 June 20-24  
 9 am-noon  
 Memorial Park  
 \$109



Coach Nics Hoop Camp  
 Grades 1-3  
 June 27-30  
 9 am-noon  
 Memorial Park  
 \$69

Survival Camp  
 Ages 9-13  
 New Camp Added!  
 July 11-15  
 9 am-4 pm  
 Mary S Young Park  
 \$285

# Proposed Recreation & Aquatic Center to be Voted on in November Election

City Councilor Scott Starr is presenting information about the City Council's plans to hold a bond measure vote in November to fund a proposed recreation and aquatic center on Wednesday, June 8,

**Recreation and Aquatic Center Presentation**  
 • June 8, 6 pm  
 City Hall, 29799 SW Town Center Loop East

6 pm, at the Planning Commission meeting, to be held at City Hall in the Council Chambers, 29799 SW Town Center Loop

**Recreation and Aquatic Center Open House**  
 • June 29, 6–7:30 pm  
 Parks and Rec Admin Bldg, 29600 SW Park Place

The City is also hosting a public Open House for the proposed recreation and aquatic center on Wednesday, June 29, 6–7:30 pm in the Parks and Rec Admin. Building, in Town Center Park, 29600 SW Park Place.

## Amenities

The approximately 80,000 square-foot recreation and aquatic center is to be used for both leisure and competitive activities and offers a wide variety of amenities for community members of all ages. In particular the center includes:

- A **warm-water pool** suitable for water exercise activities and therapy classes for seniors and adults
- An **indoor 25-yard six-lane pool** suitable for lap swimming, swim meets and other aquatics programs
- A **6,000 square-foot leisure pool** with a slide and lazy river along with a whirlpool spa
- A **fitness center**
- Two **multi-purpose fitness class rooms**
- Two **full-size hardwood courts**
- An **indoor walking track**
- A **rock climbing wall**
- Multi-purpose **community meeting rooms**



The proposed center includes a community fitness center.

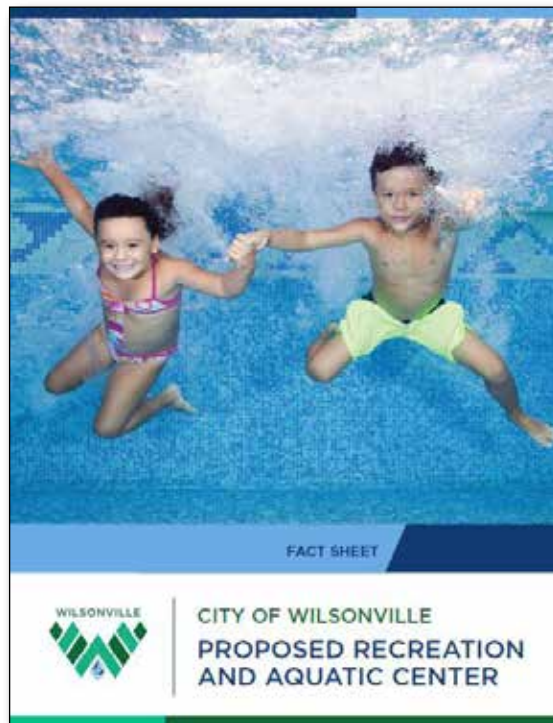
## Cost and Funding

To fund the proposed center, the Wilsonville City Council is placing a 20-year capital bond measure for voters' consideration on the November 2016 ballot that, if approved, will fund the total cost to buy land, design and build the \$30 million indoor recreation and aquatic center. If approved, a home with an assessed value of \$300,000 would see a property tax increase of about \$19 a month.



The proposed center includes an indoor six-lane lap pool.

Wilsonville does not have a public recreational and aquatic center. Community members who seek a public pool for indoor year-round lap swimming, lessons and other recreational activities must travel to facilities in other cities.



A four-page fact sheet for the proposed center is available online, at the Library and City Hall.

## Background and Feasibility Study

In 2014 the City contracted with The Sports Facilities Advisory, a firm that specializes in analyzing the financial viability of building and operating public athletic facilities. A 14-member community task force solicited input and provided feedback, from which the consultant produced a feasibility study. The task force reviewed the study before recommending approval to

the City Council. The Council adopted the study in December 2014 and has decided to move forward with a proposed ballot measure to design and construct a recreation aquatic center. The feasibility study is available for review at [www.WilsonvilleParksandRec.com/WRAC](http://www.WilsonvilleParksandRec.com/WRAC). The financial feasibility analysis concludes a center could be operationally self-sustaining after a



The proposed center includes warm-water pool.

five-year start-up phase that includes City support of approximately \$1.5 million in total spread over the five-year period.

## Location

A specific site has not yet been secured; the plan is to locate the recreation and aquatic facility in the Wilsonville Town Center area.



The proposed center includes two full-size hardwood courts.

## Operation of the Center

The City is issuing a Request for Proposals (RFP) to identify qualified and experienced firms to operate the center in the best and most cost-effective manner. The selected operator is to be identified prior to the November 2016 election. If a private operator is selected, a management contract with the City is contingent upon passage of the capital bond measure.

The City retains ownership of the facility. The cost to operate the facility is to be primarily covered by membership and user fees.



The proposed center includes a multi-purpose community room for meeting and parties.

## Membership and User Fees

The feasibility study assumed specific membership rates; the exact cost of memberships is to be determined through negotiations with the operator. Wilsonville residents would pay lower rates for membership than non-residents.

## Ballot Information

Funding for the proposed recreation and aquatic center is subject to passage of a general obligation capital bond for voter consideration in the November 2016 election. The 20-year bond would be used for land acquisition and subsequent design and construction of the facility. If the bond is approved, the recreation and aquatic center design is expected to begin in 2017.

For more information call the Parks and Recreation Department at 503-783-7529 or visit [www.WilsonvilleParksandRec.com/WRAC](http://www.WilsonvilleParksandRec.com/WRAC).

## TVF&R Offers Tips to Prevent Window Falls

As we welcome warmer spring and summer weather, Tualatin Valley Fire & Rescue reminds parents, grandparents, and babysitters that window falls are preventable.

Each year, window falls account for about eight deaths and more than 3,300 injuries to young children in the United States. In Oregon, 26 children ages 5 and younger fell from windows in 2015.

TVF&R firefighter paramedics responded to an incident in May involving a 3-year-old boy who fell from a second-floor window of his Tigard home. The child was transported to an area hospital for injuries he sustained when he landed on a concrete sidewalk after pushing out a window screen.

Window screens are not strong enough to keep a child from falling out of a window. They are designed to keep bugs out, not kids in.

Because children frequently land on their heads, many require hospitalization for fall injuries and may have permanent disabilities. A majority of falls occur between May and September, but can happen anytime windows are open.

The encouraging news is that simple steps can help prevent injuries. As a partner in the Stop at 4 Inches

Campaign to Prevent Window Falls, TVF&R urges parents and families to continue their commitment to window safety.

Please share the following tips with friends, family, or anyone else who has children living in or visiting their home.

- Keep windows closed and locked when not in use.
- Keep play areas, furniture, and anything a child can climb on away from windows.
- Do not open windows more than 4 inches and install window stops. Window stops prevent windows from opening wide enough to allow a child to tumble out.
- Window stops should be easily removed by an adult in the event of a fire or other emergency.

- For a double-hung window, open the top portion near the ceiling for ventilation while keeping the bottom portion closed.

- Actively watch children near windows — no device can replace adult supervision.

Visit [www.StopAt4.com](http://www.StopAt4.com) for more information about window safety, appropriate safety devices for your style of windows, and locations to purchase window stops and guards. For more safety tips, visit [www.tvfr.com](http://www.tvfr.com).





## Reports from City Boards and Commissions

### City Council Considers Transportation Systems Plan Amendments

At the City Council meeting on May 2, the City Council opened a public hearing on a set of proposed amendments to Wilsonville's 2013 Transportation Systems Plan; known collectively as the "2016 TSP Amendment." The City Council voted to continue the public hearing and keep open the public record until the May 16 City Council meeting in order to provide the public additional time to review the proposed changes and provide public comment.

The TSP is the City's long-term policy and planning document for all types of transportation improvements including vehicular, bicycle, pedestrian, transit and freight projects. The Plan also features higher priority projects that are planned to be implemented over a 20-year timeframe through the City's Capital Improvement Program (CIP), development review processes and occasionally by other agencies.

The recommended TSP amendments include the following project modifications grouped by location:

#### 1. For the proposed Coffee Creek Industrial Area of northwest Wilsonville:

- Replacing the northern unconstructed Kinsman Road segment between Ridder Road and Day Road with a three-lane collector along the existing Garden Acres Road alignment as a higher priority truck and

bike route.

- Adding the proposed east-west Java Road collector, including a bicycle route.
- Adding a sidewalk infill project on Boones Ferry Road from Commerce Circle Loop to Day Road.

#### 2. For the proposed Frog Pond residential area:

- Adding designated collector roadways and updating the proposed trail locations included in the recently adopted Frog Pond Area Plan.
- Adding the collector roadways and site improvements associated with the new Meridian Creek Middle School and City park site off of Advance Road.

#### 3. Adding Printer Parkway on the former Xerox campus as a public street and classifying as a collector, a bike route and a higher priority project.

On April 13 the Planning Commission conducted a public hearing on the proposed TSP amendments and forwarded a unanimous recommendation of approval to the City Council.

For more information about the TSP amendments visit [www.ci.wilsonville.or.us/TSPAmendment](http://www.ci.wilsonville.or.us/TSPAmendment) or contact Eric Mende, PE, Capital Projects Engineering Manager, at 503-570-1538; [mende@ci.wilsonville.or.us](mailto:mende@ci.wilsonville.or.us).



Eighth grade students from Inza R. Wood Middle School pose for a photo after participating in the *We the People National Invitational* — a competition that tests the students' knowledge, understanding and ability to apply constitutional principles.

### City Council Adopts Tourism Development Strategy and Advances Tooze Road Widening and Improvement Project

On April 18, the City Council adopted the first "Five-Year Action Plan and Annual One-Year Implementation Plan for the Wilsonville Tourism Development Strategy" developed by the new Tourism Promotion Committee. The plan recommends three primary sets of actions and studies to be executed starting this year that implement the longer-range, five-year components of the larger "Visit Wilsonville" tourism strategy. Major elements of the plan include: identifying a dedicated staff person or contractor to implement the plan; creating a website for Wilsonville tourism; and conducting various studies, including a visitor profile study, destination marketing strategy plan and feasibility studies for an all-weather/indoor, multi-purpose facility and a hotel/conference center.

At the meeting the City Council also approved a resolution authorizing the City to acquire approximately 17,624-square-feet of right-of-way, 9,271-square-feet of permanent easement area and 23,130-square-feet of temporary easement needed for the Tooze Road widening and improvement project on the unimproved section of the roadway east of the intersection with SW Grahams Ferry Road. The project, which extends along the northern edge of the Villebois neighborhood, is identified as a priority in the City's Transportation System Plan and right-of-way acquisitions are planned to be completed this year with construction beginning in the second half of next year.

The Council also approved a resolution authorizing the City to refinance water revenue bonds on or after June 1, 2016, with the expectation of saving the City approximately \$150,000 in interest.

The City Council continued until May 16 a public hearing planned for a proposed 14-lot subdivision on Canyon Creek Road South. The continuation is needed in order to provide the Development Review Board (DRB) Panel B more time for additional consideration of the land-use applications and development

proposal prior to City Council consideration and the public hearing.

During the Mayor's Business portion of the meeting, Mayor Tim Knapp issued three proclamations. The first proclamation declares May 1-7 as Construction Safety Week in order to call attention to the important role that safety plays in the construction industry and building trades. The second proclamation identifies May 1-7 as Drinking Water Week in order to acknowledge the City's important role as a provider of clean and abundant drinking water. The third proclamation declares May 1-7 as Municipal Clerks Week and extends the City Council's appreciation to City Recorder Sandra King for the vital services she provides the community.

Also under Mayor's Business, the Mayor announced that for the 19th consecutive year since 1997, the Government Finance Officers Association of the United States and Canada (GFOA) has awarded the City of Wilsonville with the Certificate of Achievement for Excellence in Financial Reporting for the City's Comprehensive Annual Financial Report (CAFR). Attainment of the Certificate of Achievement, the highest form of recognition in the area of governmental accounting and financial reporting, represents a significant accomplishment by the City.

During the Communications portion of the meeting, the City Council heard a report about federal affairs from Megan McKibben, Clackamas County Field Representative for the Office of Congressman Kurt Schrader.

Prior to the regular meeting at the work session City Council heard reports on the Transit Master Plan Update currently underway by the South Metro Area Regional Transit (SMART), the Information Technology Strategic Plan and an update on the Road Maintenance Fee.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during June. Scheduling changes can and do occur; check for updated meeting information at the City's website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, June 6:** Emergency exercise; Water Treatment Plant Master Plan update; Wilsonville Road Kinsman Road truck turning radius results; Ozone Coalition letter; Basalt Creek update; Library Board appointment; Tourism Promotion Committee appointments; Washington County Visitors Association; Republic Services SORT Bio-energy project; Comp. Plan Amendment/Zone Change 14-lot subdivision; proposed recreation and aquatic center operator award.

**Monday, June 20:** Fiber Business Plan; Sewer lateral repair grant program; sidewalk repair grant program; Neighborhood Traffic Management Plan; Frog Pond Master Plan/infrastructure funding; Town Center Redevelopment Plan consultant contract award; City Manager employment contract; Final Supplemental Budget Adjustment; Adopting the FY 2016-17 Budget and related items; 2nd reading Republic Services SORT Bio-energy project; Resolution Ballot Measure for proposed recreation and aquatic center; Regional Park 7 & 8-Development Agreement Villebois Polygon; franchise agreement.

### Planning Commission Update

At the Planning Commission meeting on May 11, the Commission discussed and provided feedback and direction on draft Code language proposed to implement the Frog Pond Master Plan.

At the Planning Commission meeting on Wednesday, June 8, the Commission will get a report on the proposed recreation and aquatic center and an update on the Basalt Creek Concept Plan.

### Development Review Board Update

At the Development Review Board (DRB) Panel A meeting on May 9, the board approved an annexation, a Comprehensive Plan Map amendment, a Zone Map amendment and other related land-use applications to allow for the expansion of Republic Services Ridder Road campus and the development of facilities that collect and convert commercial food waste into biogas and use the biogas to generate electricity. The DRB's approval advances the project to the City Council for further consideration on June 6 and June 20.

Also at the May 9 meeting the DRB-Pane B approved the renewal of Republic Services's two-year Temporary Use Permit for two modular office structures on the east side of the company's Ridder Road property.

On April 25, the board approved a Comprehensive Plan Map amendment, a Zone Map amendment, other related plans and a tentative subdivision plat for a 14-lot single-family subdivision located at 28500 and 28530 SW Canyon Creek Road South.

### Library Board Update

During the Library Board meeting in April various reports included:

- The library hosts a Game Night teen event each quarter with board games, and video games for teens.
- The Library's Teen Advisory Board is preparing for the final teen event of the school year Dis-night with Disney themed games and activities.
- The Children's non-fiction section has been shifted down creating more space giving the Spanish language collection a higher profile.
- Also in Children's series books are being integrated into the J collection. Series with one author are under the author's name and series with multiple authors are under the name of the series.
- On behalf of LINCC, the library submitted a grant proposal for creating a consortium of smaller libraries in Oregon that would send representatives to the International Book Fair in Guadalajara, Mexico to review and purchase Spanish-language books.
- The children's collection circulation is up 6% in the first quarter, perhaps due to recent improvements in the children's room.

The next regular meeting is scheduled for Wednesday, June 22, 6:30 pm, at the Library.

## Mayor and Two City Council Positions Open in November Election

Three of five City Council positions are nearing the end of their four year-terms that expire on December 31, 2016. These positions are to be filled based on the results of the fall general election being held on Tuesday, Nov. 8, 2016.

All City Council positions are four-year terms, non-partisan and elected at-large. Candidates need to be a registered voter and have lived within the City limits for 12 months prior to the Nov. 8 election. The first day for a candidate to file the appropriate paperwork with the City is June 1; the last day to file is August 30 by 4:30 p.m. Candidates are asked to file at least one week prior to the filing deadline in case some signatures are not valid. Signa-



City Recorder  
Sandra King

tures are invalid if the person is not a registered voter, has recently moved to the community and did not re-register, or lives outside the city limits.

Anyone interested in running for City Council is encouraged to first meet with the City Recorder to learn about the process and pick up a packet outlining the election process.

One of the first steps is to prepare a petition that must be approved by the Recorder and then circulated to collect 20 signatures. The petition pages are then submitted to the City Recorder who verifies the signatures are valid. Also, in accordance with certain guidelines in the packet, a contribution and expenditure report may need to be submitted to the Secretary of State.

For more information contact Sandra King, City Recorder, at 503-570-1506; king@ci.wilsonville.or.us.

## Formation of the Voter-Supported Coffee Creek Urban Renewal District Advances

A plan to implement the voter-supported Coffee Creek Urban Renewal District in northwest Wilsonville continues to make progress and is expected to be ready for City Council consideration late this summer.

In April, the City held an Open House and an Urban Renewal Task Force meeting to seek feedback and support for the draft plan. Open house attendees included many local property owners and developer representatives who learned about and provided feedback on the proposal. After the Open House, the Urban Renewal Task Force reviewed and then voted unanimously to forward the plan to the Wilsonville Urban Renewal Agency for their consideration on June 20.

Other key steps for the plan include a proposed review by the City's Planning Commission on July 13 and then onto City Council on August 1 where public comment is welcome.

The plan establishes how the City's Urban Renewal Agency proposes to fund and build the public infrastructure that is needed to transform the now largely undeveloped area into a new employment center with approximately 1,800 jobs and an estimated annual payroll of \$55 million. The plans shows how urban renewal funding is to be generated and invested in

building public infrastructure such as new modern roads, sidewalks, utility lines, bike lanes and paths.

When complete the tax assessed value of the district is estimated to rise from a tax base of \$62 million to about \$790 million which then generates additional



The Coffee Creek Urban Renewal District is located near the Coffee Creek Correctional Facility in northwest Wilsonville, south of Day Road, west of Interstate 5, north of Ridder Road and east of Grahams Ferry Road.

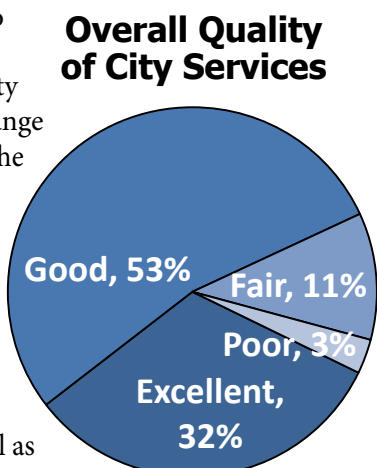
urban renewal funding to repay the initial infrastructure investment.

Additional updates and information about the formation of the Coffee Creek Urban Renewal District will be in upcoming issues of *The Boones Ferry Messenger* throughout the summer.

For more information contact Nancy Kraushaar, Community Development Director at kraushaar@ci.wilsonville.or.us or 503-570-1562.

## Research Firm Seeks Community Member Input to Study Wilsonville's Quality of Life, Governance and Civic Participation

Once again the City of Wilsonville is contracting with the National Research Center (NRC) of Boulder, Colorado, to perform a statistically-valid random-sample survey to gather feedback from Wilsonville community members on a wide range of important topics. The confidential survey assesses community member views about the quality of life in Wilsonville, the City's performance regarding governance and service delivery, as well as the level of participation in local services, events and meetings.



Source: 2014 Wilsonville Community Survey.

In May the survey, referred to as the National Citizen Survey™, was mailed to 1,200 households in Wilsonville and recipients are asked to complete and return the questionnaire as soon as possible. In July, the survey is to be published online for all community members to complete.

The 2016 survey questions mirror the prior survey questions conducted in 2012 and 2014 so that the results can be compared and any changes in resident views can be identified. The results from the first two community surveys are currently available on the City's website at

www.ci.wilsonville.or.us/CommunitySurvey.

The data from the 2016 survey is to be compiled this summer and then early this fall the firm is delivering the results to the City. The 2016 survey results are to be published in the *Boones Ferry Messenger* and online this fall.

For more information, contact Angela Handran, Community Outreach Specialist, at 503-570-1503; Handran@ci.wilsonville.or.us.

## Wilsonville is Re-Designated as a Walk Friendly Community

In May, the Pedestrian and Bicycle Information Center (PBIC) based in Chapel Hill, North Carolina announced that the City of Wilsonville has been re-designated as a bronze level Walk Friendly Community (WFC). Wilsonville is being recognized as a "Walk Friendly Community" for working to improve a wide range of conditions related to walking, including safety, mobility, access, and comfort. Wilsonville is one of only 58 Walk Friendly Communities across the nation. In Oregon other WFC communities include Corvallis, Eugene, Bend, Sisters and Tigard. The WFC program is funded by FedEx and the U.S. Department of Transportation Federal Highway Administration. For more information visit [www.WalkFriendly.org](http://www.WalkFriendly.org).



## Community Enhancement Committee Supports Local Projects

In April, the City's new Wilsonville-Metro Community Enhancement Committee met for a second occasion to consider projects and recommends to the City Council awarding a total of \$69,543 in funding to four different projects in Wilsonville.

The community enhancement projects recommended for funding include:

- **Memorial Park "Dog Park" Relocation Project:** \$25,000
- **"Bee Stewards" Wilsonville Pollinator Improvement Project:** \$21,433
- **Multifamily Waste-Reduction and Recycling Project:** \$16,000
- **Fluorescent Mercury-Lamp Business Recycling Program:** \$7,110

Mayor Tim Knapp states, "This new funding source allows the City to engage with local partners and leverage additional resources in order to address important community improvement initiatives that are above and beyond the current scope of City services and programs."

A fifth project, Frog Pond Church Campus Restoration Project, is still under consideration by the committee

to allow committee members more time to research and gather information about the proposal prior to reaching a final funding decision. This project is planned to be reviewed for funding



The Dog Park is being relocated within Memorial Park to make room for the new nine-hole disk golf course.

at the committee's next meeting that is to be scheduled for May or June.

Project nominations from the public and public agencies were first reviewed by City staff and then forwarded for consideration to the new seven-member Wilsonville-Metro Community Enhancement Committee, which is composed of four community members—Chair Brad Hughbanks, Vice Chair Larry Beck, Kate Johnson and Jimmy Lee; Mayor Tim Knapp and City Councilor Susie Stevens; and Metro District Three Councilor Craig Dirksen. The committee was formed over the 2015-16 period as a part of the new Wilsonville-Metro Community Enhancement Program.

Funded by a per-ton charge on biodegradable solid-waste, the Community Enhancement Program was extended by Metro regional government effective July 2015 to cover all cities with a waste-transfer facility, including the Willamette Resources Inc. facility operated by Republic Services in Wilsonville. Metro estimates that about \$85,000 per year in community enhancement funds would be available to Wilsonville; current trends and potential changes in the Metro solid-waste franchise disposal program over the next few years may result in increased volumes being transferred that eventually yield \$100,000 per year of enhancement funds. Funds may be used immediately on smaller projects or accumulated for up to three years to underwrite a larger project.

The Community Enhancement Program funding is used for "enhancing the host community of the facility from which the fees have been collected" to fund projects that "rehabilitate and enhance the area within the city." Eligible projects can improve the appearance or environmental quality of the community, increase reuse and recycling opportunities or improve recreational areas and programs. Program proposals will be accepted again next winter, Dec. 1, 2016, through Jan. 31, 2017, with potential project awards next spring.

For more information about the program or the 2016 projects recommended for funding approval visit [www.ci.wilsonville.or.us/CommunityEnhancement](http://www.ci.wilsonville.or.us/CommunityEnhancement).

## Tonkin Challenge Fun Run Benefits Local Schools on June 5

Ro<sup>n</sup> Tonkin Family of Dealerships is holding the Fifth Annual "Tonkin Challenge-For The Love Of Schools" fun run on Sunday, June 5, with the first race starting at 8 am. The event, held at Audi Wilsonville, 26600 SW 95th Avenue, raises money for educational programs of the West Linn-Wilsonville School District and other school districts throughout the region.

**5th Annual Tonkin Challenge for the Love of Schools**  
 • Sun, June 5, 7:30 am  
 26600 SW 95th Avenue

## Water Feature Season Begins

W<sup>e</sup>dnesday, June 1, 10 am-8 pm, is the first day the City's two water features located at Murase Plaza and Town Center Park are open. The popular attractions remain open all summer long until Sunday, Sept. 11. To make sure that the features are up and running, please call the Water Feature Hotline 503-685-6356.



.5-mile Mascot Dash for younger kids. Sign up at [www.TonkinChallenge.org](http://www.TonkinChallenge.org) to participate.



## Help Boys Scouts Go to Camp Buy Plants at Annual Sale

T<sup>h</sup>e local Boy Scouts Troop 194 is hosting the Annual Plant Sale and Silent Auction on Saturday, June 18, 9 am-3 pm, on the lawn near the Art Tech High School, 29796 Town Center Loop E. The sale includes a wide variety of plants, trees and more donated from local nurseries. Proceeds from the sale provide scholarships to send Boy Scouts to camp.



## Solar Energy Workshop Helps Residents Go Green and Save

S<sup>o</sup>lar Oregon is offering a free "Basics of Going Solar" workshop in Wilsonville for community members who are interested in learning more about solar energy and are considering installing solar panels on their home.

The workshop is **Saturday, June 11, 10-11:30 am, at the Wilsonville Public Library**, 8200 SW Wilsonville Road.

Attendees learn why solar can work in Oregon's climate; how to determine good sites for solar; how to qualify for tax credits, incentives and low-interest loans; and what questions to ask when selecting a contractor.

"Often homeowners don't know that incentives and tax credits may cover up to 80% of the cost of solar

systems," said Joshua Baker, Solar Oregon Program Manager. "With the federal tax credit set to expire at the end of 2016, now is a great time to take advantage of affordable solar prices."



As a non-profit organization since 1979, Solar Oregon provides workshops, training and outreach to businesses and residents throughout Oregon and southwest Washington to promote the adoption of solar energy. Solar Oregon was recognized as the best source for neutral, unbiased information on solar energy in Oregon. Pre-registration is encouraged. For more information and to

pre-register call Solar Oregon at 503-231-5662 or [www.SolarOregon.org](http://www.SolarOregon.org).

## Time to Register for Fall Recreational Soccer and Summer Camps

W<sup>i</sup>llamette United Soccer Club (WUSC), primarily serving Wilsonville and the surrounding communities, is accepting online registrations for kids to play fall soccer. Generally players registered by June 15 get an early-bird discount.

Parents are strongly encouraged to register as early as possible to ensure a spot for their child on a team.

WUSC offers teams for boys and girls in grades kindergarten to eighth grade, as well as, coed teams for high school-aged soccer players.

Practices begin in late August and are held one to two days a week depending on the child's age.

Fall games begin Saturday, Sept. 10 and end Oct. 29

for most teams. Kindergarten-2nd grade teams, play in West Linn or Wilsonville and older ages play on local fields or in the surrounding areas including Clackamas, Lake Oswego, Oregon City, Tigard and West Linn.



The non-profit soccer club's recreational and coed high school programs rely on parent volunteer coaches and offers support to new coaches through free coaching clinics. The club also offers club soccer for advanced players, summer soccer camps for players of all ages and abilities.

To register for fall soccer or learn more about coaching opportunities and summer soccer camps visit [www.WUSC.net](http://www.WUSC.net).

# City Calendar

| JUNE   |   |
|--|---|
| 1 Wed  | Budget Community Meeting, 6-9 pm City Hall  |
| 4 Sat  | Wilsonville Festival of the Arts 10 am-6 pm, Town Center Park   |
| 5 Sun  | Tonkin Challenge-The Love Of Schools Run, 8 am, Audi Wilsonville, <a href="http://www.TonkinChallenge.org">www.TonkinChallenge.org</a><br>Wilsonville Festival of the Arts 10 am-5 pm, Town Center Park |
| 6 Mon  | City Council, 5 pm Work Session, 7 pm Meeting, City Hall  |
| 7 Tue  | Municipal/Traffic Court, 5 pm, City Hall  |
| 8 Wed  | Planning Commission, 6 pm, City Hall<br>Budget Community Meeting (If needed), 6-9 pm City Hall  |
| 9 Thurs  | Parks and Recreation Board Meeting 4:30 pm, Parks and Rec. Admin Bldg.  |
| 11 Sat   | Basics of Going Solar Workshop 10-11:30 am, Wilsonville Public Library  |
| 13 Mon   | DRB Panel A, 6:30 pm, City Hall   |
| 14 Tues  | Flag Retirement Event, Town Center Park, 10 am  |
| 16 Thurs   | Wilsonville Leadership Academy 5:30 pm-6:30 pm, City Hall<br>Volunteer Fair (Open to the Public), 6:30-8 pm, City Hall  |
| 18 Sat   | Boy Scouts Annual Plant Sale & Silent Auction, 9 am-3 pm, Comm. Center<br>Korean War Remembrance Ceremony, Town Center Park, 10 am  |
| 20 Mon   | City Council, 5 pm Work Session 7 pm Meeting, City Hall   |
| 21 Tue   | Municipal/Traffic Court, 5 pm, City Hall  |
| 22 Wed   | Library Board, 6:30 pm, Wilsonville Public Library  |
| 27 Mon   | DRB Panel B, 6:30 pm, City Hall   |
| 29 Weds  | Rec and Aquatic Center Open House, 6-7:30 pm, Parks and Rec Admin Bldg  |
| JULY   |   |
| 4 Mon  | July 4 <sup>th</sup> Holiday-City Offices Closed  |
| 5 Tue  | Municipal/Traffic Court, 5 pm, City Hall  |
| 7 Thurs  | City Council, 5 pm Work Session 7 pm Meeting, City Hall   |
| 9 Sat  | Hazardous Waste Collection Day, 9 am-2 pm, City Hall Parking Lot  |
| 11 Mon   | DRB Panel A, 6:30 pm, City Hall   |
| 13 Wed   | Planning Commission, 6 pm, City Hall<br>Wilsonville Community Seniors Inc, 6 pm, Wilsonville Community Center   |
| 15 Fri   | Movie In The Park-Movie starts at dusk, Memorial Park River Shelter   |
| 18 Mon   | City Council, 5 pm Work Session, 7 pm Meeting, City Hall  |
| 19 Tue   | Municipal/Traffic Court, 5 pm, City Hall  |
| 21 Thurs   | Rotary Concert Series-Johnny Limbo and the Lugnuts, 6:30 pm, Town Center Park   |
| 24 Fri   | Movie In The Park-Movie starts at dusk, Memorial Park River Shelter   |
| 25 Mon   | DRB Panel B, 6:30 pm, City Hall   |
| 27 Thurs   | Library Board, 6:30 pm, Wilsonville Public Library  |
| 28 Thurs   | Rotary Concert Series-The June Bugs, 6:30 pm, Town Center Park  |
| 29 Fri   | Movie In The Park-Movie starts at dusk, Memorial Park River Shelter   |
| All dates and times are tentative; check the City's online calendar for schedule changes at <a href="http://www.ci.wilsonville.or.us">www.ci.wilsonville.or.us</a> . |   |

## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and the meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and replayed periodically. Meetings are also available to stream live and by video-on-demand online at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at all City Council meetings.



**Tim Knapp**  
Mayor  
[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



**Scott Starr**  
City Council President  
[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



**Julie Fitzgerald**  
City Councilor  
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**Susie Stevens**  
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**Charlotte Lehan**  
City Councilor  
[lehan@ci.wilsonville.or.us](mailto:lehan@ci.wilsonville.or.us)

### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)

### Wilsonville City Hall

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)



# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

March 2016

## State Department Sponsors Official Visit of Albanian Mayors

In February, five mayors from Albania, on an official visit sponsored by the US State Department, visited the City of Wilsonville as well as Astoria and Portland.

During the Albanian delegation's half-day visit to Wilsonville, the group toured the community with Mayor Tim Knapp and City Councilor Julie Fitzgerald. The official visit was arranged with the help of World Affairs Council of Oregon.

The purpose of the delegation's visit was to learn about the inner workings of local government in the United States. In particular the group specifically desired to observe municipal finance strategies, waste water treatment techniques, water treatment facilities, urban and regional planning tools, community

engagement strategies, urban renewal and redevelopment approaches, capital financing strategies, environmental protection measures and other strategies and tools utilized by local government in the United States.

In Wilsonville, Mayor Knapp and Councilor Fitzgerald met with the mayors at City Hall and then provided the delegation a tour of the community including a tour of the waste water treatment plant, water treatment plant, Villebois neighborhood and other important sites around town.

Albania is located northwest of Greece and has a population of approximately 2.7 million people.

In 2015, Wilsonville hosted 10 mayors from South Korea on a State Department-sponsored visit.



Wilsonville officials Mayor Tim Knapp (third from right), City Councilor Julie Fitzgerald (third from left) and City Manager Bryan Cosgrove (fourth from left) met with the five Albanian Mayors of Berat, Fier, Korca, Shkodra and Vlora municipalities who visited Wilsonville on an official visit sponsored by the US State Department and the World Affairs Council of Oregon.

## Mayor Knapp Presents '2016 State of the City' Address on March 7

Wilsonville Mayor Tim Knapp is preparing to present the "2016 State of the City Address" at the start of the City Council meeting on Monday, March 7, 7:00 p.m., at Wilsonville City Hall, 29799 SW Town Center Loop East.

In general Mayor Knapp is planning to speak about how the City is working to "Build a Complete Community" and is expected to showcase both recent and future projects and initiatives that contribute to maintaining Wilsonville's quality of life for community members of all ages. In particular the address intends to highlight a broad range of major projects and initiatives the City recently completed or is beginning this year.

Community members are invited to attend the

City Council meeting to hear the address or to watch a live broadcast either on the City's website at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV) or on Xfinity/Cast Ch. 30 or Frontier Ch. 32.



The new accessible play structures installed at Murase Plaza.

## 'March for Meals' Recognizes Importance of Senior Meal Program

At the City Council meeting on Feb. 18 Mayor Tim Knapp proclaimed that March 2016 is "March for Meals" month. The proclamation seeks to raise awareness about the long-standing meal program in general and in particular to call attention to the Home Delivered Meals program that has been

administered by the City, with the help of community volunteers, since 1985.

Mayor Knapp explains, "The City's senior meal program not-only provides great nutritious food, but also offers a chance for our seniors to socialize with others and for our volunteers to do a quick safety-check for those homebound seniors who are not able to come to the Community Center for meals."

In 2015 the program prepared a total of 12,085 meals of which 6,191 meals were served at the center and an additional 5,894 meals were delivered to local homebound seniors and individuals with disabilities

in the community. There was a 30% increase in the number of meals delivered from 2014 to 2015.

The meals are prepared each weekday in the commercial kitchen at the Wilsonville Community Center and then are served at the center and also are delivered every weekday to homebound seniors by a team of 35 volunteers.

Evie Proctor, the City's Nutrition Coordinator, explains, "This program delivers a healthy meal and personal contact as much as five times a week to some of our most vulnerable community members — both the meal and the contact improve their lives."

At noon on Mondays, Tuesdays, Wednesdays and Fridays the program staff and volunteers prepare and serve meals at the Wilsonville Community Center for a suggested donation of \$3.50 for those 60 and over or \$4.25 for those under 60; however, a recipient may pay whatever he or she can afford.

To qualify for the meal delivery program, a person must be 60 or older and be homebound by reason of injury, illness, an incapacitating disability, or be otherwise isolated. In addition, disabled persons under 60 also qualify under certain circumstances.

The program accepts donations in the form of a check payable to: City of Wilsonville noting "Home Delivered Meals" or "Nutrition Program" in the memo line. For information about the program or other referral services, contact Sadie Wallenberg, Information & Referral Specialist, at 503-570-1526, [wallenberg@ci.wilsonville.or.us](mailto:wallenberg@ci.wilsonville.or.us).



Andrea Bowles and her daughter delivering a meal to Cecil Romero.

### Mayor's Message

## Community Members Volunteer for City Positions

As we start the new year, the City Council confirms the appointment of volunteer community members to fill vacancies on the City's various boards and commissions. Recently we had 13 positions to fill and we were fortunate to have 15 community members interview for the openings. Applicants included five current board and commission members seeking reappointment and 10 community members seeking new appointments, including five participants of the 2015 and 2016 Wilsonville Leadership Academy class sponsored by the City.



During December and January, City Councilors Julie Fitzgerald and Susie Stevens and I met with

*We were very impressed with the level of interest, commitment, qualifications and passion each community member shared for our town.*

the applicants in order to become better acquainted and understand where volunteers might best serve. We were very impressed with the level of inter-

est, commitment, qualifications and passion each community member shared for our town.

While we would have liked to have a position for everyone who applied, ultimately the City Council did settle on the following appointments:

- **Budget Committee:** Andrew Karr and Paul Bunn to three-year-long terms
- **Development Review Board:** Samy Nada, Fred Ruby and Samuel Scull to two-year terms
- **Parks and Recreation Advisory Board:** Elaine Swyt and Steve Benson to four-year terms
- **Planning Commission:** Eric Postma, Peter Hurley and Kamran Mesbah to four-year terms
- **Wilsonville-Metro Community Enhancement Committee:** Larry Beck, Katherine Johnson, Jimmy Lee and Roseann O'Laughlin to one-year terms

This process of appointing volunteers to serve on City boards affirms what a talented and caring community we have. And one where the people's participation in local government provides the very foundation of our democracy to create a better place for all of us to live, work and play. Thank you to everyone who applied for these positions and also to the many current and prior board and commission members who volunteer their time to serve Wilsonville.

*Tim Knapp, Mayor*

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...and more!

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## Clackamas County Rolls Out New Leaders in Sustainability Program for Local Businesses

Clackamas County is rolling out a new business certification program, Leaders in Sustainability. This certification is offered at three levels: basic certification, Silver and Gold. The program offers customized on-site assistance to businesses making efforts to prevent waste, reduce energy and water consumption, use fewer toxic products, and support employees and their community. The assessment identifies more than 80 best practices focused on three areas of benefit: financial, environmental and people – and helps connect businesses to one another through a mentoring program.

Increasingly, businesses are seeking out resources and assistance to excel in and be recognized for a variety of best practices. Many of these practices save money but also conserve resources, reduce environmental impacts, help businesses recruit and retain talent, mitigate risk and attract new customers. Studies also show that corporations that prioritize responsible, sustainable operations and products outperform their peers.

Clackamas County has recently certified 25 businesses county-wide with many more in the works. The following are three Wilsonville businesses that have made outstanding efforts to achieve Leaders in Sustainability certification at the Gold level:

### Coffee Creek Correctional Facility

CCCF is a multi-custody prison accommodating all of the State of Oregon female inmates. The correctional facility provides intake and evaluation of all female and male inmates committed to state custody by the courts for nearly 1,700 inmates.

#### Sustainability Efforts

- Since 2014, reduced food expenses by \$539,616 by reducing food waste
- Waste audits have saved \$1800 off monthly garbage bill.
- Energy conservation by education and awareness, implementing behavior changes, replacing lighting to LED and procuring energy efficient equipment.
- Purchasing and using green janitorial chemicals.
- Garden produces over 6,000 lbs. of organic vegetables that is used to supplement fresh and healthy

food alternatives. Unused produce is donated to the Oregon Food Bank.

- Provide Sustainable Gardening, Seeds to Supper, Roots of Success, and Beekeeper Apprenticeship classes to the adults in custody.
- Active green team and adults in custody green teams.

including building design, occupancy sensors, lighting selection and heating and cooling optimization.

- Replaced LCD projectors in conference rooms with LED projectors and flat screen TVs, which reduced energy consumption.
- Trained warehouse employees and janitorial staff to recycle plastic shrink wrap and bubble wrap.



Business leaders from across Clackamas County at the Leaders in Sustainability Award Ceremony held in February.

### Rockwell Collins

Rockwell Collins has a global team of nearly 20,000 employees that shares a vision to be the most trusted source of aviation and high-integrity solutions in the world. Around 500 employees work in the Rockwell Collins Head-up Guidance Systems (HGS) facility in Wilsonville.

#### Sustainability Efforts

- Implemented composting program in Aug. 2014 with Aramark. In one year, over 3000 pounds of food scraps were collected, which equates to 800 pounds of compost.
- Joined 22 other Wilsonville businesses in the EPA “Green Power Communities” program by signing up for the Silver Level Clean Wind program through PG&E, which offsets around 18% of our electricity usage with wind power.
- Set-up employee education sessions, including electric vehicle show-and-tell, recycling education with Clackamas County, and the Earth Day Fair on April 1, 2015 with tables staffed by local environmental organizations.
- Have applied continuous improvement and Lean principles to the energy management of the facility,

- All storm drains in parking lot have been marked with pollution prevention labels.

### Microsoft Surface HUB

Microsoft Surface HUB employs about 200 employees and manufactures Microsoft Surface HUB, a multi-touch large display: a powerful team collaboration device designed to advance the way people work together naturally.

#### Sustainability Efforts

- ISO 14001 certified for environmental management.
- LED office lighting.
- Electric vehicle charging stations.
- Onsite shower facilities and covered bike parking.
- Raised employee awareness of commuting options.
- Reduced waste from catering events.
- Set office recycling goals.
- Baseline energy audit and roadmap for energy reductions.

For more information on Leaders in Sustainability: visit [www.clackamas.us/recycling/lis](http://www.clackamas.us/recycling/lis) or contact Julane Potter, Sustainability Coordinator, 503-742-4456, [julanepot@co.clackamas.or.us](mailto:julanepot@co.clackamas.or.us).



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

### Special event: Wee Sing March 24

Wee Sing co-creator Pam Beall will share some of her favorite “Wee Sing” songs and activities in a “Music and Movement” program at the library on Thursday, March 24, at 10:30 am.



The award-winning “Wee Sing” books, CDs, and DVDs have been filling children’s lives with music for over 35 years. From sing-along songs and nursery rhymes to fun fingerplays and campfire favorites, the Wee Sing materials brings the joy of music, singing, and movement into the lives of generations of children and families.

Children from birth to six are invited to join the fun in this free event, along with their parents and caregivers. The library will have plenty of “Wee Sing” music available for checkout, so don’t forget your Clackamas County Library Card.

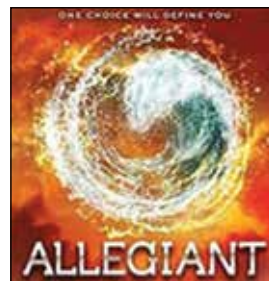
For more information, contact Youth Services Librarian Steven Engelfried at 503-570-1592 or [engel-fried@wilsonvillelibrary.org](mailto:engel-fried@wilsonvillelibrary.org)

**\*Please note: Regular Youth programs are on break March 14 - 25.**

### Teen “Divergent-Allegiant Night” Friday, March 18

Get ready for the film release of “The Divergent Series: Allegiant” at this event. “I wonder if fears ever really go away, or if they just lose their power over us.” Find out at this exciting event.

This free event will be held in the Oak Room on Friday, March 18, from 6:30 - 8:30 pm. For students in grades 6-12.



### Dulcina Ensemble performs Saturday, March 12

The Dulcina Ensemble is set to perform music from the late Middle Ages through the Renaissance at



our March Book Notes Concert on Saturday, March 12, at 2 pm in the Fiction stacks.

Enjoy free live music in the Fiction stacks!

[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

### Free Tax Help Available

A volunteer CPA is going to be at the library ready to answer your questions and/or help you fill out your tax forms on Saturdays, February 13 through April 9, 10 am - 3:30 pm.

Please bring all of your necessary documentation and your 2015 tax records.

**Saturdays through April 9, 10 am - 3:30 pm. Free.**



## Upcoming Classes and Events

### First Friday Film

Friday, March 4, 6 - 8  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

### Book Club

*Unlikely Pilgrimage of Harold Fry*  
Thursday, March 10, 6 - 8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

### Book Notes Concert

*Renaissance Music by Dulcina*  
Saturday, March 12, 2 - 3 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

### Great Books Discussion Group

*Gospel of Mark*  
Tuesday, March 15, 6 - 8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

### K-2 Book Adventures

*Silly Stories*  
Thursday, March 17, 4 - 4:45 pm  
[www.WilsonvilleLibrary.org/k2](http://www.WilsonvilleLibrary.org/k2)

### Genealogy Club

Monday, March 21, 1 - 2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

### History Pub at Wilsonville McMenamins

Tuesday, March 29, 6:30 - 8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

### Game Night

Wednesday, March 30, 6 - 8 pm  
[www.WilsonvilleLibrary.org/games](http://www.WilsonvilleLibrary.org/games)



### Public Feedback Needed to Update City's Transit Master Plan

This year Wilsonville's public transit agency, South Metro Area Regional Transit (SMART), is undertaking a big project to update the City's Transit Master Plan by the end of the year. In order to ensure the plan captures the community's priorities and needs, SMART is eager to hear from as many community members as possible.

Throughout March residents, employers, employees and visitors are all encouraged to share feedback on the future of the City's transit service by completing an online survey at [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com). The survey is also available to be taken at mobile survey stations that will pop up in March at various popular locations around town.

Community feedback is needed to help SMART better understand and prioritize the type of public transit services the community feels is most needed. To do so the survey asks tough questions like:

- If you can't have it all... Should SMART focus on travel within Wilsonville or on helping people get between Wilsonville and other cities?
- What's important to you? Transit that helps peak hour weekday commuters get to work, or transit that is available all day and all week, for many different kinds of people and trips?

After the survey closes, SMART plans to begin studying the survey results and drafting potential rec-

ommendations for the public, transit staff and City leaders to further consider.

This summer community members will again be invited to complete a second survey in order to provide feedback on the various transit service alternatives and priorities under consideration.



In early March mobile survey stations like this will pop up at City Hall and the Library and then be moved around town.

Finally, this fall SMART plans to release a draft Transit Master Plan for public review and comment prior to City Council consideration later this year.

To take the survey, share feedback and stay informed about the Transit Master Plan visit [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com).

Think SMART. Ride SMART. Plan SMART.

### New Toastmasters Club Forms

Graduates of the 2015 Wilsonville Leadership Academy are forming a new Toastmasters club in Wilsonville. The club's focus will be on leadership and volunteering in the community. The club is geared toward residents of Wilsonville, but is open to anyone who would like to get involved. An informational meeting is being held on



WHERE LEADERS ARE MADE  
[www.toastmasters.org](http://www.toastmasters.org)

Saturday, March 19, 9 am at Boone Town Bistro and Bar, 25425 SW 95th Ave, Wilsonville, located next to Wilsonville Holiday Inn. For more information, contact Angela Handran, Community Outreach Specialist, at 503-570-1503; [Handran@ci.wilsonville.or.us](mailto:Handran@ci.wilsonville.or.us).

### Choral Group Seeks Singers

Wilsonville's I-5 Connection, a local choral group seeks singers to join and sing in the group's spring concert on June 4, 2016. There is a nominal fee and members are required to attend rehearsals that are held every Thursday, 10 am -12 pm at the Wilsonville Community Center, 7965 SW Wilsonville Road.

For more information contact director, Jocelyn Higgins, at [jphiggins@comcast.net](mailto:jphiggins@comcast.net), leave your name and phone number at the front desk in the Community Center, or call Patty Brescia, 503-682-3727.

### Building Permit Data Correction

Due to a clerical error, the February 2016 issue of the *Boones Ferry Messenger* reported inaccurate building permit data. The correct number of new single-family building permits issued in 2015 is 316 with a valuation of \$79.0 million and the correct two-year total for 2014 and 2015 is 562 with a valuation of \$131.2 million. The correct number of commercial/industrial/multi-family alteration permits issued in 2015 is 264 with a valuation of \$21.3 million. The correct number of multi-family building permits issued in 2015 is 16 with a valuation of \$4.9 million for a total of 37 units.

## SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

### 1X & 3 Bus Route Clarifications

Please note that the 1X & 3 bus routes do not stop on Kinsman Road either northbound or southbound. The 1X southbound bus stops on Wilsonville Road at O' Reilly Auto Parts and Walgreens. The 1X northbound bus has "Drop Off Only" at Bullock Family Dental and at the Fred Meyer Fueling Station. The 3 southbound bus has only one stop at Walgreens and the 3 northbound bus has only one "drop off only" stop at Bullock Family Dental.



### Additional Service to 1X Schedule Begins March 7

In order to relieve overcrowding and encourage commuters to ride transit, SMART and Salem Cherrits will be adding additional trips on the 1X bus route. In the morning from Wilsonville to Salem (southbound), 20 minute service will occur between 6:30 am and 7:30 am. Trips will leave Wilsonville at 6 am, 6:30 am, 6:50 am, 7:10 am, 7:30 am, and 8:05 am. In the afternoon, an additional trip at 4:37 pm will occur, allowing for 30 minute frequency from 3:37 pm to 6:37 pm.

Northbound in the morning from Salem, trips will leave Salem at 5:00 am, 6:25 am, 7:00 am, 7:50 am, 8:15 am, and 9:05 am. In the afternoon, a new trip will depart Salem at 3:35 pm. Following trips remain the same, providing approximately 30 minute service from 4:00 pm to 7:05 pm.

In addition to the 1X bus route changes, there is also an added trip at the beginning of the route 6 from Wilsonville station at 3:03 pm northbound to Argyle Square.

The 2X bus route southbound from Barbur Transit Center has an additional 5 minutes built into the run leaving Barbur Blvd. Transit Center at 2:00 pm for better on time performance. Please visit our website or pick up a new schedule to find full details.



### SMART Tip: Stay Bright at Night

Three simple tips for riding the bus at night:

1. Wear bright-colored clothing and/or reflective gear when walking and biking, especially near dawn and dusk.
2. Stand up when your bus approaches so you're more visible.
3. If you have a mobile phone, flashlight or anything bright, wave it so the bus operator can more easily see you.



## Resolve to be Ready in 2016

During stormy weather we can lose power. Sometimes the interruption lasts for only a few minutes, but other times you might have to go without electricity for a few hours or longer. Here are some basic tips for what to do before, during, and after a power outage.

### Before a Power Outage

- **Build or restock your emergency preparedness kit**, including a flashlight, batteries, cash and first aid supplies.
- Know where the **manual release lever of your electric garage door opener** is located and how to operate it.
- **Keep your car's gas tank full** - gas stations rely on electricity to power their pumps.

### During a Power Outage

- **Only use flashlights** for emergency lighting, candles can cause fires.
- **Keep refrigerator and freezer doors closed.** Most food requiring refrigeration can be kept safely in a closed refrigerator for several hours. An unopened refrigerator keeps food cold for about 4 hours. A full freezer keeps the temperature for about 48 hours.
- **Turn off or disconnect appliances and other equipment in case of a momentary power "surge"** that can damage computers and other devices. Con-

sider adding surge protectors.

- **Put on layers of warm clothing** if it is cold outside.
- **Never burn charcoal for heating or cooking indoors.**
- **Never use your oven as a source of heat.** If the power may be out for a prolonged period, plan to go to another location (the home of a relative or friend, or a public facility) that has heat to keep warm.

### After a Power Outage

- **Throw away any food that has been exposed** to temperatures of 40 degrees F or above for two or more hours or more or that has an unusual odor, color or texture. When in doubt, throw it out.
  - If food in the freezer is colder than 40 degrees F and has ice crystals on it, you can refreeze it.
  - **Restock your emergency kit** with fresh batteries, canned foods and other supplies.
- For more information visit [www.ready.gov](http://www.ready.gov).



## Parks and Rec Still Accepting Opportunity Grant Applications

The City of Wilsonville's Parks and Recreation Department is accepting applications for the Community Opportunity Grant Program until Friday, March 4, 5 pm.

### Community Opportunity Grant Due Date

- **Fri, March 4, 5 pm**  
Parks and Recreation Office,  
29600 SW Park Place

The **Community Opportunity Grant Program**, with funds distributed twice per year for a total of \$25,000, provides funding for a wide range of local

programs and projects in Wilsonville. There is \$18,000 available during the upcoming grant cycle. Funds for the Opportunity Grant Program come from the City's general fund.

Successful applications for the Community Opportunity Grant Program include proposals aimed at promoting education, diversity, arts or community involvement in the city.

For more information, go online to [www.WilsonvilleParksandRec.com/GrantPrograms](http://www.WilsonvilleParksandRec.com/GrantPrograms); or contact Brian Stevenson, Recreation Coordinator, at 503-570-1523, [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

## Clackamas Community College Offers Scholarships

The Clackamas Community College Foundation awards more than \$500,000 in scholarships every year, providing vital financial support for students. Scholarships are available to all students planning to attend CCC for the 2016-17 academic year, including current students, future students and high school seniors. Open enrollment began Jan. 30.

CCC offers 275 different scholarships that range from funding a portion of tuition or supplies to a full-ride scholarship. They are available for students with at least a 2.0 grade-point average who are taking six or more credits. CCC scholarships are offered in nearly every area of study including career-technical fields such as automotive, welding, horticulture, health sciences and water and environmental studies.

"Scholarships have never been more important," said Greg Fitzgerald, executive director of the CCC Foundation. "Scholarships can mean the difference between starting your post-college career owing money and starting it relatively debt free."

With declining public support, students are carrying an increasingly larger portion of the cost of higher education. With community colleges offering a quality education at a fraction of the cost, scholarship recipients at CCC face less debt at graduation.

Some scholarships are dedicated to students pursuing certificates, while others benefit students who plan to transfer to a four-year program. There are scholarships available for students who begin at CCC and move on to Portland State University. Others support students who are the first in their families to attend college. **Scholarship applications are submitted online at [www.clackamas.edu/scholarships](http://www.clackamas.edu/scholarships) through March 30.** Scholarship awards are announced in May.

The CCC Foundation is a tax-exempt, nonprofit corporation that raises funds and friends to support the college. For more information about CCC scholarship opportunities, contact Darcie Iven, scholarship coordinator, at 503-594-3421 or email [darciei@clackamas.edu](mailto:darciei@clackamas.edu).



29600 SW Park Place

Administrative Office: 503-783-7529

Community Center: 503-682-3727

[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

## Park Rental Season Opens Soon

Memorial Park is a popular location for company gatherings, family parties, weddings and a variety of sporting events. Wilsonville Parks and Recreation offers a number of park rental options including: the historic Stein Boozier Barn, the River Shelter, the Forest Shelter, Splash Shelter and sport fields.



Reservations for Wilsonville residents and businesses will be accepted starting on March 14 for the 2016 season. The rental season runs April 9 - Oct. 9.

For more information on availability and fees please visit the Parks and Recreation website or contact Ahsamon at 503-570-1530.

## Spring Break Science Camp

Wilsonville Parks and Recreation has partnered with Mad Science to offer a Spring Break inventors camp. This week long camp runs from 9 am - noon and is being held at the Tauchman House. Campers will probe the world of machines and their evolution from simple to the complex world of robotics. Learn about electricity, physics and automation. On the last day, campers will put all their skills together to create their very own invention. The program is open to children ages 6 - 12 and costs \$175.

## Community Garden Sign Up

The Community Garden opens for registration on Thursday, March 17. Gardeners are asked to come to the Parks and Recreation Admin. Office to register and pick their garden plot. Registration opens at 8:00 am.

In-ground plots are \$22 for the season. Raised beds are \$25 for the season. Please visit the Parks and Recreation website or contact Brian Stevenson at 503-570-1523 for more information.

## Spa Saturday at the Center

Mark your calendar for the 3rd annual Spa Saturday at the Center. This event is scheduled for Saturday, April 16, 10 am - 1 pm at the Community Center and this year's theme is "Island Paradise." Let your cares melt away as you sample free offerings of healthy and relaxing services like massage and meditation, sip on coconut water and snack on tropical fruit, enjoy the sweet tunes of Hawaiian music as you peruse the health & craft vendor area, and return home wearing your free lei and feeling rejuvenated. Practitioners and vendors interested in participating may call 503-570-1530 for more info. Deadline for submissions has been extended to March 18. Appointments, highly recommended for some offerings, are being accepted starting April 1. See [WilsonvilleParksandRec.com/SpaSaturday](http://WilsonvilleParksandRec.com/SpaSaturday) for more information.



## Upcoming Classes and Events

### Pilates Sculpt

Wednesdays, March 9 - May 4  
6 - 7 pm

Community Center  
\$59

### Qi Gong

Thursdays, March 10 - May 5  
6 - 7 pm

Community Center  
\$39

### Soccer Shots

Saturdays, March 12 - May 14  
Ages 2 to 3, 9 - 9:30 am

Ages 3 to 5, 9:40 - 10:15 am

Ages 3 to 5, 10:30 - 11:05 am

Ages 5 to 8, 11:15 am - 12 pm

Memorial Park  
\$153

### Beginner Learn to Ice Skate

Saturdays, April 2 to May 28  
Ages 6 to 10

11:05 - 11:35 pm

Sherwood Ice Arena

\$90 (plus \$15 annual fee to ice arena)

## Wilsonville Egg Hunt

Join **Wilsonville Parks & Recreation** and **Wilsonville Albertsons** for this year's Egg Hunt. Numerous prizes & gift baskets are being given away.

Saturday  
March 26  
10 am  
1 - 11 yr olds  
Memorial Park  
No Charge



Thanks to our Sponsors:



**CHIEF'S CORNER – MOST COMMON VIOLATIONS CITED IN 2015**

This month, I thought *Boones Ferry Messenger* readers would be interested in seeing what the most common violations for which citations were issued by the Wilsonville Police Department in 2015. This list should also serve as a reminder of the rules of the road.



Of course, this list does not include all 129 different violations for which 1643 moving citations were issued, but it does give you a good idea of what the most common violations were.

Our purpose in educating you about the traffic citations we issued is to deter these infractions in the future and to increase vehicular and pedestrian safety. Please drive safely.

*Jeff Smith, Chief of Police*

**Most Common Moving Violations Cited in 2015 by Wilsonville Police Department**

|   |     |   |    |
|---|-----|---|----|
| 1. Speeding/violation of the Basic Rule   | 422 | 14. Failure to carry vehicle registration     | 14 |
| 2. Driving with a suspended license       | 195 | 15. Unsafe movement from lane                 | 14 |
| 3. No driver's license                    | 177 | 16. Unsafe distance from emergency vehicle    | 10 |
| 4. Failure to carry proof of insurance    | 135 | 17. Skateboarding in a park                   | 10 |
| 5. Driving uninsured                      | 131 | 18. Inoperable lighting equipment             | 10 |
| 6. Expired vehicle sticker                | 130 | 19. Failure to register vehicle in Oregon     | 9  |
| 7. Illegal use of cell phones             | 122 | 20. Failure to drive within lane              | 8  |
| 8. Failure to obey traffic control device | 84  | 21. Violation of open container               | 7  |
| 9. Driving under the influence            | 42  | 22. Illegal standing/stopping                 | 7  |
| 10. Failure to renew registration         | 27  | 23. Illegal u-turn                            | 6  |
| 11. Failure to change name/address        | 18  | 24. Failure to stop for pedestrian            | 6  |
| 12. Following too closely                 | 18  | 25. Failure to display registration plates    | 6  |
| 13. Careless driving                      | 14  | 26. Failure to obey control device/pedestrian | 4  |

**Go Red Campaign Raises Awareness of Heart Attack Symptoms**

During "National Wear Red Day" in February, Tualatin Valley Fire & Rescue (TVF&R) firefighter medics, training officers and fire prevention staff, exchanged their blue-duty uniform shirts for red ones in recognition that heart disease is the No. 1 killer of women in the United States, claiming more lives than all forms of cancer combined.



At a TVF&R event in 2015 Sharon Steiger, a Wilsonville resident, talks about the life-threatening cardiac event she experienced in 2008.

The campaign seeks to raise awareness about the importance of knowing the signs and symptoms of a heart attack—which can be different for men and women—and when to call 9-1-1.

Identifying the symptoms of a heart attack and calling 9-1-1 triggers a potentially life-saving continuum of care for a cardiac patient. Firefighter medics can begin advanced medical care in the field and hospitals, with a

catheterization lab and chest pain center, provide an expert diagnosis and fast, effective treatment.

**Heart disease is the No. 1 killer of women in the United States.**

While the message is equally important for both men and women, public awareness of heart disease and gender-specific heart symptoms in women has lagged behind that of men. Only one in five American women believes it is her greatest health threat.

As with men, the most common heart-attack symptom in women is **chest pain or discomfort**. But women are more likely to experience the other common symptoms — particularly **shortness of breath, nausea, vomiting, and back or jaw pain**. They are also symptoms

that women often brush off as the flu, stress, or simply feeling under the weather, which could put their lives in jeopardy.

**If you feel you may be having a heart attack, dial 9-1-1!**

"If you feel you may be having a heart attack, dial 9-1-1! Emergency responders will be able to care for you immediately upon their arrival and all the way to the hospital in case your condition worsens," explains Sharon Steiger, a resident of Wilsonville, who experienced a life-threatening cardiac event in 2008. "I tell

all my friends to listen to your body, and if you have concerns, talk to your doctor about the warning signs and steps you can take to prevent a heart attack."

**Common Heart Attack Symptoms**

**Men often experience: Chest pain and discomfort**

**Women often experience: Chest pain and discomfort and also shortness of breath, nausea, vomiting and back or jaw pain.**

**What are your risks for getting heart disease? Ask yourself these three questions.**

**1 How many risk factors do I have?**

**Major risk factors** are those that research has shown significantly increase the risk of heart disease. The more risk factors you have, the greater your chance of developing it.

- Non-Modifiable: age, gender, family history
- Modifiable: high blood cholesterol, high blood pressure, physical inactivity, obesity and overweight, smoking, diabetes, poor diet

**2 Do I know Life's Simple 7® keys to prevention?**

- CONTROL CHOLESTEROL**: 98.9 million Americans age 20 and older have total blood cholesterol levels of 200 milligrams per deciliter (mg/dL) or higher.
- MANAGE BLOOD PRESSURE**: Approximately 90% of all Americans will develop hypertension over their lifetime and 1 in 3 adults has high blood pressure, yet, many people don't even know they have it.
- REDUCE BLOOD SUGAR**: Heart disease and stroke are the No. 1 causes of death in people with type 2 diabetes.
- EAT RIGHT**: When you eat a heart-healthy diet you improve your chances for feeling good and staying healthy – for life!
- LOSE WEIGHT**: More than 2/3 of our American adult population is overweight, with 1/3 of us in the obese category.
- GET MOVING**: Over 50% of American adults, age 18 and older, do not get the recommended at least 150 minutes a week of moderate to vigorous physical activity.
- STOP SMOKING**: Among Americans age 18 and older, more than 19 million women and 24 million men are smokers.

Get your My Life Check® Assessment now at [heart.org/mylifecheck](http://heart.org/mylifecheck)

**3 Am I making an effort or making excuses?**

Top "barriers" to healthy living from a recent survey of website visitors to [heart.org](http://heart.org):

- 14%: Costs too much to eat healthy. Think again! Try our tips to save you money and time: [heart.org/healthytips](http://heart.org/healthytips)
- 18%: Don't want to stop eating foods they like. Then DON'T! Just find healthy ways to prepare them here: [heart.org/recipes](http://heart.org/recipes)
- 12%: Too busy taking care of others to take care of myself. To do the best for our loved ones, we must make an effort to be in the best possible health ourselves. [heart.org/caringforyourself](http://heart.org/caringforyourself)
- 7%: I don't really know how to take care of my heart. You're in luck! We have all the heart-health information you need. [heart.org/conditions](http://heart.org/conditions)
- 27%: I live a heart-healthy lifestyle. Great job! Learn more about preventing heart disease at any age: [heart.org/atanyage](http://heart.org/atanyage)
- 14%: Don't like exercising. Keep trying! Here are our top 5 tips to loving exercise... or at least not hating it: [heart.org/lovingactivity](http://heart.org/lovingactivity)
- 17%: Don't have time to exercise regularly. Don't worry, 10 minutes, 3 times a day is all it takes. [heart.org/take10](http://heart.org/take10)



## Reports from City Boards and Commissions

### Council Amends West Side Urban Renewal Plan, Appoints Volunteer Board and Commission Members and Clarifies SMART's Future Service Area

On Feb. 1, the City Council approved a substantial amendment to the West Side Urban Renewal Plan that includes the Villebois neighborhood and extends along Brown Road and further to the east to include a section of the Old Town neighborhood. The amendment increases the maximum indebtedness of the district by \$9.4 million to \$49.4 million in order to complete a set of primarily road construction projects that were already specified in the Plan. The amendment is consistent with the Wilsonville Urban Renewal Strategic Plan adopted by Council in October 2014 as recommended by a community task force.

City Council approved a resolution that clarifies the City's intent to align the service area boundary for the City's public transit system, South Metro Area Regional Transit (SMART), to be the city limits of Wilsonville. The resolution provides SMART support to begin plans to provide transit services to areas that have been, or are planned to be, annexed into the city limits. Some areas of Wilsonville, such as the Coffee Creek Correctional Facility, are currently in the Wilsonville city limits but are in the TriMet service area, so SMART staff plan to begin working with TriMet to transition these areas to be served by SMART. The resolution also allows SMART to move forward with plans to serve newly-annexed, or soon to be annexed areas that are not currently served by public transit.

The City Council appointed volunteers to fill a number of vacancies on the City's various boards and commissions. The appointments include:

- **Andrew Karr** and **Paul Bunn** to three-year terms on the **Budget Committee**
- **Sammy Nada**, **Fred Ruby** and **Samuel Scull** to two-

year terms on the **Development Review Board**

- Reappointment of **Eric Postma** and **Peter Hurley** and the appointment of **Kamran Mesbah** to four-year terms on the **Planning Commission**
- Reappointment of **Elaine Swyt** and **Steve Benson** to four-year terms on the **Parks and Recreation Advisory Board**
- Appointment of **Larry Beck**, **Katherine Johnson**, **Jimmy Lee** and **Roseann O'Laughlin** to one-year terms on the **Wilsonville-Metro Community Enhancement Committee**.

The Council also thanked other members of the public who applied for, but were not selected to fill, the various vacancies.

Under the consent agenda the Council authorized SMART to enter into a contract to expand and replace the existing Compressed Natural Gas (CNG) station that currently fuels four SMART buses located at the SMART office and bus yard on Boberg Road. The expansion of the CNG station is paid for in part by a Congestion Mitigation-Air Quality grant from the Oregon Department of Transportation and in part from City resources.

During the communications portion of the meeting the Council heard a report on the results of a Strategic Energy Management initiative conducted in partnership with the Energy Trust of Oregon to identify energy savings opportunities at the City's Waste Water Treatment Plant and the Willamette River Water Treatment Plant. Overall, with the help of contractors from CH2M and Veolia North America who separately operate the two treatment plants, the initiative identified over \$26,000 in annual energy savings.

#### Basalt Creek Concept Plan Update

Staff has drafted a resolution for both Wilsonville and Tualatin City Councils' consideration at



upcoming meetings, acknowledging the discussions and agreements from the Dec. 16, 2015 joint Council meeting. The Wilsonville City Council is tentatively scheduled to hear this resolution at their meeting on March 7. Staff is working toward holding a public open house in April (look for details in the April *Boones Ferry Messenger*.)

If you are interested in knowing more, please visit this project's website at [www.BasaltCreek.com](http://www.BasaltCreek.com) or email Long Range Planning Manager Miranda Bateschell at [Bateschell@ci.wilsonville.or.us](mailto:Bateschell@ci.wilsonville.or.us).

### City Council Grants Zone Changes, Authorizes Annexations and Finalizes Sale of Surplus Property

On Thursday, Jan. 21, the Wilsonville City Council held a public hearing and adopted an ordinance to make a substantial amendment to the West Side Urban Renewal Plan (see article above for details).

The Council held a second reading and approved a set of ordinances that amend the City's Zone Map zoning designation for three parcels of land located in the central area of the Villebois neighborhood. The zone change requested by applicant Polygon Homes Northwest seeks to permit construction of 132 row houses. The first planned development, Brookside Terrace, includes 50 row houses on approximately 3.2 acres located northwest of SW Villebois Drive North between SW Costa Circle West and SW Berlin Avenue. The second development, Camden Square and Royal Crescent at Villebois, includes 82 row houses located in the Villebois Village Center between Costa Circle and SW Villebois Drive. Both developments were previously reviewed and approved by the Development Review Board Panel A on December 14, 2015.

The City Council held a second reading and approved an ordinance that annexes into the city limits specific segments of SW Grahams Ferry Road and SW Tooze Road and a 22-acre undeveloped property located within the Villebois Master Plan Area on the south side of Tooze Road. The City already has authority for the road and is responsible of the road's maintenance. The approved annexation is needed in part to allow the investment of urban renewal funds in the Tooze Road improvement project, which is currently being designed to widen and improve the western section of Tooze Road just east of SW Grahams Ferry

Road. A portion of the other property to be annexed is to be used, in part, for a public park.

The City Council endorsed a resolution approving the sale price of a 10-acre parcel of surplus property that the City is selling to Polygon WLH LLC for an approved purchase price of \$4.90 million.

The City Council also held a second reading and approved an ordinance that amends the City's Public Work Standards to require: the use of warm mix asphalt for road improvements; changes to the storm water design standards; the use of iron pipes and fittings that are "made in the USA"; sanitary design changes to comply with the City's odor code; and changes to the colors used to paint public and private fire hydrants to make identification easier and faster.

During the communications portion of the meeting staff and consultants presented the IT Strategic Plan Survey, which is a new initiative that seeks to guide the City's Information Technology investments and actions over the next five years. The project was in the "discovery" phase and sought input from community members via an online survey in January.

At work session prior to the City Council meeting, the Council heard updates and discussed projects and upcoming issues, including a review of the financial sustainability of the City's Community Development Fund; a proposed garbage/recycling rate increase requested by waste-hauler Republic Services; the public-involvement process for the pending Transit Master Plan update; and the communications plan for the proposed recreation and aquatic center tentatively scheduled for the November 2016 election.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during March. Scheduling changes can and do occur; check for updated meeting information at the City's website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, March 7:** State of the City Address Mayor Knapp; Utility agreement with WWSP/ODOT - Kinsman Project; Nims property annexation/zone change; Basalt Creek Planning Area; Universal Health Services proposed hospital; Trocadero Park development agreement; and Bike Wayfinding Signage Plan.

**Monday, March 21:** Affordable housing property tax exemption recertification; supplemental budget adjustment; franchise fee code amendment (second reading); Nims property annexation/zone change (second reading); road maintenance fee update.

### Planning Commission Update

At the Planning Commission meeting on February 10, the Commission reviewed and discussed the Transit Master Plan public involvement strategy and the proposed Bike Wayfinding Signage Plan.



### Development Review Board Update

On Jan. 25, the DRB Panel B approved Universal Health Services application for annexation, Zone Map amendment, Comprehensive Plan amendment and other plans and designs for a behavioral healthcare hospital on the corner of Boones Ferry Road and Day Road.



This is a rendering of the Universal Health Services' proposed behavioral healthcare hospital.

At the DRB Panel A meeting on February 8, the panel approved West Linn-Wilsonville School District's plans, designs and other related items for the proposed new middle school south of at SW Advance Road between SW Wilsonville Road and SW 60th Avenue. During the meeting the panel also approved an annexation and Zone Map amendment for a 1 acre property at 11700 SW Tooze Road as well as and other plans and designs for applicant Polygon Homes NW to construct a 63-lot single-family development on the annexed property and adjacent property to the east, referred to as Calais East at Villebois, located south of SW Tooze Road and east of SW Grahams Ferry Road.

City Council will hold public hearings on the two proposed projects annexations and zone map amendments before considering and/or granting final approval of the pending land use applications.

### Library Board Report for January

During the Library Board meeting in January various reports included:

- Increased participation in the Teen Advisory Board.
- A Winter Party for teens featured marshmallow and spaghetti sculptures and a "snowball" fight.
- Deborah Gitlitz is working with Washington and Clackamas County librarians to explore the possibility of sharing costs to send a staff person to Guadalajara, Mexico to buy Spanish Language books.
- February is Food for Fines month. \$1.00 will be credited against fines for each can of food brought to the library. In the past this has raised 1200 - 1500 lbs of food for Wilsonville Community Sharing.
- The board reviewed the Library's proposed budget and specific budget line items were highlighted showing the enormous financial support provided by the Friends of the Library and the Library Foundation. In general, programs are supported by the supplemental contributions, while the staff, building, and materials costs come out of the general fund.

The next regular meeting is scheduled for Wednesday, March 23, 6:30 pm, at the Library.

## Protect the Environment and Loved Ones by Limiting Pesticide Use

The most commonly applied pesticides are insecticides (to kill insects), herbicides (to kill weeds), rodenticides (to kill rodents) and fungicides (to control fungi, mold and mildew). Harmful environmental impacts of pesticide use include: loss of biodiversity and elimination of key species (e.g., pollinator bees), water pollution, soil contamination, and pest resistance that leads to an increase in the application of pesticides. Studies directly link the use of many pesticides with human health problems and chronic conditions.

In your own backyard you can do your part to eliminate or reduce the use of pesticides that might end up in streams, soil, food chains and on children's skin. In many cases, the only "active ingredient" you'll need is some elbow grease; hand-pulling weeds, for instance, can eliminate the use of herbicides and the risks they pose to the broader environment.

Other tips for eliminating or reducing pesticide use and other unintended effects include:

- Be aware that **pesticides** applied in gardens can move from your intended application area by **drifting in the air** or **washing off into storm drains** or streams.
- **Be aware of the weather** and do not apply pesticides prior to rainfall or during windy conditions.
- **Avoid applying pesticides to surfaces like sidewalks or driveways**, where they can easily be washed off.
- **Check pesticide labels** for warnings regarding

their appropriate use and limitations.

- **Never dispose of pesticides in storm drains, sinks or toilets.**
- **Under no circumstances should pest control equipment be cleaned** in a location where **rinse water could flow into gutters, storm drains or open waterways.**
- **Never exceed application rates** listed on a pesticide label.
- **Purchase only enough pesticides for immediate needs**, which eliminates pesticide storage.
- **Always keep pesticides in their original containers.**

Unwanted pesticides, even diluted ones, should be taken to an approved waste facility, such as the Metro Household Hazardous Waste Disposal Facility, open Monday - Saturday, 9 am - 4 pm, and located at 2001



Washington St., Oregon City (503-234-3000).

Unused rinse liquid is also considered a hazardous waste and must be disposed of properly at a hazardous waste facility. For more information, contact Luke Bushman, Stormwater Management Coordinator, at 503-570-1552: bushman@ci.wilsonville.or.us.

## Utility Repair Projects Planned for Charbonneau this Spring

This spring the City of Wilsonville is kicking off the first phase of the Charbonneau Consolidated Improvement Plan, a 25-year plan to upgrade and improve deficient Charbonneau infrastructure and compromised sewer, storm, water and street utilities. This initial phase of work, called the Charbonneau High-Priority Utility Repair Project, includes refurbishment of the 15 highest priority sewer and storm pipelines in Charbonneau. This project is grouped into three separate phases, to be constructed over the next three years, beginning in the spring of 2016.

The project includes correction of structural deficiencies within the pipelines — such as cracks, broken,

tural deficiencies. Because the liner follows the path of the existing pipe, deficiencies with grade or sags cannot be corrected with CIPP. With CIPP, construction has less cost and occurs more quickly, completing a couple of blocks per day. However, CIPP generates a lot of noise, steam and a styrene odor during the curing process that can be annoying, but not harmful.

The first phase of work is scheduled to begin later this spring with work occurring at Boones Bend Road (9), Winchester Way (9), Estates Post Road (2), Molalla Bend Court (3) and Armitage Road (6 and 11) (see the circled projects on the map below). Most of the work, because these are pipes in the worst condition,

consists of open-trench replacement with some CIPP installation at a few locations.

During construction, local residents can expect that roads immediately around the construction areas are going to be impacted by construction and are going to be closed to through

traffic on the weekdays, 8 am - 5 pm. Access to the surrounding homes is to be maintained at all times, but some delays are to be expected. Garbage, delivery and emergency services are to be provided at all times during construction. On-street parking is going to be restricted during the day. Generally, interruption to utility service is not anticipated. However, there may be instances where the contractor is going to need to request that impacted residents not run appliances with heavy water use (dish or clothes washer), flush toilets or take showers for a couple of hours at a time. Expect noise, dust and an occasional portable toilet in the construction areas.

As construction nears and a contractor is selected, the City is planning to provide additional information to those residents affected by the construction. For more information contact the project manager, Zach Weigel, at 503 570-1565; Weigel@ci.wilsonville.or.us.



This map shows all the high-priority repair projects and the circled projects that are to be done in 2016.

crushed or corroded sections of pipe and removal of sags, flat grades and root intrusion within pipelines - to improve flow throughout the pipeline. The project also includes replacement of sewer lateral connections underneath the roadway and repair of deficient structures such as manholes and catch basins.

The utility line repair project uses two methods of construction: open excavation and replacement, and cured-in-place pipe (CIPP). Open Excavation consists of digging a new trench and completely replacing the pipeline with new pipe. This method corrects all of the existing pipe deficiencies, but typically costs more and has a greater impact on nearby neighbors due to the need to dig trenches and more complex installation.

CIPP consists of inserting a resin-filled liner into the existing pipe that is then inflated and steam cured to create a new pipe inside the existing one. This method can only be implemented on existing pipes with struc-

## VOLUNTEER: MEGAN CHUINARD

Megan Chuinard, a member of the Wilsonville Library Board for the past two years, is not your typical board and commission volunteer.

For starters Megan is among the youngest board and commission members that currently volunteers for the City. In explaining her motivations for serving on the Wilsonville Library Board so early in her career she states, "I bring a fresh perspective to the Wilsonville Library Board — a new understanding of how different generations may or may not use libraries, in hopes that we could accommodate all of the Library's patrons. I see the Library as more than a place for finding copies of books, but a general staple in our community."



Though Megan is young, she already has an impressive history of community and public service. Soon after graduating from the University of Oregon with a Bachelors of Arts in Political Science, Megan

*I see the Library as more than a place for finding copies of books, but a general staple in our community.*

began working for various members of the Oregon State Legislature including State Senator Frank Morse, State

Representative Vic Gilliam, and most recently as the Chief of Staff for State Representative John Davis until January 2015. Currently, she works as a Policy Analyst for the Oregon State Legislature in Salem where she focuses on the Business and Transportation, Education, and Environment and Natural Resources Committees.

In addition to working at the State Legislature, Megan has also worked on a number of various political campaigns including serving as Campaign Coordinator for the Forest Grove Library Foundation where the successful campaign raised \$250,000 to support the local public library.

In reflecting on Megan's contribution to the Library Board, Library Director Pat Duke explains, "Megan brings an inquisitive mind that keeps me sharp and helps us improve our services. Her passion for the library is evident in her work on the Library Board as well as her contributions to the Wilsonville Public Library Foundation. We are fortunate that she applies herself to the library. Plus, she's a delight to be around."

Having grown up and graduated from high school in Forest Grove, Megan was naturally drawn to Wilsonville's small-town feel. Wilsonville's location also offers her easy access to her work, family and friends. "As I was looking for a place to relocate, I really wanted to find a location that was close to the freeway, but still feels like home," she explains.

In addition to having a successful career and being an engaged community member, Megan also has big plans in her personal life. She and her fiancé, Baird Chrisman, are getting married this June and they are also in the process of buying their first home in Wilsonville. She explains, "We love Wilsonville because it has many beautiful parks, strong community involvement, great schools, and offers excellent recreational opportunities. We enjoy the many amenities our city has to offer."

## Free Filing Software for Taxpayers Earning \$62K or Less

The Internal Revenue Service (IRS) is partnering with brand-name tax software providers to offer "Free File" which makes available free federal tax return products to taxpayers whose adjusted gross income was \$62,000 or less during 2015. For taxpayers who earned more than \$62,000, the IRS is offering Free File fillable forms, the electronic version of IRS paper forms. To access the free software visit [IRS.gov/FreeFile](http://IRS.gov/FreeFile).

Some of the software providers are offering both free federal and state tax return preparation for states with income tax requirements, while others charge a fee for offering state tax return preparation.

**The tax filing deadline is Monday, April 18, 2016.**

## Grace Chapel Rummage Sale to Benefit Community Groups

The annual Community Rummage Sale sponsored by Grace Chapel is set for Friday, March 11, 5 - 9 pm, and Saturday, March 12, 9 am - noon, when all merchandise is half off. Proceeds from the sale are

**Community Rummage Sale**  
 • Fri, March 11, 5 - 9 pm  
 • Sat, March 12, 9 am - noon  
 Grace Chapel, 9600 SW Boeckman Rd

provided to community organizations that in the past have

included school-support groups, Random Kindness, Wilsonville Community Sharing and others.

Donations of clothing, merchandise and appliances are welcome, except for mattresses, car seats and ripped or broken furniture and maybe dropped-off at

Grace Chapel, 9600 SW Boeckman Road on Monday, March 7 or Tuesday, March 8.




For more information, visit [www.WilsonvilleRummageSale.com](http://www.WilsonvilleRummageSale.com) email [wilsonvillecrs@gmail.com](mailto:wilsonvillecrs@gmail.com).

## City Calendar

| MARCH    |   |
|----------|---|
| 1 Tue    | • Tourism Promotion Committee Meeting, 10 am - 12 pm, City Hall Council Chambers<br>• Municipal/Traffic Court 5 pm, City Hall         |
| 7 Mon    | • City Council, 5 pm Work Session 7 pm 2016 State of the City Address and Meeting, City Hall  |
| 9 Wed    | • Wilsonville Community Seniors Inc. Advisory Board, 1 - 3 pm, Wilsonville Community Center<br>• Planning Commission, 6 pm City Hall  |
| 10 Thurs | • Parks and Recreation Advisory Board Meeting, 4:30 pm, City Hall   |
| 14 Mon   | • DRB Panel A 6:30 pm, City Hall  |
| 15 Tue   | • Municipal/Traffic Court 5 pm, City Hall   |
| 17 Thurs | • Wilsonville Leadership Academy, 6 - 9 pm, City Hall   |
| 21 Mon   | • City Council 5 pm - Work Session, 7 pm - Meeting, City Hall   |
| 23 Wed   | • Library Board, 6:30 pm Wilsonville Public Library   |
| 26 Sat   | • Wilsonville Egg Hunt, 10 am, Wilsonville Memorial Park  |
| 28 Mon   | • DRB Panel B 6:30 pm, City Hall  |
| APRIL    |   |
| 2 Sat    | • Volunteer Tree Planting Event 9 am - 1 pm, Tranquil Park  |
| 4 Mon    | • City Council 5 pm Work Session 7 pm Meeting, City Hall  |
| 5 Tue    | • Municipal/Traffic Court 5:30 pm, City Hall  |
| 11 Mon   | • DRB Panel A 6:30 pm, City Hall  |
| 13 Wed   | • Wilsonville Community Seniors Inc. Advisory Board, 1 - 3 pm, Wilsonville Community Center<br>• Planning Commission, 6 pm, City Hall |
| 14 Thurs | • Parks and Recreation Advisory Board Meeting, 4:30 pm, City Hall   |
| 16 Sat   | • Spa Day, 10 am - 1 pm, Wilsonville Community Center   |
| 18 Mon   | • City Council 5 pm Work Session 7 pm Meeting, City Hall  |
| 19 Tue   | • Municipal/Traffic Court 5:30 pm, City Hall,   |
| 21 Thurs | • Wilsonville Leadership Academy 6 - 9 pm, City Hall  |
| 25 Mon   | • DRB Panel B 6:30 pm, City Hall  |
| 27 Wed   | • Library Board 6:30 pm, Wilsonville Public Library   |


All dates and times are tentative; check the City's online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).



### Wilsonville Egg Hunt

Saturday, March 26  
Memorial Park, 10 am  
Ages 1 - 11  
No Charge!

Thanks to our Sponsors:


## Jr. Scoop Hosts \$1 Clothing Sale and Seeks Clothing Donations

On Saturday, April 23, 8 am - 2 pm, Jr. Scoop, a business run by Wood Middle School students, is hosting their fourth annual \$1 clothing and ice cream sale in the school's gym located at 11055 SW Wilsonville Road. Proceeds from the

**Jr Scoop \$1 Clothing Sale**  
 • Sat, April 23, 8 am - 2 pm  
 Wood Middle School Gym  
 11055 SW Wilsonville Road

annual fundraising event benefit Doernbechers Children's Hospital as well as provide scholar-

ships to send Wood students to Outdoor School.

At the sale every item of clothing is sold for only \$1 and features clothes and styles for all ages and sizes. In addition, the club is also selling their premium ice cream for \$3 a scoop on homemade waffle cones.

Community members are also invited to donate clothes prior to the sale. Clothing donation bins are located at various locations throughout Wilsonville including City Hall and the Library and clothing dona-

tions are also being accepted at Wood Middle School until April 22.



Jr. Scoop clothing donation bins, like this one at City Hall, are placed around town.

Jr. Scoop is both a community service club and a premium ice cream company made up of over 45 middle school students who learn business skills, discuss and organize upcoming events and volunteer their time. The club is dedicated to helping members learn how to be successful entrepreneurs while also helping their community.

Each month Jr. Scoop club members make and sell two flavors of high-quality, artisan ice cream at [www.JrScoop.com](http://www.JrScoop.com) and at other events in Wilsonville.

## Tree and Shrub Care Volunteers Needed on Saturday, April 2

On Saturday, April 2, 9 am - 1 pm, join Friends of Trees and the City of Wilsonville in planting native trees and shrubs and removing invasive English ivy at Tranquil Park, 10580 SW Brown Road in

**Volunteer Tree Care Event**  
 • Sat, April 2, 9 am - 1 pm  
 Tranquil Park, 10580 SW Brown Road

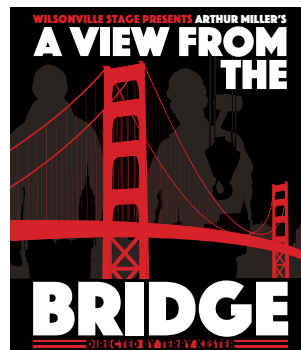
Wilsonville. Please arrive around 8:45 to be registered and assigned to a crew. Breakfast snacks and hot chocolate/coffee, as well as gloves, tools

and guidance are provided. Dress for field conditions. All ages welcome.

RSVPs are requested for groups of five or more people. Register online at [FriendsOfTrees.org/volunteering/group-registration](http://FriendsOfTrees.org/volunteering/group-registration). Contact Jenny or Randi 503-595-0213 at Friends of Trees if you have any questions or need more information.

## Wilsonville Stage Performs A View From the Bridge

Wilsonville Stage, also known as Wilsonville Theater Company, continues the 2015-16 season with a spellbinding American drama by one of America's most famous playwrights, Arthur Miller. Play director Terry Kester explains, "There is no more powerful, painful, dramatic play, than *A View From the Bridge*. It is an emotionally raw drama about a low-middle-class immigrant family who lives in fear of deportation." In March the play runs Thursday - Saturday, March 3 - 5, 7:30 pm, at the Frog Pond Grange, 27350 SW Stafford Road. For more information visit [www.WilsonvilleTheater.com](http://www.WilsonvilleTheater.com).



## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on cable TV channels 30 (Xfinity/Comcast) and 32 (Frontier) and replayed periodically. Meetings are also available to stream live online on Web TV and replays in the video vault by clicking on [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.



**Tim Knapp**  
Mayor  
[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



**Scott Starr**  
City Council President  
[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



**Julie Fitzgerald**  
City Councilor  
[fitzgerald@ci.wilsonville.or.us](mailto:fitzgerald@ci.wilsonville.or.us)



**Susie Stevens**  
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**Charlotte Lehan**  
City Councilor  
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### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)



### Wilsonville City Hall

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)

# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

May 2016



Members of the State Champion 2015-16 Wilsonville High School Boys Varsity Basketball team and Wilsonville City Council; front row (l-to-r) Councilors Lehan, Fitzgerald, Stevens, Starr and Mayor Knapp with Coach Roche holding proclamation

## Council Honors State Champion High School Boys Basketball Team: Proclaims Wilsonville Wildcats Week

On Monday, April 4, the Wilsonville City Council honored the Wilsonville High School Varsity Boys Basketball Team for winning the 5A High School Boys Basketball State Championship in March. The Mayor and Council acknowledged the team's success by declaring the week of April 3-9 as "Wilsonville Wildcats Week."

In issuing the proclamation Mayor Tim Knapp declared, "Your work, diligence and success is an exceptional accomplishment that will be remembered by the community for a very long time."

The City Council also commended the team for their academic accomplishments for placing second in the 5A Boys Basketball Academic All-State Awards with an average GPA of 3.64.

In addition to recognizing the team for winning the state championship, the Council also recognized the team for winning the Northwest Oregon Conference with a conference record of 15-1, and an overall season record of 25-3.

At the meeting Council President Scott Starr commended the team by stating, "Your ability to consis-

tently deliver each game night throughout the season is clear evidence of your team's preparation, perseverance and high level of skill. The City Council appreciates how your team showed courage, commitment, determination, and competitive drive throughout the season. Your success has brought positive exposure to our city, created a high-level of community pride, and served as an inspiration to all of Wilsonville."

### 2015-16 Wilsonville High School Boys Varsity Basketball Team

**Players:** Sam Bullock, AJ Burkhead, Draco Bynum, Tyler Hieb, Dylan Jordan, Caleb Larsen, Ryan Napoli, Nathan Overholt, Zach Reichle, Chris Riewald, Jack Roche, Zac Rossetti, Harrison Steiger and Zac Woodworth

**Coach:** Chris Roche

**Assistant Coaches:** John Roche, Michael MacKelvie, Ryan Schwarzer and Tom Haaga

**Manager:** Kylie Gelfand

**Athletic Director:** Dennis Burke

## City Awards Grants to Support Community Programs

On April 14, 2016 the City of Wilsonville's Parks and Recreation Advisory Board awarded a total of \$15,000 in grants to support community programs with funding from the City's Opportunity Grant Program. In all, the following three local organizations were awarded \$15,000 in grants:

- Jr. Scoop (Wood Middle School): \$5,000
- Relay for Life of Wilsonville: \$5,000
- Wilsonville Farmers Market: \$5,000

The Opportunity Grant program is funded by the City's general fund to assist local individuals and organizations that further educational or artistic opportunities; encourage and foster diversity; foster advances in education, art or

community leadership; or involve youth or elderly population in community activities. A total of \$25,000 is made available each year, with two deadlines in October and March for \$10,000 each and \$5,000 as discretionary funds for smaller, time-sensitive applications.



For information about these grant awards and future funding opportunities, contact Brian Stevenson, Recreation Coordinator, at 503-570-1523; [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

## Wilsonville Community Members Celebrate Arbor Day



In early April over 50 local volunteers joined City and Friends of Trees staff to celebrate Arbor Day by planting over 360 native trees and shrubs and also pulling ivy at Tranquil Park in Wilsonville.

### Mayor's Message

## Showing Appreciation to Our Volunteers

I am always impressed by our residents and employees of local businesses who donate so generously of their time and resources to improve our community — and so we proclaimed April as "Volunteer Appreciation Month." However, volunteering in Wilsonville is a year-round endeavor that makes our community a better place.



There are many examples of how volunteers make Wilsonville a better community. For example, recently over 50 residents pitched in for an Arbor Day event and planted more than 360 trees and shrubs in Tranquil Park, bolstering our City's Tree City USA designation.

Every month more than two dozen residents are actively participating in the City's Leadership Academy. This six-month-long program provides participants a foundation in local-government civics and is preparing community members to serve in positions of leadership on volunteer City boards and commissions or other local organizations.

Every year volunteers of all ages donate their time to perform a wide variety of tasks that help improve the quality and expand the range of services available from our Library. Volunteers help to serve meals at our Community Center, to deliver meals to homebound seniors, help prepare income taxes and assist seniors getting to medical appointments.

And WERK Day, set for May 14, attracts hundreds of community members every year for the annual

**WERK Day**  
• Sat, May 14, 8 am-1 pm  
Wilsonville Community Center  
7965 SW Wilsonville Road

park clean-up event to spread bark, rake-up debris and remove invasive plants.

What a great way to show you care about our parks.

During the first weekend June, the 17th annual Wilsonville Festival of Arts is being held thanks to substantial contributions from a team of hard-working volunteers and art enthusiasts. And really, this is true to a considerable extent with all of our major community events — volunteers are the underpinning of success.

Throughout our community, day in and day out, volunteers contribute in countless ways to help Wilsonville be the place we want it to be, and carry forward a vision of a positive future. Please join me in showing our appreciation for all of our volunteers who so freely give their time to strengthen and improve our community!

*Tim Knapp, Mayor*

City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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**Our History: Wilsonville Athletic Club Wins 1914 State Championship**



The Wilsonville Athletic Club, won the 1914 state baseball championship. The team included the three Baker Brothers — Chick, Roy and Tom — whose older brother Del (not shown) played and coached for the Detroit Tigers.

**Community College Local Artist Exhibit Runs Through June 10**

During spring term, the Clackamas Community College Wilsonville campus features the work of local artist Kelsey Hinton.

Hinton paints in celebration of the vibrancy of the everyday. Originally from the Alpine Desert of Colorado, she traded her crisp blue skies for soft light and rich, cool greens. She is most interested in exploring the radiance of a scene, where shapes, forms, lines and light are more intriguing than the subject itself. Hinton paints everyday environments that are familiar to all.

Her mother (also a local artist) is an Oregon native, born in McMinnville, with a deep family history in the area. Hinton learned



to paint as soon as she could hold a paintbrush, and she said her interest in art began before she can remember.

Hinton mostly uses oil paints, however she also dabbles in a variety of other media, including fabric art and paper forms.

Hinton's work is on display in the Commons Area Gallery of the CCC Wilsonville campus, 29353 SW Town Center Loop East, through June 10. The show is open to the public.

For more information, contact Joyce Gabriel, CCC Student Services & Facility Coordinator, at 503-594-0950; joyceg@clackamas.edu.

**School District and CREST Offer Summer Camps**

**CREST Camps and Programs**

**EnviroCamp-for students entering grades 1-5 in the fall**

- Session 1: Aug. 1-5, Mary S. Young Park, West Linn
- Session 2: Aug. 8-12, CREST, Wilsonville

**Garden Camp-for students entering grades 1-5 in the fall**

- Session 1: July 11-15, CREST, Wilsonville
- Session 2: July 25-29, Cedaroak Park Primary School, West Linn

**Middle School Adventure Camp-for students entering grades 6-8 in the fall**

- Session 1: July 18-22, location varies
- Session 2: Aug. 22-26, location varies

**Teen Leadership Training-for students entering grades 8-10 in the fall**

- June 29-30, CREST, Wilsonville

**Farm Internship-for students entering grades 8-10 in the fall**

- Session 1: June 20-July 8, CREST Farm, Wilsonville
- Session 2: July 11-28, CREST Farm, Wilsonville
- Session 3: Aug. 1-18, CREST Farm, Wilsonville

Information and registration for CREST Summer Camps and Programs are available at [www2.crest.wlww.k12.or.us/register/](http://www2.crest.wlww.k12.or.us/register/).



**Children's Art Institute-for students entering grades 1-8 in the fall**

- July 11-15, Rosemont Middle School, West Linn
- Information and registration for Children's Art Institute is available at [www.wlww.k12.or.us/domain/116](http://www.wlww.k12.or.us/domain/116). For more information contact Vickie Lee at 503-673-7010.



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

**May is National Bike Month**

As we gear up for summer, now is the perfect time to discover (or re-discover!) bicycling. Not only can bicycling make you healthier, it can also save you time and money.

Need some inspiration? Youth librarian Terri Wortman is bicycling across the country. Follow her model and explore your own community this month. Ride your bike to the library and come check out some wheely good books!

[www.WilsonvilleLibrary.org/biking](http://www.WilsonvilleLibrary.org/biking)

**Theodore Roosevelt Talk on Thursday, May 5**

History comes to life with this multi-media presentation! Born to wealth and privilege, Theodore Roosevelt

became an advocate of the working man and populist reformer, taking on big oil, banks and railroads.

A combat leader and proponent of military strength, he also won the Nobel Peace Prize for ending a major conflict in the Far East.

The free presentation is being held on Thursday, May 5, 1-2:30 pm, in the Oak Room.



**Spring Cleaning Your Debt Workshop on Monday, May 9**

Spring is a great time to clean your house, but have you considered cleaning up your finances? Local retired CPA Alan Steiger will help you gain financial peace of mind with tips for reducing and eliminating your debt.

The free workshop takes place on Monday, May 9, 6-7 pm, in the Rose Room.



**Teen "Dis-Night" Friday, May 20**



Nah, we're not dissing anybody! This event is all things Disney. (Which now includes Marvel, the Muppets, and Star Wars-Jiminy Cricket!) We'll try our hand at

Disney origami, and maybe even sing some Disney-oke!

The free teen event takes place on Friday, May 20, 6:30-8:30 pm.

[www.WilsonvilleLibrary.org/teens](http://www.WilsonvilleLibrary.org/teens)

**Summer Reading Program Coming in June**

Get ready for a summer of olympic proportions with "On Your Mark, Get Set, READ!" starting Monday, June 6.

Fun for ages 0-100!

[www.WilsonvilleLibrary.org/srp](http://www.WilsonvilleLibrary.org/srp)



**Upcoming Classes and Events**

**Local Artist of the Month: Victory Academy student artwork**

\*No First Friday Film\*

**Book Club**

*Looking for Me* by Beth Harman

A woman puts the pieces of her family's secrets together in a novel of deep dramatic impact with Southern charm.

Thursday, May 12, 6-8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

**Genealogy Club**

Monday, May 23, 1-2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

**Great Books Discussion Group**

*What Is War?* by von Clausewitz

One of the most important treatises on political-military analysis and strategy ever written, and remains both controversial and an influence on strategic thinking.

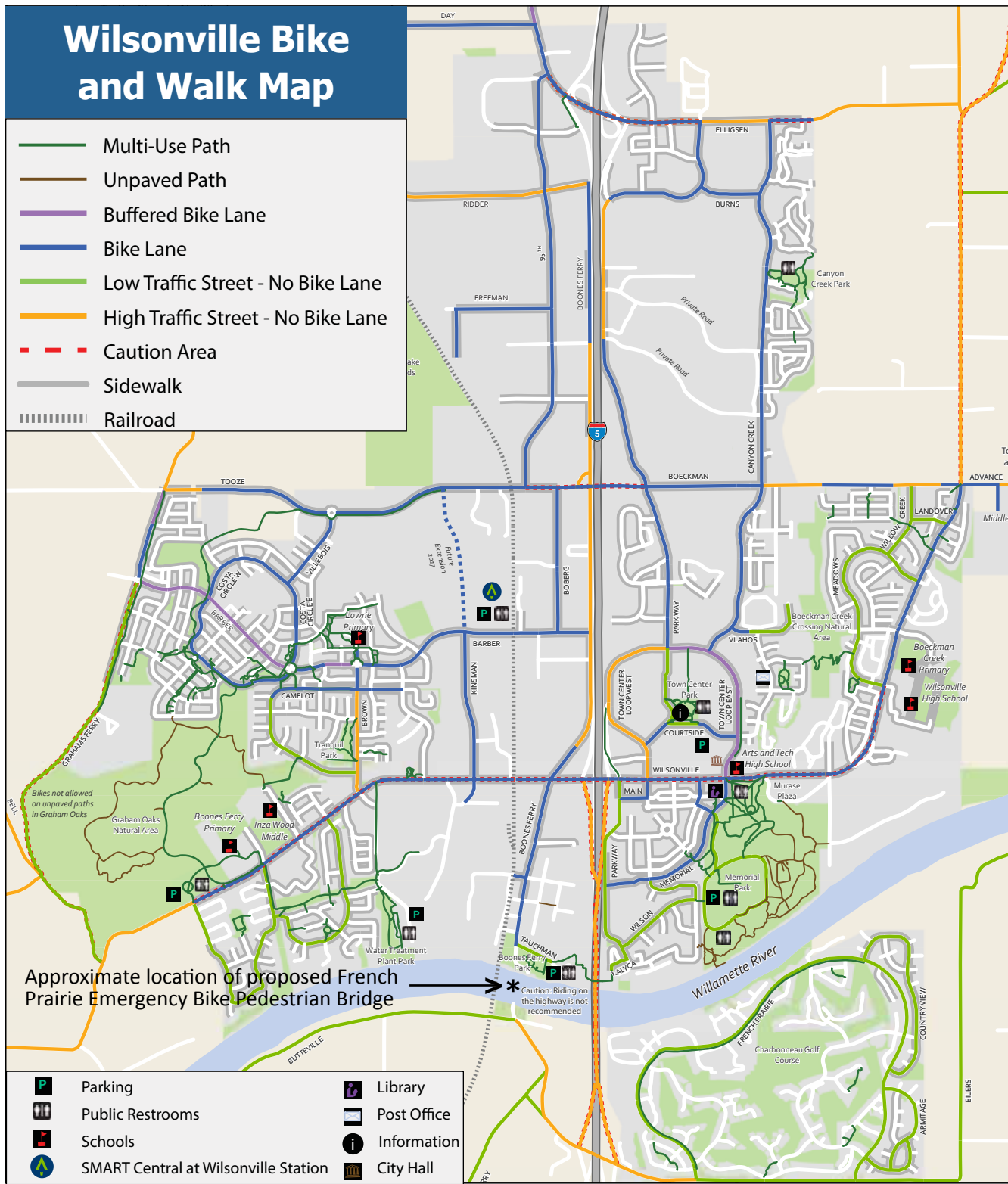
Tuesday, May 17, 6-8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

**History Pub at Wilsonville McMenamins**

Tuesday, May 31, 6:30-8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

**Game Night**

Wednesday, May 25, 6-8 pm  
[www.WilsonvilleLibrary.org/games](http://www.WilsonvilleLibrary.org/games)



### CHIEF'S CORNER- BIKE SAFETY

With the warmer weather on the way and May having been declared as Bike Month, this is the perfect time to share some bike-riding safety tips to ensure you and your family have safe and enjoyable bike rides throughout the summer. For your bike-riding safety and enjoyment remember to:



- Follow the **rules of the road**
- **Ride with traffic** and obey the same laws as motorists
- Use the **furthestmost right lane** that heads in the direction that you are traveling
- **Obey all traffic control devices**, such as stop signs, lights and lane markings
- Always look back and use **hand and arm signals** to indicate your intention to stop, turn or merge
- Be visible, **predictable**, alert and anticipate conflicts
- Ride where **drivers can see you**
- Wear **brightly colored clothing** at all times
- At night, use a **white front light and red rear light or reflector**
- Wear **reflective tape or clothing**
- Ride in a **straight line** and don't swerve between parked cars
- Make **eye contact** with motorists so they see you
- **Do not ride on the sidewalk**
- Be **aware of traffic** around you and be prepared to take evasive action
- Learn **braking and turning techniques** to avoid crashes
- Be **extra alert at intersections**
- Make sure that the **helmet fits** on top of your head, not tipped back or forward
- If you crash or have any impact that affects your helmet, visible or not, **replace it**
- Riders younger than 16 must wear a helmet and everyone should **wear a helmet** regardless of their age

Jeff Smith, Chief of Police

## SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
smart@ridesmart.com  
www.RideSMART.com

### WalkSMART on Wednesday, May 25

Join Walk SMART at noon on Wednesday, May 25. Walk SMART is meeting in May at Coherent work site at 27650 SW 95th Ave—rain or shine. We are meeting out front of the main entrance, and begin our walk at 12:15 pm. The walk leads us through Villebois and over the new Barber Street bridge. Plan on an

hour for the walk, but quick-paced walkers may be done in less time. All who take part walk away with a small gift just for participating. Meet new friends and do some lunchtime networking. Remember to bring your walking shoes to work with you!



### Market Shuttle Returns May 19



New this year, the Wilsonville Farmers Market shuttle will now serve the Charbonneau area. The Charbonneau Country Club area will serve as a park-and-ride location where you can hop aboard the Market Shuttle in front of the main club house each Thursday afternoon and evening.

#### To Farmers Market

| Charb Country Club | Town Center Park | City Hall | Farmers' Market |
|--------------------|------------------|-----------|-----------------|
| -----              | 3:40 pm          | 3:43 pm   | 4:00 pm         |
| 4:35 pm            | 4:45 pm          | 4:48 pm   | 5:00 pm         |
| 5:35 pm            | 5:45 pm          | 5:48 pm   | 6:00 pm         |
| 6:35 pm            | 6:45 pm          | 6:48 pm   | 7:00 pm         |

#### From Farmers Market

| Farmers' Market | Town Center Park | City Hall | Charb Country Club |
|-----------------|------------------|-----------|--------------------|
| 5:02 pm         | 5:12 pm          | 5:14 pm   | 5:24 pm            |
| 6:02 pm         | 6:12 pm          | 7:14 pm   | 4:24 pm            |
| 7:00 pm         | 7:12 pm          | 7:14 pm   | 7:24 pm            |
| 8:00 pm         | 8:10 pm          | 8:12 pm   | 8:22 pm            |

### Join the 2016 Bike More Challenge

May is National Bike Month, sponsored by the League of American Bicyclists and celebrated in communities from coast to coast. Established in 1956, National Bike Month is a chance to showcase the many benefits of bicycling — and encourage more folks to giving biking a try. Bike to Work Week is May 16-20, with Bike to Work Day on Friday, May 20.

The Bike More Challenge is a friendly, month-long competition to see who can #bikemore during the month of May! Join the Challenge today:

1. Register your workplace team or as an individual at [BikeMoreChallenge.com](http://BikeMoreChallenge.com).

2. **Every ride counts** this year! Ride your bike to work, the grocery store or for fun and log all of your trips.
3. Challenge your coworkers, friends, family and yourself to **#bikemore in May**.
4. **Connect with your community** as you ride! Give each other high fives, share your photos, and meet your #bikemore goals with new features of the website.
5. **Log your trips** to earn badges and unlock prizes and discounts from our sponsors including Portland Design Works and Chinook Book!

Join the Bike More Challenge today, #bikemore this May, and pedal you and your team to glory!



## Community Enhancement Committee Reviews Projects

The City's new Wilsonville-Metro Community Enhancement Committee seeks to advance a potential range of projects that improve the appearance, environment, natural qualities, recycling options, recreational opportunities or safety of the community. Projects nominated by public agencies and community members in early 2016 were then reviewed for compliance with the Community Enhancement Program guidelines and other considerations, and then forwarded to the committee.

As this issue of *The Boones Ferry Messenger* went to press, the committee was scheduled to meet in late April to review project nominations and select potential projects to advance over the next year. Funds for the program originate from a \$1/ton fee on biodegradable waste that is transferred in Portland-area communities that hosts a "garbage" facility; Metro extended the program region-wide effective 2015 to include Republic Services' waste-transfer and recycling reclamation facility in Wilsonville.

Created by City Council resolution and an agreement with Metro regional government in 2015, the committee is composed of four residents, two City Council members and the Metro Councilor for the Wilsonville district. For more information, see [www.ci.wilsonville/CommunityEnhancement](http://www.ci.wilsonville/CommunityEnhancement).

## City Receives 19th Consecutive Annual Financial Reporting Award

For the 19th consecutive year since 1997, the Government Finance Officers Association of the United States and Canada has awarded the City of Wilsonville with the Certificate of Achievement for Excellence in Financial Reporting for the City's Comprehensive Annual Financial Report (CAFR). Attainment of the Certificate of Achievement, the highest form of recognition in the area of governmental accounting and financial reporting, represents a significant accomplishment by the City. The City's CAFR is available online at [www.ci.Wilsonville.or.us/CAFR](http://www.ci.Wilsonville.or.us/CAFR).



29600 SW Park Place  
 Administrative Office: 503-783-7529  
 Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

## Evening Yoga is Back!

Instructor Tracy Cohen has rejoined the Parks and Recreation team and will once again be offering her evening yoga class.

After discussion with Tracy and yoga class participants, the decision was made to offer this popular

program two days per week. The \$88 class fee covers attendance at both

Tuesday and Thursday evening classes and works out to \$7.33 per class session. The next 12-session class begins on May 17 and runs through June 23 with a second session beginning on June 28.

**Tuesday and Thursday Yoga Class**  
 • May 17-June 23, 5:30-6:30 pm,  
 Wilsonville Community Center

## Help Prevent Kitchen Fires Before They Start

In April Tualatin Valley Fire & Rescue firefighters responded to an accidental, unattended cooking fire at the Village at Main Street Apartments in Wilsonville.

Fortunately, the apartment complex had a commercial fire sprinkler system that activated and kept the fire under control in the kitchen, allowing Tuala-



This kitchen fire was caused by unattended cooking at a home in Wilsonville this April.

tin Valley Fire & Rescue (TVF&R) crews to quickly extinguish the fire.

A TVF&R investigator determined the fire originated from a pan of coconut oil left unattended on an electric stovetop. The fire extended to the microwave and cabinetry above the stove before activating two fire sprinkler heads.

The fire sprinkler system limited damage to the kitchen and kept the fire from spreading to other rooms and apartments in the building. When a fire sprinkler system is present, the likelihood of injuries and costly damage is dramatically reduced.

Cooking fires are the leading cause of residential fires. To reduce your risk, we recommend the follow-

ing safe cooking tips:

- Keep a **close eye** on what you're cooking.
- **Never leave food unattended** on the stovetop — turn off burners or set a timer if you must leave the room for any reason.
- **Heat cooking oil slowly** and never leave it unattended. Turn down the heat if it begins to smoke.
- **Keep combustibles away** from the stove, and keep your cooking area clean, including the exhaust fan, oven and stovetop.
- **Keep a large lid close at hand** in case of a pan fire.
- **Unplug small appliances** when not in use.
- If you experience a cooking fire, and the fire is **large** or appears to be growing, **do not attempt to extinguish it**. Leave the house quickly and call 911 from a cellphone or neighbor's house.
- If the fire is still in the pan or oven and has not spread to nearby counters or cabinets, you may be able to **extinguish it yourself**.

### To extinguish a pan or oven fire:

- **Put a lid on it.** The easiest way to extinguish a small pan fire is with a pan lid. Turn off the burner and carefully slide a pan lid over the pan from the side. The lid will smother the fire. Do not move the pan until the fire is completely extinguished and the pan is cool. If you do not have a pan lid, you may also use a baking sheet.
  - Use a **fire extinguisher** only if the fire is small and has not spread outside the pan. The discharge of a portable fire extinguisher lasts between eight and 10 seconds.
  - **Shut the door on oven or microwave fires** and turn off the heat.
  - **Never transfer a burning pan** from the stovetop to the sink or out the back door. The fire may grow in size and burn you or spread to countertops.
  - Also, **never pour water on a grease fire**. The flames will "splash," causing the fire to spread and likely result in burns to your body.
- For more information about fire sprinkler systems and additional safety tips, visit [tvfr.com](http://tvfr.com).

## Partnership with Lee's Martial Arts Results in New Programs

Wilsonville Parks and Recreation and Lee's Martial Arts have partnered to offer new programs to the community. The partnership will result in special Parks and Recreation pricing for Beginner Tae Kwon Do programs and the introduction of a new Personal Safety and Self-Defense Workshop for women.

"We are really excited about the expanded opportunities the partnership provides," said Brian Stevenson, the City's Recreation Coordinator.

Beginning Tae Kwon Do is available for ages 4-adult with class times dependent on age. The month long sessions meet on Tuesdays and Thursdays for a cost of \$79. The Self-Defense Workshop is open to females ages 13 and older, and is being held on June 11, 2-5pm with a cost of \$89.

## Mother's Day Brunch

On Monday, May 9, the Community Center will be hosting a special Mother's Day brunch to celebrate (and commemorate) all moms. Lunch will be served at noon, and Moms and Grandmothers will receive a special treat.



## W.E.R.K. DAY

SAT, MAY 14  
 8 AM-1 PM

MEET AT COMMUNITY CENTER



WERK Day is an annual event to assist with a variety of park projects.

Each year the projects differ but are typically focused within Memorial Park.

[WilsonvilleParksandRec.com/WERKDay](http://WilsonvilleParksandRec.com/WERKDay)

## Camps, Camps, Camps!

Wilsonville Parks & Recreation offers a variety of summer camps for children of all ages. In June, the program offerings get started with a number of camp options. Full list of camp offerings and registration available at: [WilsonvilleParksandRec.com/SummerCamps](http://WilsonvilleParksandRec.com/SummerCamps)

### Coach Nics Hoop Camp

Grades 1-3  
 June 13-16  
 June 27-30  
 9 am-noon  
 Memorial Park  
 \$69



### Babysitters Bootcamp

Ages 11-15  
 June 15 and June 16  
 9 am-5 pm  
 Tauchman House  
 \$210

### Intro to Survival Camp

Ages 6-9  
 June 20-24  
 9 am-1 pm  
 Mary S Young Park  
 \$220

### Theater Workshop Scene Study

Ages 13-19  
 June 20-24  
 Noon -5 pm  
 Tauchman House  
 \$50

### Wild West Art Camp

Ages 7-13  
 June 27-30  
 9-11:30 am  
 Tauchman House  
 \$150  
 (+\$25 material fee)

## Committee Takes Tourism Development Strategy to City Council

The City's new Tourism Promotion Committee has crafted the first "Five-Year Action Plan and Annual One-Year Implementation Plan for the Wilsonville Tourism Development Strategy" for City Council adoption. The plan recommends three primary sets of actions and studies to be executed starting this year that implement the longer-range, five-year components of the larger "Visit Wilsonville" tourism strategy.

**1. Organizational Framework / Staffing Resource:** The committee seeks a dedicated human resource as staff or contractor for tourism development and promotion in order to advance the Strategy and plan. The committee also intends to assume responsibility for overseeing the tourism grant programs in Fall 2016.

**2. Online / Marketing:** The Committee seeks staff or contractor focus on online/Internet website products and processes as a primary task that the strategy

*Creating an authoritative website for Wilsonville tourism is a top recommendation.*

website for Wilsonville tourism is a top recommendation and can also complement City efforts to advance

identified as a top priority and the committee strongly supports. Creating an authoritative

website for Wilsonville tourism is a top recommendation and can also complement City efforts to advance

business recruitment and to welcome new residents.

### 3. Study Efforts for City to Advance Tourism

**Development:** The committee recommends four study efforts to advance tourism:

- Visitor profile study
- Feasibility study for an all-weather/indoor, multi-purpose playing fields facility for sports tournaments and other recreational/entertainment purposes
- Destination marketing strategy plan
- Hotel/conference center study as part of Town Center Master Redevelopment Plan.

The longer-range five-year plan shows an increasing workload and level of activity, progressing towards the launch of the recommended nonprofit Destination Marketing Organization (DMO) to be named "Visit Wilsonville."

Created by Council resolution in June and with volunteers appointed in October 2015, the 12-member Tourism Promotion Committee is composed of seven voting members drawn from the area hospitality and tourism industry and five (5) ex-officio members who provide expertise, advice and assistance to the committee.

For more information, see [www.ci.wilsonville.or.us/tourism](http://www.ci.wilsonville.or.us/tourism).

## Oregon State University Ag Center Offers Research-Farm Tours

The OSU North Willamette Research & Extension Center (NWREC), located just south of the Charbonneau District in Wilsonville, offers free public tours and other events this summer.

From 2 to 4 pm every last Friday of the month, from May through October, NWREC staff and students

provide two-hour afternoon farm tours for anyone interested

**Free Tours of the North Willamette Research & Extension Center**  
• Last Friday of the month, May-Oct, 2-4 pm, 15210 NE Miley Rd

in seeing the latest agricultural research and education activities. Bring friends, family or neighbors. Larger groups (up to 24) can be accommodated, but are ad-

vised to call ahead to schedule a convenient time.

For more information or to RSVP, call 503-678-1264 or go online to [www.oregonstate.edu/dept/NWREC/](http://www.oregonstate.edu/dept/NWREC/).



NWREC is offering free tours of OSU's experimental farm on May 27, June 24, July 29, Aug. 26, Sept. 30 and Oct. 28

## City Invites Non-Profits to Participate in Free Volunteer Fair

The City of Wilsonville is hosting a Non-Profit/Volunteer Fair on Thursday, June 16, 7-8 pm at City Hall, 29799 SW Town Center Loop East. Local non-profit organizations, located in Wilsonville or Clackamas County and in need of volunteers, are invited to have a free information table at the event. The goal of the fair is to connect community members with volunteer opportunities.

For more information, contact Angela Handran, Community Outreach Specialist, at 503-570-1503; [Handran@ci.wilsonville.or.us](mailto:Handran@ci.wilsonville.or.us).

## On Tues, May 17, the following ballot measures are being voted on:

- **Measure 3-476** Clackamas County Emergency Radio Bond; Title: General Obligation Bonds to Replace Obsolete Emergency Communications System
  - **Measure 3-478** Clackamas County Road Maintenance Funding Advisory Vote. Title: Advisory Vote on Voter Approved County Road Maintenance
  - **Washington County Measure No. 34-243** General Obligation Bonds for County Emergency; Title: Communication and Response Facilities.
- Ballots are due by 8:00 pm on May 17, and may be dropped off at the Wilsonville Public Library.

## Wilsonville Festival of Arts is Set for the First Weekend in June

The Wilsonville Arts & Culture Council presents the 17th annual Wilsonville Festival of Arts. This free community event is held on Saturday, June 4, and Sunday, June 5, at Town Center Park. The festival begins at 10 am both days and lasts until 6 pm on Saturday and 5 pm on Sunday.

The festival includes original art from over 50 professional artists and 200 West Linn-Wilsonville

School District students. The Authors Pavilion features over 10 local authors, who represent a wide variety

**Wilsonville Festival of Arts**  
• June 4 & 5, opens 10 am  
Town Center Park, 29600  
Park Place, Wilsonville

of genres, discussing writing and selling their books. Listen to live music from the Minidoka Swing Band, Dina y los Rumberos, Michele Van Kleef, Sweet Adeline quartet, B. Joy Blues Jazz, and Mary Flower and the BBQ Boys while enjoying delicious food from Dogzilla, Dos Gringos Grill, Lupita's, The Local Grind, Steal Your Plate, Slow Squeeze Juice and premium ice cream from Jr. Scoop.

On Saturday, South Metro Area Regional Transit (SMART) sponsors historic trolley tours of art installations around Wilsonville.

On Saturday and Sunday, the Drummer's Corner, features a drum circle, 3 pm-5pm, and on Saturday,

10 am-1 pm, there is a drum-making workshop which requires pre-registration.

The Wilsonville Festival of Arts is supported by grants from the City of Wilsonville Community Tourism Grant Program, as well as donations from Fred Meyer, Pamplin Media Group, Argyle Square, The Laue Team, Frontier, Findlay of Wilsonville, NW Natural, Republic Services and Walmart.



The Wilsonville Festival of Arts offers something for everyone of all ages including face painting and more.

Visit [www.WilsonvilleArts.org](http://www.WilsonvilleArts.org) for more detailed information about this exciting two-day event.

## COUNCILOR'S CORNER: CHARLOTTE LEHAN

May is cemetery season as we get ready to honor the memory of our veterans on Memorial Day. But it is also when many people visit cemeteries to honor friends and relatives whether they served in the military or not. By May 30th the profusion of flowers and flags turns many historic cemeteries into an inspirational display of color.



### Pleasant View Cemetery

I serve on the Oregon Commission on Historic Cemeteries and also as president of the Pleasant View Cemetery Association. There are three cemeteries near Wilsonville where you can find many of the names that are part of our history. All three are worth visiting even if you have no relatives residing there. The largest is Pleasant View Cemetery, located 2 miles west of Villebois. Here you will find Inza R. Wood, Charles Wilson, John Wallace Graham, and Phil Balsiger, as well as many local road names: Westfall, Baker, Seely, Tooze, Parrott, Vlahos, Mulloy, Young, Morgan, Bell, Barber and others.

### Meridian Cemetery

Meridian Cemetery, just north of Elligsen on SW 65th Avenue, is owned and managed by volunteers from the Frog Pond United Church of Christ. Names you can find at Meridian include Boeckman, Boberg, Ridder, Frobase, Clutter, and Koellermeier.

### Robert Bird Cemetery

Robert Bird Cemetery is on SW Newland Road near the intersection with SW Gage Road. Like Pleasant View this cemetery has a dedicated group of volunteers who are responsible for its care. Names to recognize here include Elligsen, Borland, Delker, Kruse, Gage, Athey, and others from the Stafford area.

Like most historic cemeteries in Oregon, these three cemeteries are maintained by volunteers and donations without government support. May is the perfect time to volunteer to help before Memorial Day. Even if most of your ancestors are buried far away, you can still honor them by caring for the monuments of someone else's ancestors where you live now.



**Pleasant View Cemetery** has a **volunteer work day May 14, 9 am-2 pm, 14250 SW Westfall Road.**

We have jobs that range from yard work to monument cleaning where your main tools are little sticks and toothbrushes for cleaning out engravings. We provide most of the tools and instruction. Call me or visit our website for more information.

**Meridian Cemetery** is holding two **work days** in May, one on **May 7** and one on **May 14**, both **10 am-2 pm.** Contact **Ron Boeckman** at **503-881-1288** for more information.

**Robert Bird Cemetery** is having a **community work day on May 21** starting at **9 am** and breaking for a **meeting and potluck lunch at 11:30.** Contact **Barbara Stinger** at **503-645-1596** for more information.

Before you attempt to care for a cemetery monument, you should check with the cemetery managers for instruction in monument care. Even people with the best of intentions, can easily damage these old artifacts with improper cleaning methods.

I look forward to chatting with anyone about our local cemeteries, and about the history and artwork that they hold for us. If you can't join us for a work day, be sure to stop by one or all of these local cemeteries over Memorial Day weekend. You can always leave a few flowers for somebody's grandmother who doesn't have any.

**Charlotte Lehan**, 503-313-8040  
[www.PleasantviewCemetery.org](http://www.PleasantviewCemetery.org)



## Reports from City Boards and Commissions

### Mayor Proclaims Wilsonville Wildcats Week and City Council Approves Financing for Urban Renewal Projects

At the April 4 City Council meeting Wilsonville Mayor Tim Knapp proclaimed the week of April 3-9 as “Wilsonville Wildcats Week” in honor of Wilsonville High School Boys Varsity Basketball team that won the 5A Boys Basketball State Championship in March (see article on page 1.)

The City Council approved a resolution authorizing the City to make a short-term “overnight” loan of an amount up to \$2 million to the City’s Urban Renewal Agency. The “overnight” loan is funded by the City General Fund and is to be paid back by tax increment collected within the Year 2000 Urban Renewal District. State urban renewal rules mandate that urban renewal funding may only be used to repay debt and is not allowed to directly fund urban renewal project expenses. The financing strategy makes available

urban renewal funds to pay for costs associated with the Boones Ferry-to-Kinsman Connector road project (aka the proposed “Old Town Escape” roadway) and the Town Center Redevelopment Plan that are both within the Year 2000 Urban Renewal District.

During the meeting, Mayor Knapp also issued three other proclamations. The first proclamation requested by the Parkinson’s Action Network declares April as Parkinson’s Awareness Month. The second proclamation honors Arbor Day and the City of Wilsonville’s 18-year-long designation by the National Arbor Day Foundation as a Tree City USA.

The third proclamation declares April 2016 as “Volunteer Appreciation Month” and honors the hundreds of community members in Wilsonville and surrounding

areas who volunteer not only for the City, but also for local schools and non-profit organizations.

During the Communications portion of the meeting the City Council heard about an upcoming Document Shredding Event that the City is co-sponsoring along with U.S. Bank and Wood Middle School.

The Council also heard from members of the Clackamas 800 Radio Group that is advocating for the passage of the May 17 Ballot Measure 3-476, Clackamas County Emergency Radio Bond, that would fund improvements to the emergency communications network used primarily by emergency services first-responders such as the police and fire-fighters.

Councilor Scott Starr presented information about the City’s proposal to hold a ballot measure vote in November that, if approved, will fund construction of

an 80,000-square-foot recreation and aquatic center. For more information about the proposed center visit [WilsonvilleParksandRec.com/WRAC](http://WilsonvilleParksandRec.com/WRAC).

Prior to the regular meeting at the work session, City Council heard reports on a cost-saving proposal to refund water bonds, an update regarding the Tourism Promotion Committee’s Annual Action/Implementation Plan and held a discussion on affordable housing programs and opportunities resulting for the recent adoption of new laws by the Oregon state legislature.

Community members can watch a replay of City Council meetings on Comcast/Xfinity Ch. 30, Frontier Ch. 32 or on the City’s video-on-demand service at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV).



Mayor Tim Knapp and City Planner Jennifer Scola holding Arbor Day Proclamation.

### Council Adopts Annexation and Zone Change for Villebois, Adjusts Budget and Supports Low-Income Housing Option

At the March 21 meeting of the Wilsonville City Council, the Council held a second reading and adopted a pair of related ordinances for an annexation and a Zone Map amendment of a one-acre parcel, located at 11700 SW Tooze Road. The Council actions rezoned the parcel from Rural Residential Farm Forest 5 to Village (V) in order to allow a residential use consistent with the adjacent parcels of land that already have the Village zoning designation. As the last piece of land that completes the master-planned Villebois neighborhood, the parcel is part of a larger previously-approved 63-lot subdivision by applicant/homebuilder Polygon Northwest known as Calais.

The City Council held a public hearing and approved a resolution authorizing a supplemental budget adjustment for Fiscal Year 2015-16, which moves expenditures from one category to another within a specific fund and does not increase the overall budget. The Council action provides the needed budget authority to advance a number of capital projects and other expenses.

During the Mayor’s Business portion of the meeting the City Council approved the appointment of Arthur Park to the Budget Committee to fill a vacancy of an unexpired term ending Dec. 31, 2017. Mayor Knapp indicated that Mr. Park has an impressive educational background, 15 years of experience as a credit officer and has lived in the Villebois neighborhood since 2010.

Under the Consent Agenda, the City Council adopted five resolutions that renew property-tax exemptions under state law totaling \$48,300 for five, nonprofit multifamily communities with 366 affordable rental-housing units serving low-income and senior residents in Wilsonville. These five multi-family housing properties offer low-income families, seniors and individuals below-market rents that are subsidized annually by approximately \$318,600 as the result of local, state and federal affordable housing subsidies.

Also on the Consent Agenda the City Council authorized the City Manager to sign an inter-governmental agreement between the City and Metro for a \$320,000 Metro grant to fund the Wilsonville Town Center Redevelopment Plan.

A proposed ordinance regarding amending the City Code related to franchise fees charged for use of the public right-of-way was continued to the City Council meeting on May 16.

During the work session prior to the regular meeting the City Council heard reports on the Bicycle Wayfinding System, and discussed potential future improvements and signage along Parkway Avenue Drive south of Town Center Loop.

### Frog Pond Master Plan Open House

Wednesday, May 11,  
5-6:30 pm, Wilsonville City Hall  
29799 SW Town Center Loop East

The City is holding an Open House for the Frog Pond Master Plan on Wednesday, May 11, 5-6:30 pm, at City Hall. The event is an opportunity to view drawings, talk with the project team and provide early feedback on work in progress. Draft materials include working ideas for zoning, residential design guidelines, street designs and parks and open space.

The Planning Commission is holding a work session immediately following the Open House and the public is welcome to attend. Materials for the open house are available on the project website at:

[www.ci.wilsonville.or.us/FrogPond](http://www.ci.wilsonville.or.us/FrogPond)



### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during May. Scheduling changes can and do occur; check for updated meeting information at the City’s website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, May 2:** Transportation Systems Plan public hearing to be held and then continued on May 16.

**Monday, 16:** Metro Councilor Dirksen update (tentative); Laura McKinney-Oregon Tech Update; East-West Connector Corridor Plan (aka Old Town Escape) consultant agreement; Charbonneau High Priority Repair Projects contract ; franchise agreement public hearing; zone change 14-lot subdivision; Transportation Systems Plan minor amendments; Information Technology Strategic Plan; Regional Park 7 development agreement; Community Enhancement Program recommendations; LED lighting; and affordable housing discussion.

### Planning Commission Update

At the Planning Commission meeting on April 13, the Commission held a public hearing, discussed and approved the proposed Transportation System Plan (TSP) Amendments. The Commission also reviewed and discussed the Transit Master Plan Update being undertaken by the City’s public transit agency South Metro Area Regional Transit (SMART) and the reviewed and discussed the Annual Housing Report.

### Development Review Board Update

At the DRB Panel B meeting on March 28, the board heard a request for a Zone Map amendment, subdivision platt and other applications for a 14-lot single-family subdivision to be located at 28500 and 28530 SW Canyon Creek Road South and continued consideration of the application to their April 25 meeting.

At the April 11 meeting of the DRB Panel A, the board approved applications and plans for a 35,120-square-foot expansion of the Coca Cola warehouse located at 9750 SW Barber Street that adds new automobile and bicycle parking spaces, landscaping and upgraded exterior lighting.

### Budget Committee Meets in May

The Budget Committee is scheduled to hear the City Manager’s proposed budget for Fiscal Year 2016-17 on May 18, June 1 and if necessary, on June 8. These

#### Budget Committee Meetings

- Weds, May 18, 6-10 pm
- Weds, June 1, 6-10 pm
- Weds, June 8\*, 6-10 pm

City Hall, 29799 SW Town Center Loop East, \*If needed

meetings are open to the public and public comment is encouraged. Proposed budget documents are expected to be available to the public on May 9 and will be posted online at [www.ci.wilsonville.or.us/budget](http://www.ci.wilsonville.or.us/budget).

### March Library Board Report

During the Library Board meeting in March various reports included:

- A teen event based on Divergent, the latest movie from that franchise, was held in March.
- Steven Engelfried was announced as the new Library Services Manager.
- The librarians held a brainstorming session about new programs and idea. Those ideas are being sorted into broad themes and begin to show where the library team is wanting to go in the future.
- Food For Fines in February was a great success bringing in more than 1900 lbs of food for Wilsonville Community Sharing.
- The Library also offered food for fines during National Library Week in April.
- The Library is reviewing their policy and will present any possible changes if warranted.
- Alan Steiger’s term on the Library Board is ending on June 30 and the Library Board is in the process of seeking a new member.
- The Library Board Foundation is planning a spring fundraiser luncheon on May 24, at the Charbonneau Country Club with the Oregon State librarian MaryKay Dahlgreen as the guest speaker.

The next regular meeting is scheduled for Wednesday, May 25, 6:30 pm, at the Library.

## City Celebrates Drinking Water Week May 1-7 with Proclamation

At the City Council meeting on April 18, Mayor Tim Knapp proclaimed the week of May 1-7 as Drinking Water Week, “Your Water—To Know it is to Love it”. This year the national awareness campaign is encouraging municipal water suppliers like the City of Wilsonville to educate water customers about the importance of having high-quality and ample supply of drinking water.

Public Works Director Delora Kerber explains, “We know community members are very interested in where their water comes from and how it is treated because water plays a critical role in to our health, economy and safety which is why each year we participate in National Drinking Water Week.”

There are a number of ways the City keeps community members informed about the quality of drinking

water the City produces and distributes throughout the community.

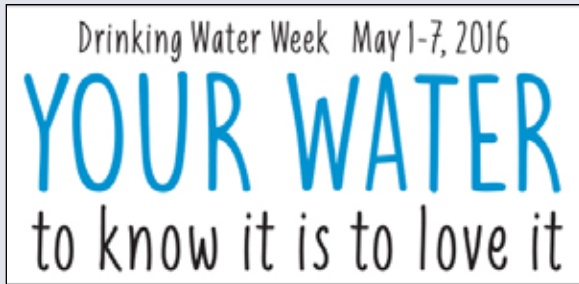
Each July the City publishes an “Annual Water Quality Report,” which documents the prior year’s water-quality test results. The current report is available online at [www.ci.wilsonville.or.us/WaterQualityReport](http://www.ci.wilsonville.or.us/WaterQualityReport) and is updated each year by July 1. The annual report shows that the City is not only meeting, but surpassing all state and federal drinking water standards.

The City also welcomes local community and school groups to take a tour of the City’s water

treatment plant by contacting Teresa Bradshaw, Administrative Assistant with Veolia Water, at 503-582-9655; [teresa.bradshaw@veolia.com](mailto:teresa.bradshaw@veolia.com).

Here are a few more interesting and educational facts about Wilsonville’s drinking water:

- Since 2002, the Willamette River has been the sole



source for drinking water in Wilsonville.

- The City’s state-of-the-art water treatment plant is staffed 24 hours per day and is operated through a public-private partnership with Veolia Water.
- Wilsonville’s water rights allow the city to draw up to 20 million gallons of water per day from the Willamette River.
- On average Wilsonville’s water treatment plant produces 5 million gallons of water per day, but peak demand in the summer can be as high as 11 million gallons per day.
- The water produced by the plant is monitored continuously and instrument readings are verified by in-house laboratory tests at least every four hours.
- City treated water is tested on an accelerated schedule by third-party labs to allow for more frequent testing than is required by regulatory agencies.
- Wilsonville has been selling treated water to the City of Sherwood since 2011.

For more information, contact Delora Kerber, Public Works Director, at 503-570-1542; [kerber@ci.wilsonville.or.us](mailto:kerber@ci.wilsonville.or.us).

## Public Works is Always There to Deliver Essential Services

The City’s Public Works crews play a critical role in delivering Wilsonville residents essential services such as high-quality drinking water, clean public spaces, well-cared for landscaping, good quality roads, exceptional sanitary sewer services and much more.

**Public Works Week Proclamation**  
 • May 16, 7 pm  
 City Hall, 29799 SW Town Center Loop East

In recognition of the department’s hard work and high-quality services, Mayor Tim Knapp is to proclaim the week of May 15-21

as Public Works Week at the City Council Meeting on May 16. This year’s theme of “Always There” recognizes the consistent and reliable nature in which the Public Works staff fulfills their mission of delivering essential services to the community.

Mayor Tim Knapp says, “No matter the time of day or the enormity of the challenge, our Public Works crews are always there and available to not only meet, but exceed, the needs of our community for clean water, sanitation and other critical public services that are vital to maintaining our community’s high quality of life.”

The Public Works Department, directed by Delora Kerber, is comprised of 20 staff people in seven different sections that include Building & Grounds Maintenance, Industrial Pretreatment, Roads, Storm Water Maintenance, Street Lighting, Wastewater Collections and Water Distribution.

In addition, Public Works is also responsible for delivering the residents clean drinking water and overseeing the City’s water treatment plant. Public Works manages the contract with Veolia Water

North America, which performs the day-to-day operation of the plant that on average produces 5 million gallons of water daily. The department is also responsible for maintaining and repairing over 100 miles of water lines that run throughout town along with over 1,000 fire hydrants.

Another area of responsibility for the Public Works Department is the operation and maintenance of the sanitary sewer system, including the City’s wastewater treatment plant and the entire sewer collection system. Public Works Director Kerber oversees the contract with CH2M for the day-to-day operation of the City’s wastewater treatment plant that processes on average about 4 million gallons of sewage daily.

In addition to City water and sewer services, Public Works staff manages and maintains city facilities, city-owned streets, landscaping, street trees, storm water systems and graffiti removal. The Public Works Department also manages the Adopt-a-Road Sign, Emergency Management, Bulk Water Meter and Backflow Prevention, Industrial Pretreatment Programs and the Drug Take-Back Box (see article below).

The department also maintains the water-quality at the City’s two interactive water features located at Murase Plaza and Town Center Park. Every few hours during water feature season that begins this year on Wednesday, June 1, and runs until Sunday, September 11, staff monitors and maintains water-quality to meet health code standards.

For more information about the services provided by the Public Works Department visit [www.ci.Wilsonville.or.us/PublicWorks](http://www.ci.Wilsonville.or.us/PublicWorks).



## City Finds Large Energy Cost Savings at Water and Sewer Treatment Plants



CH2M Project Manager Robert Watts (left) and Public Works Director Delora Kerber (right) hold one of two incentive checks the City received for participating in an Energy Trust of Oregon energy savings program.

## Emergency Preparation Tips from April’s National PrepareAthon! Day:

- Sign up for local alerts and warnings, download apps, and/or check access for emergency alerts
  - Develop and test emergency communication plans
  - Assemble or update emergency supplies
  - Learn about local hazards
  - Take preparedness training classes
  - Collect and safeguard critical documents
  - Document property and obtain appropriate insurance for relevant hazards
  - Make property improvements to reduce potential injury and property damage (mitigation)
  - Practice emergency response actions
  - Plan how to help each other and share resources
- America’s PrepareAthon! is asking everyone to take action, be counted, and spread the word. Visit [www.ready.gov/prepare](http://www.ready.gov/prepare) to find out how you can participate and register your actions.

## Deposit Unused and Expired Medicines in Drug Take-Back Box at Public Works Building

The drugs in your medicine cabinet pose a serious health threat to humans and the environment. If these drugs fall into the wrong hands or are improperly disposed of, bad things happen.

**Clear Your Cabinet**—All types of drugs left in medicine cabinets or anywhere in homes, are often abused and frequently stolen. Studies show that a majority of abused prescription drugs are obtained from family and friends, including from home medicine cabinets.

**Drug Take-Back Box**  
 • Monday-Friday excluding Holidays, 8 am-4:30 pm  
 Public Works/Police  
 30000 SW Town Center Loop East

Proper disposal of expired, unused, and unwanted prescription drugs helps

prevent drug abuse, child poisonings, and theft. It is also important to keep prescription medications in safe places locked away from children.

**Don’t Flush Drugs Down the Drain**—When medications are flushed down the drain they end up at the Wilsonville Wastewater Treatment Plant. Since the facility can only partially remove chemical com-

pounds, they pass through the treatment plant and are released into the river where they contaminate the waterways and environment.

**Drop-Off Unwanted Drugs**—To help residents responsibly dispose of unused or expired prescription drugs, the City of Wilsonville has a Drug Take-Back Box located in the Public Works/Police building at 30000 SW Town Center Loop East in Wilsonville.



Community members can drop off unwanted prescription drugs, Monday-Friday, 8 am-4:30 pm, excluding holidays. The City does not accept any sharp objects or needles at this location. The drug take-back service is free and anonymous.

For more information about the City’s Drug Take-

Back Box, contact Randy Watson, Industrial Pretreatment Coordinator, at 503-570-1551; [watson@ci.wilsonville.or.us](mailto:watson@ci.wilsonville.or.us).



Industrial Pre-Treatment Coordinator Randy Watson at the Drug Take-Back Box in the Public Works Building.

### City, Metro and Republic Services Sponsor Bulky Waste Day, May 7

In partnership with Metro and Republic Services, the City of Wilsonville is holding a Bulky Waste Day on Saturday, May 7, 10 am-2 pm, at 10295 SW Ridder Road in Wilsonville.

Community members, with proof of Wilsonville residency and toiletries to donate, are invited to drop off large, bulky items that don't fit into standard-sized garbage cans.

**Bulky Waste Day**  
• Sat, May 7, 10 am-2 pm  
Republic Services  
10295 SW Ridder Road

The types of waste that will be accepted include: dishwashers, televisions, refrigerators, computers, monitors, stoves, dryers, water heaters, couches, mattresses, scrap metal, tables and chairs. No construction materials will be allowed including scrap wood, flooring materials, walls, siding, concrete and rebar, insulation materials, electrical, fire

doors, fire brick or fire proofing. Other prohibited materials include paints, solvents, thinners, batteries, propane bottles/canisters, household garbage, tires, and any other non-bulky waste deemed hazardous. Loads containing any of these items will not be accepted. For more information contact Matt Baker at mbaker@ci.wilsonville.or.us; 503-570-1548.



The event is free, but participants are asked to donate toiletries such as shampoo, soap and deodorant to benefit Wilsonville Community Sharing's Food Bank and the low-income households they serve.

### Time to Roll Up Your Sleeves at WERK Day on Saturday, May 14

The City's 16th annual community park improvement and clean-up event, Wilsonville Environmental Resources Keepers (WERK) Day, is on Saturday, May 14, 8 am-1 pm.

Registration and a free omelet breakfast provided by the Wilsonville Rotary Club runs 8-9 am in the Wilsonville Community Center, 7965 SW Wilsonville Road.

During the event participants focus on improving and maintaining City parks in Wilsonville. According to Parks Supervisor Tod Blankenship, this year's focus includes a partnership with Pac-West Disc Golf Alliance. The day consists of prepping and constructing tee pads and bench sites and anchoring basket sleeves for a new Disc Golf Course in Memorial Park.

Other projects on WERK day include spreading bark dust, removing invasive species, painting soccer goals and resurfacing a small wooden walking bridge.

Participants should bring work gloves, water bottles, sun screen and be dressed appropriately for weeding, raking and shoveling. Participants of all ages are welcome and encouraged to attend, but children

**WERK Day**  
• Sat, May 14, 8 am-1 pm  
Wilsonville Community Center  
7965 SW Wilsonville Road

under the age of 14 must be accompanied by an adult. Sponsors of the event include the Wilsonville Rotary Club, Wendy's, Starbucks, the Family Fun Center, Fred Meyer and Lee's Martial Arts.

Participants in the rain-or-shine event receive coupons from Wendy's restaurants and the Family Fun Center and a free WERK Day t-shirt while supplies last.

Participants and volunteer groups are encouraged to register in advance on the City's Parks and Recreation website at www.WilsonvilleParksandRec.com/WERKdAy where they can download and sign the WERK Day volunteer liability release form.



To sign up a group, please contact Patty Brescia, Recreation Program Manager, at 503-570-1525, brescia@ci.wilsonville.or.us.

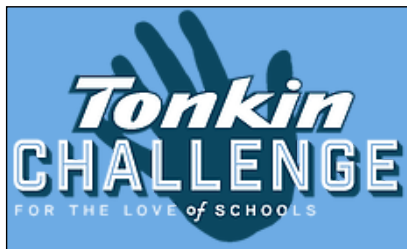
### Tonkin Challenge Fun Run Benefits Local Schools

Ron Tonkin Family of Dealerships is holding the Fifth Annual "Tonkin Challenge-For The Love Of Schools" fun run on Sunday, June 5, with the first race starting at 8 am. The event, held at Audi Wilsonville, 26600 SW 95th

Avenue, raises money for educational programs of the West Linn-Wilsonville School District and other school districts throughout the region.

All of the entry fees, \$45 per adult and \$15 per child 14 and under, support local schools.

The runs start at 8 am with a 5 mile run, followed by a 5K race and a .5 mile Mascot Dash for younger kids. Sign up at www.TonkinChallenge.org to participate.



### Garden Club Holds Annual Plant Sale on Saturday, May 7

The Wilsonville Garden Club is holding an annual plant sale on Saturday, May 7, at the Wilsonville Public Library, 8200 SW Wilsonville Road. The sale begins at 9 am and lasts until the all the plants are sold, which at past sales is

usually before 2 pm. The club's sale offers large selection of the very popular sun and shade hanging baskets, blooming annuals, perennials plus cuttings and divisions from club member's gardens and an assortment of Garden Gallery yard art.



All of the proceeds from the event go to support local horticulture education and scholarships.

**Wilsonville Garden Club Annual Plant Sale**  
• Sat, May 7, 9 am  
Wilsonville Public Library  
8200 SW Wilsonville Road

## City Calendar

| MAY      |  |
|----------|--|
| 2 Mon    | • City Council, 5 pm-Work Session CANCELLED, 7 pm-Meeting, City Hall   |
| 3 Tue    | • Municipal/Traffic Court, 5 pm, City Hall   |
| 7 Sat    | • Wilsonville Garden Club Annual Plant Sale, 9 am-2 pm, Wilsonville Library<br>• Bulky Waste Day, 10 am-2 pm, Republic Services, 10295 SW Ridder Rd  |
| 9 Mon    | • DRB Panel A, 6:30 pm, City Hall  |
| 10 Tue   | • Wilsonville Area-Legislators Town Hall, 7-8 pm, City Hall, Council Chambers  |
| 11 Wed   | • Frog Pond Master Plan Open House, 5-6:30 pm, City Hall<br>• Planning Commission*Time Change*, 7 pm, City Hall<br>• Wilsonville Community Seniors Inc. Advisory Board, 6 pm, Wilsonville Community Center |
| 14 Sat   | • WERK Day, 8 am-1 pm, Wilsonville Community Center  |
| 16 Mon   | • City Council, 5 pm-Work Session, 7 pm-Meeting, City Hall   |
| 17 Tue   | • Election Day, Ballots Due by 8 pm  |
| 17 Tue   | • Municipal/Traffic Court, 5 pm, City Hall   |
| 18 Wed   | • Budget Community Meeting, 6-9 pm City Hall   |
| 19 Thurs | • Wilsonville Leadership Academy, 6-9 pm, City Hall  |
| 21 Sat   | • Queen of the High Road Half Marathon and 10K, 8 am-3 pm, Wilsonville Memorial Park River Shelter   |
| 23 Mon   | • DRB Panel B, 6:30 pm, City Hall  |
| 25 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library   |
| 30 Mon   | • Memorial Day Holiday- All City Offices CLOSED  |
| JUNE     |  |
| 1 Wed    | • Budget Community Meeting, 6-9 pm, City Hall  |
| 4 Sat    | • Wilsonville Festival of the Arts, 10 am-6 pm, Town Center Park   |
| 5 Sun    | • Tonkin Challenge-The Love Of Schools Run, 8 am, Audi Wilsonville, www.TonkinChallenge.org<br>• Wilsonville Festival of the Arts, 10 am-5 pm, Town Center Park  |
| 7 Tue    | • Municipal/Traffic Court, 5 pm, City Hall   |
| 8 Wed    | • Budget Community Meeting, 6-9 pm, City Hall<br>• Planning Commission, 6 pm, City Hall  |
| 9 Thurs  | • Parks and Recreation Board Meeting, 4:30 pm, Parks and Recreation Admin Building   |
| 13 Mon   | • DRB Panel A, 6:30 pm, City Hall  |
| 16 Thurs | • Wilsonville Leadership Academy, 6-9 pm, City Hall<br>• Non-Profit Volunteer Fair, 7-8 pm, City Hall, Willamette River Room   |
| 21 Tue   | • Municipal/Traffic Court, 5 pm, City Hall   |
| 22 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library   |
| 27 Mon   | • DRB Panel B, 6:30 pm, City Hall  |

All dates and times are tentative; check the City's online calendar for schedule changes at www.ci.wilsonville.or.us.

### Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and the meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and replayed periodically. Meetings are also available to stream live and by video-on-demand online at www.ci.wilsonville.or.us/WilsonvilleTV. Public comment is welcome at all City Council meetings.



**Tim Knapp**  
Mayor  
knapp@ci.wilsonville.or.us



**Scott Starr**  
City Council President  
scottstarr97070@gmail.com



**Julie Fitzgerald**  
City Councilor  
fitzgerald@ci.wilsonville.or.us



**Susie Stevens**  
City Councilor  
stevens@ci.wilsonville.or.us



**Charlotte Lehan**  
City Councilor  
lehan@ci.wilsonville.or.us

#### City Manager

Bryan Cosgrove  
503-570-1504  
cosgrove@ci.wilsonville.or.us



#### Wilsonville City Hall

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: www.ci.wilsonville.or.us  
E-mail: webmaster@ci.wilsonville.or.us

**City Council:** 503-570-1501; council@ci.wilsonville.or.us

# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

November 2016



In October, Edge Family Fitness in Wilsonville celebrated a grand opening with staff, community members and clients.

## Wilsonville Voters Consider Ballot Measures to Fund Recreation/Aquatic Center, Continue Ban on Marijuana Businesses and Others

In the upcoming general election on Tuesday, Nov. 8, Wilsonville voters are considering a number of important local ballot measures. The key local questions Wilsonville voters are being asked on the ballot this year include:



**City of Wilsonville Measure 3-485:** Shall Wilsonville be authorized to issue up to \$35,000,000 of general obligation bonds for a Community Recreation and Aquatic Center?

**City of Wilsonville Measure 3-486:** Shall Wilsonville continue to prohibit medical marijuana processors, medical marijuana dispensaries, recreational marijuana producers, processors, wholesalers, and retailers in Wilsonville?

**Clackamas County Measure 3-509:** Shall Clackamas County adopt a seven-year, 6-cent-per-gallon motor vehicle fuel tax dedicated to road maintenance projects?

**Clackamas County Measure 3-502:** Shall Clackamas County fund law enforcement; maintain rate of 24.8 cents per \$1,000 assessed value for five years beginning 2017-2022? This measure renews current local option taxes.

**Metro Measure 26-178:** Shall Metro protect water quality, fish, natural areas, parks; renew 5-year operating levy, 9.6¢ per \$1,000 assessed value, beginning 2018? This measure renews current local option taxes.

**Tualatin Soil & Water Conservation District (Washington County) Measure 34-269:** Shall District adopt permanent rate limit authorizing levy of up to \$0.09 per \$1,000 assessed value, beginning fiscal year 2017-18?

For more information on each measure, voters should consult an official Voters' Pamphlet that was mailed to all registered voters in mid-October. For many measures the Voters' Pamphlet also includes both arguments in favor and arguments opposed to

many of the measures.

Wilsonville voters are also electing three members of City Council including the position of Mayor. Mayor Tim Knapp, City Councilor Susie Stevens and DRB Member Kristin Akervall are all running unopposed. City Councilor Julie Fitzgerald is not seeking reelection.

More information, including the Voters' Pamphlet, is available online at [www.clackamas.us/elections](http://www.clackamas.us/elections) and at [www.co.washington.or.us/elections](http://www.co.washington.or.us/elections).



The Wilsonville Public Library is an official Ballot Drop Site—ballots may be deposited up until 8 p.m. on Tuesday, Nov. 8, Election Day.

## Volunteers Needed to Plan Wilsonville Festival of Arts

The Wilsonville Arts & Culture Council is seeking volunteers to help plan the 2017 Wilsonville Festival of Arts. The annual art show takes place in Town Center Park on Saturday, June 2 and Sunday, June 3. The festival's Planning Team begins meeting twice monthly in January until the event in June. To volunteer or for more information contact Theonie Gilmore at [Theonie@WilsonvilleArts.org](mailto:Theonie@WilsonvilleArts.org); 503-638-6933.



Art enthusiast Theonie Gilmore

## SMART Partners with TriMet on 'Be seen. Be safe.' Campaign

With daylight-saving time ending on Sunday, Nov. 6, SMART has again teamed up with TriMet to help promote safety and visibility for commuters as part of the **Be seen. Be safe.** campaign. To promote nighttime safety SMART and TriMet staff are handing out safety lights, at transit stations around the region – look for them at a transit station near you.

The safety campaign encourages the use of:

- Reflective jackets, vests and accessories
- Reflective arm, leg and ankle bands
- Bike lights and reflectors. Bicyclists are required by law to have a light in the front and a red reflector in the back, before sunrise and after sunset.

Learn more at [www.trimet.org/beseen](http://www.trimet.org/beseen)



### Mayor's Message:

## City is Working Actively on Transportation Issues to Improve Mobility

As Oregon's economy has moved into full job-creation recovery from the Great Recession and we continue to be the No. 1 move-to state in the U.S., area highways are becoming increasingly congested. The West Coast's major transportation arterial, I-5, is both a boon and bust for Wilsonville. When I-5 is flowing smoothly, life is good; when it's not, it's a bear.



For better or worse, we live or die by I-5.

On page seven of this issue, we go more in-depth on some of the issues that impact traffic flow and congestion, and offer a number of suggestions for improved mobility. The City Council takes seriously the concerns of residents and businesses on transportation matters, and we are working to advance solutions on multiple levels.

Over the past decade, the federal government has backed away from continued funding for major roadway improvements. As seen across the country, states and localities are having to bootstrap themselves to fund desired transportation enhancements.

We are working with a statewide coalition known as the Oregon Transportation Forum—composed of local governments, transportation associations, freight haulers and nonprofit organizations—to advance a potential "transportation package" at the Oregon Legislature in the 2017 session. Similar to

the 2009 *Jobs & Transportation Act* that funded the I-5 northbound auxiliary lane from North Wilsonville to I-205, this new legislation would focus on adding new transportation capacity. A special state legislative committee has heard testimony from around Oregon complaining about traffic congestion in the greater Portland metro area as well as requesting more transit services.

**For better or worse, we live or die by I-5.**

The region recently came together to advance planning efforts on three long-time congested highways with major "bottlenecks": I-5/I-84 Rose Quarter, Highway 217 corridor, and I-205/Abernathy Bridge to Stafford Road. We are working to position the South Metro I-5 corridor as the next major bottleneck planning effort.

We are updating our SMART Transit Plan to offer improved local transit services and connections outside of town to Beaverton, Portland, Canby and Salem. We are urging TriMet to encourage further service enhancements for the South Metro region. We are also lobbying to extend WES commuter rail service south to Salem in order to provide another transit alternative.

On local, regional, state and federal levels, Wilsonville is working on your behalf to improve transportation options in-town and regionally. It will, of necessity, be an on-going effort.

*Tim Knapp, Mayor*

### In This Issue...

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- Tips and Ideas for Trips/Commutes . . . 3
- Your Guide to Recycling . . . . . 5
- Many Factors Impact I-5 . . . . . 7
- Road Project Update. . . . . 7

...and more!

City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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## Wilsonville Hires Dan Carlson as City Building Official

The City of Wilsonville announced the hiring of Dan Carlson as the City's new Building Official, assuming the position previously held by long-term Building Official Martin Brown who retired this summer.

Dan comes to Wilsonville from the City of Corvallis where he has served as the Building Official for the past 12 years. Prior to serving as Building Official, Dan also held related, progressively responsible positions during his 20-year career with the City of Corvallis as the City's Inspection Services Manager, Commercial Plans Examiner, Commercial Building Inspector and as a Residential Building Inspector.

In Corvallis Dan served as Building Official for a number of large-scale building projects on the campus of Oregon State University and the HP (formerly Hewlett-Packard) campus, among others. During this time Dan also served as the President of the state-wide Oregon Building Official Association in 2011-2012 and also was named Building Official of the Year for



Building Official  
Dan Carlson

Oregon in 2013. In 2010 Dan was appointed by Governor Kulongoski to serve on the Oregon Building Code Structures Board where he continues service today.

Dan studied Building Inspection Technology at Chemeketa Community College in Salem where he earned a degree in Building Inspection Technology and prior to that Dan spent five years working for a custom homebuilder in Dallas, Oregon.

Community Development Director Nancy Kraushaar states, "We are so pleased to have someone with so much experience join our ranks. Dan is a tenured professional who is a great fit for the position and is highly qualified to oversee commercial and residential building in Wilsonville."

Dan explains, "I have long admired Wilsonville's building inspection program and have always felt it would be a great place to continue my career working with the development community."

In his free time Dan enjoys spending time in his new home that he recently built, hiking the Pacific Crest Trail and being with his wife and three adult children.

Dan Carlson, Building Official, may be reached at 503-570-1557; carlson@ci.wilsonville.or.us.

## Reminder: Don't Rake Leaves into the Street

Residents are reminded to not rake leaves into the street. The City does not pick up leaves raked into the street nor is the street sweeper able to remove piles of leaves. Leaves that enter the storm drain system can also block up the system resulting in localized flooding. Community members who see a blocked storm drain are encouraged to rake the leaves away from the drain.

Instead community members should rake up and dispose of their leaves in the green landscaping cart, bring their leaves to the Leaf Drop Off Day at City Hall on Saturday, Nov. 19, use the leaves as compost, or bring to a landscaping debris recycling facility.



The public is encouraged to remove leaves and other debris from storm drains to prevent localized flooding.

## Rotary Club Accepting Nominations to Recognize Exceptional Community Members/Volunteers

The Rotary Club of Wilsonville is now accepting nominations for the 2016 First Citizen Award to honor a local community member or couple for exceptional volunteerism and community service. Nominations are due no later than Dec. 15.

Four or five finalists are to be announced in late January and the winner is being presented at the Rotary's Heart of Gold Dinner and Auction on Saturday, Feb. 25, 2017, at the Wilsonville Holiday Inn.



2015 First Citizens Sherine and Jaimy Beltran take photo with last year's Rotary Club President Kyle Bunch.

To nominate a community member who is an exceptional volunteer go to [www.WilsonvilleRotary.com/heart/nominate](http://www.WilsonvilleRotary.com/heart/nominate) or send an e-mail to [president@wilsonvillerotary.com](mailto:president@wilsonvillerotary.com). There is also a PDF nomination form that can be downloaded from the site and mailed in.

"Our Heart of Gold Dinner and Auction exists so we can celebrate Wilsonville's great volunteers and support Rotary's great service projects," Rotary Club President Richard Martens said. "But to do that, we need the

community's help so we can identify the next deserving honoree."

At the last year's Heart of Gold Celebration, Sherine and Jaimy Beltran were awarded the 2015 Wilsonville First Citizen award from Rotary Club of Wilsonville past president Kyle Bunch. Prior winners include Donna Bane, Kathryn Whittaker, Jay Puppo, Dick Spence, Debi Laue, Lonnie and Julie Gieber, Theonie Gilmore, Anne Easterly, Ron Anderson, Bob and Judy Woodle, Alan Kirk, Doris Wehler, Sue Ludlow, John Ludlow, Charlotte Lehan and Dr. Laura LaJoie Bishop.

For more information, to purchase tickets or to become a sponsor of the annual event contact any Rotary member, visit [www.WilsonvilleHeart.com](http://www.WilsonvilleHeart.com), or contact Club President Richard Martens, [president@wilsonvillerotary.com](mailto:president@wilsonvillerotary.com).



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

## Vote at the Library!

Ballot boxes are now available through Tuesday, Nov. 8, for the General Election and our "Picture Book Presidents" election.



Vote for your Picture Book President candidate (The Cat in the Hat, Max from *Where the Wild Things Are*, or write-in). No age limit for voters, but only one ballot per voter. It's your civic duty!

And don't forget to follow the antics of our mischievous candidates on our Facebook page. Learn more at: [www.WilsonvilleLibrary.org/elections](http://www.WilsonvilleLibrary.org/elections).

## Managing Diabetes Naturally

Diabetes is a global epidemic in the noncommunicable diseases sector. Learn about self-care techniques, focusing on diet modifications and stretching exercises, that are both economical and effective. Share ideas on ways to keep the damage caused by this chronic and resistant illness under control.



The free workshop is on Saturday, Nov. 5, 1-2 pm. Learn more at: [www.WilsonvilleLibrary.org/classes](http://www.WilsonvilleLibrary.org/classes)

## International Games Day

Celebrate International Games Day with a day of gaming at the library! Choose from a variety of board and card games, including new games just for this event, or bring your own.



The free all-ages event is on Saturday, Nov. 19, 12-4 pm.

## Book Notes Concert: West African harp Nov. 12

Sean Gaskell performs traditional West African songs on the Kora, a 21-string harp that he learned to play over multiple visits to its homeland in Gambia, West Africa. This free concert is sponsored by the Friends of the Wilsonville Library.

The free concert is on Saturday, Nov. 12, 2-3 pm, in the magazine reading area.



## Library Holiday Closures

The Library will be closed the following dates in November:

- Friday, Nov. 11, Veterans Day
- Thursday, Nov. 24 & 25, Thanksgiving

To renew items, select "My Account" on the LINCC website ([www.lincc.org](http://www.lincc.org)) or call our automated phone service at 503-659-8634.



## Upcoming Classes and Events

**Local Artist of the Month: Lake Oswego Reads traveling display**

**The Zone (for grades 3-5)**

Thursday, Nov. 3, 4-5 pm  
[www.WilsonvilleLibrary.org/kids](http://www.WilsonvilleLibrary.org/kids)

### Book Club

**Author Comparison: Isabel Allende**  
SPECIAL DATE Thursday, Nov. 3, 6-8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

### First Friday Film

Friday, Nov. 4, 6-8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

### Sábado de Cine (Spanish Movie Saturday)

Saturday, Nov. 5, noon-2 pm  
[www.WilsonvilleLibrary.org/kids](http://www.WilsonvilleLibrary.org/kids)

### Book Notes Concert: Sean Gaskell

Saturday, Nov. 12, 2-3 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

### K-2 Book Adventures: Outer Space!

Thursday, Nov. 17, 4-4:45 pm  
[www.WilsonvilleLibrary.org/kids](http://www.WilsonvilleLibrary.org/kids)

### Teen Event: Hawaiian Night

Friday, Nov. 18, 6:30-8:30 pm  
[www.WilsonvilleLibrary.org/teens](http://www.WilsonvilleLibrary.org/teens)

Visit our website for more events:  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

## Smoke Alarms Save Lives

Tualatin Valley Fire & Rescue wants to remind you to check your smoke alarms and to “hear the beep where you sleep”—make sure every bedroom has a working smoke alarm.

The majority of people who die in a fire succumb to heat and smoke inhalation, not flames. Having working smoke alarms in your home greatly increases your chances of surviving a fire. Most fatal fires happen at night when people are sleeping. Working smoke alarms are designed to activate early to help ensure that you and your family have time to safely escape from your home.

The National Fire Protection Association states smoke alarms should be replaced every 10 Years. To find out how old a smoke alarm is, look at the date of manufacture on the back of the alarm.



### TVF&R also advises community members:

- To ensure maximum protection, install smoke alarms in every sleeping room, outside each separate sleeping area and on every level of the home.
- Check your alarms monthly by pushing the test button and replace smoke alarms that are 10 or more years old.
- Hard-wired alarms (wired directly into home electrical systems) should have a battery back-up.
- Never disconnect/remove batteries from an alarm.
- Develop and practice a fire escape plan.
- Designate a safe meeting place outside.

If your smoke alarm sounds, immediately exit your home, call 911, and stay outside.

Oregon law requires working smoke alarms in all rental dwellings. If you rent and do not have working smoke alarms, contact your landlord or property manager. If working smoke alarms are not installed after you have notified your landlord or property manager, call TVF&R at 503-649-8577.

Smoke alarms are also available for people who are deaf or hard of hearing.

For more fire prevention tips and tools to create a fire escape plan, visit [www.tvfr.com](http://www.tvfr.com).

## Chief's Corner: Inclement Weather Driving Tips

With fall in full swing and another winter right around the corner, this is a good time to remind everyone of a few tips for driving in inclement weather.

Of course, the best tip in bad weather is to not drive at all—stay home or take public transit. However, if driving is your only or best option, here are a few other tips:

### Before your trip:

- Visit [www.TripCheck.com](http://www.TripCheck.com) for up-to-date travel and road conditions.
- Clear all snow and ice from the vehicle windows and hood to improve visibility.
- Make sure you have a full tank of gas before your trip.
- Ensure you have an emergency kit in your car in case you get stuck or stranded.
- Carry chains that fit your vehicle and practice chaining up ahead of time.
- Leave early and allow yourself enough travel time when going to work or to an appointment.
- Always leave enough room between you and the car in front of you; leave at least one car length for every ten miles per hour you are traveling.
- Avoid spinning or locking your wheels and accelerate and decelerate slowly, with or without chains.
- If you have to install chains, put them on in a safe and level area. Carry a waterproof tarp to keep dry when installing the chains and keep children and pets safe in your car to avoid distraction or injury. Shortly after installation, pull over in a safe place,



Police Chief Jeff Smith

check and retighten the chains to ensure they fit properly. Remember to pull over and stop immediately if any part of your chain fails or comes loose.

- Be alert to and prepared for black ice.
- If your front wheels begin to slide during a turn, straighten the steering wheel slightly to allow them to regain rolling friction. If your back end begins to slide, do NOT use your brakes; this will shift weight to the front of the car and make the slide worse.
- If you have to pull over for any reason, pull over on the right-hand shoulder of the road and as far away from traffic as possible.
- During nighttime driving, your vision may be impaired slightly so drive defensively and at a safe distance. And remember that daylight savings time ends this month so it will begin to get darker earlier in the evening.
- Finally, remember that driving in inclement weather isn't necessarily difficult but stopping usually is - so just slow down.

I wish you and your family a safe, healthy and happy fall and winter season.

**Jeff Smith, Chief of Police**

### PUTTING ON TIRE CHAINS

**Get the right size!**  
*Consult your owner's manual*

**Practice!**  
*Know how to install your chains before you go*

**Pull over!**  
*Choose a safe and level area that's away from traffic*

**Double check!**  
*Retighten your chains after you drive a short distance*

**Slow down!**  
*You can still slide with chains — don't drive faster than 30 mph*

**When installing chains...**

**Wear protective gear:**

- A brightly colored jacket or a reflective vest
- A hat
- Warm gloves

**Stay dry!**

- Use a tarp or plastic sheet to protect yourself

**Want to learn more?**  
Watch our YouTube video:  
[www.youtube.com/oregondot](http://www.youtube.com/oregondot)

# SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

## Holiday Service Hours

### Friday, Nov. 11, Veterans Day

Regular service  
for all SMART bus routes

### Thursday & Friday, Nov. 24 & 25

SMART offices & bus service closed  
in observance of Thanksgiving Day.  
Regular service resumes  
Saturday, Nov. 26

## RideWise Offers Free Public Transit Training and Assistance

SMART is now partnering with Ride Connection, Inc. to offer RideWise, a new program that provides travel training and mobility support in the Wilsonville area. RideWise promotes safe and independent travel of older adults and people with disabilities. The program supports travel on all of the transit modes in the tri-county area, and provides people with the tools they need to ride on their own. To get help planning your next trip call Sydney, a RideWise Travel Trainer, at 503-226-0700 or visit the website [www.rideconnection.org/services/travel-training](http://www.rideconnection.org/services/travel-training).



## Tips and Ideas for In-Town Trips and Out-of-Town Commutes

Understanding that larger, major issues such as West Coast population growth and I-5 traffic congestion are realities that we have to deal with while we seek longer-term solutions, Wilsonville residents and commuters have a number of transportation options to improve their mobility and daily trips — and even health and well-being.

### Walk/bike or take the bus for in-town errands:

Most Wilsonville residents live within a mile of commercial centers, which makes for a nice 20-minute walk or 10-minute bike-ride. Or try taking a free SMART bus trip to run your errands in town; quite a few residents can be seen using a small push-cart and loading their weekly groceries onto buses.

**Use transit options to commute to work:** In addition to offering free in-town bus service, SMART offers low-fare rides out-of-town to Portland, Canby and Salem. Finding routes and schedules is easy online or via an app, or you can also chat with a friendly customer-service rep who can help you schedule a ride.

The TriMet Westside Express Service (WES) commuter train runs from Wilsonville's SMART transit center through Tualatin and Tigard to Beaverton, and transfers to other bus routes or to the MAX light-rail system at the Beaverton transit center. For those commuting to or from the metro westside, WES offers reliable weekday service that allows you to bypass the Highway 217 or 26 slog while enjoying free wi-fi and legal, non-driving browsing time.

### Reduce school auto trips and encourage walking together:

Everyday, lines of cars can be seen at local schools as parents drop-off or pick-up their children. Consider working with neighbors to have kids walk together to school; not only will we reduce trips, but we offer kids an opportunity to get some exercise that helps improve health.

### Avoid congested areas — take alternative routes:

During peak rush-hours, avoid roads that pass-by or are near the I-5 interchanges. If needing to cross I-5, consider Boeckman Road that slices through the middle of town and connects the east- and westsides of Wilsonville. To the north of Wilsonville, the Norwood Road overpass offers another I-5 crossing free of interchange-related congestion.

**Be safe and courteous — do not block intersections:** Avoid placing yourself in a dangerous situation of turning on a yellow-light and possibly blocking an intersection. In addition to being an illegal offense that irks other drivers, vehicles that block an intersection impede traffic that produces more back-ups and can lead to an accident.



## City Hires Amanda Guile-Hinman as Assistant City Attorney

The City of Wilsonville announces the hiring of Amanda Guile-Hinman as the City's new Assistant City Attorney, in the position previously held by Barbara Jacobson before she was promoted to the City Attorney position.



Amanda Guile-Hinman

Amanda comes to Wilsonville from Bateman Seidel, a private law firm based in Portland, where she specialized in business litigation for the past seven years as an attorney and where she also served as a legal clerk for a year during law school. While at the firm she represented numerous clients on real estate and other business related cases. In practicing law, Amanda has

also actively offered pro-bono legal services in partnership with Legal Aid Services of Oregon's Domestic Violence Project, where she represented two dozen domestic violence clients.

Amanda also volunteers as a board member on the Portland Art Museum's Northwest Arts Council and a board member of the Wilsonville-based nonprofit Mark and Monica Nyman Foundation that promotes education and leadership opportunities for youth.

Amanda is a member of the Oregon Bar Association, as well as a member of Federal Bar Association, Multnomah Bar Association, American Bar Association, Oregon Women Lawyers and Oregon Trial Lawyers Association.

Amanda studied and graduated with a Juris Doctor (JD) Degree from the University of Denver after attending and earning a Bachelor of Arts degree from Willamette University in Salem where she studied politics and history. Amanda grew up and attended high school in Keizer.

City Attorney Barbara Jacobson states, "Amanda's variety of legal experience, including construction and business litigation, real estate and general business law make her a great addition to our Wilsonville legal team."

## Our History: Boones Ferry Carries Cars, People and Products Across the Willamette River for over 100 Years, 1847–1954



This Boones Ferry Historical Society photo, taken in 1940 by Ralph Gifford, shows the ferry landing in Wilsonville carrying four cars. The ferry was started and opened by Alphonso Boone, the grandson of frontiersman Daniel Boone in 1847. The Boones Ferry was operated until 1954 and served as a direct trade route between Portland and the Willamette Valley.

Amanda explains, "Working for the City of Wilsonville is an excellent opportunity for me to help people, support the needs of the community and meet the day-to-day legal challenges of a fast-growing and developing community."

In her free time Amanda likes spending time with her family and being outdoors with her husband where they enjoy hiking, running and kayaking.

Amanda Guile-Hinman, Assistant City Attorney, may be reached at 503-570-1561; guile@ci.wilsonville.or.us.

### Get Five Free Trees from the City

The City is offering up to five native tree seedlings for any community member or business within the City limits, for planting on their property. The City encourages plantings along streams and other water bodies to reduce stream temperature and pollutants. The trees are obtained at a local nursery. For more information and to get a tree coupon for five trees, contact Luke Bushman, Stormwater Coordinator, at 503-570-1552; bushman@ci.wilsonville.or.us.



29600 SW Park Place  
Parks Programs: 503-783-7529  
Community Center: 503-682-3727  
www.WilsonvilleParksandRec.com

## Sign up today for the Reindeer Romp - Kids Dash and 5k.

The Reindeer Romp is a Wilsonville Tradition! This year's 5K Fun Walk/Run and Kid's Dash takes place on Saturday, Dec. 3. The start and finish line are at the Family Fun Center, and there is a new route for the 5k run.

Pre-registration for both events is available at WilsonvilleParksandRec.com. The Kid's Dash is free and the 5K is just \$10



(day of race registration is \$15). To guarantee a t-shirt you must register by Nov. 25.

Race check-in starts at 8 a.m. at the Family Fun Center, the Kid's Dash starts at 8:45 am and the 5K Walk/Run begins at 9 am. The races are not timed and there are no street closures. Strollers and leashed dogs are allowed.

After the run, stay for the Bullwinkle Bash! Race participants receive a free \$5 game card courtesy of the Family Fun Center. There are also planned visits from Santa Claus and Bullwinkle the Moose.

### Serenity Bereavement Group

The Community Center is pleased to offer a new bi-monthly support group that assists community members with grief therapy, peer support, and education as they grieve the loss of a loved one. Facilitated by LCSW, Maudeen Jordan, grief counselor and bereavement coordinator, the Serenity Bereavement Group is offered on the 2nd and 4th Tuesdays of the month at 1:15 pm and is open to everyone. It is our hope that group members will realize that they are not alone in their loss and find care and validation through others who have experienced loss under the careful guidance of a highly-qualified therapist. Pre-registration is not necessary.

### Upcoming Classes and Events

#### Hatha Yoga

Ages 18 and older  
5:30–6:30 pm  
Community Center  
Tuesdays, Nov. 1–Dec. 13  
\$51  
Thursdays, Nov. 3–Dec. 15  
\$44

#### Medicare Assistance

Wednesday, Nov. 2  
11 am–noon  
Community Center  
No Charge

#### Fishing Workshop

Ages 8–12  
Saturday, Nov. 5  
9 am–4 pm  
Mary S. Young Park  
\$62

#### Oil Painting

Ages 12 and older  
Saturday, Nov. 12  
10 am–2 pm  
Parks and Rec Admin Office  
\$50

#### Pre-Teen and Teen Learn to Ice Skate

Ages 11 to 17  
Saturdays, Dec. 3–Jan. 28  
12:15–12:45 pm  
Sherwood Ice Arena  
\$80 + \$15 annual fee to ice arena

## WILSONVILLE HOLIDAY EVENTS

**Community Tree Lighting -**  
Wednesday, Nov. 30, 5:45 pm at Town Center Park

**Reindeer Romp 5k and Fun Run -**  
Saturday, Dec. 3, 8:45 am at Town Center Park

**Holiday Fun Fest -**  
Wednesday, Dec. 14, 4 pm at The Community Center

**Toy Drive -**  
New toy donations are being accepted Nov. 7–Dec. 14  
at The Parks and Recreation Admin Building



WilsonvilleParksandRec.com/Holiday

## Your Guide to Recycling in Wilsonville

### What Can I Put in My Green Landscaping Barrel?

Fall is a time of year when the green yard waste carts get a lot of use and a good time to remind community members about their proper use. When Republic Services collects your yard waste at the curb, biodegradable material is taken to local compost facilities to be turned into a nutrient-rich soil amendment used by farmers and landscapers. Grass clippings and weeds can get quite heavy, so only certain types of yard waste are allowed in the yard waste carts. The carts are picked up mechanically by a collection truck and cannot pickup carts that are too heavy or compacted.



To keep your yard waste cart within a reasonable weight and to properly have the material composted, the following items are acceptable:

- Grass clippings
- Leaves
- Fir/Pine needles
- Weeds
- Small branches (under 4 inches in diameter)
- Flowers and other yard plants

The following items are NOT acceptable in a yard waste cart due to weight and/or compost restrictions:

- Dirt
- Food scraps
- Rocks or gravel
- Pet waste
- Sod (even if it has grass on it)
- Branches or wood larger than 4 inches in diameter
- Plastic bags of any type
- Painted, stained or treated lumber or wood

Also, do not compact your yard trimmings and yard waste in your cart. The material gets stuck in the carts and the trucks cannot empty them. If your yard waste cart gets full before your collection day you can purchase Kraft paper lawn bags at any home, garden or hardware store and set the bag(s) next to your cart for pickup, for a nominal fee.

For questions about yard waste or other recycling and waste collection services, contact Republic Services at 503-682-3900.

### Save the Drains — Freeze the Grease!

The holidays are right around the corner and that means extra guests, extra food and extra cleanup. Avoid extra costs by keeping leftover gravy, pan drippings and grease out of the sink drain — otherwise, you may end up with an extra guest that charges by the hour. According to Roto-Rooter, incoming calls for service the day after Thanksgiving increase almost 50 percent.

To combat the fat, the City is offering a free “Freeze the Grease, Save the Drain Kit” to residents by dropping by the Public Works Office at 30000 SW Town Center Loop East in Wilsonville. Kits are available in the front lobby and include a pan scraper and lid.

Follow these three simple steps:

- Scrape grease and food scraps into a can.
- Freeze it.
- Toss it into the trash.



### Where do I put my recycling and waste carts on pick up day?

The City’s waste and recycling hauler is Republic Services which picks up and disposes waste, landscaping and mixed recyclables on a weekly basis in Wilsonville. In order to help the waste haulers

efficiently complete their routes and avoid unnecessary accidents, the company wants to remind residents when and where to place their carts on pick up. The company recommends placing the carts:

- At the street by 6 a.m.
- Four feet from other objects including for example other bins, mail boxes, and poles.
- Place cart approximately 10 feet in front or behind parked vehicles.



Following the above practices ensures the safe and efficient pick-up of your materials. Trucks use an automated arm to grab your carts and need space between the carts to do this properly.



For more information contact Republic Services’ customer service at 503-682-3900 or visit them at [www.RepublicServices.com](http://www.RepublicServices.com)



### YES! DO RECYCLE.

**Mixed Paper, Cardboard and Cartons:** Junk mail, office paper, newspaper, magazines, phone books, catalogs, cereal & cracker type boxes, shredded paper in a paper bag, milk cartons, juice cartons, and soup cartons (rinse and drain).

**Plastic Bottles, Tubs and Buckets:**

Clean bottles, tubs six ounces and larger, buckets five gallons or smaller, rigid nursery pots four inches across and larger.



**Metal Cans, Foil & Scrap Metal:**

Clean cans, foil, pie plates & trays, empty aerosol spray cans, scrap metal 30 lbs and less and no longer than 30 inches.

**Glass:** Clean glass bottles and jars go in a separate recycling bin.

**Motor Oil:** Recycle in a marked, unbreakable container no larger than two gallons, with a lid.

### Recycling Dos and Don'ts



### NO. DO NOT RECYCLE.

**Non-Recyclable Paper**

- Plastic bags and plastic film
- Frozen food boxes
- Paper plates
- Pizza boxes
- Coffee cups
- Pet food bags
- Paper towels
- Napkins/tissues



**Non-Recyclable Plastics**

- Plastic bags
- Styrofoam
- Packing peanuts
- To go containers
- Plastic cups and plates
- Plastic utensils
- Clamshells
- Blister packaging
- Bioplastics
- Plastic lids
- Motor oil bottles
- Hazardous materials containers



### Don't Flush Unwanted or Expired Medicine Down the Drain

The drugs in your medicine cabinet pose a serious health threat to humans and the environment. If these drugs fall into the wrong hands or are improperly disposed of, bad things happen.

• **Clear Your Cabinet**—All types of drugs left in medicine cabinets or anywhere in homes, are often abused and frequently stolen. Studies show that a

majority of abused prescription drugs are obtained from family

and friends, including from home medicine cabinets. Proper disposal of expired, unused and unwanted prescription drugs helps prevent drug abuse, child poisonings and theft. It is also important to keep prescription medications in safe places locked away from children.

• **Don't Flush Drugs Down the Drain**—When medications are flushed down the drain they end up at the Wilsonville Wastewater Treatment Plant. Since the facility can only partially remove chemical compounds, they pass through the treatment plant and are released into the river where they contaminate the waterways and environment.

• **Drop-Off Unwanted Drugs**—To help residents responsibly dispose of unused or expired prescription drugs, the City of Wilsonville has a Drug Take-Back Box located in the Public Works/Police building at 30000 SW Town Center Loop East in Wilsonville.

Community members can drop off unwanted prescription drugs, Monday-Friday, 8 am-4:30 pm,

excluding holidays. The City does not accept any sharp objects or needles at this location. The drug take-back service is free and anonymous.

For more information about the City’s Drug Take-Back Box, contact Randy Watson, Industrial Pre-Treatment Coordinator, at 503-570-1551; [watson@ci.wilsonville.or.us](mailto:watson@ci.wilsonville.or.us).

### Make National Drug Take Back Day EVERY DAY

Help keep medications out of our water.



Properly dispose of unused prescription drugs.

Do not flush drugs down the sink or toilet.

Waste water treatment plants are not designed to remove chemicals.



The chemicals can harm fish and wildlife. Dispose of prescription drugs at this safe, convenient place:

City of Wilsonville 30000 S.W. Town Center Loop East  
Public Works Lobby / Monday - Friday 8 a.m. - 4:30 p.m.

\*\*\*\* We do not accept Sharps\*\*\*\*



## Reports from City Boards and Commissions

### City Council Seeks to Prohibit Semi-Trucks from Passing Through Wilsonville on Eastern Segment of Wilsonville Road

On Oct. 3, the Wilsonville City Council held a first reading and approved a “Restricted Truck Zone” ordinance that prohibits semi-trucks from passing through an eastern segment of Wilsonville Road between Town Center Loop East and Boeckman Road. The truck restriction allows local deliveries, but does not allow semi-trucks to use Wilsonville Road as a cut through alternative to the freeway. The through-truck restriction strives to address concerns about noise, safety and vibrations as large freight trucks pass through the largely residential neighborhoods and schools along this portion of Wilsonville Road. The proposed restriction also seeks to discourage the use of Wilsonville Road as an alternate route between Interstate 5 and Interstate 205. City Council intends to evaluate the restriction for a 90-day period after adoption to determine whether the ban is effective.

The Wilsonville City Council also held a first reading and approved an ordinance to address evolving wireless communications facilities, including small cell/distributed antenna systems siting. The City Council recognizes the need to balance the concern over potential aesthetic issues related to the proliferation of wireless communications facilities with the benefit that these providers can offer in enhancing cellular coverage for Wilsonville residents, businesses and visitors. The new code provides flexibility for the siting of needed wireless communications facilities while balancing the need to preserve City aesthetics and be in compliance with City, state and federal law. The ordinance had a second reading on Monday, Oct. 17.

The City Council adopted a resolution for a supplemental budget adjustment for the 2016-17 fiscal year that increases the overall budget and shifts funds between various funds and line items. The adjustment is needed to update the budget for an occurrence or condition that was not known at the time the budget was adopted. The adjustment includes additional funding available from last fiscal year to increase the capital project program as well as to the materials and services and capital outlay categories.

During the communications portion of the meeting City Natural Resource Manager and staff from North-

west Center for Alternatives to Pesticides gave an update on the City’s new Bee Stewards Program that is focused on improving bee habitat and promoting the health and abundance of pollinators in Wilsonville.

Under New Business the Council adopted a resolu-



Natural Resource Manager Kerry Rappold and Northwest Center for Alternatives to Pesticides staff Sharon Selvaggio share information about the Bee Stewards Program.

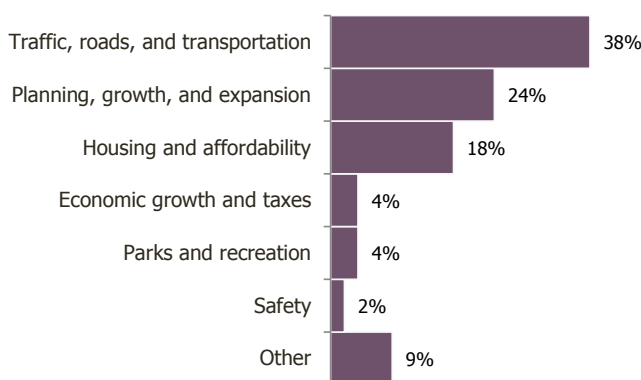
tion that approves South Metro Area Regional Transit (SMART) to purchase a seventeen-passenger bus using a \$68,195 grant from Oregon Department of Transportation (ODOT) and a matching contribution from the City of \$10,267. The bus is to be delivered within about six months of being ordered.

During work session prior to the meeting, City Council heard reports regarding infrastructure funding for the Frog Pond West planning area and an update on the road maintenance fee task force’s recommendations.

Community members are able to watch a replay of all or a portion of the City Council meetings on Comcast/Xfinity Ch. 30, Frontier Ch. 32 or on the City’s video-on-demand service at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV).

### Community Survey Results Announced, City Council Adopts Information Technology Strategic Plan

At the Sept. 19 City Council meeting findings from the recently completed National Citizen Survey™ (NCS) of Wilsonville were presented to the Wilsonville City Council by Ashley Perez de Tejada, Survey Consultant for the National Research Center (NRC) based in Boulder, Colorado. She reported the NRC found that of the 127 different standard community measurements included in the survey, 26 of Wilsonville’s ratings exceeded national benchmarks, 96 were similar and only five ratings were lower than other cities in the U.S. which also participate in the survey. The survey also asked community members what they think are the biggest priorities for the City over the next five years which were reported to be: 1. Traffic, roads and transportation (38%); 2. Planning, growth and expansion (24%), and 3. Housing and affordability (18%).



Results from the 2016 Community Survey question, “What do you think are the biggest priorities for the City over the next five years?”

The 2016 survey is the third statistically-valid questionnaire conducted for the community over the past six years, providing the opinions of a representative sample of 536 residents of Wilsonville. The local

survey also compares current results with results from over 500 communities across the U.S. as well as to past local surveys completed in 2014 and 2012. The survey is used by City Council and management as an objective means of gathering community member feedback regarding key aspects of quality of life, City governance, City services, civic participation and other special topics. For more information and complete survey results visit [www.ci.wilsonville.or.us/CommunitySurvey](http://www.ci.wilsonville.or.us/CommunitySurvey).

Under new business, the City Council adopted the Wilsonville Information Technology Strategic Plan which aims to create a plan for prioritizing technology investments into the future while providing a framework for Wilsonville to become a leader in efficient and innovative IT service delivery. For more information contact Holly Miller, IT Manager, at 503-570-1514; [miller@ci.wilsonville.or.us](mailto:miller@ci.wilsonville.or.us).

The City Council continued until the date certain of October 3 an ordinance that amends City Code to clarify and expand control of wireless communications facilities including the siting of small cell/distributed antenna systems to reflect changing technology and new regulations that will soon govern the siting of such facilities.

On the Consent Agenda the Council adopted a resolution that approves the City to extend an Intergovernmental Agreement (IGA) with Metro, Washington County and the City of Tualatin for Basalt Creek Concept Planning project that seeks to establish a jurisdictional boundary between Wilsonville and Tualatin.

During work session prior to the meeting, City Council heard reports regarding addressing truck traffic on Wilsonville Road, an update on the Boones Ferry Road to Brown Road Connector Plan, and upcoming changes to City Code to govern siting of new forms of wireless communication facilities.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during November. Scheduling changes can and do occur; check for updated meeting information on the City’s website, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, Nov. 7:** Fiber Business Plan; Transit Master Plan update; Boones Ferry Road to Brown Road Connector update; Equitable Housing Grant update; Road Maintenance Fee appeal process; Metro Councilor Dirksen update; IGA Road Authority Advance and Stafford Roads; Marijuana time, place and manner ordinance; and Zone Map amendment.

**Monday, Nov. 21:** Year 2000 Urban Renewal District; Planning Permit fees; Frog Pond infrastructure funding; Transportation System Development Charge update; Road Maintenance Fee; and Planning permit fees.

### Planning Commission Update

At the Planning Commission meeting on Wednesday, Oct. 12, the Commission asked questions and provided feedback on the Infrastructure Funding Plan for the Frog Pond Master Plan. The Commission also heard an update Basalt Creek Concept Plan the French Prairie Bridge project.

### Development Review Board Update

On Sept. 12, DRB Panel A unanimously approved the development of 10 “detached” row houses on the south side of Barber Street at Costa Circle West in Villebois.



During their September 26 meeting DRB Panel B unanimously approved the development of 16 “detached” row houses on the northeast corner of Barber Street and Costa Circle West in Villebois. As part of the request they recommend City Council approve a Zone Map Amendment to the V “Village” Zone for the subject property.



### Library Board Report for September

During the Library Board meetings in September reports included the following:

- There was a Book-to-Movie teen event in September featuring Fantastic Beasts and Where to Find Them, Doctor Strange, Miss Peregrine’s Home for Peculiar Children and Star Wars material.
- In October there was a Teen Murder Mystery event based on a Stranger Things theme.
- The search for a new Library Operations Manager is underway and there is a large, very qualified pool of candidates that is being narrowed down to a finalist.
- The Library has issued a draft Request For Proposal to identify an architect to design the Library renovations.
- The Library is rearranging the book collections



Library Director Pat Duke promotes the library during Banned Book Week.

- so there are more genre collections, better visibility of popular collections, and a reduction of underutilized collections such as magazine back issues.
- The 2016 Oregon Public Library Statistical Report was shared and discussed by the Library Board.
- In October the Library Foundation held a fundraiser and wine tasting event at the Library.

The next regular meeting is scheduled for Wednesday, Dec. 7, 6:30 pm, at the Library.

# Many Factors Impact I-5 Traffic Situation

By Nancy Kraushaar, PE,  
Community Development Director

The City of Wilsonville is taking multiple steps to improve mobility within the community and to address issues around traffic congestion and safety. Accompanying articles in this issue summarize a number of important road and alternative transportation projects being advanced by the City that provide more routes and travel options and offer suggestions on how to deal with congestion.

One specific area that frustrates many travelers is the major traffic-convergence zone of the I-5/Wilsonville Road interchange and nearby intersections of Boones Ferry Road to the west and Town Center Loop to the east. In the mid-2000s, the City worked with ODOT on a \$21 million project to improve the I-5 interchange capacity by 50% for vehicles passing through the interchange on Wilsonville Road or entering/leaving I-5. Despite these substantial improvements, when I-5 reaches maximum traffic-flow capacity during morning/evening peak-commute hours, on some weekends or when there is an incident on I-5, the interchange area can clog-up, similar to many popular I-5 or I-205 interchanges in the Portland metro region.

With the greater Portland/Vancouver metro population increasing rapidly over the past five years to 2.4 million—over 3,300 people move here every month—

**While the City's public investment in local transportation system improvements can readily handle our town's growth, the most significant traffic-congestion issues in Wilsonville arise when I-5 traffic reaches the highway's capacity.**

coupled with significant employment recovery since the end of the Great Recession, we are collectively dealing with more drivers and vehicles than ever before. More than half the region's new residents moved here from other parts of the country, the

13th largest domestic migration among U.S. metro areas. Metro regional government predicts that another 400,000 new residents are expected by 2035.

While the City's public investment in local transportation system improvements can readily handle our town's growth, the most significant traffic-congestion issues in Wilsonville arise when I-5 traffic reaches the highway's capacity. Wilsonville is especially impacted by traffic on I-5 since all of our east-west arterials lead to/from I-5 and the only freeway crossings are at Elligsen, Boeckman and Wilsonville Road. As I-5 fills during rush hour, traffic in the area of the interchanges is impacted; congestion then spreads out from the interchange area to local streets.

A main issue for Wilsonville in relation to I-5 concerns both regional/interstate traffic-movement and the local-area design of the freeway—major factors over which the City has no control.

- **Very large volume of traffic:** As the West Coast's major arterial, I-5 carries more vehicles than any other road in Oregon. Over 200,000 vehicles pass by Wilsonville each day; the I-5 Boone Bridge carries nearly as much traffic as the I-5 "Columbia River Crossing" Interstate Bridge (only four percent less) and one-third more semi-trucks. As the state and especially the greater Portland area continue to grow in population, jobs and housing costs, more people are commuting to metro-area jobs from outside the region, including the Willamette Valley.

- **Too many I-5 exits too close together:** Just south of Wilsonville I-5 has four separate sets of on/off ramps located within a span of just under two miles: Wilsonville Road, Miley Road, Highway 551 and the Rest Area. This quantity of freeway exits is double the number now allowed, since each freeway on/off introduces automobile weaving-patterns or lane-changes that cause conflicts and result in congestion—the primary cause of highway accidents. Over one-third of the traffic on I-5 in this area is entering or exiting the highway, which creates tremendous conflicts and adds to congestion.

- **Roadway conditions are inconsistent:** The Boone Bridge represents a very different roadway cross-section than the interstate conditions to the north and south. The Willamette River and constrained shoulder edge conditions introduce driver-distraction from the typical I-5 landscape and wide shoulder conditions to cause traveler uncertainty that contributes to congestion.

- **I-5 geometry design:** I-5 makes a sweeping curve with substantial elevation change near the Willamette River in the vicinity of all these on/off ramps that further aggravates traffic flow and can induce incidents such as rear-end collisions the tie-up the freeway.

- **Major highway interchange merges on I-5:** Wilsonville is sandwiched between two major highways that merge on or off I-5. Highway 551 (Wilsonville-Hubbard Cut-Off, or the "I-5/99E Connector") just south of Wilsonville and I-205 to the north both start/terminate at I-5, resulting in an additional lane of traffic seeking to merge onto or depart I-5. In the case of merges, however, I-5 has no additional capacity to accept a whole other lane of traffic. These major highway connections induce further weaving lane-changes on I-5 that result in slowing and incidents that result in traffic back-ups.

These issues illustrate why Wilsonville closely monitors and may oppose proposals such as new urban-level development immediately south of Wilsonville and future potential I-5/99W Connector connections to I-5 in North Wilsonville.

Wilsonville is not giving up, however. In addition to local street improvements and quality transit services, the City is working with regional and state partners to advance a number of transportation strategies, as Mayor Tim Knapp details on page 1. Additionally, Wilsonville residents and commuters can consider other transportation strategies around routing/timing of trips and utilizing transit and alternative transportation options. Especially important for community mobility is that Wilsonville continues to enhance the transportation grid as

we move from a small rural community with "farm-to-market" roads to a modern urban city.

For more information, contact Nancy Kraushaar, PE, Community Development Director, at 503-570-1562; kraushaar@ci.wilsonville.or.us.



Map illustrating I-5 exits in the Wilsonville area. Each interchange on/off ramp introduces automobile weaving lane-changes that are a major contributor to traffic congestion and highway incidents.

## Road Projects Update: More Road and Pathway Investments Planned for 2016-17

The next 12 months are going to be very busy for the City of Wilsonville's Engineering Division. Following are some of the projects that City staff is currently working on:

**Boones Ferry to Brown Road Connector Project:** Concept planning for an extension of Brown Road to Boones Ferry Road is well underway and scheduled to be complete in early 2017. This roadway design projects seeks to help alleviate motor-vehicle congestion on Wilsonville Road west of I-5. To learn more about the proposed roadway, go online to Boones FerryToBrownRoad.org.

**French Prairie Drive Pathway:** This recently-completed walkway project (see photo below) included repair of the pathway along French Prairie Drive in Charbonneau. Trip hazards, drainage issues and sidewalk ramp areas have been made to meet ADA standards and address safety concerns.

**French Prairie Bridge Project:** This bridge design project determines the final location, alignment,



and bridge design type for a new bicycle/pedestrian/emergency responder crossing of the Willamette River west of the I-5 Boone Bridge. This fall the City is convening a task force to begin evaluation and preliminary design of the proposed bridge. In addition to daily bike/ped use, the bridge is to serve as an alternate route for emergency responders during I-5 incidents. The project includes public open houses and other opportunities for public input throughout the various stages of the process. For more information visit [www.ci.wilsonville.or.us/FrenchPrairieBridge](http://www.ci.wilsonville.or.us/FrenchPrairieBridge).

**Kinsman Road Extension:** This summer construction began on the road extension project between Barber and Boeckman with completion expected by the June of 2018. The road extension project provides an additional north-south roadway connection on the City's growing westside, linking Boeckman Road with Barber Street. The project features a two-lane roadway with bike lanes, sidewalks, and transit stop improvements, along with construction of a roundabout at the Kinsman Road/Boeckman Road intersection.

**Tooze Road Widening Project:** The road widening project between Villebois Drive and Graham's Ferry Road is planned to begin in the summer 2017 and to be complete by fall 2018. The project, located along the northern boundary of Villebois, is west of the Villebois Drive/Boeckman Road roundabout. The street-widening project includes a two-lane road with median, bike lanes, sidewalks and a traffic signal at Grahams Ferry Road.

**Villebois Drive:** The remaining portion of the roadway through the center of the Villebois is to be completed by Polygon NW and open in 2017.

**124th Avenue Extension/Basalt Creek Parkway:** Funded by a special property-tax in Washington County, the project which is now under construction just north of the Coffee Creek Correctional Facility, includes a 2.3-mile, two-lane arterial between Tualatin-Sherwood Road in Tualatin and Grahams Ferry Road in Wilsonville. The project also constructs safety improvements on Tonquin Road and Grahams Ferry Road, for a grand total of 4.38 miles of new or improved roadways. For more information, see [www.124thProject.com](http://www.124thProject.com).

For more information about any of these projects visit [www.ci.wilsonville.or.us/projects](http://www.ci.wilsonville.or.us/projects) or call 503-682-4960.



Excavator at work on the Kinsman Road Extension project.

## Bring your Leaves to Leaf Drop-Off Day on Saturday, November 19

The City of Wilsonville in cooperation with Republic Services is holding the fifth annual Leaf Drop-Off Day on Saturday, Nov. 19, 9 am–2 pm, in the City Hall parking lot, 29799 SW Town Center Loop East.

Republic Services, Wilsonville's waste hauler, is supplying large drop boxes for the event. City employees will be on hand to assist residents with disposing of leaves. To haul and drop off the leaves the City suggests using

**Wilsonville Leaf Drop-Off Day**  
 • Sat, Nov 19, 9 am–2 pm  
 City Hall Parking Lot  
 29799 SW Town Center Loop E

large recyclable paper bags that are available at local hardware stores — rather than using large plastic bags that are not recyclable.

Community members are reminded to not rake leaves into the street as they become a major problem for the storm sewer systems. The leaves clog grates and pipes that contribute to back-ups and/or flooding and they reduce flow when accumulating in pipes. When leaves reach our creeks and rivers they also do harm by using

oxygen that can affect the viability of aquatic life. Excess accumulation of leaves on streets also causes a safety problem for motorists.



The event is also a fundraiser for Wilsonville Community Sharing (WCS), a local non-profit emergency service provider and referral agency in Wilsonville. City staff will be on hand to help unload leaves and a WCS volunteer will be collecting cash donations.

## Donated Toys Brighten Holidays for Families in Need

The Parks and Recreation Department is pleased to announce the third annual toy drive begins on Monday, Nov. 7! Last year, more than 200 new toys were donated by community members. Bring new, unwrapped toys to the Parks and Rec Admin Building in Town Center Park, 29600 SW Park Place, Monday–Friday, 8 am–5 pm, Nov. 7 – Dec. 14. Toys are also being accepted during the Holiday Tree Lighting Ceremony on Wednesday, Dec. 7, as well as, the Reindeer Romp Fun Run on Saturday, Dec. 10. The toys are to be donated to Toy & Joy/Compassion in Action Clackamas County, a charitable organization that delivers toys to local children in need.



## I-5 Connection Holiday Concert

The I-5 Connection Chorus is holding a holiday concert at 3 pm on Saturday, Dec. 3, at the Meridian United Church of Christ, 6750 Boeckman Road in Wilsonville.

**I-5 Connection Holiday Concert**  
 • Sat, Dec 3, 3 pm  
 Meridian United Church of Christ  
 6750 SW Boeckman Road

Director Jocelyn Higgins leads the group in singing a mix of sacred and secular songs for the afternoon's program, "Home for the Holidays," with talented accompanist, Katie Turner. Come early to enjoy special music performed by musicians and sing a few Christmas songs before the concert.



The group, comprised of 50 singers, practices Thursdays at the Community Center, 10 am–12 pm. For information, contact Jocelyn Higgins at [jphiggins@comcast.net](mailto:jphiggins@comcast.net) or Patty Brescia, Recreation Program Manager at 503-682-3727; [brescia@ci.wilsonville.or.us](mailto:brescia@ci.wilsonville.or.us).

## Christmas Decoration Sale Saturday, December 3

Wilsonville Garden Club's annual Swag Sale returns on Saturday, Dec. 3, 9 am–2 pm, while supplies last in the Oak Room at the Wilsonville Public Library, 8200 SW Wilsonville Road.



The items for sale, hand made by club members using fresh greens, include a beautiful selection of handmade swags, centerpieces, wreaths and Yule logs to brighten your holiday spirits and décor.

Wilsonville Garden Club is tax exempt and a member of Oregon State Federation of Garden Clubs.

All proceeds from this sale go to their annual winter fund raiser to support horticulture education and scholarships

# City Calendar

## NOVEMBER

|          |  |
|----------|--|
| 1 Tue    | • Municipal/Traffic Court, 5 pm, City Hall<br>• Transit Master Plan Task Force Meeting, 6–7:30 pm, City Hall                                   |
| 7 Mon    | • Toy Drive Begins, Drop Off Toys at the Parks and Rec Admin Building, 8 am–5 pm.<br>• City Council, City Hall 5 pm Work Session, 7 pm Meeting |
| 8 Tues   | • General Election Ballots due by 8 pm. The Library is an official Drop Off Site   |
| 9 Wed    | • Planning Commission, 6 pm, City Hall<br>• Wilsonville Community Seniors Inc. Advisory Board., 6 pm, Wilsonville Community Center             |
| 11 Fri   | • Veterans Day Holiday, City Offices and Library CLOSED  |
| 14 Mon   | • DRB Panel A, 6:30 pm, City Hall  |
| 15 Tue   | • Municipal/Traffic Court, 5 pm, City Hall   |
| 17 Thurs | • DEQ Hearing on biogas facility, 6:30 pm, Wilsonville Public Library  |
| 19 Sat   | • Wilsonville Leaf Drop-Off Day, 9 am–2 pm, City Hall Parking Lot  |
| 21 Mon   | • City Council, City Hall 5 pm Work Session, 7 pm Meeting  |
| 24 Thurs | • Thanksgiving Holiday, City Offices and Library CLOSED, no SMART service.   |
| 25 Fri   | • Thanksgiving Holiday, City Offices and Library CLOSED, no SMART service.   |
| 28 Mon   | • DRB Panel B, 6:30 pm, City Hall  |



## DECEMBER

|         |   |
|---------|---|
| 1 Thurs | • Tourism Promotion Committee, 10 am–12 pm, City Hall   |
| 5 Mon   | • City Council, City Hall 5 pm - Work Session, 7 pm – Meeting   |
| 6 Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |
| 7 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library  |
| 8 Thurs | • Tourism Promotion Committee, 10 am–12 pm, City Hall<br>• Parks and Recreation Board Meeting, 4:30 pm, Parks and Rec Admin Building  |
| 12 Mon  | • DRB Panel A, 6:30 pm, City Hall   |
| 14 Wed  | • Last Day for the Toy Drive, Drop off toys at the Parks and Rec Admin Building by 5 pm.<br>• Planning Commission, 6 pm, City Hall<br>• Wilsonville Community Seniors Inc. Advisory Board, 6 pm, Wilsonville Community Center |
| 19 Mon  | • City Council, City Hall 5 pm - Work Session, 7 pm – Meeting   |
| 20 Tue  | • Municipal/Traffic Court, 5 pm, City Hall  |
| 26 Mon  | • Christmas Holiday, City Offices CLOSED  |
| 31 Sat  | • New Year's Eve  |

All dates and times are tentative; check the City's online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and are replayed periodically. Meetings are also available to stream live and by video-on-demand at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.



**Tim Knapp**  
Mayor  
[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



**Scott Starr**  
City Council President  
[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



**Julie Fitzgerald**  
City Councilor  
[fitzgerald@ci.wilsonville.or.us](mailto:fitzgerald@ci.wilsonville.or.us)



**Susie Stevens**  
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**Charlotte Lehan**  
City Councilor  
[lehan@ci.wilsonville.or.us](mailto:lehan@ci.wilsonville.or.us)

### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)

### Wilsonville City Hall

29799 SW Town Center Loop E  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)

**Police Non-Emergency Dispatch:** 503-655-8211



# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

September 2016

## City Recruits Members for 2017 Citizens Academy

The City of Wilsonville is now recruiting community members to participate in the 2017 Citizens Academy, formerly known as the Wilsonville Leadership Academy. The monthly City-hosted community-engagement program is designed to prepare community members for roles on local boards and commissions, as well as area nonprofits.

Mayor Tim Knapp states, "This program is a great way for community members to become more familiar with the City and to explore the many volunteer opportunities that exist to make a difference in Wilsonville."

The six-month-long program runs January to June

and offers participants the opportunity to learn about the City's programs and operations, meet City staff and elected officials and get to know fellow community members. The monthly meetings will be held on the third Thursday of each month, 6-9 pm, at City Hall.

Applications for the program are now being accepted through the end of October and the program is limited to no more than 20 participants.

For more information, contact Angela Handran, Community Outreach Specialist, at 503-570-1503; Handran@ci.wilsonville.or.us; or visit [www.ci.Wilsonville.or.us/Academy](http://www.ci.Wilsonville.or.us/Academy).



Members of the 2016 Academy pose for a photo with firefighters at Tualatin Valley Fire and Rescue Station 56.

## SMART Wants to Hear Your Ideas on Improving Transit Service

As part of the local public transit agency's update to the Wilsonville Transit Master Plan, South Metro Area Regional Transit (SMART) is asking community members and commuting workers:

- How should Wilsonville balance intercity and local service?
- Does Wilsonville need more bus service for trips out of town or more trips within the City?

The answers to these questions and others are



needed to help SMART update the plan that guides the City's transit service over the next five years.

As part of the project that began early this year, SMART has heard from over 750 participants through surveys, workshops, focus groups, presentations and

other outreach. Key themes that are emerging from public feedback include:

- Expand Saturday and evening service
- Faster east/west crosstown service
- Increase service to Villebois
- Expand service to Salem
- Better and more frequent connections to the north
- Improve timing of connections with Canby Transit for Oregon City

Based on community input, SMART transit service planners have developed two alternatives to help gauge what best suits Wilsonville.

SMART invites community members to complete a survey by either visiting a mobile survey station located at City Hall, the Library, SMART offices or by taking the survey online at [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com). A Spanish version is also available.

This fall a draft of the Transit Master Plan is to be presented to City Council for discussion and potential adoption by the end of the year. For more information or to sign up for e-updates, visit: [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com).

## South Korean Delegation Visits Wilsonville

A nine-member delegation from Yeosu City of South Korea visited Wilsonville in mid-August as part of a business and trade mission to the United States that also included a stay in Chicago. Yeosu City Mayor Won Kyeong Hei and visitors, along with City officials and Korean War veterans, visited the Oregon Korean War Memorial in Town Center Park.

In honor of the visiting delegation, Mayor Tim Knapp proclaimed August 15, 2016 as "Yeosu City Day in Wilsonville" to Celebrate International Peace and Cooperation," and provided each visitor with an Honorary Wilsonville Community Citizen card.



Yeosu City Mayor Won Kyeong Hei (center with red tie) is flanked by his team and Wilsonville City Council members.

### Mayor's Message:

## Nine Out of 10 Residents Prefer to Keep French Prairie Rural

Over the summer months just prior to the 4th of July holiday, Clackamas County held public open-house events on removing the Rural Reserve protections for over 1,600 acres of prime farmland, including 800 acres of the French Prairie Rural Reserve just south of Wilsonville. Hundreds of area residents attended to provide comments at the open houses and subsequently via online survey; of the total set of 736 comments submitted, over 73% originated from the Aurora-Wilsonville area.



Mayor Knapp speaks with constituents at the Open House.

And the results were as follows: over 90 percent of area residents "strongly disagree" with the proposal by the Clackamas County Board of Commissioners to remove the protection of the French Prairie Rural Reserve.

The Wilsonville City Council was pleased to see such a high level of public interest and direct engagement on this important public-policy issue. Wilsonville continues to support the original 2010 French Prairie "Rural Reserve" designation, and opposes efforts to overturn a thorough, two-year-long public-input process that resulted in a reserves agreement with Clackamas County, Metro and the City to protect the high-value farmlands of French Prairie.

As succinctly stated in 2009 by seven state agencies that unanimously supported the protection of "lands south of the Willamette River (French Prairie) as a rural reserve":

"The reasons for a rural reserve designation include: threat of urbanization, high suitability for agriculture, very significant transportation limitations (Boone Bridge capacity and no alternate river crossing, poor multimodal connectivity), poor suitability for urbanization (services and distance to existing population), and concerns about encouraging urban development moving south along I-5 into prime agricultural lands."

Continued growth throughout the region since 2009 has highlighted the limitations of our combined transportation system, and the continuing need for thoughtful, responsible planning for industry and employment growth in appropriate areas. Wilsonville continues to do exactly that with our planning for the regionally recognized Coffee Creek and Basalt Creek industrial employment areas. We see no good reason to abandon this responsible work, or to replace it with high public cost, unplanned and unsupported development in a problematic area south of the Willamette River.

*Tim Knapp, Mayor*

### In This Issue...

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- City Calendar of Events. . . . . 8

...and more!

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Wilsonville, OR 97070

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## Imagination Library Helps Young Kids Prepare for School

The Wilsonville Library and the Kiwanis Club are helping parents and caregivers with young children prepare children for school by promoting participation in a free book club called Imagination Library.



Through this program, founded by Country Music Singer Dolly Parton, Wilsonville kids under the age of five get a free high-quality, age-appropriate book mailed to their home every month until their 5th birthday. In fact if a child is signed up as a new born they can get up to 60 free books.

To start building your baby a personal library register at [www.ImaginationLibrary.com](http://www.ImaginationLibrary.com) or in person at the Wilsonville Public Library.

Research shows that children who receive these high-quality books are more prepared for kindergarten and are more successful students than those who were not enrolled.

Even if you do not have kids at home help Wilsonville families learn about this program by liking “Wilsonville Imagination Library” on Facebook to help share the news about this exceptional program.

## Talk to Kids About Fire and Life Safety Before They are Home Alone

It's that time again — when Wilsonville students will head back to class. And for many students, this may be the school year where they will be trusted to be home alone after school.

Tualatin Valley Fire & Rescue encourages parents and caregivers to review fire and life safety tips with them.

Talk to your kids about how and when to call 911 for police, fire, or medical assistance in the event of an emergency. Children should know their address, phone number, and your contact information so that they can provide that information to the emergency dispatcher.



It's also a good idea to have phone numbers for family and neighbors posted on the refrigerator, near the phone, or on a designated bulletin board in the event kids need assistance but it's not an emergency.

This is also a good time to set house rules on using the stove or microwave. Cooking is one of the leading causes of home fires. Most of the fires start on the stovetop due to food or oil being left unattended while being heated. The following tips may help your child avoid a fire:

- Allow cooking privileges based on age and maturity. Consider allowing younger children to only use a microwave or oven to heat food (versus the stovetop). However, use caution when heating food in a microwave due to the potential for burn injuries. If allowed to use the stovetop, never leave food (especially oil) unattended – even for a moment! Remind children to stay by the stovetop when cooking. If they need to leave – turn off the burner.
- Keep combustibles such as potholders, dishtowels, food boxes, magazines away from the stovetop.
- Water and oil do not mix! Never throw water on a

grease fire or try to move the flaming pan. Either action can cause the fire to spread or cause burns to a body.

- If a fire occurs, leave the home and call 911 from a cellphone or a neighbor's home.
- Other safety tips to share include:**
- Never use matches, lighters or candles. Keep these items away from children and remind them that they are not to be touched by anyone other than an adult.
  - Know two escape routes from every room in the house. One of those could be a window. Create and practice a home fire escape plan.
  - Do not place combustibles like clothes, blankets, or scarves, over a lamp.
  - Do not play with electrical cords, and do not insert anything other than a plug into an electrical socket.
  - Never mix cleaning products.
  - Know where first-aid supplies are located, and consider signing up older children for a first-aid and CPR class.

For additional fire and life safety information, visit [www.tvfr.com](http://www.tvfr.com).

## Handwoven Textiles Featured at Wilsonville Library

In September the Wilsonville Library is featuring *Weaving for the Home & Me too!* by Oregon handwoven textile artist Marlene Lloyd. The art show runs Sept. 1–30 in the main lobby of the library located at 8200 SW Wilsonville Road.

Marlene Lloyd draws her color and texture inspiration from nature, her walks in urban neighborhoods, parks, mountains and the desert. Ms. Lloyd has earned multiple awards for her textiles and has been featured in *Handwoven Magazine*. Her work is in private collections across the country. The free art exhibit is open during Library hours.



## Be the First to Know!

Sign up for City eNotify (e-news):  
[www.ci.wilsonville.or.us/enews](http://www.ci.wilsonville.or.us/enews)

Follow the City on Twitter:  
[@WilsonvilleOR](https://twitter.com/WilsonvilleOR)

Like the City's Facebook page:  
[www.Facebook.com/CityofWilsonville](http://www.Facebook.com/CityofWilsonville)



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

## Children's Fall Programs Return Sept. 6

Starting Monday, Sept. 6, story times for all ages return for the fall session.

**Baby Time** features books, rhymes, and songs for parent and baby ages 0–12 months. Baby Time occurs Tuesday mornings, 11–11:45 am (including playtime).

For children ages 1–2 years old, **Toddler Time** offers stories, bubbles and parachute time. Toddler Time occurs Tuesday mornings, 10–10:30 am.

Bring the whole family to **Storytime** for crowd-pleasing fun with books, stories, and songs.

Storytime occurs four times a week:

- Tuesday evenings 6:30–7 pm
- Wednesday mornings 10:30–11 am
- Wednesday afternoons 1–1:30 pm
- Thursday mornings 10:30–11 am

For more information about these programs, contact Youth Services Librarian Steven Engelfried at 503-570-1592; [engelfried@wilsonvillelibrary.org](mailto:engelfried@wilsonvillelibrary.org) or visit [www.WilsonvilleLibrary.org/kids](http://www.WilsonvilleLibrary.org/kids).



## New Workshops Focus on Memoir, Finance and Wellness

The library has a variety of free workshops in September for improving your life.

Jump into memory and let your stories emerge with “**The Story Collector**” memoir workshop on Saturday, Sept. 17, 1–3 pm. Bring a journal, your favorite pen or pencil and a beloved item.

Start building a passionate life with “**Creating a Wellness Vision**” on Thursday, Sept. 29, 6:30–7:30 pm. Learn how to create a wellness vision to help make your aspirations for a healthy and passionate life come true.

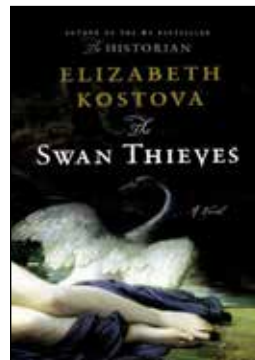
Worried about affording college? “**Financing College Costs**” discusses options and considerations for paying for college without going broke on Thursday, Sept. 22, 6:30–7:30 pm.

For more information about the free workshops visit or contact the library at 503-682-2744 or online at [www.WilsonvilleLibrary.org/classes](http://www.WilsonvilleLibrary.org/classes).

## “The Marriage of Art and Fiction” Class

What happens when art and fiction interact? How does their relationship evolve or devolve? Local author/illustrator Nicole Rubel leads this innovative class which examines novels with dominant themes of art or an artist and their work. Please read “The Swan Thieves” before the first class.

The \$40 class series is held Wednesdays, Sept. 7–Oct. 26, 6–8 pm. Texts available for purchase at a discount. Register at the Circulation Desk, call the Library at 503-682-2744 or visit [www.WilsonvilleLibrary.org/classes](http://www.WilsonvilleLibrary.org/classes).



## Upcoming Classes and Events

**Library closed Monday, Sept. 5**

**Local Artist of the Month:**  
Marlene Lloyd

### First Friday Film

The story of pioneer Indian mathematician, Srinivasa Ramanujan, and his friendship with mentor, Prof. G.H. Hardy.  
Friday, Sept. 2, 6–8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

### Spanish: Beginning Level 2

Wednesdays, Sept. 7–Nov. 16, 6–8 pm  
[www.WilsonvilleLibrary.org/classes](http://www.WilsonvilleLibrary.org/classes)

### Book Club

*Short Nights of the Shadow Catcher*  
by Timothy Egan  
Thursday, Sept. 8, 6–8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

### Genealogy Club

Monday, Sept. 19, 1–2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

### Great Books Discussion Group

*Principles of Government* by Montesquieu  
Tuesday, Sept. 20, 6–8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

### History Pub at Wilsonville McMenamins

Tuesday, Sept. 27, 6:30–8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

### Game Night

Wednesday, Sept. 28, 6–8 pm  
[www.WilsonvilleLibrary.org/games](http://www.WilsonvilleLibrary.org/games)

## Chief's Corner: Be Safe as Kids Head Back to School

Each September our schools are met with children ready and eager to learn. Of course this also means that many of our streets, sidewalks and bike lanes around schools will also be occupied by kids, especially right before and after school.

In order to ensure that our children arrive and return safely to and from school, now is a good time of the year for some back-to-school safety tips for both kids and adults.



### For kids headed to school remember:

- Walk or ride in groups to be seen by drivers.
- If possible, walk or ride to school with an adult family member or caregiver.
- Learn about and take the "Safe Route to School."
- Cross streets at major intersections with crosswalks, school safety guards and stop lights.
- Before crossing an intersection, check for vehicles by looking left, right and left again.
- Wait for vehicles to come to a complete stop before crossing in front of traffic.

If everyone does their part and follows these safety tips, I am confident we can have an accident-free school year.

**Chief Jeff Smith**

P.S. The chart below shows how long it actually takes to stop when you are traveling over 20 miles per hour.

### For motorists remember that:

- Pedestrians always have the right of way, whether or not the pedestrian is in a crosswalk.
- Crosswalks exist at any public street intersection, whether they are marked or unmarked.
- Crosswalks may also exist between intersections if they are marked with white painted lines.
- Drivers must stop and remain stopped for pedestrians until they are out of the lane in which you are traveling/turning and the next lane.
- Drivers must stop and remain stopped for pedestrians until they are cleared the lane into which your vehicle is turning and at least six feet of the next lane.
- As a driver, always signal your intentions when turning.
- When driving through a school safety zone, the speed limit is 20 MPH during times or conditions as outlined on the speed sign.



See the following link for further information:  
<https://www.portlandoregon.gov/transportation/article/126818>

**At 20 mph** you will stop in front of the first child.

**At 25 mph** you not only will have hit the first two children, but you will have increased their chances of a major injury\* to 60%.

**It doesn't matter** how good a driver you are. The faster you go, the longer it takes to stop.

**Driving 5 miles over** the speed limit may not seem like a big deal, until someone steps out in front of you.

**Killing or injuring a child – now that's a big deal.**

**Show you CARE! Drive the Speed Limit.**

\*A major injury is an incapacitating injury such as broken bones, severe cuts, and unconsciousness, with the potential for being permanently disabling.

|                  |                     |
|------------------|---------------------|
| 64 feet to stop  | Traveling at 20 mph |
| 86 feet to stop  | Traveling at 25 mph |
| 112 feet to stop | Traveling at 30 mph |
| 138 feet to stop | Traveling at 35 mph |
| 170 feet to stop | Traveling at 40 mph |

Source: Oregon Department of Transportation

## SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
 28879 SW Boberg Road (office)  
 503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

### Walk @ Lunch - Sept. 7, 2016

Join SMART at noon on Wednesday, Sept. 7, at the Murase Plaza water feature on Wilsonville Road. The walk begins at 12:15 pm.

This walk is through the park, community gardens, dog park and along the wooded trail of Memorial park along the river. Most of this walk is not paved and takes about 50 minutes to complete. Come join the walk and get an award for participating. Meet new friends and do some lunchtime networking. Remember to bring your walking shoes to work with you!



### Enjoy Wilsonville trails? Want to help improve them?

Wilsonville is looking for volunteers Sept. 13–17 to help take part in the Metro region trail counts. Every September, volunteers throughout the region gather along trails to count and survey people biking and walking on the area's system of trails, parks and streets. These counts are part of the National Bicycle and Pedestrian Documentation Project's ongoing effort to gather accurate trail use data to decide where and when to build new trails and respond to the needs of trail users.



Walking is the number one recreational activity in the State of Oregon and bicycling is quickly becoming a key mode of commuting. The growing network of bicycle

and pedestrian pathways supports active transportation, offers recreational opportunities and helps the region grow in a way that improves livability. Wilsonville expands on these efforts and was recently designated as a Walk Friendly Community by the Bicycle Information Center. SMART would love to have help gathering data so we can continue to improve our city's walkability and connectivity.



SMART is looking for volunteers to help count at various locations within Wilsonville. Sept. 13–17, 9–11 am and 4–6 pm.

**Contact Nicole Hendrix**  
[Hendrix@ridesmart.com](mailto:Hendrix@ridesmart.com)  
 503-682-7790 x1497 or sign up online at [rideSMART.com/BikePed2016](http://rideSMART.com/BikePed2016)

### 8X & 9X Routes are Discontinued

Due to low ridership on the early morning 8X and late night 9X, these routes are to be discontinued effective Friday, Sept. 2.

### New Departure Time From Salem

The departure time at the 1X's Market Street/Savage Road stop in Salem is being adjusted to be four minutes earlier. These changes take effect on Sept. 6. See the online schedule for full details at [www.ridesmart.com/1Xschedule](http://www.ridesmart.com/1Xschedule).



**Our History: Emery Aden Works as West Linn School Bus Driver**



This Boones Ferry Historical Society photo shows Emery Aden next to a West Linn School District Bus in 1937 or 1938.

**Freeman Named New Principal at Boones Ferry Primary**

The West Linn-Wilsonville School District announced that Ms. Angela Freeman is the new principal at Boones Ferry Primary School in Wilsonville. Ms. Freeman began as a teacher in Portland schools, serving at both the primary and middle levels. She then moved to Washington where she served as an assistant principal at a middle school in the Evergreen School District and as a primary school principal in Ridgefield School District. Most recently Angela has been the principal of a K-8 school in the Beaverton School District. Soon to complete her doctorate, Angela states her love for students and the pursuit of equity drive her to be a school leader.

Angela is replacing former Principal Jennifer Patterson who accepted a leadership role with the Portland Public Schools.

**Eight-Year-Old Wilsonville Boy Sells Berries to Help Foster Kids**

During the summer months many kids open a lemonade or other similar stands to raise money to buy a toy or a tasty treat. Last summer Brayden Arsenault also dreamed of having a stand of his own. With the help of an abundance of berries from his grandpa's berry patch, he opened up Brayden's Berry Stand in front of his home in Wilsonville.

Shortly after opening the stand, at church he learned about a program called Welcome Boxes led by Embrace Oregon, a Portland-based nonprofit that supports kids in foster care in partnership with the State of Oregon's Department of Human Services. Welcome Boxes are given to children entering foster care while they await placement in a home. The boxes include age-appropriate activities, toys

and other items to enjoy or that they may need at the foster home.

When Brayden's mom, Juliana, asked him if he would like to give some of the money from his sales to support the Welcome Boxes program he concluded, "that was a good plan."

Sales were a bit slow at first, but soon they started picking up thanks to a bit of social media promotion from his mom. Soon, in addition to sales to neighbors, friends and family began placing berry orders. When people learned Brayden would use all of his profits to make Welcome Boxes sales increased dramatically. In addition to sales, soon many community members began donating items to be included in the Welcome Boxes like hygiene items, toys, art materials and school supplies.

During the first summer Brayden set a goal of filling four Welcome Boxes, but he ended up filling 28. This summer he hoped to ex-

ceed his total last summer and fill 40 boxes, but ended up filling 102 Welcome Boxes!

What is next for Brayden? He wants to encourage other kids to "step in where they see a need."

To learn about supporting foster kids in the Portland-Metro area visit [www.EmbraceOregon.org](http://www.EmbraceOregon.org).



Boones Ferry Primary student Brayden Arsenault prepares to bring Welcome Boxes to the Department of Human Services.

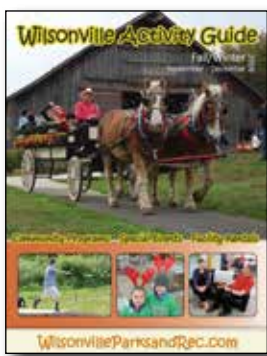
*When people learned Brayden would use all of his profits to make Welcome Boxes sales increased dramatically.*



29600 SW Park Place  
Parks Programs: 503-783-7529  
Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

**Fall Registration Now Open**

Registration is now open for fall activities and participants can register online on the Parks and Recreation website. Wilsonville Parks and Recreation offers a variety of programs and services for citizens of all ages with recreation offerings ranging from sport and fitness to life-long learning and meditation.



**Volunteer Retirement -Jean Tsokos**

After many years of service, Jean Tsokos is retiring from her volunteer work with the City. Jean has served in a variety of roles including, Wilsonville Community Seniors, Inc. Vice Chair, Senior Trips Coordinator, Acting Class Instructor and was an active participant in fundraising and special events at the Community Center. Jean also completed the Wilsonville Leadership Academy, and served on the SMART Transit Task Force. Her son and many friends attended a lunch in her honor at the Center in July, and she was presented with a poster, a card and flowers to celebrate her generosity and years of service.



**Volunteer Opportunity**

Do you enjoy helping others? Would you like to make a positive contribution to your community? The Community Center currently has an opening for a community group or business with 3-4 people who would like to help out with the senior lunch program. The commitment is on the 4th Monday of each month, 10:30 am-1 pm.

The position involves setting up tables and chairs, serving lunch to seniors, sharing lunch, and cleaning up afterward. If you are interested, please complete a volunteer application at the Community Center, or contact Recreation Program Manager Patty Brescia at [brescia@ci.wilsonville.or.us](mailto:brescia@ci.wilsonville.or.us) for more information.

**Mini Hoopers Basketball**

Mini Hoopers Basketball is a noncompetitive basketball league for 1st and 2nd graders centered around fun, skill development and fair play. Games are played 4-on-4 with an 8-foot-tall hoop.

Registration Fee: \$50  
Deadline Sunday, Sept. 25  
Season runs Oct. 24-Dec. 17



More information:  
[WilsonvilleParksandRec.com/MiniHoopers](http://WilsonvilleParksandRec.com/MiniHoopers)  
Brian Stevenson at 503-570-1523

**Upcoming Classes and Events**

**Fit Girls!**

Ages 11 and older (and bring mom!)  
Mondays, Sept. 5-Oct. 24  
7-8 pm  
Community Center  
\$59

**Hatha Yoga**

Ages 18 and older  
5:30-6:30 pm  
Community Center  
Tuesdays, Sept. 6-Oct. 25  
\$59  
Thursdays, Sept. 8-Oct. 27  
\$59

*Save \$10 when you sign up for both Tues and Thurs*

**Soccer Shots**

Saturdays, Sept. 10-Nov. 12  
Memorial Park - near tennis courts  
\$170  
Ages 2-3  
9-9:30 am  
Ages 3-4  
9:40-10:15 am  
Ages 4-5  
10:25-11 am  
Ages 5-8  
11:10-11:55 am

**Dynamic Science**

Ages: 5-12  
Friday, Oct. 14  
9 am-noon  
Tauchman House  
\$39

## Wilsonville Community Says 'Thank You for Your Service' on National Night Out in August



## September is National Preparedness Month "Don't Wait, Communicate. Make Your Emergency Plan Today"

### Why Make an Emergency Plan?

Your family may not be together if a disaster strikes, so it is important to think about the following situations and plan just in case. Consider the following questions when making a plan:

- How will my family get emergency alerts and warnings?
- How will my family get to safe locations for relevant emergencies?
- How will my family get in touch if cell phone, internet, or landlines are not working?
- How will I let loved ones know I am safe?
- How will my family get to a meeting place after the emergency?

### Here Are a Few Easy Steps to Start Your Emergency Communication Plan:

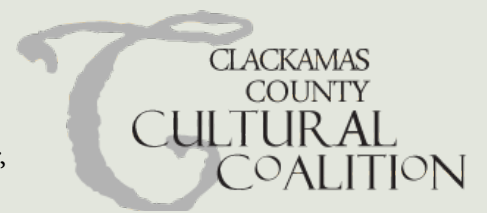
- **Understand how to receive emergency alerts and warnings.** Make sure all household members are able to get alerts about an emergency from local officials. Learn more about alerts by visiting: [www.ready.gov/alerts](http://www.ready.gov/alerts).
- **Discuss family plans for disasters that may affect your area and plan where to go.** Plan together in advance so that everyone in the household understands where to go during a different type of

disaster like a flood, earthquake or wildfire.

- **Collect information.** Create a paper copy of the contact information for your family that includes:
  - phone (work, cell, office)
  - email
  - social media
  - medical facilities, doctors, service providers
  - school
- **Identify information and pick an emergency meeting place.** Things to consider:
  - Decide on safe, familiar places where your family can go for protection or to reunite.
  - Make sure these locations are accessible for household members with disabilities or family members with other access and functional needs.
  - If you have pets or service animals, think about animal-friendly locations.
- **Examples of meeting places:**
  - In your neighborhood: a mailbox at the end of the driveway, or a neighbor's house.
  - Outside of your neighborhood: library, community center, place of worship or a family friend's home.
  - Outside of your town or city: home of a relative or family friend. Make sure everyone knows the address of the meeting place and discuss ways to get there.
- **Share information.** Make sure everyone carries a copy in his or her backpack, purse or wallet. You should also post a copy in a central location in your home, such as your refrigerator or family bulletin board.
- **Practice your plan.** Have regular household meetings to review your emergency plans, communication plans and meeting place after a disaster and then practice, just like you would a fire drill.

## Clackamas County Cultural Coalition Offers Grants

The Clackamas County Cultural Coalition seeks proposals for the 2017 Community Cultural Participation Project Grant Program. Any Clackamas County-based organization or individual working in the fields of arts, heritage and/or humanities can apply. Tax-exempt status is not a prerequisite. Deadline for grant applications is October 6, 2016. Grants are awarded in January 2017 for projects occurring between January-December, 2017. The County Cultural Plan, project grant applications, and complete guidelines are all available at: [www.clackamasartsalliance.org/grants/project-grants](http://www.clackamasartsalliance.org/grants/project-grants).



Projects related to the following funding priorities will be eligible for application:

- Creative people and organizations
- Heritage and history
- Participation in the arts, heritage and humanities
- Public art
- K-12 education and lifelong learning
- Cultural tourism

The Coalition Grant Program is made possible through funding from the Oregon Cultural Trust which is a statewide program designed to raise significant new funds to invest in Oregon's arts, humanities and heritage. For more information about this grant opportunity contact Elizabeth Klein, Public Arts Management, at 503-655-0525; [elizabeth@clackamasartsalliance.org](mailto:elizabeth@clackamasartsalliance.org).

**DON'T WAIT. COMMUNICATE.  
MAKE YOUR EMERGENCY PLAN TODAY.**



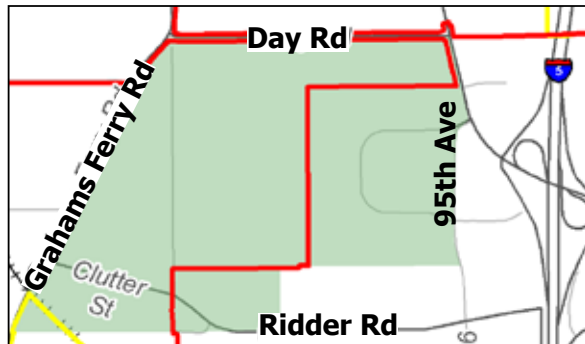
**SEPTEMBER IS NATIONAL PREPAREDNESS MONTH!**



## Reports from City Boards and Commissions

### City Council Adopts Coffee Creek Urban Renewal Plan and Approves Grant Applications

On August 1, the Wilsonville City Council held a first reading, public hearing and adopted an ordinance to approve the Coffee Creek Urban Renewal Plan. The plan, which covers approximately 258 acres of land located between Day Road, Ridder Road, 95th Avenue and Grahams Ferry Road, establishes how the City's Urban Renewal Agency proposes to fund and build the public infrastructure that is needed to transform the now largely undeveloped area into a new employment center with approximately 1,470 jobs and an estimated annual payroll of \$55 million. The plan establishes a maximum indebtedness of \$67 million, which sets the limit on urban renewal funding also known as tax increment financing, that is to be invested to build public infrastructure, including roads, sidewalks, bike lanes, and water and wastewater services. A citizen task force recommended creation of the urban renewal district, which was approved by Wilsonville voters in a November 2015 advisory vote. A second reading for the ordinance is planned for the City Council meeting on Thursday,



This map shows the tentatively approved Coffee Creek Urban Renewal District in north Wilsonville.

September 8. Prior to final approval by City Council, the plan must also be approved by the Washington County Board of Commissioners.

During the Communications portion of the meeting Tualatin Valley Fire and Rescue (TVF&R) Chief Duyck gave a "State of the Fire District" report to the City Council.

On the Consent Agenda the Council approved three resolutions. The first authorizes the City Manager to enter into a construction contract to complete street maintenance and repairs at the Wilsonville Road railroad crossing in August. Another of the resolutions authorizes staff to apply to Metro regional government for a federal transportation funding grant for the design

and construction of a proposed pedestrian and bike bridge over I-5 connecting Town Center Loop and Barber Street. Finally the Council approved a resolution authorizing the City to apply for a Metro Equitable Housing and Development Grant to fund creation of an Equitable Housing Strategic Plan.

### City Council Approves November Ballot Measures for Recreation and Aquatic Center and Continued Ban on Marijuana Facilities

At the July 7 Wilsonville City Council meeting, the Council approved two resolutions related to funding and operating a proposed recreation and aquatic center in Wilsonville. The first resolution approved by City Council places a ballot measure on the November ballot asking Wilsonville voters to consider issuing General Obligation Bonds in the amount not to exceed \$35 million to finance the design and construction of a recreation and aquatic center. If approved the approximately 80,000-square-foot facility would include community event space, aquatic, athletic, fitness and recreational facilities. For more information about the proposed center visit [www.WilsonvilleParksandRec.com/WRAC](http://www.WilsonvilleParksandRec.com/WRAC).

Later in the meeting the City Council also approved a resolution selecting Sports Facilities Management (SFM), LLC as the preferred operator to manage the proposed recreation and aquatic center if, and when the facility is approved and constructed. The firm was selected based on a Request for Proposal selection process that included interviewing four different organizations who responded to the City's recent solicitation. Among the responders was a proposal from the City of Wilsonville to operate the facility, but reviewers deemed the City's proposal was not the best alternative. The proposed partnership with SFM to operate the facility is subject to voter-approval of the capital bond measure to be voted on in November and requires the successful negotiation of a final agreement with the City. SFM is a Florida-based firm that specializes in managing and operating recreation and aquatic centers throughout the United States. SFM's proposal also includes management services to be offered through a sub-contract with by Counsilman Hunsaker, a firm based in St Louis, Missouri, that

has extensive expertise in operating aquatic facilities throughout the United States.

The City Council also approved a second ballot measure title for the November election that asks Wilsonville voters to consider whether or not the City should continue the current ban on operating marijuana-related facilities located in the City of Wilsonville. A "yes" vote continues the ban on marijuana facilities in Wilsonville and a "no" vote removes the ban.

The Council also approved a resolution authorizing the City Manager to execute a purchase and sale agreement with Washington County for 7.58 acres on SW Boberg Road to potentially become the Public Works new office, storage and equipment yard. The Public Works Department is currently co-located with the Police Department at 30000 SW Town Center Loop E in a building that does not provide adequate space for both functions.

Related to the property purchase, the City Council approved two resolutions authorizing five-year capital interfund loans from the Water Fund to the Stormwater Capital Fund (\$500,000) and the Street Capital Fund (\$400,000). The internal loans are to be used in part to purchase the site that is proposed to be the City's new Public Works facility.

During Mayor's Business, the Council approved a standard one-year employment contract extension for City Manager Bryan Cosgrove and appointed Miriam Pinoli to a four-year term on the Wilsonville Library Board ending June 30, 2020.

The City Council continued a resolution pertaining to a construction contract with Polygon Homes NW to build the Tooze Road Storm Sewer Project and the future Tooze Road Improvement Project until the Council's July 18 meeting.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during September. Scheduling changes can and do occur; check for updated meeting information on the City's website, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Thursday, Sept. 8:** Wilsonville Road traffic; Planning and Community Development fund review; Marijuana ordinance discussion; Central Villebois Zone Change; Coffee Creek Urban Renewal; IT Strategic Plan; Willamette Falls Locks; and Brayden's Berry Stand.

**Monday, Sept. 15:** Road Maintenance Fee update; Community Survey results; Wilsonville Leadership Academy; and Basalt Creek IGA extension.

### Planning Commission Update

At the Planning Commission meeting on July 13, the Commission reviewed and approved the Coffee Creek Urban Renewal Plan and also reviewed and discussed the Frog Pond Master Plan.

### Development Review Board Update

On July 25, DRB Panel B approved two 24-unit condominium buildings, one 34-unit condominium unit and 10 row houses and garages, parking areas, streets and parks in the Villebois Village Center.

At the July 11 DRB Panel A meeting, the Board approved a 12-lot subdivision, associated parks, open space and other improvements on Maxine Drive.

### Library Board Report for June & July

During the Library Board meetings in June and July reports included the following:

- Held two well-attended teen events in June, Movie/Sundae Monday and Nerf Day.
  - Announced Library Operations Manager Kim Robben is retiring in August after 27 years of service.
  - Acknowledged Alan Steiger at his last meeting after eight years of service on the board.
  - Reviewed the Library Board Report Card results.
  - Approved the Library Board 2016-17 calendar.
  - The Library Foundation is planning a fundraiser on Oct. 8.
  - Welcomed new board member Miriam Pinoli.
  - Introduced Outreach Librarian Deborah Gitlitz.
  - Reported the Teen Advisors blew up a watermelon at the last teen summer reading event and held three summer teen events including the Library After Dark, the Game Day and Party in the Park.
  - Announced that the Children's summer reading programs had a record number of sign-ups with about half of the participants also turning in the Science Adventure logs.
  - Reported the Adult Summer Reading Program also had record participation.
  - Looking for staff and community members to serve on the Boones Ferry Historical Society.
  - Installing a radio frequency identification (RFID) barcode system this winter.
  - Public Works is getting bids to install a new HVAC and other energy saving upgrades.
  - Reviewed and discussed annual library statistics.
- The next regular meeting is scheduled for Wednesday, Sept. 28, 6:30 pm, at the Library.



Miriam Pinoli

### City Council Adopts Annual Budget, Approves 10-Acre Annexation and Rezoning, Renews Social Services Annual Grant and Endorses Community Enhancement Projects

On June 20, the Wilsonville City Council held public hearings and adopted four resolutions as part of approving the City's annual budgets.

The City Council approved on second reading a set of three ordinances for an annexation, a Comprehensive Plan amendment and a Zone Map amendment for a 10.2-acre site on SW Garden Acres Road adjacent to and north of Republic Services' campus located on SW Ridder Road. The parcel is to be the future site of Republic Services' facility expansion for the proposed SORT Bio-Energy Facility that is designed to collect and convert commercial food-waste into electricity and Compressed Natural Gas and a

compost product.

Council members continued until October 3 consideration of a proposed ordinance that would amend City Code pertaining to right-of-way easement management and franchise fees.

The Council reviewed and approved two separate matters under New Business. The Council adopted a resolution that authorizes continuation of a support grant over the next fiscal year for the community-assistance and utility-relief programs operated by the nonprofit Wilsonville Community Sharing food-bank and social-services referral organization.

The Council also adopted the project funding recommendations made by the new Wilsonville-Metro

Community Enhancement Committee to advance five projects that total just under \$80,000, including: Memorial Park "dog park" relocation (\$25,000), "Bee Stewards" Wilsonville pollinator improvement (\$21,433), multifamily communities waste-reduction and recycling (\$16,000), historical Frog Pond Church streetscape improvement (\$10,000) and a fluorescent mercury-lamp business recycling program (\$7,110).

During Mayor's Business of the Council meeting, the Council approved a contract extension for the City's special-projects attorney and rescheduled the City Manager employment contract renewal for the next Council meeting on July 7.

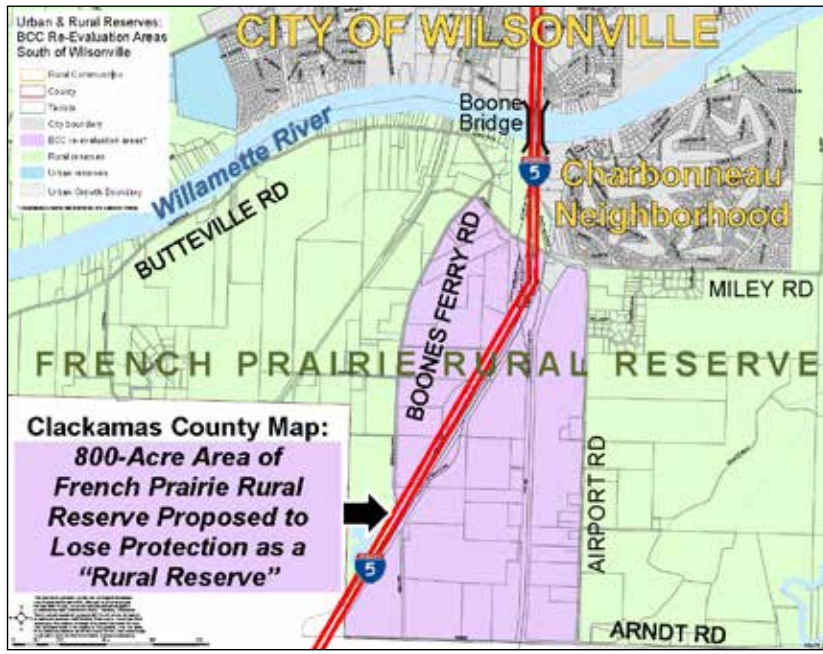
# Hundreds Attend Open House on Proposal to Remove Protection of French Prairie Rural Reserve

An open-house event held in late-June in Wilsonville to provide the public with information on a proposal to remove the protection of the French Prairie Rural Reserve drew approximately 400 area residents. The Clackamas County Board of Commissioners are attempting to remove the “rural reserve” protection of 800 acres of French Prairie “foundation farmland” immediately south of Wilsonville, and an additional 825 acres near Canby and Springwater area.

The City of Wilsonville opposes efforts to overturn the results of a thorough, two-year long public-involvement process that produced reserves agreements with Clackamas County, Metro and Wilsonville to protect the high-value farmlands of French Prairie. The City seeks to preserve the farmland and economy of French Prairie, location of Oregon’s best agrarian lands, that sustain an active, traded-sector agricultural industry; removing the “rural reserve” designation eliminates the area’s long-term protection. The state has estimated that road transportation costs alone to develop south of the Willamette River at well over \$500 million, bringing considerable new traffic congestion to the South I-5 corridor and the Boone Bridge

area. The City seeks to develop already-designated, planned industrial lands to the north towards the population centers of the Portland metro region (Coffee Creek and the Basalt Creek areas), and advocates for developing the 8,000 acres of vacant “greenfield” and redevelopable industrial sites that are sitting idle and awaiting capital investment.

For more information, see the City’s website at [www.ci.wilsonville.or.us/reserves](http://www.ci.wilsonville.or.us/reserves).



## City Works with Utility Customers to Maintain Service

One of the most frequent phone calls the City receives is about the delinquency procedures for water and sewer service. The City’s utility bills are mailed on a monthly basis. Each customer receives a detailed monthly statement with the due date noted in the account information section of the bill, which is typically the last working day of the month that the statement is received.

If a payment is not received prior to the next billing cycle, a warning notice appears in the information section of the next month’s statement. This is the first delinquent notice a customer receives to remind them they have a past due balance.

Below is an explanation of the City’s current billing process. In total, the City allows an additional 25 days from the original due date before the service is disconnected. On shut off day, water service will be restored when the past due balance is paid in full by 3:30 pm. If a customer attempts to

restore water service on their own, they could be assessed a \$500 fee for tampering with a meter.

**The City works with utility customers by providing the following options:**

- Extended due dates upon a customer request.
- Provide payment plans for people with

water leaks or unexpected high usage.

- Referral to Wilsonville Community Sharing for customers experiencing a financial hardship.

**Things customers can do to help:**

- Call if they are missing their monthly bill.
- If they receive a delinquent notice, call to verify current account status.
- Report new mailing addresses, email addresses, and phone numbers.
- If moving, complete a stop service form.

The vast majority of Wilsonville’s 5,900 utility customers pay their bills on time. The City grants approximately 50 payment extensions per month. On average the City shuts off less than 25 homes each month.

The City strives to provide excellent customer service and if there is ever a billing question please don’t hesitate to contact us at 503-570-1610 or [utility@ci.wilsonville.or.us](mailto:utility@ci.wilsonville.or.us).

| July '16 |    |    |    |    |    |    |
|----------|----|----|----|----|----|----|
| Su       | M  | Tu | W  | Th | F  | Sa |
|          |    |    |    |    | 1  | 2  |
| 3        | 4  | 5  | 6  | 7  | 8  | 9  |
| 10       | 11 | 12 | 13 | 14 | 15 | 16 |
| 17       | 18 | 19 | 20 | 21 | 22 | 23 |
| 24       | 25 | 26 | 27 | 28 | 29 | 30 |
| 31       |    |    |    |    |    |    |

| August '16 |    |    |    |    |    |    |
|------------|----|----|----|----|----|----|
| Su         | M  | Tu | W  | Th | F  | Sa |
|            | 1  | 2  | 3  | 4  | 5  | 6  |
| 7          | 8  | 9  | 10 | 11 | 12 | 13 |
| 14         | 15 | 16 | 17 | 18 | 19 | 20 |
| 21         | 22 | 23 | 24 | 25 | 26 | 27 |
| 28         | 29 | 30 | 31 |    |    |    |

| September '16 |    |    |    |    |    |    |
|---------------|----|----|----|----|----|----|
| Su            | M  | Tu | W  | Th | F  | Sa |
|               |    |    |    | 1  | 2  | 3  |
| 4             | 5  | 6  | 7  | 8  | 9  | 10 |
| 11            | 12 | 13 | 14 | 15 | 16 | 17 |
| 18            | 19 | 20 | 21 | 22 | 23 | 24 |
| 25            | 26 | 27 | 28 | 29 | 30 |    |

Water used

Billed usage  
Due date for bill

Delinquent notice sent  
Warning call  
Final warning call  
Shut off

## Don't Forget to Register to Vote By October 18

Oregon’s General Election is Tuesday, Nov. 8, 2016. Oregonians have until Tuesday, Oct. 18, 2016 to register to vote or change their party affiliation before the November election.

**You may register to vote if you are:**

- A resident of Oregon;
- A United States citizen; and
- 17 years of age or older, although a ballot will not be issued until an election occurs on or after your 18th birthday.

If you move (even if you just move to a new apartment in the same complex) or if you change your mailing address you need to update your voter registration card. Also if you change your name or if you wish to change your political party affiliation.

You can check if you are registered or change your party affiliation online at either the Secretary of State’s



website or the Clackamas County Election’s Division website using the following links: [www.OregonVotes.gov](http://www.OregonVotes.gov) or at Clackamas County Election’s Division website at: [www.clackamas.us/elections](http://www.clackamas.us/elections).

Hard copy voter registration forms can be found at post offices, public libraries, city halls, schools, DMV offices and Clackamas County Elections, located at 1710 Red Soils Court, Suite 100, in Oregon City. Also, you can download and print a form from the Secretary of State’s web site at the Oregon Voter Registration link, which can be found at [www.OregonVotes.gov](http://www.OregonVotes.gov). (Make sure your printer is set up for “landscape” printing!)

If you have any questions about registering to vote, contact Sandra King, City Recorder, at 503-570-1506 or at [king@ci.wilsonville.or.us](mailto:king@ci.wilsonville.or.us).

## Kinsman Road Extension Project Now Underway

Construction of the Kinsman Road extension project that connects Barber Street and Boeckman Road is now underway by the project’s contractor Emery and Sons Construction.

The project, scheduled for completion by June 2018, provides an additional north-south roadway connection through Wilsonville, linking the western residential neighborhoods with the City’s industrial employment areas. The extension of Kinsman Road is a key component of the City’s vision for an interconnected system of transportation choices.

As part of the project, the City plans to up-size and relocate a 30-inch sanitary sewer pipe and also install a 66-inch water line for the Willamette Water Supply Program (WWSP). The WWSP is a partnership between Tualatin Valley Water District and the City of Hillsboro to develop the mid-Willamette River as an additional water supply for customers primarily in Hillsboro and Washington County.

The complex project includes threading the roadway through the Coffee Lake Creek wetlands, under the Bonneville Power utility lines, away from forested habitat, and close to existing industrial development.



Construction worker spreads gravel over a culvert that runs underneath the Kinsman road extension project.

The WWSP water line is being constructed across the Barber and Kinsman Road intersection just east of the Barber Street Bridge. Doing so requires nighttime closures of Barber Street, 9 pm–6 am for approximately two months. During the daytime commuting hours, however, the contractor is going to maintain at least one lane of traffic in each direction.

The start date for the nighttime intersection closure is not set, but may begin as soon as mid-September and last until approximately mid-November depending on when the 66-inch water pipe is available for installation. As soon as the precise dates of the intersection closure are known the City is plans to post reader board signs at the intersection and share information with community members online and in the media.

Next spring and summer the project also includes construction of a roundabout at the north end of the project where the extension of Kinsman Road and Boeckman Road are planned to intersect. This stage of the project requires a three-month closure of Boeckman Road to build a new concrete roundabout.

For more information, contact Zachary Weigel, PE, Civil Engineer at 503 570-1565, [weigel@ci.wilsonville.or.us](mailto:weigel@ci.wilsonville.or.us).

## Frog Pond Master Plan Open House

**Wednesday, Sept. 14, 5-6:30 pm, Wilsonville City Hall 29799 SW Town Center Loop East**

The City is holding an Open House for the Frog Pond Master Plan on Wednesday, Sept. 14, 5-6:30 pm, at City Hall. The event is an opportunity to view drawings, talk with the project team and provide feedback. Draft materials include working recommendations for zoning, residential design guidelines, street designs, the Boeckman Trail and parks and open space.

The Planning Commission is holding a work session immediately following the Open House and the public is welcome to attend. Open Houses materials are available on the project website at: [www.ci.wilsonville.or.us/FrogPond](http://www.ci.wilsonville.or.us/FrogPond).



### Market Runs Until September 29

This year the Wilsonville Farmers Market remains open throughout most of September closing for the season on Thursday, Sept. 29. The market is open 4–8 pm at Sofia Park in Villebois, 28836 SW Costa Circle.



The Wilsonville Market is supported in part by a grant from the City of Wilsonville. For more information, including updated lists of vendors and musicians, visit [www.WilsonvilleMarket.com](http://www.WilsonvilleMarket.com).

### Golf Tournament Raises Money for Cancer Research

Charbonneau's 11th annual Pink Ball Tournament on Tuesday, Sept. 13, is a fundraising event for cancer research and treatment that benefits the Knight Cancer Institute at Oregon Health Sciences University. Last year the tournament, hosted by Charbonneau Women's Niners, raised \$47,000 and this year organizers hope to raise even more.



The tournament, open to all, is a four-person scramble beginning at 8:15 am on Sept. 13, followed by lunch and silent and oral auctions. Golfers, who are encouraged to wear pink, can sign up to play individually, as a couple or as a foursome. Community members, who do not golf, are invited for lunch at 11:30 am at the Charbonneau Country Club. The cost to golf is \$15 and for lunch is \$20. Register by Sept. 6, 5 pm at the Charbonneau Pro Shop's Club Room. For more information contact Pink Ball Chairman Robyn Andrus at 503-593-2454.

### Chorus Seeks New Singers and Pianist to Join Group

The I-5 Connection, a community chorus, is inviting new singers including both men and women with all different types of voices to join the local group. Music experience is helpful, but not necessary. Rehearsals are every Thursday, 10 am–noon, at the Wilsonville Community Center, 7965 Wilsonville Road, Wilsonville.



The group also needs a substitute pianist who is able to play a variety of music with the ability to improvise and work with a small string combo. This is a paid position. If you are interested, please contact Jocelyn Higgins at [jphiggins@comcast.net](mailto:jphiggins@comcast.net) or call contact Patty Brescia at the Wilsonville Community Center, 503-682-3727; [communitycenter@ci.wilsonville.us](mailto:communitycenter@ci.wilsonville.us).

### 2016 Water Feature Season Ends

Sunday, Sept. 11, 10 am–8 pm, is the last day the City's two water features located at Murase Plaza and Town Center Park will be open.



### School Starts on September 6 & 7

Students in Wilsonville's public primary and high schools head back to school on Tuesday, Sept. 6, but only 6th grade students begin on the same day at Wood Middle School. Wood's 7th and 8th graders start school the next day on Weds., Sept. 7.



### Electric Vehicle Event Set for Saturday, September 17

The Wilsonville National Drive Electric Week Event is on Saturday, Sept. 17, 10 am–2 pm, at the (former) Albertsons parking lot, 30299 SW Boones Ferry Road in Wilsonville. Over 100 electric vehicles of every kind are on display, with the opportunity to drive many of them. Preregister at [www.driveelectric-week.org](http://www.driveelectric-week.org) to see specific vehicles and reserve your test drive(s).

EV owners will be on hand to talk about the advantages of driving an EV. Some highlights you can expect to hear about:

- Fun to Drive—quiet and vibration free, and surprisingly quick and responsive
- Cheap to Fuel—even with the recent low gas prices, EV's cost a fraction of an ICE (Internal Combustion Engine) to fuel



- Virtually no Maintenance—No oil changes, spark plugs, transmission fluid, fuel filter, serpentine belt, timing belt/chain, muffler, etc. A typical factory schedule is: Rotate the tires every 7500 miles, cabin air filter every 15K, change brake fluid at 3 years. Repeat as necessary.
- Environmentally friendly—Our grid power is much cleaner than burning gas (or diesel) in your vehicle, and utilities offer even cleaner energy options like solar or wind.
- Come see the new Tesla Model X.

EV curious? Confused about terms like BEV, PHEV, MPGe? Come talk to owners and enthusiasts to find all the answers.

Have an EV already? Bring it by and share your experiences with those looking to learn.

National Drive Electric Week is an annual celebration with the goal to promote the use of electric vehicles. Plug In America, the Sierra Club, and the Electric Auto Association have organized National Drive Electric Week since its debut in 2011.

## City Calendar

### SEPTEMBER

|         |   |
|---------|---|
| 5 Mon   | • Labor Day Holiday<br>City Offices CLOSED  |
| 6 Tue   | • Municipal/Traffic Court<br>5 pm, City Hall  |
| 8 Thurs | • City Council<br>5 pm - Work Session<br>7 pm - Meeting<br>City Hall  |
| 11 Sun  | • Last Day To Play In Water Features<br>Murase Plaza and Town Center Park   |
| 12 Mon  | • DRB Panel A<br>6:30 pm, City Hall   |
| 14 Wed  | • Frog Pond Open House,<br>5–6:30 pm, City Hall<br>• Planning Commission<br>6:30 pm, City Hall<br>• Wilsonville Community Seniors Inc.,<br>6 pm, Wilsonville Community Center |
| 19 Mon  | • City Council<br>5 pm - Work Session<br>7 pm - Meeting<br>City Hall  |
| 20 Tue  | • Municipal/Traffic Court<br>5 pm, City Hall  |
| 26 Mon  | • DRB Panel B<br>6:30 pm, City Hall   |
| 28 Wed  | • Library Board, 6:30 pm,<br>Wilsonville Public Library   |

### OCTOBER

|          |  |
|----------|--|
| 3 Mon    | • City Council<br>5 pm Work Session<br>7 pm Meeting<br>City Hall   |
| 4 Tue    | • Municipal/Traffic Court<br>5:30 pm, City Hall  |
| 10 Mon   | • DRB Panel A, 6:30 pm,<br>City Hall   |
| 12 Wed   | • Planning Commission, 6 pm,<br>City Hall<br>• Wilsonville Community Seniors Inc.,<br>6 pm, Wilsonville Community Center |
| 13 Thurs | • Park and Recreation Board Meeting<br>4:30 pm, Park and Recreation<br>Administrative Offices                            |
| 17 Mon   | • City Council<br>5 pm, Work Session<br>7 pm, Meeting<br>City Hall   |
| 18 Tue   | • Municipal/Traffic Court<br>5:30 pm, City Hall  |
| 24 Mon   | • DRB Panel B<br>6:30 pm, City Hall  |
| 26 Wed   | • Library Board , 6:30 pm,<br>Wilsonville Public Library   |
| 29 Sat   | • Fall Harvest Fest, 9:30–11:30 am,<br>Stein-Boozier Barn, Murase Plaza  |
| 31 Mon   | • Halloween  |

All dates and times are tentative; check the City's online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and are replayed periodically. Meetings are also available to stream live and by video-on-demand at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.



**Tim Knapp**  
Mayor  
[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



**Scott Starr**  
City Council President  
[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



**Julie Fitzgerald**  
City Councilor  
[fitzgerald@ci.wilsonville.or.us](mailto:fitzgerald@ci.wilsonville.or.us)



**Susie Stevens**  
City Councilor  
[stevens@ci.wilsonville.or.us](mailto:stevens@ci.wilsonville.or.us)



**Charlotte Lehan**  
City Councilor  
[lehan@ci.wilsonville.or.us](mailto:lehan@ci.wilsonville.or.us)

### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)



### Wilsonville City Hall

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)



## Wilsonville Subaru Celebrates Grand Opening with Large Donation



At the grand opening for Wilsonville Subaru in March, the new car dealership donated \$30,886 to Wilsonville Community Sharing (WCS), the local non-profit food bank and social service referral agency (l to r): Wilsonville Subaru Dealer Principal Dave Jachter, WCS Vice Chair Taft Mitchell, Mayor Tim Knapp, WCS staff Leigh Crosby, Wilsonville Subaru General Manager and WCS Chair Ron Owens, WCS Treasurer Tammy Pupo, and WCS board members Wes Morris and Dick Spence.

## Mayor's Message

### Wilsonville: Creating a Complete Community for All Ages of Life

Recently, I had the honor of delivering the "2017 State of the City Address" at a March City Council meeting. I reviewed City Council goals and spoke about a number of high-profile issues and initiatives that the City is actively engaging in with the public.



Mayor Knapp

The essence of the goals of the City Council focus on creating a "complete community for all ages of life." An intentional community is one that offers a range of residential living options for different stages of life, a variety of good local jobs, an assortment of shopping selections, quality recreational opportunities, an array of ways to get around, and encourages social connections with neighbors.

I addressed transportation concerns that the City Council is focusing on, including building the town's grid of streets from what were formerly 'farm-to-market' roads, as well as lobbying for increased regional, state and federal investments to improve I-5 and arterials and enable more transit options.

I also discussed three major projects—all so far funded primarily by federal, state or Metro grants—that the City is undertaking. All three are designed to improve mobility, community connections and the local economy.

The proposed **French Prairie Bike-Ped-Emergency Bridge** over the Willamette River is intended to serve daily as a bike/pedestrian facility for recreational and tourism purposes, connecting the Portland-area Ice Age Tonquin Trail with the Willamette Valley Scenic Bike Route. Importantly, the seismically reinforced bridge would also be designed to withstand a major earthquake, making it a significant element of transportation resiliency for the I-5 corridor. And the bridge can be used by emergency responders when I-5 is clogged to reach incidents on the freeway or Charbonneau south of the river.

The **Wilsonville Town Center Plan** project represents a community-driven vision to 're-make' our town center as a more commercially vibrant, attractive, walkable mixed-use district that is easily accessible. As major new shopping centers—Argyle Plaza and Old Town Square—were developed, they attracted private-sector investment and diversified our commercial choices. We now have the opportunity to engage the community to re-envision the center, and the role it will play in the center of our community.

Our **Coffee Creek employment area** of north Wilsonville has been concept planned for roads, public services and transit access, all focused on business and employment land-uses. A first-ever industrial 'form-based code' can offer a quicker option for timely advancing development and creation of new high-wage jobs. We are seeing the beginnings of private-sector business investments in this area.

Wilsonville is becoming the kind of community where one can grow up in great neighborhoods, benefit from quality educational opportunities, get a good job, raise a family, and retire — all centered on a thoughtful, interconnected, desirable small city: A complete community for all phases of life.

*Tim Knapp, Mayor*

Editor's Note: Mayor Knapp's "State of the City Address" may be viewed online at [www.ci.wilsonville.or.us/video-on-demand](http://www.ci.wilsonville.or.us/video-on-demand), and selecting March 6 City Council meeting.

## Open House Kicks Off Parks & Recreation Master Planning Process

As part of the Parks and Recreation Master Plan study that is now underway, the City of Wilsonville is hosting a public open house on Thursday, April 20, 6 pm–8 pm at City Hall, 29799 SW Town Center Loop East. The City seeks community member and stakeholder feedback on the types of parks and recreation

programs, services and facilities the community desires to support in Wilsonville.

The purpose of the master plan is to provide a community-driven vision and long-range guide for the Parks and Recreation Department covering all parks, facilities and programs.

To help develop the plan the City hired GreenPlay, LLC, a national firm based in Colorado with extensive experience across the U.S. facilitating, researching and developing similar parks and recreation master plans.

The 12-month planning process seeks to compare current parks and recreation facilities, programs and services with the community's future needs and desires. The plan is going to recommend potential changes and/or enhancements for City Council consideration early next year.

The project's work plan includes extensive community engagement with residents, business owners, stakeholders and people of all ages. In addition to the upcoming open house, the City is also planning to provide more opportunities for community feedback that include additional public meetings, a public online survey, stakeholder interviews, focus-group discussions and a public hearing prior to the City Council adoption early next year.

Parks and Recreation Director Mike McCarty shares, "We recognize that providing high-quality parks and

***We want as many people as possible to participate in this planning process to ensure the plan is supported by the public and is consistent with the community's high expectations.***

recreational opportunities is extremely important to the overall quality of life within Wilsonville and we want as many people as possible to participate in this planning process to ensure the plan is supported by the public and is consistent with the community's high expectations."

For more information, visit [www.WilsonvilleParksandRec.com/ParksPlan](http://www.WilsonvilleParksandRec.com/ParksPlan) contact Mike McCarty, Wilsonville Parks and Recreation Director at 503-570-1579 or at [mccarty@ci.wilsonville.or.us](mailto:mccarty@ci.wilsonville.or.us).

## City Awards Tourism Grants to Support Local Events

In February, the City of Wilsonville Tourism Promotion Committee awarded \$25,000 in grants to support local tourism events and programs with funding from the City's Community Tourism Grant Program.

Five local nonprofit organizations that sought \$30,000 in funding were awarded \$25,000 in grants to produce major community events during the 2017 summer season:

- **Wilsonville Celebration Days** for the **Fun in the Park** festival: \$7,000
- **Wilsonville Arts & Culture Council** for the **Wilsonville Festival of Arts**: \$5,000
- **Wilsonville Brewfest**: \$5,000
- **Wilsonville Kiwanis Club** for the **Kids Fun Run**: \$4,000
- **Wilsonville Rotary Club** for the **Summer Concert Series**: \$4,000

Tourism Promotion Committee Chair Jeff Brown said, "The Tourism Promotion Committee is excited to be able to support these five different community events that attract not only local residents, but also draw visitors and tourists from outside of town and the region."



City tourism funds support the annual Fun in the Park event in August.

City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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### Fun Times at the 2017 Daddy Daughter Hawaiian Luau Dance



### Transit Master Plan Public Hearing Continued to May 10

The City of Wilsonville’s public transit agency, South Metro Area Regional Transit (SMART), is seeking public comment on the draft Transit Master Plan prior to potential City Council adoption.

The purpose of the Transit Master Plan is to capture the community’s priorities and needs and to guide the transit agency’s future changes, investments and operations over the next five years. Copies of the plan are now available online at [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com).



Members of the public—including residents, employers, employees and visitors—can comment on the draft plan in various ways:

- Online at [Plansmartwilsonville.com](http://Plansmartwilsonville.com)
- Mail to City of Wilsonville, SMART TMP Comments, 29799 SW Town Center Loop East, Wilsonville, OR 97070
- Call (503) 682-7790 x1497

The Wilsonville Planning Commission is scheduled to hold a public hearing on the draft Transit Master Plan on **Wednesday, May 10, 6 pm**, at Wilsonville City Hall Council Chambers. Public testimony is welcomed.

For more information, contact Nicole Hendrix, Transportation Program Coordinator, at 503-682-7790; [hendrix@ridesmart.com](mailto:hendrix@ridesmart.com).



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

### April is National Poetry Month

Join us for our second annual celebration of National Poetry Month.

Discover your inner poet with activities and interactive displays throughout the library, and join us for a delightful performance by Cowboy Poet of the Year Tom Swearingen at our Curiosity Cafe on Saturday, April 15, at 1 pm.

Sponsored by the Wilsonville Public Library Foundation.

[www.WilsonvilleLibrary.org/poetry](http://www.WilsonvilleLibrary.org/poetry)

### Beginning Spanish Class Starts Monday, April 3

Discover the world of Spanish language and culture! This eight-week course provides you with the initial tools you need to start using the Spanish language to communicate and function in a variety of real-life situations. Develop skills in reading, listening, speaking, and writing in Spanish, and gain insight into the richness and beauty of Spanish-speaking culture.



**Mondays, April 3–May 22, 6–8 pm, in the Rose Room.** \$40 for class series. Sign up at the Circulation Desk or call 503-682-2744.

[www.WilsonvilleLibrary.org/classes](http://www.WilsonvilleLibrary.org/classes)

### Teen Event: Comic Book Workshop Friday, April 14

Teens assemble! At this comic book workshop, learn to write, draw, and color your own comic book, test your superhero trivia knowledge, and get your supercraft on with fun activities.



For students in grades 6–12.  
**Friday, April 14, 6–8 pm. Free.**

### Curiosity Cafe: Cowboy Poetry Saturday, April 15

Tom Swearingen, Cowboy Poet of the Year, tells stories of the people and land of the American West through original cowboy poetry that is often inspired by his own experiences and observations from the saddle. Some are humorous, others serious, and all entertaining.



**Saturday, April 15, 1–3 pm, in the Oak Room. Free.**

### American Red Cross Blood Drive Thursday, April 20

The Library will be hosting American Red Cross blood drives throughout the year. Donate blood and save a life at our Spring Blood Drive on Thursday, April 20.



**Thursday, April 20, 10 am–3 pm. Free.**

### Upcoming Classes and Events

**Local Artist of the Month: Jeannie Branscom**

#### First Friday Film

Friday, April 7, 6–8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

#### Book Notes Concert

*I-5 Connection Choir*  
Saturday, April 8, 2–3 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

#### K-2 Book Adventures

Thursday, April 13, 4–4:45 pm  
[www.WilsonvilleLibrary.org/k2](http://www.WilsonvilleLibrary.org/k2)

#### Book Club

*“At the Water’s Edge” by Sara Gruen*  
Thursday, April 13, 6–8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

#### Genealogy Club

Monday, April 17, 1–2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

#### Great Books Discussion Group

*The Indestructibility of Our Inner Nature*  
by Arthur Schopenhauer  
Tuesday, April 18, 6–8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

#### History Pub at Wilsonville McMenamins

*“Alien Skulls: The Ultimate Archeological Find” with Faye Sweeney*  
Tuesday, April 25 6:30–8 pm

## Boeckman Road near Villebois to Be Closed New Kinsman Road Extension to Relieve Congestion, Improve Mobility

Construction of the Kinsman Road extension project that connects SW Boeckman Road and Barber Street requires a three-month-long closure of Boeckman Road, April 24 through early August. The project builds a concrete roundabout at the new intersection of Boeckman and Kinsman.

During this time, Boeckman Road between Villebois Drive and the railroad tracks west of SW 95th Avenue is to be closed to through traffic, with access to local businesses maintained. A detour route along Boberg Road, Barber Street, Costa Circle and Villebois Drive is planned.

The project extends Kinsman Road one mile from Barber Street near the Barber Street Bridge and the SMART/WES Commuter Rail Transit Center to connect with Boeckman Road to the north.

The project is a key component of City's Transportation Systems Plan (TSP) that is building a grid network of streets for an interconnected system of transportation choices in Wilsonville to provide improve mobility and create new routing options. The new roadway provides an additional north-south roadway connection and is designed to accommodate movement of large trucks through one of Wilsonville's industrial districts. The new section of Kinsman Road helps to alleviate traffic congestion on Boones Ferry and Boberg roads, provides more direct access to the SMART/WES transit and links the western residential neighborhoods with the City's industrial employment areas.

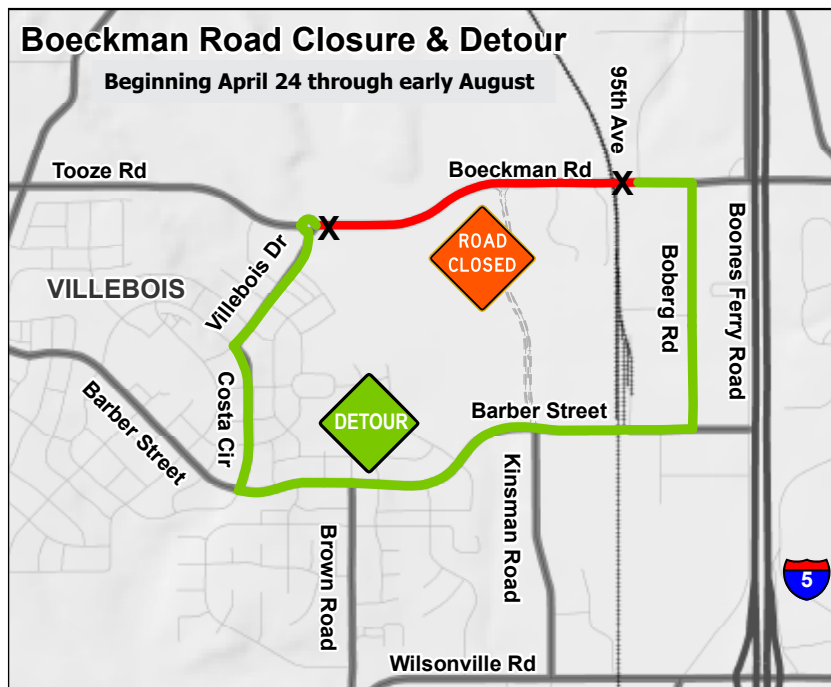
Incorporated into the project are bike lanes, sidewalks, a 10-foot-wide multi-use pathway and overlook along the Coffee Lake Creek wetlands. The project also includes 2.6 acres of habitat enhancement and wetlands restoration. Stormwater culverts and wildlife crossings are being installed at regular intervals to preserve habitat and wetlands that exist on both sides of the roadway.

As part of the project, a 30-inch sanitary sewer pipe is being upsized and relocated under the new roadway to serve future growth in the Coffee Creek area of North Wilsonville.

In coordination with the Willamette Water Supply Program (WWSP), a 66-inch water line is being installed under the road as well. The WWSP is a partnership between Tualatin Valley Water District (TVWD) and the City of Hillsboro to develop the mid-Willamette River as an additional water supply for customers primarily in Hillsboro and Washington County.

The \$11.3 million construction project is funded with \$3.9 million by TVWD, \$3.3 million in City systems development charges paid by new development, a \$2.2 million Oregon Statewide Transportation Improvement Program (STIP), \$1.4 million in waste-water utility funds and \$0.5 million Metro Transportation Improvement Program grant

For more information, contact Zachary Weigel, PE, Civil Engineer, at 503 570-1565, weigel@ci.wilsonville.or.us.



## Advance Road at 60th Ave Closed April 29–May 19

Advance Road is planned to be closed to all through-traffic between Wilsonville Road and 60th Avenue for a three-week period, Saturday, April 29–Friday, May 19. The intersection at Wilsonville Road and Stafford Road remains open.

Local access to homeowners is to be maintained, but short delays are to be expected.

The closure is needed to allow complete reconstruction of a section of the roadway in association with the new Meridian Creek Middle School being constructed by the West Linn–Wilsonville School District. A detour route using Mountain and Home-steadier roads will be in effect. The City and School District apologize for any inconvenience this may cause citizens and businesses.

For more information, contact Steve Adams, PE, Development Engineering Manager, at 503-682-4960; adams@ci.wilsonville.or.us or Tony Vandenberg with the School District at 503-673-7000 x7990.

## Bell Road Slide Repair Planned for April

Clackamas County plans to repair the damage from a landslide on Bell Road between Grahams Ferry Road and Baker Road in April. The slide occurred in mid-February and the road has been closed since.

The three-part plan to permanently repair the Bell Road slide includes:

- Removing the slide debris
- Building a rock buttress and shoulder to hold the slope in place
- Replacing the failing culvert next to the slide to help avoid future problems related to drainage

Once repairs begin, they are expected to be completed within three weeks. The detour that has been established from Grahams Ferry Road to Tooze Road to Baker Road remains in effect until the repair is completed.

For more information, contact Project Manager Vince Hall at 503-742-4650.

## SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
smart@ridesmart.com  
www.RideSMART.com

### SMART Adds New Bus to Fleet

In late February, SMART added a new 17-passenger bus to the transportation agency's fleet. The purchase of this vehicle allows SMART to retire a similar 2003 model bus. SMART continues to successfully pursue grant opportunities, which make these vehicle purchases possible. The grant which was utilized to purchase this bus funded 80% of the \$78,000 purchase price.

Consistent with all buses in the SMART fleet, the new vehicle is ADA compliant, having space for two mobility devices.



### Bike Repair Station Now Available

SMART is excited to announce our recent addition to SMART Central.

A green Dero bike repair station and air pump can be found next to the bike lockers at Wilsonville Station. SMART is pleased to be able to provide additional service to Bike Smart and for our cycling community. SMART implemented the project based on a community member's recommendation last year.



### Walk @ Lunch – April 26

Join Walk SMART at noon on Wednesday, April 26, for a 30-minute walk during lunch and take the first steps towards a healthier you. Walk SMART is coordinating the walk on National Walk @ Lunch Day and is taking place at City Hall in Wilsonville—rain or shine starting in the lobby at City Hall. Meet new friends and do some lunch-time networking. Come meet our partners for this walk, who provide rewards for participating. Each walk has different partners, with fresh and exciting rewards. See you there!



### Interested In Forming a Worksite Vanpool?

SMART is working to expand the current transportation options network with the introduction of the use of vanpools at Wilsonville worksites. A solution to the congestion along the Wilsonville I-5 corridor is to reduce the amount of single occupancy vehicles. In addition to reducing traffic congestion the commute in and out of Wilsonville on a daily basis for many folks is not the typical 8–5 work schedule. With SMART transit servicing the typical commute time there is a void of a travel choice in the travel time that could best be utilized by opening up a vanpool service. By introducing vanpool programs, folks would be able to commute from point



A to point B alongside other commuters who share the same route to and from work/school.

If you believe your Wilsonville business may have interest or simply would like to learn more about vanpool options please contact us, we would be happy to come out and meet with you or a representative of your organization.

For more information, contact Nicole Hendrix, Transportation Program Coordinator, at 503-682-7790; hendrix@ridesmart.com.



### Historic Butteville Store Opens for Season, Features Live Music

The Historic Butteville Store located at 10767 Butte Street NE in Butteville opens for the 2017 Summer Season on April 8 with a new manager, Dori Brattain, a whole new look and feel and a new menu emphasizing house baked pastries, fresh salads, house made soups, deli sandwiches and barbeque. Operated by the nonprofit Friends of Historic Butteville, the store is hosting a special dinner and live music on Saturday evenings from Memorial Day through and Labor Day. Local bands are featured playing bluegrass, old time fiddle & guitar, and Celtic music, along with occasional Americana/country performers.

Owned by Oregon Parks and Recreation Department, the Butteville Store is Oregon's oldest continuously operating retail establishment and is located in Butteville's old town, once the main riverport on the Willamette River upstream from Wilsonville.

Located at the east end of the hiking trail that runs along the river through Champoeg State Park, the store continues a famous tradition of hand-dipped ice cream.

The store is open weekends until Memorial Day, and then open every day 9 am–6 pm. For more information, go online to [www.butteville.org](http://www.butteville.org) or contact Ben Williams at 503-568-5670 or Dori Brattain at 503-678-1605.



Butteville Store, circa 1961 [Photo courtesy Oregon Historical Photo Collection, Salem Public Library].

### Take a Break Stay Awake to Prevent Drowsy Driving

The Oregon Department of Transportation (ODOT) is reminding Oregonians of the dangers of drowsy driving and providing tips on how to stay awake when behind the wheel.

Research shows that nearly a third of drivers admitted to driving within the prior thirty days when they were so tired that they had trouble keeping their eyes open. This lack of sleep slows reaction time, impairs judgment, and increases the risk of dozing off while driving. A new report from AAA's Foundation for Traffic

Safety measures the relationship between



the number of hours that a driver had slept in the past 24 hours and the risk of crash involvement. The report finds that, "Drivers who had slept for less than 4 hours, 4-5 hours, 5-6 hours, and 6-7 hours in the past 24 hours had an estimated 11.5, 4.3, 1.9, and 1.3 times the crash rate, respectively, of drivers who had slept for 7 hours or more in the past 24 hours."

Getting good sleep on a regular basis is the best defense. But if you do find yourself driving while drowsy, take a break. Rolling down the windows, turning up

**Getting good sleep on a regular basis is the best defense. But if you do find yourself driving while drowsy, take a break.**

the radio or drinking a caffeinated beverage are not enough to stave off drowsiness.

Sleep is the best remedy. If you are drowsy while behind the wheel, find a safe, legal place off the roadway to take a quick nap. Or take a break to recharge with exercise. Physical activity such as a brisk walk or moving around offers a natural boost of energy. Never risk driving when you are drowsy. Take a Break. Drive Awake. It may just save a life.

### Mayor Knapp Lobbies Legislature with Other Metro-Area Mayors



Wilsonville Mayor Tim Knapp speaking about the need for transportation funding at a press conference in March at the Oregon State Capitol. Mayor Knapp was one of 15 Portland-area mayors who lobbied state legislators on a variety of issues of concern to cities. Pictured with Mayor Knapp (center) are (left to right) Happy Valley Mayor Lori DeRemer, Milwaukie Mayor Mark Gamba and Sherwood Mayor Krisanna Clark.

### State Legislators to Hold Town Hall

Representative Rich Vial and Senator Kim Thatcher, are holding a public Town Hall meeting to share and discuss policies currently under consideration by the Legislature including transportation issues. The meeting is scheduled for Thursday, April 20, 6:30–7:30 pm, at Wilsonville City Hall.

Rep. Vial represents House District 26, which includes areas of Wilsonville, Sherwood, King City, Hillsboro, Beaverton, Aloha and Tigard. Sen. Thatcher represents Senate District 13, which includes the areas of Keizer, Newberg, Wilsonville, Sherwood, King City, Hillsboro, Beaverton, Aloha and Tigard. For more information, contact Michelle Felton, Legislative Director, at 503-986-1426; [rep.richvial@oregonlegislature.gov](mailto:rep.richvial@oregonlegislature.gov).

**Legislative Town Hall**  
• **Thur, April 20, 6:30–7:30 pm**  
Wilsonville City Hall  
29799 SW Town Center Loop E



29600 SW Park Place  
Parks Programs: 503-783-7529  
Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

### Save 10% on Spring and Summer Registration

Enroll by May 1 and receive 10% off program tuition costs. Registration for classes and camps opens on April 17 and all registrations completed between April 17– May 1 receive 10% off the cost of the program or camp (excluding Skyhawks Sports Academy). All programs can be viewed and registered for online.



### Park Reservations Now Being Accepted

Applications for Memorial Park Shelters and Fields are now being accepted. Memorial Park is a popular location for company gatherings, family parties, weddings and a variety of sporting events. Wilsonville Parks and Recreation offers a number of park rental options including: the historic Stein-Boozier Barn, River Shelter (SW corner of Memorial Park), Forest Shelter (SE corner of Memorial Park), Splash Shelter (adjacent to the Murase Plaza water feature), Grove Shelter (near the Murase Plaza playground) and sport fields.

The 2017 rental season runs April 8–Oct. 15. For more information on availability and fees please visit [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com) or contact Ahsamon at 503-570-1530.

### Wilsonville Egg Hunt

Saturday  
April 15  
10 am  
1–11 y olds  
Memorial Park  
No Charge



Over 10,000 candy and toy filled eggs plus toy giveaways!

### Join WERK Day on May 13

Join Wilsonville Parks and Recreation for this annual community event. Come together with 300 of your neighbors to help clean up and beautify Wilsonville parks and facilities. The Wilsonville Rotary is serving up their famous omelette breakfast starting at 8 am with participants heading to their work sites at 9 am. Please wear long pants, closed toe shoes and bring gloves and water. Participants under the age of 18 need a parent/guardian signature to participate.

For more information and to pre-register, please visit [WilsonvilleParksandRec.com/WERKDay](http://WilsonvilleParksandRec.com/WERKDay).

### Upcoming Classes and Events

#### Zumba Gold

Ages 18 and older  
Mondays, April 3–May 22  
1:30–2:15 pm  
Community Center  
\$55

#### Tai Chi Studio

Ages 18 and older  
Tuesday and Thursdays  
April 11–June 1  
2–4 pm  
Community Center  
\$80

#### Pilates: Healthy Back and Flexibility

Ages 18 and older  
Wednesdays, April 12–May 31  
10:30–11:30 am  
\$65

#### Gentle Hatha Yoga

Ages 18 and older  
Tuesdays, April 18–June 6  
8:30–9:30 am  
Community Center  
\$47

### HOLISTIC HEALTH FAIR

(formerly Spa Saturday)

Saturday, April 22, 10 am–1 pm  
at the Community Center  
No Charge



For more information: [WilsonvilleParksandRec.com/HolisticHealthFair](http://WilsonvilleParksandRec.com/HolisticHealthFair)

## Wilsonville Town Center Question of the Month

Every month throughout the 18-month-long planning initiative the Wilsonville Town Center Plan is asking community members a new Question of the Month. The questions are designed to gather feedback on a wide-range of issues related to developing the plan that captures a community-driven vision and strategies to guide the development of Wilsonville's Town Center.



### Here is April's Question of the Month:

#### What type of restaurants would add the variety you desire in Town Center?

(Select all that apply.)

- a) Upscale, sit-down restaurants
- b) National chain restaurants
- c) Diverse, local restaurants
- d) Farm-to-table restaurants
- e) Food carts

To share your feedback and participate in the Question of the Month stop by City Hall, the Library or the Community Center or visit the project's website below.

On Wednesday, April 19, the Wilsonville Town Center Task Force is meeting for the second time, 6–9 pm, at City Hall. The meeting is open to the public, but public comment will not be received at the task force meetings.

To answer the Question of the Month, sign up for project updates, provide input and to get more information visit [WilsonvilleTownCenter.com](http://WilsonvilleTownCenter.com) or contact Miranda Bateschell, Long-Range Planning Manager, at [bateschell@ci.wilsonville.or.us](mailto:bateschell@ci.wilsonville.or.us) or 503-570-1581.

## CHIEF'S CORNER – WILSONVILLE POLICE WELCOMES NEW MEMBERS

As your Chief of Police, I am excited to welcome three new Deputies and one new Sergeant to our staff. One of the biggest benefits of the City's relationship with Clackamas County Sheriff's Office (CCSO) is in how we complete our recruiting and training process. When the City needs new personnel to join the police force in Wilsonville we simply look for candidates from within CCSO's existing pool of trained and qualified staff. This efficiency allows the City to quickly get a wide variety of police personnel with a full range of experiences in a relatively short period of time and minimal expense by not having to manage lengthy recruitment and training periods to obtain new staff.



Police Chief Adam Phillips

Now let's meet the new CCSO Police Officers who are now onboard in Wilsonville.

**Terry Colbert** is a tenured law enforcement officer with over 23 years of experience who is the City's new swing shift Deputy. Terry first became a police officer in December 1993 in Cannon Beach, but during his career has also worked for the Portland Police Bureau for 14 years and also previously for CCSO from 1995–2002. His professional experience includes serving as an undercover drug crimes investigator, a member of the Auto Theft Task Force and on the SWAT Team. In his free time he enjoys rooting for the Oregon State Beavers and watching and/or participating in outdoor sports.



Terry Colbert enjoys fishing.

**Marc Griffith** is the City's new graveyard shift Sergeant with CCSO where he has worked for the past ten years. Prior to working for CCSO he spent 13 years in the United States Air Force/Air Nation Guard as a

combat veteran with multiple deployments and where he was also assigned to the 142nd Fighter Wing in Portland where he served as a Combat Arms Instructor that passed Basic Sniper and Advanced Sniper School. While at CCSO in 2011 he joined the Clackamas County Water Rescue Team (WRT) as a Rescue Swimmer and now supervises a team of ten rescue swimmers who are members of the Clackamas County Water Rescue Consortium (WRC). In his free time he also enjoys being a husband and father of three children.



Marc Griffith during a water rescue of a 10-year-old boy on the Clackamas River.

**Scott Krause** is a new graveyard shift Deputy who began his law enforcement career with CCSO last February after being a full-time student at Portland State University and graduating from the State Police Academy in August. In his free time he enjoys spending time as youth Jiu Jitsu coach, outdoor activities, playing sports and spending time with his family.



Scott Krause

**Chris Thomas** recently joined the Wilsonville Police Department as a graveyard shift Deputy. Chris has a total of five years of experience in law enforcement with CCSO and the West Linn Police Department. Prior to becoming a Police Officer he served in the US Army by way of the Oregon Army National Guard as a Combat Engineer.



Chris Thomas

Please join me in welcoming these new members to our team when you see them out and around town.

*Adam Phillips, Chief of Police*

## Pilot Program Allows Off-Duty Firefighters to Save Lives

A coalition of first responders, clinicians, researchers and a medical equipment manufacturer aims to make this region the frontrunner in sudden cardiac arrest response and survival.

The PulsePoint Foundation, Tualatin Valley Fire & Rescue, International Association of Firefighters Local 1660 and automated external defibrillator (AED)



manufacturer Philips Healthcare launched the Verified Responder Pilot Program on Valentine's Day that activates off-duty, professional firefighters to respond to cardiac arrest calls in public and private settings.

Participating professional firefighters are certified emergency medical technicians or paramedics who receive background checks. Philips Healthcare is loaning

every participating firefighter an AED so that if they respond, they can employ the same technology used by emergency responders and physicians to restart a heart that has stopped beating.

"Four years ago, we were the first fire department in Oregon to launch PulsePoint's app for citizen responders," said TVF&R Fire Chief Mike Duyck. "We are humbled to partner with them again as the first agency to pilot the Verified Responder program and hope that it's the beginning of a national movement."

The effort combines important data with existing technology and clinical insights to inform future lifesaving strategies and the potential expansion to additional communities across the nation.

"Firefighters know all too well that their skills are sometimes needed when off-the-clock," said Richard Price of the PulsePoint Foundation. "In some ways, PulsePoint Verified Responder simply formalizes the 'always in service' dedication and full-time commitment that comes with the badge."

"We understand that the moments between someone's heart stopping and when the emergency responders get to the scene are crucial," added Joe Sovak of Philips Healthcare. "The faster help is able to intervene, the greater the opportunity for another life saved."



## Local Hood-to-Coast Team Runs and Fights Cancer

Twelve local runners from Wilsonville are teaming up again this year to run the 2017 Hood-to-Coast relay and to raise funds to fight cancer in partnership Providence Portland Medical Foundation.

The local team, "Cherries and Nutz!" is led by local resident Katie Green and plans to embark upon the grueling 199-mile relay race in late August from Timberline Lodge in Government Camp (Hood) to Seaside (Coast). Over approximately 30 hours each member of the team runs a total on average of over 16 miles including three different legs that take place all hours of the day and night. The runners will begin early morning on Friday, Aug. 25, and end in the afternoon on Saturday, Aug. 26.

In addition to being a personal challenge, the team is also challenging itself to raise \$10,000 for the Providence Portland Medical Foundation to help cancer patients who are battling the disease that adds approximately 1.6-1.7 million new cancer cases each year nationally.

Katie Green explains, "We are running to honor those who have lost the hard fight. We want to run and fight alongside those currently fighting. We are also fighting for those that will possibly face this horrific disease in the future. On our team we have lost brothers, mothers, uncles and aunts including a team member who had a scare with cancer, but is thankfully in the clear, for now."

The team is asking community members to join in the fundraising effort by pledging funds at <https://finishcancer2017.everydayhero.com/us/cherries-and-nutz/members>. For more information contact Team Captain Katie Green at 503-686-3557.



Local Hood-to-Coast team, "Cherries and Nutz!" front row in black (l to r): Katie Green, Tonya Emmett, Stephanie Goff, Shelley Weise, Carrie Roelofs, and Donielle Crowley. Back row (in gray) Tony Frey, Neil Roelofs, Nicholas Emmett, Stephen Weise, Tim Crowley and Josh Goff.



## Reports from City Boards and Commissions

### Mayor Delivers State of the City Address with Theme of 'Building a Complete Community'

On March 6, at the beginning of the City Council meeting, Mayor Tim Knapp presented the "2017 State of the City Address," providing an overview of the City's current activities and future plans. During the presentation, the Mayor shared a vision for creating a complete community for all ages of life. The presentation included a review of the City Council's goals and objectives, an update on major new residential developments that include the Villebois and Frog Pond neighborhoods, an overview of the Wilsonville Town Center Plan and the French Prairie Bridge Project, along with several other proposed transportation and economic development projects. In summary Mayor Knapp stated, "Our ongoing goal is to create a safe, desirable, and aesthetically pleasing community that nurtures, sustains, and connects us all." For more information see p. 1.



Later in the meeting City Council, which included three of the five City Council members in attendance, decided to continue a discussion on the potential adoption of the 2017-19 City Council key objectives and goals and an updated City Council Protocol Manual until the next meeting on Monday, March 20, when a full City Council is expected to be present.

Under Citizen Input and Community Announcements numerous community members encouraged the City Council to adopt an inclusivity resolution to promote Wilsonville as a city that supports and promotes diversity.

During work session prior to the meeting, the City Council heard reports and discussed an analysis by SMART, the City's public transit agency, that examines the cost of providing the services proposed in the current draft of the Transportation Master Plan. The Council also received a report on the Frog Pond West Master Plan Residential Neighborhood Zone and also a briefing on coordinating the Water Treatment Plant Master Plan and Willamette Water Supply Program.

Community members can watch a replay of all or a portion of the City Council meeting on Comcast/Xfinity Ch. 30, Frontier Ch. 32 or on the City's video-on-demand service at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV).

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during April. Scheduling changes can and do occur; check for updated meeting information at the City's website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, April 3:** During work session: red light camera; adult diversion program; Willamette Falls Locks update by West Linn Mayor Russell Axelrod. During the regular meeting: Arbor Day proclamation

**Monday, April 17:** During work session: community engagement; cell tower lease. During the regular meeting: Construction Safety Week proclamation; cell tower lease; Frog Pond Master Plan public hearing.

### Planning Commission Update

At the Planning Commission meeting on March 8, the Commission held a public hearing and discussed the Frog Pond Master Plan recommending the plan for City Council adoption with minor modifications to the related plans and documents. The Commission also agreed to continue the public hearing on the Transit Master Plan to the meeting on May 10.

### Development Review Board Update

At the regularly scheduled DRB Panel A meeting on Monday, March 13, the Board elected Ron Herberlein as Chair and Fred Ruby as Vice Chair to serve one-year terms. At the regularly scheduled DRB Panel B meeting on Monday, February 27, the Board elected Shawn O'Neil as Chair and Richard Martens as Vice Chair to serve one-year terms.

### City Council Approves Purchase of Sewer/Storm-Drain Cleaning Truck and Renews Employment Contract with City Attorney

On Feb. 23, the Wilsonville City Council approved the City to enter into a contract to purchase a combination sewer/storm catch-basin cleaning truck, also known as a Vactor Truck. The new vehicle replaces the City's current truck, which is over 23 years old and needs to be replaced due to being in poor mechanical condition. The City uses the truck to perform regular maintenance and spot repairs on the City's sewer, storm and water utility lines and underground infrastructure throughout the City.

The City Council renewed a one-year employment contract with City Attorney Barbara Jacobson who is in her second year in the position. The City Attorney provides legal advice to the City's boards and commissions including the City Council, reviews legal documents, drafts complex ordinances and resolutions, directs city litigation (including that of city prosecutor), negotiates and drafts contracts, assists in risk management and oversees the Legal Department.

During the communications portion of the meeting the Council heard an update from Nancy DeSouza, Executive Director of the Oregon Travel Information

Council/Oregon Travel Experience. She spoke about the state agency's role managing many of rest areas in Oregon such as the French Prairie Rest Area south of Wilsonville on I-5, the State Heritage tree program which includes Wilsonville's Robert Valentine Short Fir tree, the highway business sign program and the Oregon Historical Marker Program. The agency's Chief Community Assets Officer, Annie Von Domitz, also shared how the agency is leading an improvement project to restore the Grove of the States located at the southbound I-5 French Prairie Rest Area that features a grove of 50 trees that represents the 50 different states in the U.S.

During work session prior to the meeting, the City Council also heard reports and discussed the City's upcoming annual review of low-income housing property tax exemptions, considerations with using red light cameras at two key commonly-congested intersections in Wilsonville and also potential adjustments to the City's Community Development Planning fees to more fully recover costs associated with service delivery.

### Library Board Report for February

During the Library Board meeting on Wednesday, February, 22, the following reports were given:

- In February the Library hosted a Teen Lego night and is planning to hold a camp-themed game night.
- The Dolly Parton Imagination Library celebrated its 5 year anniversary at the Findlay auto dealership.
- The Library celebrated its 35th birthday on Feb. 14 with cake and coffee for the public.
- Library renovation plans continue to move along with planning meeting in March to review initial designs and quotes.
- The RFID tagging project for library materials is moving forward and the materials needed to complete the project have been ordered.
- In February the Library hosted Family Night events to get more parents and school kids acquainted with the Library and the Library Foundation donated books for each child who attended.
- The Library's proposed budget for the upcoming fiscal year is primarily status quo, but includes additional funding for RFID, software and renovation funds.

The next regular Library Board meeting is scheduled for Wednesday, April, 26, 6:30 pm, at the Library.

### Mayor Tim Knapp Awarded 2016 Wilsonville First Citizen

Wilsonville Mayor Tim Knapp was announced as the 2016 winner of the Wilsonville First Citizen Award in February at the Rotary's annual Heart of Gold Celebration that honors community volunteers.

Knapp was among four finalists for the award, which is given by the Rotary Club of Wilsonville, and draws upon nominations submitted by the community. The award recognizes local leaders who have a history of making a difference and having a positive impact in the Wilsonville community. The other finalists were John Budiao, CEO of Military Coach; Kyle Bunch, agent with Kyle Bunch Agency of American Family Insurance; and Jean Tsokos, long-time Wilsonville community volunteer and leader with WilsonvilleSTAGE.

This year's event is included a Hawaiian theme, Mahalo To You, which means "thanks" as well as "respect." This year's event the 16th annual edition of the banquet and fundraiser, which was started in 2002 to honor Wilsonville's community leaders who enrich the lives of all local residents.

"Tim Knapp has contributed to the community over the past several decades as a volunteer and an elected official," said Richard Martens, club president, who chaired the First Citizen Selection Committee. "He has contributed a great many hours of service over the years, in a variety of roles from committee member on up to mayor. The Rotary extends its congratulations to

Mayor Knapp, and to all of the finalists who have made our city a better place for all of our residents."

Heart of Gold is the Rotary's largest fundraiser of the year, with funding supporting education and community enrichment efforts in Wilsonville. While the final tally is still underway, early estimates demonstrate the generosity of sponsors and attendees, from which education and social support efforts in the city are funded.

"It is a tremendous compliment to receive the 2016 First Citizen award from the Rotary Club of Wilsonville and be among finalists who all have made significant contributions to our community," Knapp said. "Year after year, the Rotary contributes to our city in important ways as a partner and friend. I am pleased to accept this award not just on my behalf, but for all the leaders and citizens of Wilsonville who work with such devotion to make this city the wonderful community that it is."



Mayor Knapp accepting the First Citizen award from Rotary president Richard Martens.

Knapp moved to Wilsonville in 1986 and served on several city boards, commissions and task forces before being appointed to the City Council in 2003. He was elected to the council in 2004, then elected mayor in 2008. He was re-elected in 2012 and 2016. His vision for Wilsonville as a connected, supportive community for all its citizens and businesses has been an underlying theme throughout his government and personal work.

During the banquet and celebration, attendees heard from a former inmate at the Coffee Creek Correctional Facility who was released in 2016. She and her daughter talked about how they benefitted from the Through A Child's Eyes partnership, which strengthens families and helps female inmates be better parents both before and after release. Besides TACE, proceeds from the event support numerous other projects of the Wilsonville Rotary Foundation, including local omelet breakfasts to support schools; the Wilsonville Chess Program; the free Wilsonville Rotary Summer Concerts; scholarships for Wilsonville high school graduates; international student exchange programs; and annual projects that have brought solar energy to Africa and clean water to schools and clinics in the Third World.

For those looking ahead to next year, the club is already accepting nominations for the 2017 award, which will be awarded in early 2018. Nominations may be submitted to [www.wilsonvillerotary.com/heart/nominate](http://www.wilsonvillerotary.com/heart/nominate), or sent to [president@wilsonvillerotary.com](mailto:president@wilsonvillerotary.com).

EXHIBIT C

### Wilsonville Named a Tree City USA for 19th Year in a Row, Celebrates with Upcoming Proclamation and Planting Events

The City of Wilsonville has been recognized for the 19th consecutive year as a Tree City USA by the National Arbor Day Foundation. The award reflects Wilsonville's nearly two-decade-long commitment to maintaining and promoting an ongoing investment in the community's urban-forestry efforts.

Mayor Tim Knapp said, "Trees help clean the air, conserve the soil and water, reduce heating and cooling costs and bring nature close to where we live – ultimately contributing to a sense of place and quality of life in Wilsonville." City

Councilor Charlotte Lehan, a long-time supporter of the City's role as a Tree City USA, added, "Trees also improve the visual appeal of a neighborhood, increase property values and provide wildlife habitat."

Maintaining the Tree City USA designation requires the City to have a comprehensive urban forestry

#### Volunteer Planting Event

- Sat, April 1, 9 am–1 pm  
Water Treatment Plant  
10350 Arrowhead Creek Ln.
- Sat, April 15, 9 am–1 pm  
Memorial Park

program, which consists of a tree ordinance, a City department responsible for trees, an annual community forestry budget of

at least \$2 per capita and annual Arbor Day observance and proclamation. Mayor Knapp plans to issue an Arbor Day proclamation during the City Council meeting on April 3.

To further promote Wilsonville's commitment to an urban forest and to promote habitat for pollinators, the City is hosting two different volunteer planting events in April in partnership with Friends of Trees. The volunteer planting events are being held on Saturday,

April 1, 9 am–1 pm, at the Willamette River Water Treatment Plant, 10350 Arrowhead Creek Lane in Wilsonville and on Saturday, April 15, 9 am–1 pm, at Memorial Park in Wilsonville. Volunteers are asked to arrive around 8:45 a.m. to be registered and assigned to a crew. Breakfast snacks and hot chocolate/coffee, as well as gloves, tools and guidance are provided. Dress appropriately for weather conditions and all ages are welcome.

To sign up a group of five or more volunteers contact Friends of Trees at 503-595-0213 or visit [www.FriendsofTrees.org](http://www.FriendsofTrees.org) for more information about volunteer tree planting opportunities in Wilsonville and around the region.

The volunteer planting events also support the City's Bee Stewards program which seeks to improve pollinator habitat in Wilsonville in partnership with Northwest Center for Alternatives to Pesticides; The West Linn Wilsonville School District, Center for Research in Environmental Sciences and Technology; Xerces Society for Invertebrate Conservation; Northwest Youth Corps; Habitat Landscape Design; Oregon State University North Willamette Research and Extension among others. The program is supported by a grant from the Wilsonville-Metro Community Enhancement Program.

For more information about Wilsonville's Tree City USA designation contact Jennifer Scola, Associate Planner, at 503-570-1572; [scola@ci.wilsonville.or.us](mailto:scola@ci.wilsonville.or.us).

For more information about the Bee Stewards program contact Kerry Rappold, Natural Resources Manager, at 503-570-1570; [rappold@ci.wilsonville.or.us](mailto:rappold@ci.wilsonville.or.us).



Last April over 50 local volunteers joined City and Friends of Trees staff to celebrate Arbor Day by planting over 360 native trees and shrubs and also pulling ivy at Tranquil Park in Wilsonville.

### Celebrate Earth Day—Buy and Waste Less Food at Home

By Kayla Scheafer, AmericCorps member

Do you ever feel guilty for throwing food away? Maybe you end up throwing out your old left-overs or those green vegetables that wilted before you got around to eating them.

Unfortunately in the United States we waste 40 percent of the food we grow. Food is lost all along the way from the field to our plates. Every time we throw food away we are also throwing away money, wasting fresh water and getting rid of food that could have been eaten by someone in need.

Many do not realize that we waste more food in our own homes than businesses and restaurants. So often when we clean out the fridge we find ourselves throwing out those old, unused and past-dated items that are well past their prime.

Simple tasks like making a grocery list and not going to the store hungry help reduce the amount of food we never buy and therefore do not waste. You can start by making small changes in your everyday life to waste less food and save money.



Food waste is an important issue because in Oregon, one in six people are food insecure, meaning they do not know where they will get their next meal. Whether we realize it or not, we all probably know someone who

#### Fruit and Vegetable Storage Guide

##### INSIDE THE FRIDGE:



- Apples, berries and cherries
- Grapes, kiwi and all citrus fruit
- Almost all fruit, vegetables and herbs

##### ONCE RIPE, STORE IN THE FRIDGE:

- Melons, nectarines, apricots, peaches, pears and plums
- Avocados and tomatoes

##### ON THE COUNTER OR TABLE:



- Bananas
- Basil
- Winter squash
- Once cut, store produce in the fridge

##### IN A COOL, DARK PLACE, LIKE A CUPBOARD:

- Mangos, papayas and pineapples
- Potatoes, onions and garlic



More tips to prevent food waste at [www.EatSmartWasteLess.com](http://www.EatSmartWasteLess.com)

### Happy Earth Day Wilsonville!



Saturday, April 22, 2017

#### Bee Stewards Program Restores Bumblebee Habitat

The City of Wilsonville's Bee Stewards program, in cooperation with the Northwest Center for Alternatives to Pesticides, the West Linn-Wilsonville School District, and other partners is working to restore bumblebee habitat in Wilsonville in a variety of ways.

This spring the program is holding pollinator planting events, one which took place in March, and two

- #### Pollinator Planting Events
- Sat, April 1, 9 am–1 pm  
Water Treatment Plant  
10350 Arrowhead Creek Ln.
  - Sat, April 15, 9 am–1 pm  
Memorial Park

which take place in April (see details to the left).

This year the program is also working to develop an Inte-

grated Pest Management (IPM) plan for the City that utilizes an environmentally-friendly, common-sense approach to controlling pests that relies more heavily on non-chemical methods of pest control and reduced emphasis on using chemical pesticides.

Later this year, or early next year, the City is also planning to hold a workshop to provide the public educational opportunities and materials to learn more about how to help restore pollinator habitat and the environmental benefits.

In part the "Bee Stewards" program is funded by \$81,000 in funding from the National Fish and Wildlife Foundation, the Wilsonville-Metro Community Enhancement Program and Metro's Nature in Neighborhoods Program.

For more information about the program contact Natural Resource Manager Kerry Rappold at 503-570-1570; [rappold@ci.wilsonville.or.us](mailto:rappold@ci.wilsonville.or.us).

is worried about the cost of food and how they can afford to eat.

Annually in the United States we throw 63 million tons of food into the landfill. This Earth Day make a positive impact by taking steps to reduce your food waste. Small individual changes will together have a big impact on our natural resources and community. We can all work towards providing food to those who are in need and wasting less of the food we purchase.

The Eat Smart, Waste Less initiative at Clackamas County's Resource Conservation and Solid Waste program offers tips and tools to save money, understand the 'best by' dates on food and how to develop smart shopping skills.

#### Has your food expired?



Labels cause a lot of confusion and result in people throwing away perfectly good food.

**BEST BY**

Visit [EatSmartWasteLess.com](http://EatSmartWasteLess.com) for more tips on reducing food waste or contact AmeriCorps member Kayla Scheafer at 503-742-4482 or [KScheafer@clackamas.us](mailto:KScheafer@clackamas.us).

### Wilsonville Egg Hunt is Set for Saturday, April 15, in Memorial Park

Wilsonville Parks & Recreation is hosting this year's annual Egg Hunt in Memorial Park, 8100 SW Wilsonville Road. Numerous prizes and gift baskets are being given away! Bring your basket and hunt with us for the perfect egg. The free event is for children ages 1–11 years old.

**Wilsonville Egg Hunt**  
• Sat, April 15, 10 am  
Memorial Park,  
8100 SW Wilsonville Road

The Egg Hunt takes place at 10 am at the Memorial Park ball fields and there is no charge for this activity and registration is not required.



### Academy Mortgage and Wood Middle School Host Shredding Event

Community members are invited to bring up to 10 boxes of documents to a free and secure community shredding event sponsored by Academy Mortgage, Iron Mountain and Wood Middle School. The event takes place at the Academy Mortgage's Wilsonville Branch' in the parking lot, 29100 SW Town Center Loop West on Earth Day, Saturday, April 22, 9 am–noon.

**Community Shredding Event**  
• Sat, April 22, 9 am–12 pm  
Academy Mortgage, 29100 SW Town Center Loop West

Community members should bring and shred documents that contain sensitive and confidential information in order prevent identity theft and other fraud.

Student volunteers from Wood Middle School will help unload and prepare documents to be shredded by a Iron Mountain mobile shredding truck.

The event is free, but participating community members are encouraged to make a financial contribution to a Wood Middle School program that teaches seventh graders about leadership and community involvement.



Wood Middle School students and staff volunteered at last year's community shredding event.

### Rite Aid Distribution Center Hosts 11th Annual Plant Sale

On Friday, April 28, and Saturday, April 29, 9 am– 5 pm, the Rite Aid Distribution Center in Wilsonville is hosting the 11th Annual Plant Sale and Fundraiser to benefit the Children's Miracle Network Hospitals — a non-profit organization that supports Doernbecher's Children's Hospital in Portland.

**Rite Aid Plant Sale/Fundraiser**  
• Fri–Sat, April 28–29, 9 am –5 pm  
Rite Aid Distribution Center  
29555 SW Boones Ferry Road

The sale takes place at Rite Aid's Distribution Center's parking lot located at 29555 SW Boones Ferry and is open to the public.

The plant sale includes most major gardening categories, including trees, potted plants, shrubs, annuals, bulbs, perennials, ground cover and garden

art donated by local nurseries. Local vendors are also invited to participate in the event by renting a booth. For more information contact Ricki Hackworth, event organizer, at 503-685-6069; rhackworth@riteaid.com.



Shoppers at a past year's Rite Aid Annual Plant Sale.

### Bulky Waste Day Set for May 20

In partnership with Metro and Republic Services, the City of Wilsonville is holding Bulky Waste Day on Saturday, May 20, 9 am–1 pm, at 10295 SW Ridder Road in Wilsonville.

**Bulky Waste Day**  
• Sat, May 20, 9 am –1 pm  
Republic Services  
10295 SW Ridder Road

Community members, with proof of Wilsonville residency and toiletries to donate, are invited to drop off large, bulky items that don't fit into standard-sized garbage cans.

The types of waste that **will be accepted** include: dishwashers, televisions, refrigerators, computers, monitors, stoves, dryers, water heaters, couches, mattresses, scrap metal, tables and chairs.

The following are types of items that **will not be accepted** include: construction debris of any type; pro-

pane bottles/canisters; paint; batteries; solvents; thinners; household garbage; tires; and other related items.

The event is free, but participating households will be asked to donate toiletries such as shampoo, soap and deodorant to benefit Wilsonville Community Sharing's Food Bank and the low-income households they serve.



For information contact Matt Baker, Public Works Supervisor, at mbaker@ci.wilsonville.or.us; 503-570-1548.

## City Calendar

| APRIL    |  |
|----------|--|
| 1 Sat    | • Pollinator Tree and Shrub Planting Event, 9 am–1 pm @ Willamette River Water Treatment Plant   |
| 3 Mon    | • City Council 5 pm–Work Session, 7 pm–Meeting, City Hall  |
| 4 Tue    | • Municipal/Traffic Court, 5 pm, City Hall   |
| 10 Mon   | • DRB Panel A, 6:30 pm, City Hall  |
| 11 Tue   | • Tourism Promotion Committee Meeting, 10 am–12 pm, City Hall  |
| 12 Wed   | • Wilsonville Community Seniors Inc. Advisory Board, 1–3 pm, Wilsonville Community Center<br>• Planning Commission, 6 pm, City Hall  |
| 13 Thurs | • Tourism Promotion Committee Meeting, 10 am–12 pm, City Hall<br>• Parks and Recreation Advisory Board Meeting, 4:30 pm, Parks and Rec Administration Building   |
| 15 Sat   | • Pollinator Tree and Shrub Planting Event, 9 am–1 pm, Memorial Park<br>• Wilsonville Egg Hunt, 10 am, Memorial Park   |
| 17 Mon   | • City Council, 5 pm–Work Session, 7 pm–Meeting, City Hall   |
| 18 Tue   | • Tourism Promotion Committee Meeting, 10 am–12 pm, City Hall<br>• Municipal/Traffic Court, 5 pm, City Hall  |
| 19 Wed   | • Wilsonville Town Center Task Force Meeting, 6 pm, City Hall  |
| 20 Thurs | • Parks and Recreation Master Plan Open House, 6–8 pm, City Hall<br>• Citizens Academy, 6–9 pm, Willamette River Water Treatment Plant<br>• Legislative Town Hall, 6:30–7:30 pm, City Hall, Council Chambers |
| 22 Sat   | • Holistic Health Fair, 10 am–1 pm, Wilsonville Community Center   |
| 24 Mon   | • DRB Panel B, 6:30 pm, City Hall  |
| 26 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library   |
| 27 Thurs | • Tourism Promotion Committee Meeting, 10 am–12 pm, City Hall  |
| MAY      |  |
| 1 Mon    | • City Council, 5 pm–Work Session, 7 pm–Meeting, City Hall   |
| 2 Tue    | • Municipal/Traffic Court, 5 pm, City Hall   |
| 8 Mon    | • DRB Panel A, 6:30 pm, City Hall  |
| 9 Thurs  | • Park and Rec Advisory Board Meeting, 4:30 pm, Park and Rec Admin Offices   |
| 10 Wed   | • Wilsonville Community Seniors Inc. Advisory Board, 1–3 pm, Wilsonville Community Center<br>• Planning Commission, 6 pm, City Hall  |
| 13 Sat   | • WERK Day, 8 am–12 pm, Community Center   |
| 15 Mon   | • City Council, 5 pm–Work Session, 7 pm–Meeting, City Hall   |
| 16 Tue   | • Municipal/Traffic Court, 5 pm, City Hall   |
| 18 Thurs | • Budget Committee Meeting, 6–9 pm, City Hall<br>• Citizens Academy, 6–9 pm, City Hall   |
| 20 Sat   | • Bulky Waste Day, 9 am–1 pm, 10295 SW Ridder Road, Wilsonville  |
| 22 Mon   | • DRB Panel B, 6:30 pm, City Hall  |
| 24 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library   |
| 29 Mon   | • City Offices Closed–Memorial Day   |

All dates and times are tentative; check the City's online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and are replayed periodically. Meetings are also available to stream live and by video-on-demand at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.



**Tim Knapp**  
Mayor  
knapp@ci.wilsonville.or.us



**Scott Starr**  
City Council President  
scottstarr97070@gmail.com



**Susie Stevens**  
City Councilor  
stevens@ci.wilsonville.or.us



**Charlotte Lehan**  
City Councilor  
lehan@ci.wilsonville.or.us



**Kristin Akervall**  
City Councilor  
akervall@ci.wilsonville.or.us

### City Manager

Bryan Cosgrove  
503-570-1504  
cosgrove@ci.wilsonville.or.us

### Wilsonville City Hall

29799 SW Town Center Loop E  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)

**Police Non-Emergency Dispatch:** 503-655-8211



# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

February 2017

## Mayor Tim Knapp and Councilors Susie Stevens and Kristin Akervall Are Sworn Into Office

At the Jan. 5 Wilsonville City Council meeting, Municipal Court Judge Michael Gleeson administered the Oath of Office to a re-elected Mayor Tim Knapp and City Councilor Susie Stevens and first-time Councilor Kristin Akervall, all of whom begin four-year terms of office that run through Dec. 31, 2021.

Mayor Knapp is beginning his third full four-year term as mayor and Councilor Stevens is beginning her second four-year term on the Council.

In welcoming Councilor Akervall to the City Council, Mayor Knapp stated, "Thank you so much for putting yourself out there and being willing to take on this challenge."



Members of the Wilsonville City Council are all smiles as they are sworn-in for office (l to r): City Councilor Susie Stevens, City Councilor Kristin Akervall and Mayor Tim Knapp.

In reflecting upon her new term of office Councilor Stevens stated, "I am so honored to serve on City Council for another four years."

After being sworn in Councilor Akervall shared, "I have been so impressed by the level of care by the City Council and look forward to the opportunity to work with all of you."

## City Council Adopts a New Modern Logo Designed by Local Artist

In December, the Wilsonville City Council unanimously approved a new logo for use by the City. The new logo, developed by local resident and graphic designer Jennie Hill, was inspired by the City's former logo that depicts a single hand holding a tree.

The newly-adopted logo pays homage to the City's long-term logo, "hand-in-hand with nature," by featuring a new modern design of two stylized hands along with a new leafy green tree sprout.

Hill explains, "My goal was to take the familiar elements of the City's hand and tree logo and give them a modern makeover."

When discussing the logo design at the Dec. 19 City Council work session, Councilor Charlotte Lehan stated, "I don't want to get too effusive, but I just love it



the way it is." Lehan also added, "This one looks friendly, happy— a happy W."

The winning logo emerged out of a total of 15 different logo design concepts developed by six different local and regional graphic designers that the City contracted with for the project. At the Dec. 5 Council meeting each designer presented the proposed logo concepts and the Council narrowed the options to three various logo concepts.



Jennie Hill

For more information contact Jon Gail, Community Relations Coordinator, 503-570-1502; gail@ci.wilsonville.or.us.

## Municipal Court Judge Michael Gleeson Retires after 28 Years of Service to Wilsonville; New Judge Fred Weinhouse Appointed

Prior to the Jan. 5 City Council meeting, retiring Municipal Court Judge Michael Gleeson, swore-in Fred Weinhouse as the City's newly appointed Municipal Court Judge.

Judge Weinhouse recently retired from a 36-year career as a prosecutor first with Multnomah County and then with U.S. Department of Justice.

Retiring Judge Gleeson served as the City's Municipal Court Judge for the past 28 years, presiding over the court that has jurisdiction over traffic violations, parking citations and local ordinances. Judge Gleeson also



Retiring Judge Michael Gleeson accepts a gavel and gift from Mayor Tim Knapp.

maintained a private law-practice and lived in Wilsonville for many years.

Mayor Tim Knapp thanked Judge Gleeson for his many years of service and welcomed Judge Weinhouse.

Judge Gleeson thanked the City for the opportunity to serve and shared, "One thing I have really appreciated is how the City has always respected the autonomy of the court as a separate branch of government."



Newly-appointed Municipal Court Judge Fred Weinhouse takes the oath of office.

### Mayor's Message

## Join Us in Helping Plan the Future of Wilsonville

In 2017, we look forward to a productive year planning for the future of Wilsonville. We are embarking upon or completing a number of important long-range planning processes and other initiatives that will shape and define our community well into the future.

The exceptional quality of life we enjoy in Wilsonville—and that residents constantly rate highly in community surveys—did not just happen by accident. The Wilsonville we know today is the result of thoughtful and deliberate, design, planning and active community involvement going back many years.

Important long-range initiatives that the City is undertaking in the coming year include:

- **Wilsonville Town Center Plan**, starting with a kick-off event on Tuesday, Feb. 28, is an 18-month-long project that seeks to identify redevelopment strategies for how the 100-acre Town Center area can evolve into a more attractive, pedestrian friendly and commercially vibrant district.
- **French Prairie Bridge Project** advances a long-term community goal that seeks to create preliminary designs for a proposed bicycle/pedestrian/emergency-access crossing of the Willamette River



**All of these public efforts are improved with active community engagement and feedback.**

west of the I-5 Boone Bridge. Please join us at City Hall on

Wednesday, Feb. 22, 5–7 pm, for an open house to learn more.

- **Frog Pond Master Plan**, the result of a two-year-long community-led effort that establishes design standards to guide future growth in the Frog Pond West neighborhood, is having a public hearing on Wednesday, Feb. 8, before the Planning Commission. The City is also working on an infrastructure funding plan for public input in the coming months.
- **Basalt Creek Concept Plan**, a joint effort in partnership with the City of Tualatin expected to be completed this year, establishes how the 800-acre area north of Day Road and west of I-5 is to be annexed and urbanized in the future by the two cities.
- **Transit Master Plan**, which sets forth strategies to guide the direction and services for the City's public transportation agency SMART over the next five years, is nearing completion and recommendation by a citizen task force.

These long-term planning activities are important to maintain our community's quality of life, work successfully with an evolving economy and improve transportation mobility options for residents and businesses. See inside this issue for more news on these projects around the city or online at [www.ci.wilsonville.or.us/2017projects](http://www.ci.wilsonville.or.us/2017projects).

All of these public efforts are improved with active community engagement and feedback — I invite your participation. Feel free to let me know your thoughts; reach me at 503-896-0048 or [mayor@ci.wilsonville.or.us](mailto:mayor@ci.wilsonville.or.us), or join me in attending one of the many upcoming open houses, feedback sessions and public hearings planned for these important initiatives in the coming year.

*Tim Knapp, Mayor*

City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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**Library Celebrates 35th Year of Operations on Feb. 14**

In 1982, the Wilsonville Public Library opened in a 1600-square-foot space on Wilsonville Road. That first year, the library checked out 27,000 items to Wilsonville residents. In 2017, the library has a 20,000-square-foot building and checks out 40,000 items per month. The library has grown in so many other ways as well.



The Wilsonville Public Library celebrates its 35th birthday on Tuesday, Feb. 14 at 2 pm. Join in the festivities with a birthday party! There is a birthday cake and coffee at 2 pm compliments of the Wilsonville Friends of the Library and the Wilsonville Public Library Foundation.

Since birthdays usually involve not just cake but also gifts, the library will give waivers of overdue fines (up to \$10) to library users who come to the library that day. Just bring your library card to the Circulation Desk and say, "Happy Birthday!"

**Library's 35th Birthday Party**  
 • Tue, Feb 14, 2 pm  
 Wilsonville Public Library  
 8200 SW Wilsonville Road

Looking for a way to give back to the Library? Become a volunteer, join the Wilsonville Friends of the Library, donate books, or give to the Wilsonville Public Library Foundation. Whatever way you choose to help, we thank you!

For more information, visit the library website at [www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org).

**Wilsonville Town Center Plan Begins with Feb. 28 Kick-Off Event**

How do you want Wilsonville's Town Center to look, function, and feel 20 years from now? The City wants to hear your ideas! The City is planning for the future of Town Center through the Wilsonville Town Center Plan, a community-driven process to guide development in the heart of our City.

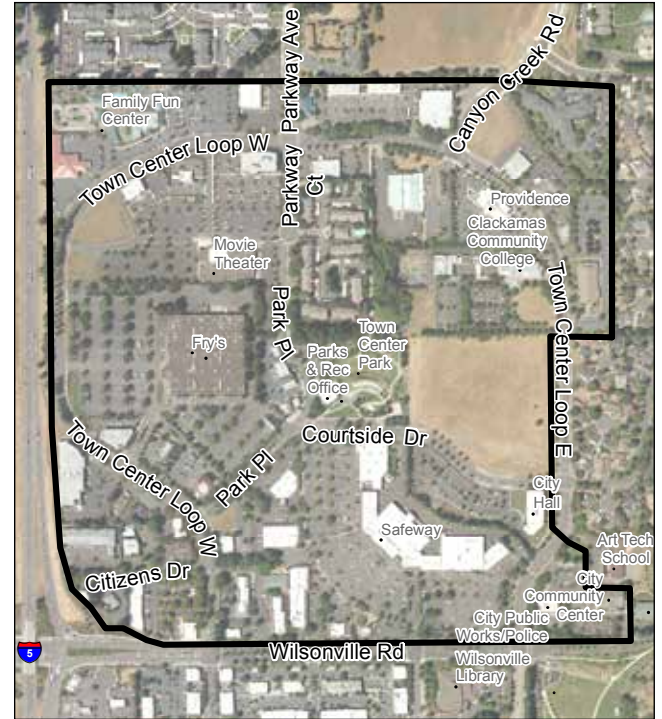


Join us at the upcoming kick-off event to share your vision for the future of Town Center. Come tell us what you value and enjoy and what you would like to see changed or improved in Town Center.

The Wilsonville Town Center Plan Community Kick-Off Workshop is on Tuesday, Feb. 28, 5:30–8 pm, at City Hall, 29799 SW Town Center Loop East. The event starts at 5:30 pm with presentations and activities 6–8 pm and light refreshments are being provided.

**Wilsonville Town Center Plan Community Kick-Off Workshop**  
 • Tues, Feb 28, 5:30–8 pm  
 City Hall, 29799 SW Town Center Loop East

Visit [www.wilsonvilletowncenter.com](http://www.wilsonvilletowncenter.com) for more information about the project or contact Miranda Bateschell, Long-Range Planning Manager, at [bateschell@ci.wilsonville.or.us](mailto:bateschell@ci.wilsonville.or.us) or 503-570-1581.



Wilsonville Town Center is approximately 100 acres and encompasses the properties north of Wilsonville Road, within and adjacent to Town Center Loop.

**School District Expands Preschool Offerings**

The West Linn-Wilsonville School District is now accepting preschool registrations for the 2017-18 school year. The local school district offers tuition based preschool at six primary schools—Boones Ferry, Bolton, Cedaroak Park, Stafford, Sunset and Boeckman Creek. Registration packets are available at the primary schools offering preschool or may be downloaded from the school district's website at [www.wilps.wlww.k12.or.us/Page/2813](http://www.wilps.wlww.k12.or.us/Page/2813).

The district's early childhood programs are based on the belief that young children "learn by doing." The

goal of the program is to engage children in experiences that enhance the natural processes of physical and intellectual growth.

In Wilsonville, the district offers preschool at Boones Ferry Primary and is now also offering a morning preschool class with a Spanish language integration at Boeckman Creek Primary. The district also offers Chinese language integration preschool at other primary schools in the district. For more information visit [www.wilps.wlww.k12.or.us/Page/2813](http://www.wilps.wlww.k12.or.us/Page/2813) or contact the primary school of your preferred program.



8200 SW Wilsonville Road  
 503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

**Family Heart Healthy Fair on Saturday, Feb. 25**

February is National Heart Healthy Month. We're celebrating at the library with a Family Heart Healthy Fair on Saturday, Feb. 25, 12–4 pm.



Try some family exercises, find your inner drummer with a taiko performance, and more. Visit the Vendor Room to discover local organizations and companies who want to help you get heart healthy. Get your heart pumping at this free community event!

**Saturday, Feb. 25, 12–4 pm**  
 Throughout the Library

**Activity Schedule:**

- **Eat Smart!** presentation in the Oak Room, 12–1 pm
- **Family Fitness class** in the Oak Room, 1–1:30 pm
- **Sheridan Taiko** in the Reading Area, 2–3 pm:
- **Family Food Crafts** in the Oak Room, 3–4 pm.
- **Local organizations and vendors** will be sharing information and answering questions in the Rose Room, 12–4 pm.

[www.WilsonvilleLibrary.org/heart](http://www.WilsonvilleLibrary.org/heart)

**Curiosity Cafe: Ukulele 101**

Have a cup of coffee and join us for Curiosity Cafe. This month, come learn to play the ukulele, a fun and simple instrument. Ukuleles will be provided.



This free event is on Saturday, Feb. 18, 1-3 pm, in the Oak Room.

**RFID is Coming to the Library**

We are excited to announce that the library will soon start using RFID for books, DVDs, CDs, and other library items.



RFID will make checking items out much faster and allow staff to focus more on customer service and offering additional programs.

Implementation takes approximately five weeks as we "tag" every item in our collection. Because of this, there may be some disruption in the library, in addition to some collection unavailability and decreased service levels.

If you have any questions, please ask us. [www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

**Baby Time Tuesdays at 11 am**

Babies up to 12 months old and their grownups are invited to come to Baby Time at the Library every Tuesday, 11–11:45 am. Fun activities include songs, rhymes and stories just right for the very youngest children. Then we bring out the toys so the babies can play!



[www.WilsonvilleLibrary.org/babytime](http://www.WilsonvilleLibrary.org/babytime)

**Upcoming Classes and Events**

**Library Closure**  
*Presidents' Day*  
 Monday, Feb. 20

**Food for Fines in February**  
 Reduce overdue fines by bringing in cans or boxes of food. 1 item = \$1. No ramen!  
[www.WilsonvilleLibrary.org/food](http://www.WilsonvilleLibrary.org/food)

**First Friday Film**  
 Friday, Feb. 3 6–8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

**Book Club**  
*"At the Water's Edge" by Sara Gruen*  
 SPECIAL DATE  
 Thursday, Feb. 2, 6–8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

**Book Notes Concert:**  
*Jason Okamoto* (Solo acoustic guitar)  
 Saturday, Feb. 11, 2–3 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

**Teen Event: Lego Night**  
 Friday, Feb. 17, 6:30–8:30 pm  
[www.WilsonvilleLibrary.org/teens](http://www.WilsonvilleLibrary.org/teens)

**Book Club**  
*"The Death of Ivan Ilyich" by Leo Tolstoy*  
 Tuesday, Feb. 21, 6–8 pm  
[www.WilsonvilleLibrary.org/gb](http://www.WilsonvilleLibrary.org/gb)

**Visit our website for more events:**  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

## CHIEF'S CORNER – MOST COMMON VIOLATIONS CITED IN 2016

This month, I thought Wilsonville residents and businesses would be interested in seeing what the most common violations for which citations were issued by the Wilsonville Police Department in 2016. This list should also serve as a reminder of the rules of the road.



Police Chief Adam Phillips

Of course, this list does not include all 129 different violations for which over 2000 moving citations were issued, but it does give you a good idea of what the most common violations were.

Our purpose in educating you about the traffic citations we issued is to deter these infractions in the future and to increase vehicular and pedestrian safety. Please drive safely.

*Adam Phillips, Wilsonville Chief of Police*

### Most Common Moving Violations Cited in 2016 by Wilsonville Police Department

|   |     |   |    |
|---|-----|---|----|
| 1. Parking                                  | 388 | 15. Failure to display registration           | 15 |
| 2. Speeding/violation of the basic rule     | 357 | 16. Careless driving                          | 14 |
| 3. Driving with a suspended license         | 231 | 17. Failure to change name/address on license | 14 |
| 4. No driver's license                      | 179 | 18. Following too closely                     | 14 |
| 5. Driving uninsured                        | 168 | 19. Failure to register vehicle in Oregon     | 13 |
| 6. Failure to obey traffic control device   | 131 | 20. Failure to renew registration             | 12 |
| 7. Illegal alteration/switched plates       | 130 | 21. Illegal standing/stopping or parking      | 10 |
| 8. Failure/improper display of sticker      | 123 | 22. Unsafe distance from emergency vehicle    | 9  |
| 9. Failure to carry proof of insurance      | 69  | 23. Failing to chain commercial motor veh.    | 8  |
| 10. Illegal use of cell phone               | 48  | 24. Failure to use safety belt                | 8  |
| 11. Driving under the influence (2015 data) | 42  | 25. Illegal u-turn                            | 7  |
| 12. Unsafe movement from lane               | 23  | 26. Driving outside of restrictions           | 6  |
| 13. Failure to drive in lane                | 22  | 27. Use of prohibited lighting equipment      | 6  |
| 14. Violating maximum weight limits/truck   | 16  | 28. Making an unlawful unsignaled turn        | 5  |

## TVF&R Encourages You to Be a Vital Link in the Chain of Survival for Cardiac Patients

Tualatin Valley Fire & Rescue seeks to create safer communities by working with community partners to help cardiac patients.

TVF&R teaches community groups hands-only CPR, encourages law enforcement partners to respond to cardiac emergencies equipped with automated external defibrillators, and invites community members to download the free PulsePoint smart-phone app that sends alerts when someone is in need of this lifesaving assistance.



During American Heart Month, TVF&R invites community members to join them in raising awareness of the signs and symptoms of a heart attack and the importance of immediately calling 911 in the event

someone is experiencing heart attack symptoms.

Unlike a cardiac arrest that strikes suddenly and includes a loss of consciousness, a heart attack can develop slow enough that you are not aware you are having a medical emergency. A heart attack occurs when the blood flow is severely reduced or cut off.

### Heart Attack symptoms typically include:

- **Chest discomfort** in the center of the chest that lasts for more than a few minutes or goes away and returns. It can feel like uncomfortable pressure, squeezing, fullness or pain.
- **Upper body discomfort**, such as pain or discomfort in one or both arms, the back, neck, jaw or stomach.
- **Shortness of breath** before, during or after other discomforts.
- **Other signs**, including breaking out in a cold sweat, nausea or lightheadedness.

The heart is a muscle and can deteriorate with each minute you delay medical treatment. Don't ignore

symptoms and don't drive yourself to the hospital.

Remember, TVF&R crews are here for you. TVF&R's highly trained firefighter paramedics can diagnose a heart attack and start treatment right away. Crews can also relay your EKG information to the hospital, enabling the hospital staff to activate special heart teams and prepare for your arrival.

A heart attack can cause sudden cardiac arrest, when the heart malfunctions and stops beating unexpectedly. Sudden cardiac arrest leads to death when the heart stops working properly. This can be reversed if CPR is performed and a defibrillator is used to shock the heart and restore a normal heart rhythm.

Hands-only CPR has just two easy steps: Call 911 and push hard and fast in the center of the chest to the beat of the disco song "Stayin' Alive."

For more information about PulsePoint, symptoms of cardiac emergencies, and CPR training, visit [www.tvfr.com](http://www.tvfr.com).

## SMART

SOUTH METRO AREA REGIONAL TRANSIT

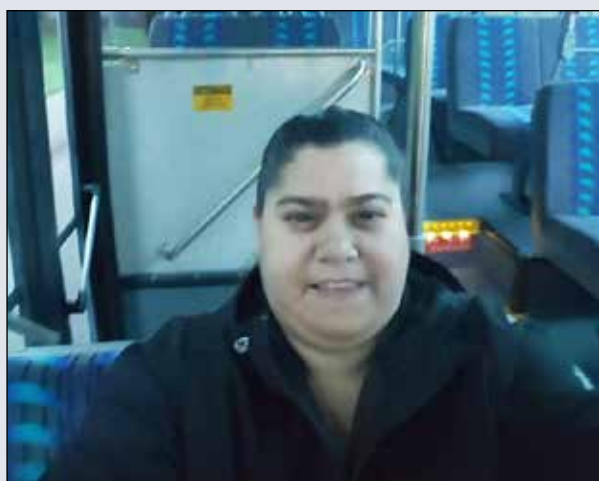
29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

### Rider Spotlight Feb 1-28

Time to shine the spotlight on our SMART riders via Facebook! Visit SMART's Facebook page and tell us:

- Why you ride SMART?
- How long you have ridden?
- Take a photo of yourself on a SMART bus, post on SMART's Facebook page.

The photo with the most likes by the of February receives their choice of monthly pass.



I ❤️ SMART!

### Travel Training Now Available at SMART

Please welcome Sydney Herbst to SMART's public-outreach team. While Sydney is an employee of Ride Connection, a tri-county area non-profit transit provider, she is working out of the SMART office on a special assignment. Sydney's job is to help train people on how to use the local transit system.

Sydney has been specifically trained in how best to work with the public in helping them feel more comfortable about using transit services. While travel training always includes a focus on helping the elderly and people with disabilities, it is also well known that many other people do not



Sydney Herbst

ride transit simply because they are unsure how to go about it. Sydney is here to help.

SMART is able to have Sydney's help because we were successful in securing a grant for this purpose.

*Sydney has been specifically trained in how best to work with the public in helping them to feel more comfortable about using transit services.*

We are fortunate to have her help and we hope lots of Wilsonville residents and employees take advantage of the opportunity to spend some time with her and get their questions answered about riding transit.

To contact Sydney directly 503-682-7790 x 1438 or email [sherbst@rideconnection.org](mailto:sherbst@rideconnection.org). Visit [www.ridesmart.com/RideWise](http://www.ridesmart.com/RideWise) to learn more.

### Community Feedback needed on Draft Transit Master Plan

The City is seeking public comment on the draft Transit Master Plan which is a broad look ahead to the type of transit system and supportive transportation options required to meet Wilsonville's mobility needs. A copy of the draft Transit Master Plan is available at [www.ridesmart.com/TMPdraft](http://www.ridesmart.com/TMPdraft).

Residents, business owners and Wilsonville commuters are invited to review the plan and provide feedback in any of the following ways:

- **Online** via our survey link: [www.ridesmart.com/TMPcomment](http://www.ridesmart.com/TMPcomment)
- **Email:** [marston@ridesmart.com](mailto:marston@ridesmart.com)
- **Phone:** 503-570-0332
- **Mail:** SMART TMP Comments  
29799 SW Town Center Loop East  
Wilsonville, OR 97070



## City Hosts French Prairie Bridge Project Open House on February 22

The City of Wilsonville is hosting a public open-house for the French Prairie Bridge Project on Wednesday, Feb. 22, 5–7 p.m., at City Hall, 29799 SW Town Center Loop East, Wilsonville 97070. The public event features members of the project management team providing an overview of the proposed bicycle/pedestrian/emergency-access crossing of the Willamette River west of the I-5 Boone Bridge.

**French Prairie Bridge Project Open House**  
 • Wed, Feb 22, 5–7 pm  
 City Hall, 29799 SW Town Center Loop East

Materials from the open house are available online at [www.FrenchPrairieBridgeProject.org](http://www.FrenchPrairieBridgeProject.org), where the public can sign-up to be on a mailing list, submit a comment or question, or view other information.



For two weeks after the live open house, the City will continue to accept comments and feedback online.

The goal of the French Prairie Bridge Project, funded by a \$1.25 million grant awarded through METRO regional government's metropolitan transportation improvement program, is to develop preliminary designs for the proposed bicycle/pedestrian/emergency access crossing of the Willamette River. The project, undertaken in partnership with Clackamas County, the Oregon Department of Transportation, seeks to engage the public in addressing key questions about the bridge location, alignment, and design and thereby help position the community and the region to decide

whether to pursue funding to create a final bridge design and construct the proposed bridge.

The project began in Fall 2016 and is planned to be complete by Fall of 2018 when the Wilsonville City Council is expected to consider the proposed bridge design project. The project includes public

open houses and other opportunities for public input regarding the preferred bridge type and landing points throughout the various stages of the process.

For more information, see [www.FrenchPrairieBridge-Project.org](http://www.FrenchPrairieBridge-Project.org), or contact Zachary Weigel, PE, Project Manager, at 503 570-1565, [weigel@ci.wilsonville.or.us](mailto:weigel@ci.wilsonville.or.us).

### City Names French Prairie Bridge Project Task Force

The City has named members of the public to serve on an advisory task force for the French Prairie Bridge Project. The first task force meeting is open to the public and scheduled for Tuesday, Jan. 31, 6–9 p.m., at Wilsonville City Hall

The French Prairie Bridge Project Task Force reviews key information and provides recommendations to the City Council, at key milestones in the planning and design process. The group's membership provides representation of a wide range of stakeholder values and interests and includes residents of Wilsonville and surrounding areas. Task force members represent affected neighborhoods and businesses, walking and cycling enthusiasts, local parks and trails interests, tourism associations and emergency service personnel. The task force members include:

- **Co-Chair Jim Bernard**, Clackamas County Board of Commissioners Chair as ex-officio/non-voting member
- **Co-Chair Charlotte Lehan**, Wilsonville City Councilor as ex-officio/non-voting member (Councilor Susie Stevens to act as alternate co-chair)
- **Jeremy Appt**, Wilsonville resident who is a civil engineer
- **Blake Arnold**, Bicycle commuter and Lake Oswego resident
- **Heidi Bell**, City Manager for the City of Donald, located just south of Wilsonville in Marion County
- **Steve Chinn**, Clackamas County resident who lives on the south side of Willamette River
- **Andrew Harvey**, retired Wilsonville resident who is an environmental geologist
- **Tony Holt**, Charbonneau Country Club President

and former member of the City Budget Committee, Wilsonville resident

- **Karen Houston**, Program Coordinator for FACT Oregon, a disability-rights organization
- **Pete Ihrig**, West Linn resident and Clackamas County Pedestrian/Bikeway Advisory Committee member
- **Douglas Muench**, Wilsonville resident and Old Town Neighborhood Association President
- **Samara Phelps**, Tourism Development Lead with Clackamas County Tourism and Cultural Affairs
- **Patricia Rehberg**, Wilsonville resident and Wilsonville Wheelers bike-group leader
- **Michelle Ripple**, Wilsonville resident and former City Councilor who chaired the prior parks and transit master plan
- **Leann Scotch**, Wilsonville resident who works as REI Operation Sales Lead
- **Brian Sherrard**, Division Chief with Tualatin Valley Fire and Rescue (TVFR)
- **Ryan Sparks**, Operations Support Manager Willamette River Trail and Scenic Bike Routes, Oregon Parks & Recreation Dept.
- **Simon Springall**, Wilsonville resident and City Planning Commissioner
- **David Stead**, Langdon Farms Golf Club General Manager and Wilsonville Tourism Promotion Committee member
- **Steven van Wechel**, Wilsonville Old Town Neighborhood resident
- **Gary Wappes**, retired Wilsonville resident active with the Kiwanis Club



29600 SW Park Place  
 Parks Programs: 503-783-7529  
 Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

### Register for Spring Classes!

Registration for the Spring session of classes is now open. Classes begin at the end of February and into early March. Activities for all ages can be viewed and registered for online at [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com).



### Park Rental Season Opens Soon

Memorial Park is a popular location for company gatherings, family parties, weddings and a variety of sporting events. Wilsonville Parks and Recreation offers a number of park rental options including: the historic Stein Boozier Barn, the River Shelter, the Forest Shelter, Grove Shelter, Splash Shelter and sport fields.



Reservations for Wilsonville residents and businesses will be accepted starting on March 13th for the 2017 season. The rental season runs April 8– Oct. 15.

For more information on availability and fees please visit [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com) or contact Ahsamon at 503-570-1530.

### Holistic Health Fair

Please join the Parks and Recreation Department and local health practitioners for the Holistic Health Fair on Saturday, April 22, 10 am–1 pm, at the Community Center. Formerly called Spa Saturday at the Center, this annual health and wellness event features mini classes, sample services and vendors, with a focus on holistic therapies and alternative medicine. Past years have featured samples of yoga, tai chi, massage, acupuncture, and vendors selling essential oils, eco-friendly cleaning products, organic bath products and more!

The event is open to all ages and is free to attend. Practitioners and vendors interested in participating may submit a vendor interest form on the event webpage. Visit [WilsonvilleParksandRec.com/HolisticHealthFair](http://WilsonvilleParksandRec.com/HolisticHealthFair) for more info.

### Nutrition Program at the Center

The Wilsonville Community Center offers a nutrition program Mon, Tue, Wed and Fri. The lunch program is by donation with a suggested amount of \$3.50 for those 60 and over and \$4.25 for those under 60. Additionally, a Home Delivered Meal Program provides daily weekday meals to home-bound seniors and persons who are disabled on a regular basis, or temporarily during recovery from surgery or illness. There is no charge for this service but donations are gladly accepted. Please call 503-682-3727 to arrange for home delivered meal service, or if you are interested in a volunteer meal driver position.

### Spring Break Camps

Parks and Recreation is happy to partner with two organizations to offer spring break camps. Skyhawks Sports Academy is offering a basketball camp for 7–12 year olds. The camp runs Monday to Friday, 9 am–3 pm and costs \$145. Coyle Outdoors is offering their popular, "Intro to Survival Camp," for 6–9 year olds. The camp runs Monday to Friday, 9 am–1 pm and costs \$215. Both camps are currently available for registration online.

### Upcoming Classes and Events

#### Oil Painting

Ages 12 and older  
 Saturday, Feb. 11  
 10 am–2 pm  
 Parks and Rec Admin Office  
 \$50

#### Daddy Daughter Dance

Limited Spaces Remain!  
 Friday, Feb. 24  
 7–9 pm  
 \$15 per person

#### Chalk Paint Project - Chalkboard

Ages 18 and older  
 Saturday, Feb. 25  
 10 am–1 pm  
 Parks and Rec Admin Office  
 \$45

#### Fit Girls!

Ages 11 and older  
 Mondays, Feb. 27 to April 24  
 7–8 pm  
 Community Center  
 \$59

#### Fishing Workshop

Ages 8–12  
 Saturday, March 4  
 9 am–4 pm  
 Mary S. Young Park  
 \$62

#### Nia

Ages 18 and older  
 Fridays, March 10 to April 28  
 9–10 am  
 Community Center  
 \$47

## Our History: 1937 Storm Drops 23 Inches of Snow on Wilsonville



These Boones Ferry Historical Society photos show (above) people standing in 23 inches of snow (unofficial total) that fell in Wilsonville on Jan. 31, 1937, and (below) horses pulling a sleigh in front of Aden's Store on Boones Ferry Road.



## Findlay Car Dealership Supports Early-Child Literacy Program

Dolly Parton's Imagination Library in Wilsonville is the February 2017 Findlay Chrysler Jeep Dodge Ram of Wilsonville's Charity of the Month.

"We are so pleased that Findlay of Wilsonville is honoring Imagination Library in this way. The program now send books to over 700 Wilsonville preschoolers each month and we have graduated over 650 children," said Jan Rippey, Executive Director of Dolly Parton's Imagination Library in Wilsonville.

The Charity of the Month is a fundraising program where Findlay of Wilsonville features a local charitable organization. The monthly program provides public exposure and a monetary donation to the selected nonprofit.

"Findlay Automotive Group places a high value on giving back to the community and supporting worthy efforts," said Laurie Tarter, Findlay's Community Relations Manager.

All children living in Wilsonville can sign up for Dolly Parton's Imagination Library to receive an age-appropriate book each month until the child turns five years old. Books are mailed directly to the child's home in their name at no cost.

To sign up a child up to receive books through Dolly Parton's Imagination Library, go to [www.imaginationlibrary.com](http://www.imaginationlibrary.com).



Partners pose for photo (l to r): Findlay of Wilsonville General Manager Jack Grimley, DPIL Executive Director Jan Rippey and Library Director Pat Duke.

Dolly Parton's Imagination Library is a project of the Wilsonville Public Library Foundation. For more information about the Wilsonville Public Library Foundation visit [www.wplf.org](http://www.wplf.org).

## State Selects Wilsonville's Drinking-Water System as an Outstanding Performer

On Jan. 5, the Oregon Health Authority's Drinking Water Program announced that the City of Wilsonville's public drinking water-system, which includes the Willamette River Water Treatment Plant, managed in partnership with Veolia North America, is as an "Outstanding Performer."

The Oregon Health Authority (OHA) staff made the determination after conducting an on-site review of the water system on Nov. 16. The review, which takes place every three to five years, evaluates the system's capability of providing safe water to the public and covers source water, treatment, storage facilities, distribution system, operation and maintenance procedures, monitoring, and management of the drinking water system.

In addition to verifying the system's capability and compliance, OHA further determined that Wilsonville's drinking water system also meets all of the following criteria:

- No Maximum Contaminant Level (MCL), Action Level, or Treatment Technique violations in the last five years;
- No more than one monitoring and reporting violation in the last three years.
- No significant deficiencies or rule violations identified during the current water-system survey; and
- No waterborne disease outbreaks in the last five years.

City Public Works Director Delora Kerber states, "OHA's thorough and independent

review confirms that Wilsonville's water system is being operated and maintained to the highest standards. We're very proud of being designated again as an Outstanding Performer."

The City of Wilsonville was previously awarded the title in November 2011.

For more information about Wilsonville's drinking water system and to review the City's Annual Water Quality Report visit [www.ci.wilsonville.or.us/water-quality](http://www.ci.wilsonville.or.us/water-quality).



City and Veolia North America staff pose with OHA award at the water treatment plant (l to r): Utilities Supervisor Jason Labrie, Public Works Director Delora Kerber, Veolia Project Manager Howard Hamilton and Veolia Plant General Manager Mike Greene.

## Grants Available for Tourism and Community Programs

The City of Wilsonville's Parks and Recreation Department is now accepting applications for the Community Tourism and the Community Opportunity Grant Programs. There are separate deadlines for the two grant programs, both of which are awarded by the Parks and Recreation Advisory Board.

The **Community Tourism Grant Program** awards funds to organizations that produce projects, programs or events that promote local business and tourism, and for festivals and special events for

### Community Tourism Grant Due Date

- **Fri, Feb 3, 5 pm**  
Parks and Recreation Office,  
29600 SW Park Place

the benefit of the Wilsonville community. **The deadline for the Tourism Grant is Friday, Feb. 3.** Funding for the program comes from the City's transient room taxes that are assessed on overnight lodging visitors.

The **Community Opportunity Grant Program**, with funds distributed twice per year for a total of \$25,000, provides funding for a wide range of local programs and projects in Wilsonville. Funds for the Opportunity Grant program come from the City's general fund.

Successful applications for the Community Opportunity Grant program include proposals aimed

### Community Opportunity Grant Due Date

- **Fri, March 3, 5 pm**  
Parks and Recreation Office,  
29600 SW Park Place

at promoting education, diversity, arts or community involvement in the city. **Applications for the Community Opportunity Grant are due Friday, March 3.**

For additional grant funding for tourism related programs and events see article on the right.

For more information, go online to [www.WilsonvilleParksandRec.com/GrantPrograms](http://www.WilsonvilleParksandRec.com/GrantPrograms); or contact Brian Stevenson, Recreation Coordinator, at 503-570-1523, [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

## Clackamas County Offers Grants to Attract Tourists

The City is accepting grant applications for local tourism projects that aim to increase the numbers

### Clackamas County Tourism Grant Due Date

- **Mon, Feb 13, 5 pm**  
Parks and Recreation Office,  
29600 SW Park Place

of tourist visitors and increase the economic impact of tourism. In partnership with Clackamas County Tourism, up

- to \$20,000 is available for projects that can:
- Increase overnight lodging stays in the County.
- Bring visitors into communities from greater than 50 miles away to shop, dine, recreate, take in arts or cultural offerings, or visit heritage sites.
- Entice visitors to "linger longer."

The maximum grant for a single project is \$20,000; the minimum request is \$500. Funding for the Clackamas County Tourism-Community Partnership Program (CPP) grants comes from the County's 6% transient room tax paid by overnight lodging visitors. Eligible projects, which must take place within Clackamas County, must be completed by Dec. 31, 2017.



The local soccer tournament received a grant in 2016.

Applications are due by end of day, Feb. 13, 2017. The on-line application form can be found at [www.mthoodterritory.com/cppapp](http://www.mthoodterritory.com/cppapp). This City-administered County tourism grant program is separate and distinct from the City's Community Tourism and Community Opportunity grant programs. Application to the County's program does not preclude application to the City's program.

Proposals are to be evaluated by the Parks and Recreation Advisory Board based on: likelihood of attracting visitors; ability to generate overnight stays; partnerships; use of additional/leveraged funds; self-sustaining growth and capacity-building potential; and measurable ROI (15%). For more information, contact Brian Stevenson, Recreation Coordinator, at 503-570-1523; [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).



## Reports from City Boards and Commissions

### Council Sworn In, Appoints New Municipal Court Judge and Updates Road Maintenance Fee

On Jan. 5, the Wilsonville City Council held a swearing-in ceremony for Mayor Tim Knapp and City Councilors Susie Stevens and Kristin Akervall who were elected to office in the November general election. Municipal Court Judge Michael Gleeson swore into office the three members of City Council who begin four-year terms of office that extend until Dec. 31, 2021.

Also during the ceremony Judge Michael Gleeson, who is retiring from the position, swore-in Fred Weinhouse as the City's newly appointed Municipal Court Judge.

At the meeting the Council approved revisions and established a new method and billing rate for the City's road-maintenance fee program to ensure that revenues keep pace with the cost to complete annual repairs needed to adequately maintain Wilsonville's public roads. The adopted changes were developed and recommended by a citizen task force composed of local residents and business representatives in response to concerns that the road-maintenance fund was not keeping pace with increasing road costs. Beginning in March and appearing on April utility bills, the current monthly charge per single-family household of \$4.03 increases to \$5.23 per month; a second adjustment moving to \$7.62 per month is set for November 2017, with annual adjustments each Nov. 1 thereafter based on changes to a northwest construction cost index. The fees for commercial and non-residential utility customers are based on the number of trips the business/establishment generates. For more information,

contact Mike Ward, Civil Engineer, at 503-570-1546; ward@ci.wilsonville.or.us.

Under Mayor's Business, Mayor Knapp issued a proclamation in honor of the upcoming 2017 Martin Luther King, Jr. Day, on Jan. 16 that declare Wilsonville to be a community that welcomes people of all ethnicities, fosters mutual respect and encourages volunteer service. In support of the proclamation, WilsonvilleSTAGE artistic Director Terry Kester discussed how the local theater company is currently preparing to perform "The Laramie Project" in late February and early March.

Also during Mayor's Business the City Council elected City Councilor Scott Starr to continue as Council President for the remaining two years of his term which lasts until Dec. 31, 2019.

The Council agreed to continue until the Council's Thursday, Jan. 19, meeting an ordinance to create a residential parking-zone program.

On the Consent Agenda the Council approved a resolution to enter into an agreement with Clackamas County to transfer roadway authority for portions of Stafford Road and Advance Road.

During work session prior to the meeting, the City Council heard reports and discussed, City Council liaison appointments, Council appointments to external committees and the Wilsonville Town Center Plan that is holding a public kick-off event on Tuesday, Feb. 28, 5:30 pm, at City Hall, 29799 SW Town Center Loop East.



Members of City Council thank outgoing Councilor Julie Fitzgerald (center) who completed a four-year term on Council.

### City Council Approves Preferred Alignment for New Boones Ferry to Brown Road Corridor and Adopts New Logo

On Dec. 19, the Wilsonville City Council held a public hearing and selected 5th Street as the preferred final alignment for the proposed Boones Ferry Road to Brown Road Corridor, formerly known as "The Old Town Escape." The proposed new roadway seeks to offer an alternative to using Wilsonville Road as the only access route for the Old Town commercial and residential neighborhood. Traffic studies show that the new roadway will provide traffic congestion relief and improved circulation at the intersection of Wilsonville and Boones Ferry Roads near I-5.

In selecting the 5th Street alignment, the Council noted extensive public outreach and technical research comparing the 5th Street route with an alternative alignment on Bailey Street. The Council found that the 5th Street alignment offers a safer route, costs less to construct, provides greater relief from traffic congestion and utilizes an existing and approved railroad crossing. The Council unanimously supported the decision by a vote of 4-0, with Mayor Tim Knapp abstaining due to a potential perception of a conflict of interest as an owner of commercial property located in the area.

Under new business the Council amended the agenda to include unanimous approval of a new City logo developed by local resident and graphic designer Jennie Hill. The newly-adopted logo pays homage to the City's long-term logo, "hand-in-hand with nature," by featuring a new modern design of two stylized side-by-side hands along with a new leafy green tree sprout. The

designer explains, "My goal was to take the familiar elements of the City's hand and tree logo and give them a modern makeover." The City is working with the Hill to develop a branding guide and is preparing to launch the new logo in the new year.

The Council also held a public hearing and approved revisions to the City's road maintenance fee program as recommended by a task force comprised of local residents and business representatives that found the program was not keeping pace with road maintenance needs.

The Council held a second reading and closed a public hearing to approve an ordinance that amends the City's right-of-way and public-easement management section of City Code. The ordinance clarifies the method of determining the fees to be charged by the City for use of the public right-of-way by utility providers that may or may not provide service to Wilsonville customers. In particular the code amendments are needed to clarify how to calculate and assess the fees the City will charge for the four miles of 66-inch-diameter water pipeline being planned and constructed to pass through Wilsonville to provide water to Tualatin Valley Water District and City of Hillsboro customers.

On the Consent Agenda the Council approved awarding a three-year street sweeping contract to Water Truck Service, Inc., which was identified and selected by the City through a formal request for proposals process.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during February. Scheduling changes can and do occur; check for updated meeting information on the City's website, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, Feb. 6:** Transit Master Plan; Frog Pond infrastructure financing; Frog Pond Master Plan; meter reading contract; accepting authority for sections of Stafford and Advance Roads; ground lease agreement (second reading); water treatment plant design, construction, operator and property ownership (second reading).

**Monday, Feb. 20:** Oregon Travel Information Council/Oregon Travel Experience presentation; Willamette Falls Locks update; design contract for Boones Ferry Road to Kinsman connector; Water Treatment Plant Master Plan (first reading).

### Planning Commission Update

The Planning Commission meeting on Wednesday, Jan. 11, was cancelled due to severe inclement weather. On Feb. 8 the Planning Commission is planning a public hearing regarding the Water Treatment Plant Master Plan update.

### Development Review Board Update

The regularly scheduled DRB Panel A meeting for Monday, Jan. 9, was cancelled. The next DRB Panel A meeting is scheduled for Monday, Feb. 13.

The regularly scheduled DRB Panel B meeting for Monday, Jan. 23, was cancelled. The next DRB Panel B meeting is scheduled for Monday, Feb. 27.

### City Seeks Feedback on Updated Water Treatment Plant Master Plan

The City of Wilsonville, in partnership with the Willamette Water Supply Program (WWSP) is completing a master plan update for the Willamette River Water Treatment Plant. The master plan update

**Planning Commission Hearing Water Treatment Plant Master Plan Update**  
• Wed, Feb 8, 6 pm  
City Hall, Council Chambers

identifies long-range water needs for the City and WWSP, the treatment technologies to be used and the capital improve-

ments needed to meet these future goals.

The Wilsonville Planning Commission is to receive a briefing on Jan. 11, and hold a formal public hearing and receive public comments on the master plan update at the commissions' meeting on Feb. 8, 6 p.m. at City Hall, 29799 SW Town Center Loop East.

The public is also invited to review and provide comments on the master plan update through an online virtual open house located at [www.ci.wilsonville.or.us/WTPMPupdate](http://www.ci.wilsonville.or.us/WTPMPupdate).

For information on the Willamette Water Supply Program, please visit [OurReliableWater.org](http://OurReliableWater.org). For more information on the master plan update, contact Eric Mende, PE, Capital Projects Engineering Manager, at 503-570-1538; mende@ci.wilsonville.or.us.

### Get Up to Five Free Trees

To promote healthy cool streams in Wilsonville, the City offers community members and local property owners up to five free native tree



seedlings to plant on their property. The City encourages plantings along streams and other water bodies to reduce stream temperature and pollutants. The trees are obtained at a local nursery.

For more information and to get a

tree coupon for five trees, contact Luke Bushman, Stormwater Coordinator, at 503-570-1552; bushman@ci.wilsonville.or.us.

## Rotary Club Nominates Four Finalists for 2016 Wilsonville First Citizen

Four leaders from the Wilsonville community have been selected as finalists for the 2016 Wilsonville First Citizen award, which is presented annually by the Rotary Club of Wilsonville. This year's nominees for the award include John Budiao, CEO of Military Coach; Kyle Bunch, Agent with Kyle Bunch Agency of American Family Insurance; Tim Knapp, Mayor of Wilsonville; and Jean Tsokos, long-time Wilsonville community volunteer and leader with WilsonvilleSTAGE.

The finalists will be honored, and a winner announced, at Mahalo to You: The 2017 Heart of Gold Dinner and Auction, which will be held Saturday,

February 25, at the Wilsonville Holiday Inn, 25425 SW 95th Ave. Tickets to the gala awards

**Heart of Gold Dinner & Auction**  
• Sat, Feb 25, 5 pm Auction,  
7 pm Dinner, Holiday Inn,  
25425 SW 95th Ave, Wilsonville

dinner and auction are \$60 each or \$480 for a table of eight. They can be purchased online at [www.WilsonvilleHeart.com](http://www.WilsonvilleHeart.com) or <https://wilsonvillerotary.ejoinme.org/tickets>, or in person from any Wilsonville Rotarian.

This is the 16th annual edition of the banquet to honor Wilsonville's community leaders who enrich the lives of all local residents. The event is the Rotary Club's largest fundraiser of the year which supports education and community enrichment efforts in Wilsonville. Richard Martens, president of the Wilsonville Rotary said, "We had an extremely competitive pool of nominees for the First Citizen award. Each of our four finalists has brought unique, valuable gifts to our town and has made Wilsonville a better place to live because of their contributions."

The four finalists are to be acknowledged at the event and at the end of the evening the First Citizen honor is announced. The four finalists include:

### John Budiao

John Budiao is currently a Logistics and Readiness Officer with the U.S. Air Force and the CEO of Military Coach, where he provides motivational speaking and mentoring to future members of the military. John has served in three branches of the military: Marines, Coast Guard, and Air Force. He is a leader and mentor for Wilsonville's Troop 194's 50+



Boy Scouts, always demonstrating how he truly cares about the boys and the community. John received the Certificate of Merit from Boy Scouts, for pulling an unconscious woman from the ocean and performing CPR till paramedics arrived on scene. He also coaches Wilsonville Youth Football. John has been referred to by those who know him as a "selfless servant" because he not only serves his local community, but also his country. John's friends and family feel strongly that he is one of the best dads and husbands in the community, even in the face of a military deployment coming in June of 2017.

### Kyle Bunch

Kyle Bunch is the immediate past president of the Rotary Club of Wilsonville, and an active volunteer in many other efforts in the city. He has participated on the City's Citizens Academy and is a strong supporter of the economic development and building community assets. Besides his many hours doing volunteer work with the Rotary, and despite having a young family and busy work life, Kyle is also on the executive committee of the Wilsonville Chamber. Kyle's love of learning and education is apparent in his educational background, which includes an MBA from Trident University International; an M.A. in Secondary Education and Teaching from the University of Portland; a B.S. in Life Science and a B.A. in Theology and Theological Science from U.P. Kyle is a graduate of Canby High School.



### Tim Knapp

Tim Knapp has given 30 years of service to the citizens of Wilsonville since moving to the city in 1986, even prior to serving as City Councilor from 2003-2008, and before his being elected mayor in 2009, and re-elected twice since that time, including recently in November 2016. His vision for Wilsonville as a connected, supportive community for all its citizens and businesses has been an underlying theme throughout his government and personal work. Tim has worked hard to develop Wilsonville's position as the most forward looking small city in Oregon. His work has helped Wilsonville to attract numerous high tech and highly regarded consumer companies to the City, while maintain-



ing high-quality development standards that ensure the community is environmentally friendly and an attractive place to live with open spaces and active parks. Starting in 1999, Tim and his wife Melodee envisioned, planned and constructed the Old Town Village Small Business Center, which houses 15 local businesses providing services to local families.

### Jean Tsokos

Jean Tsokos is a longtime Wilsonville resident, volunteer and role model for community engagement. With more than 20 years of service to the WilsonvilleSTAGE, formerly known as Wilsonville Community Theater, as well as projects that benefit seniors under her belt, Tsokos recently decided to retire and let the next generation of volunteers take over. A native of the Midwest, Tsokos, 85, has been a board member, director and acting instructor of Wilsonville Community Theater, trips coordinator and vice chair of Wilsonville Community Seniors, Inc. (WCSI), and advisory member of the SMART task force, representing Wilsonville seniors, through her past as both a theater and education professional. Tsokos' earned a degree in conservation education with a minor in theater in 1952. While working at the Field Museum in Chicago, Illinois, she took a job working for Marlin Perkins of Mutual of Omaha's "Wild Kingdom," but more importantly, began her work in community theater, at first simply as a hobby. Soon she was active with the professional children's theater, which also introduced her to her second husband, Bill Tsokos, a stage manager. Lots of moves around the Midwest meant that Jean's involvement in theater took her across the globe, including Canada, the Cayman Islands and Daytona Beach, Fla., often doing large scale musicals. Her grandson eventually got Jean to move to Oregon in 1989 and she has been an active member of the Wilsonville community since that time.



More information on the Wilsonville Rotary Club can be found at: [www.wilsonvillerotary.com](http://www.wilsonvillerotary.com).

The club is already accepting nominations for the 2017 award, which will be awarded in early 2018. Nominations may be submitted to [www.wilsonvillerotary.com/heart/nominate](http://www.wilsonvillerotary.com/heart/nominate), or sent to [president@wilsonvillerotary.com](mailto:president@wilsonvillerotary.com).

## Oregon Tech Hosts Open House in Wilsonville, Feb. 20

Ready to check out your college options? Oregon Tech – located right here in Wilsonville – is inviting the public to learn about the university's degree programs, on-campus student support services, and

tour the campus' labs and classrooms (including

**Oregon Tech Open House**  
• Mon, Feb 20, 4–7:30 pm  
Oregon Tech - Wilsonville Campus  
27500 SW Parkway Ave, Wilsonville

the cool 3-D printing lab, Optical engineering lab, Paramedic lab, and Clinical Lab Science facilities.)

Meet with current students and faculty to learn about the student experience inside the classroom and beyond, Oregon Tech's connections with local industry and the University's high student success rates for employment and starting salaries.

Come check out the exciting things going on at the #1 top rated, affordable public institution in the west (as ranked by US News & World Report Best Col-

leges). As a public polytechnic university, Oregon Tech offers Oregon Tech Wilsonville offers 13 Bachelor of Science degree programs, one Bachelor of Applied Science, one Associate of Applied Science, and two Master of Science programs to students in the Portland Metropolitan area.



Refreshments are provided at this free event. For more information go online to [www.OIT.edu/](http://www.OIT.edu/) Wilsonville; or call Oregon Tech at 503-821-1250.

## Oregon Tech Invites Companies to Career Fair

On Thursday, March 9, 4–7 pm, the Oregon Tech campus in Wilsonville is hosting a Career Fair and invites local businesses to participate in the event to recruit interns and full-time employees from a wide variety of majors, including



Mechanical and Manufacturing Engineering Technology, Software and Embedded Systems Engineering Technology, Electrical Engineering, Renewable Energy Engineering, IT, and others.

Business registration is \$300 (\$200 government and \$100 nonprofit) and includes a table, chairs, promotion of your firm to students, meals for two recruiters, and free parking and wifi.

The Wilsonville campus serves about 800 students who are uniquely prepared to make immediate contributions when hired because the university uses an applied technology education model which fosters experiential hands-on learning. In addition, the Oregon Tech faculty and staff are comprised of industry-savvy experts who have a deep understanding of workforce demands and emerging technological advances and applications.

Space is limited. To register for the career fair visit [www.oit.edu/jobs4owls/employers](http://www.oit.edu/jobs4owls/employers). Full and part-time jobs may also be posted for free on a job board by using this site.

For more information contact Jennifer Kass at 503-821-1155; [Jennifer.Kass@oit.edu](mailto:Jennifer.Kass@oit.edu).

## County Seeks Volunteers for Victim Assistance Program

While attempting to heal wounds from abuse, overcome loss due to homicide, or repair damage from a home invasion, moving forward can be overwhelming for someone whose life has been changed by crime. To varying degrees, the life of a victim of crime is changed forever and is never the same, and now experiences a 'new normal.'

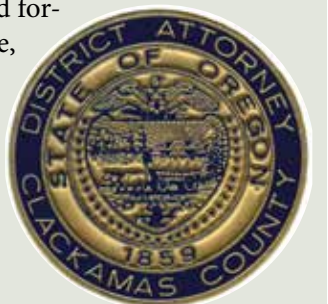
The Clackamas County District Attorney's Office – Victim Assistance Program is seeking Volunteer Victim Advocates who are interested in assisting people who have become victims of crime.

A Volunteer Victim Advocate must have the ability to empathize and care about others, possess good listening skills, ability to rapid problem solve, be at least 18 years of age, have reliable transportation and no criminal background.

Victim Advocates provide crisis intervention, call out response to an Emergency Room, school or with Law Enforcement, follow-up support, keep victims informed of their case status, accompany victims to court, encourage them to exercise their statutory victim rights and make community referrals.

Applications are currently being accepted for the April 2017 class to receive training to be a Volunteer Victim Advocate.

For more information or to download an application; visit [www.clackamas.us/da/victimassist.html](http://www.clackamas.us/da/victimassist.html) or contact Krysti Bellmore at 503-655-8616; [krystibel@co.clackamas.or.us](mailto:krystibel@co.clackamas.or.us).



## Last Call for Tickets to the Daddy Daughter Dance

A limited number of tickets for the ever-popular Daddy Daughter Dance on Friday, Feb. 24, 7 pm at the Wilsonville Community Center are available at [www.WilsonvilleParksandRec.com/Dance](http://www.WilsonvilleParksandRec.com/Dance).



## Art Exhibit at CCC Wilsonville

The Wilsonville campus of Clackamas Community College, 29353 Town Center Loop East, is featuring sculptures and paintings from artists Steve and Toni Tyree.

Steve, the sculptor, creates small steel animals, using the welding skills he had acquired while working at this brother's auto body shop.

Toni, a painter and mixed media artist, paints landscapes using vibrant and uplifting watercolors and oil.

The exhibit runs until March 17 in the college's gallery that is open Monday through Friday, 7 am–5 pm.

For more information about the exhibit, contact Joyce Gabriel at 503-594-0950 or [joyceg@clackamas.edu](mailto:joyceg@clackamas.edu).



## February is Food for Fines Month at the Library

Reduce or eliminate your library overdue fines by bringing in cans or boxes of food during the month of February. Each can or box is worth a one dollar credit toward overdue fines, subject to the approval of library staff.

The food bank prefers donations of:

- Breakfast cereals
- Canned fruit

Please do not donate Ramen noodles.

For more information about Wilsonville Community Sharing, please visit their website: [www.WilsonvilleCommunitySharing.org](http://www.WilsonvilleCommunitySharing.org).

All food is to be donated to Wilsonville Community Sharing for the assistance of needy area families. Please check expiration dates carefully! The food bank cannot accept or use expired food.



## WilsonvilleSTAGE Presents THE LARAMIE PROJECT

WilsonvilleSTAGE is presenting THE LARAMIE PROJECT by Moises Kaufman and the Tectonic Theater Directed by Terry D. Kester.

The play is being performed on Feb. 16, 17, 18, 24, 25 and March 2, 3, and 4 at 7:30 pm on the Wilsonville Campus of Clackamas Community College, 29353 SW Town Loop Center East.

Tickets are \$15 and \$12 for seniors and students. For information and to purchase tickets e-mail [Wilsonvillestage@aol.com](mailto:Wilsonvillestage@aol.com).

The play is based on hundreds of interviews conducted in Laramie, Wyoming after the 1998 murder of University of Wyoming gay student Matthew Shepard. The play includes how 60 different characters reacted to the hate crime that spurred many states to enact new hate crime laws.

Throughout 2017 the Wilsonville branch of Clackamas Community College is hosting all of WilsonvilleSTAGE's performances and providing the local non-profit theater company low-cost space for performances, auditions and rehearsals.



## Rotary's 2017 Heart of Gold Celebration Honors Volunteers

The 16th annual Rotary Club of Wilsonville Heart of Gold Celebration is being held Saturday, Feb. 25, at the Holiday Inn Wilsonville, 25425 SW 95th

Ave. Doors open and the silent auction begins at 5 pm, with the awards dinner and live auction following at 7 pm.

Tickets are \$60 per individual, \$120 per couple or \$480 per

table of eight and are available for purchase online at [www.wilsonvilleheart.com](http://www.wilsonvilleheart.com).

Every year, the Heart of Gold Celebration recognizes local volunteers. This year the finalists for the award include John Budiao, Kyle Bunch, Tim Knapp and Jean Tsokos.

The Heart of Gold Dinner and Auction celebrate Wilsonville's volunteers and the Rotary's many service projects.

At last year's celebration, Sherine and Jaimy Beltran were honored with the First Citizen Award. Prior winners include Donna Bane, Kathryn Whitaker, Jay Puppo, Dick Spence, Debi Laue, Lonnie and Julie Gieber, Theonie Gilmore, Anne Easterly, Ron Anderson, Bob and Judy Woodle, Alan Kirk, Doris Wehler, Sue Ludlow, John Ludlow, Charlotte Lehan and Dr. Laura LaJoie Bishop.

For tickets to the event and more details visit [www.WilsonvilleHeart.com](http://www.WilsonvilleHeart.com).



2015 First Citizens Sherine and Jaimy Beltran with Rotary Club past-President Kyle Bunch.

## City Calendar

### FEBRUARY

|    |       |   |
|----|-------|---|
| 6  | Mon   | • City Council, City Hall<br>5 pm–Work Session<br>7 pm–Meeting  |
| 7  | Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |
| 8  | Wed   | • Wilsonville Community Seniors Inc.,<br>1–3 pm, Community Center<br>• Planning Commission,<br>6 pm, City Hall      |
| 13 | Mon   | • DRB Panel A, 6:30 pm, City Hall   |
| 15 | Wed   | • Budget Committee: Budget 101,<br>6–9 pm, City Hall  |
| 16 | Thurs | • Citizens Academy, 6–9 pm, City Hall   |
| 20 | Mon   | • Presidents Day Holiday, City Offices<br>CLOSED  |
| 21 | Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |
| 22 | Wed   | • Library Board, 6:30 pm, Wilsonville<br>Public Library<br>• French Prairie Bridge Open House,<br>5–7 pm, City Hall |
| 23 | Thurs | • City Council, City Hall<br>5 pm–Work Session<br>7 pm–Meeting  |
| 24 | Fri   | • Daddy Daughter Dance, 7 pm,<br>Wilsonville Community Center   |
| 25 | Sat   | • Family Heart Healthy Fair, 12–4 pm,<br>Library  |
| 27 | Mon   | • DRB Panel B, 6:30 pm, City Hall   |
| 28 | Tue   | • Wilsonville Town Center Plan Kick-Off<br>Event, 5:30–8 pm, City Hall  |

### MARCH

|    |       |  |
|----|-------|--|
| 6  | Mon   | • City Council, City Hall<br>5 pm–Work Session<br>7 pm–State of the City Address<br>7:45 pm–Meeting            |
| 7  | Tue   | • Municipal/Traffic Court, 5 pm, City Hall   |
| 8  | Wed   | • Wilsonville Community Seniors Inc.,<br>1–3 pm, Community Center<br>• Planning Commission,<br>6 pm, City Hall |
| 9  | Thurs | • Park and Rec Advisory Board Meeting,<br>4:30 pm, Park and Rec Admin Offices                                  |
| 13 | Mon   | • DRB Panel A, 6:30 pm, City Hall  |
| 15 | Wed   | • Budget Committee: Mid-Year Review,<br>6–9 pm, City Hall  |
| 16 | Thurs | • Citizens Academy, 6–9 pm, City Hall  |
| 20 | Mon   | • City Council, City Hall<br>5 pm–Work Session<br>7 pm–Meeting   |
| 21 | Tue   | • Municipal/Traffic Court, 5 pm, City Hall   |
| 22 | Wed   | • Library Board, 6:30 pm, Wilsonville<br>Public Library  |
| 27 | Mon   | • DRB Panel B, 6:30 pm, City Hall  |

All dates and times are tentative; check the City's online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

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## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and are replayed periodically. Meetings are also available to stream live and by video-on-demand at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.



**Tim Knapp**  
Mayor

[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



**Scott Starr**  
City Council President

[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



**Susie Stevens**  
City Councilor

[stevens@ci.wilsonville.or.us](mailto:stevens@ci.wilsonville.or.us)



**Charlotte Lehan**  
City Councilor

[lehan@ci.wilsonville.or.us](mailto:lehan@ci.wilsonville.or.us)



**Kristin Akervall**  
City Councilor

[akervall@ci.wilsonville.or.us](mailto:akervall@ci.wilsonville.or.us)

### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)

### Wilsonville City Hall

29799 SW Town Center Loop E  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

City Council: 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)

Police Non-Emergency Dispatch: 503-655-8211



# The Boones Ferry Messenger

Monthly newsletter of the City of Wilsonville

January 2017

## Local Kids and Adults Have Fun and Run in the Reindeer Romp



In December hundreds of local people of all ages gathered at the Family Fun Center in Wilsonville for the fourth annual Reindeer Romp and Kids dash (shown here) with Parks and Recreation Coordinator and elf Brian Stevenson leading the way.

## Lieutenant Adam Phillips Appointed as Wilsonville Chief of Police

On Dec. 5 Wilsonville City Manager Bryan Cosgrove and Clackamas County Sheriff Craig Roberts have appointed Lieutenant Adam Phillips to serve as the City of Wilsonville's new Chief of Police. Chief Phillips replaces former Chief of Police Jeff Smith, who has been promoted to Captain of the Detectives Unit for Clackamas County Sheriff's Office (CCSO).

City Manager Bryan Cosgrove explains, "We are excited to welcome Chief Phillips to our community. Lt. Phillips has an extensive background in law enforcement, including prior experience working in Wilsonville. The City is fortunate to be able to draw from such a deep bench within the Clackamas County Sheriff's Office. I also want to acknowledge Chief Smith's contributions to our community during his tenure, and wish him well in his new position."

Outgoing Police Chief Jeff Smith shares, "My time here in Wilsonville has been immensely rewarding. This is such a great community and the opportunity to oversee this department of 19 law enforcement professionals has been the highlight of my career."

The newly-appointed Chief Phillips has 20 years of professional law enforcement experience all of which has been with the CCSO. Most recently Chief Phillips served as Lieutenant in the Patrol Division for the Sheriff's Office, supervising as many as five sergeants and 20 deputies in the performance of their patrol duties. While at CCSO over the past two decades Phillips has served in the role of Public Information Officer, Detec-

tive Sergeant, Patrol Sergeant, Detective, Motor Traffic Enforcement and as a Patrol Deputy.

While working for CCSO Chief Phillips has earned numerous law enforcement certifications and completed various trainings from the State of Oregon's Department of Public Safety Standards and Training (DPSST) and other organizations.

"I'm grateful to serve as Chief for the Wilsonville Police. Working closely with the City, its citizens, and our Police Department to keep Wilsonville as safe as possible was my primary draw for being here. Captain Smith has left us with a great example to follow, and I will bring my experience and professionalism to keep that forward, positive momentum going. It's a great opportunity to be part of a growing Police Department in a vibrant, expanding city!" explained Chief Phillips.

Chief Phillips started his law enforcement career as a Patrol Deputy for CCSO in 1996 after a career as a special education teacher in the Portland area. Chief Phillips graduated from The Evergreen State College earning a Bachelor of Arts and also attended Portland State University where he earned an Oregon Teaching Certificate. Chief Phillips is a graduate of Benson Polytechnic High School. He is married with two children. You'll find him exercising his two dogs, cooking, fishing, or at the gym away from work.

As Wilsonville's Chief of Police, Phillips leads a staff of 19 full-time law enforcement professionals who are employed by CCSO, but are assigned full-time to Wilsonville. The City of Wilsonville finds the long-term relationship of contracting for law enforcement with CCSO to be more cost-effective and provides a better array of services than the City is able to provide independently.



Wilsonville Police Chief Adam Phillips

## Mayor's Message

### Thanks to Volunteers Council and Community Appreciate Your Service on City Boards and Commissions

As we start a new year, on behalf of the Wilsonville City Council and greater community, I would like to offer a hearty "Thank You" to the residents and business people who serve as volunteers on various City boards and commissions. We especially want to recognize the following community members whose current terms of service ended in December.



The following people have made important contributions to our community through their recent time serving on City advisory bodies. Some of these volunteers have completed their term of service, while others remain eligible for and seek re-appointment for additional terms.

#### City Council

- Julie Fitzgerald, since January 2013

#### Budget Committee

- Tony Holt, since January 2014

#### Development Review Board Panel A

- Mary Fierros Bower, since May 2011
- Kristin Akervall, since February 2014
- James Frinell, since January 2015
- Ronald Heberlein, since January 2015

#### Development Review Board Panel B

- Aaron Woods, since January 2013
- Richard Martens, since January 2015
- Shawn O'Neil, since January 2015

#### Parks and Recreation Advisory Board

- Mary Closson, since January 2013
- Kate Johnson, since January 2013

#### Planning Commission

- Phyllis Millan, since January 2013

Moreover, I want to thank all of the additional current, on-going board and commission members who continue to serve our city. These volunteers include your elected City Councilors, who cheerfully accept the tremendous commitment of time and energy that Council positions entail.

*Wilsonville is fortunate to have so many dedicated volunteers.*

I would also like to acknowledge the positive contributions made by graduates of our Wilsonville Citizens Academy who are helping to raise funds for charity, and many of whom are now volunteering to serve on City committees or local nonprofit boards.

Wilsonville is fortunate to have so many dedicated volunteers who are willing to spend their personal time working with others helping to make our community an exceptional place to live, visit and do business. With continued strong community engagement in our civic affairs, I am confident that 2017 will be another great year for Wilsonville. Feel free to let me know your thoughts — reach me at 503-896-0048 or mayor@ci.wilsonville.or.us.

Tim Knapp, Mayor

## City Employees Raise \$5,000 for Wilsonville Community Sharing



Wilsonville City Council, City staff and board members from Wilsonville Community Sharing (l to r): City Councilor Lehan, Councilor Fitzgerald, WCS board member Taft Mitchell, Mayor Tim Knapp, City staff Angela Handran, WCS board member Michelle Ripple, WCS board member Dick Spence, Councilor Stevens and Council President Starr acknowledge City staff's success raising over \$5,000 for WCS from payroll deductions, sponsored events and other fundraisers.

### In This Issue...

- City and County Grant Opportunities . . . 2
- Holiday Event Photos . . . . . 4
- Our History: Walt Morey . . . . . 5
- Healthy Heart Tips . . . . . 5
- Patty Brescia Says Farewell . . . . . 7

...and more!

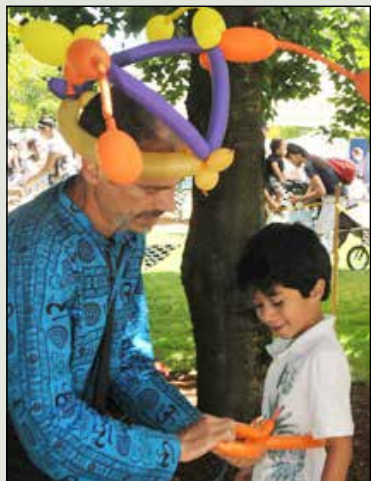
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Wilsonville, OR 97070

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## Funding Available for Tourism and Community Grant Programs

The City of Wilsonville's Parks and Recreation Department is now accepting applications for the Community Tourism and the Community Opportunity Grant Programs. There are separate deadlines for the two grant programs, both of which are awarded by the Parks and Recreation Advisory Board.



Fun In the Park got a Tourism grant from the City in 2016.

The **Community Tourism Grant Program** awards funds to organizations that produce projects, programs or events that promote local business and tourism, and for festivals and special

events for the benefit of the Wilsonville community. **The deadline for the Tourism Grant is Friday, Feb. 3.** Funding for the program comes from the City's transient room taxes that are assessed on overnight lodging visitors.

The **Community Opportunity Grant Program**, with funds distributed twice per year for a total of \$25,000, provides funding for a wide range of local programs and projects in Wilsonville. Funds for the Opportunity Grant program come from the City's general fund.

Successful applications for the Community Opportunity Grant program include proposals aimed at promoting education, diversity, arts or community involvement in the city. **Applications for the Community Opportunity Grant are due Friday, March 3.**

For additional grant funding for tourism related programs and events see article on the right.

For more information, go online to [www.WilsonvilleParksandRec.com/GrantPrograms](http://www.WilsonvilleParksandRec.com/GrantPrograms); or contact Brian Stevenson, Recreation Coordinator, at 503-570-1523, [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

## Clackamas County Offers Grants to Attract Tourists and Visitors

The City is accepting grant applications for local tourism projects that aim to increase the numbers of tourist visitors and increase the economic impact of tourism. In partnership with Clackamas County Tourism, up to \$20,000 is available for projects that can:

- Increase overnight lodging stays in the County.
- Bring visitors into communities from greater than 50 miles away to shop, dine, recreate, take in arts or cultural offerings, or visit heritage sites.
- Entice visitors to "linger longer." The maximum grant for a single project is \$20,000; the minimum request is \$500. Funding for the Clackamas County Tourism-Community Partnership Program (CPP) grants comes from the County's 6% transient room tax paid by overnight lodging visitors. Eligible projects, which must take place within Clackamas County, must be completed by Dec. 31, 2017.



The soccer tournament with over 600 teams received a grant in 2016.

Applications are due by end of day, Feb. 13, 2017. The on-line application form can be found at [www.mthoodterritory.com/cppapp](http://www.mthoodterritory.com/cppapp). This City-administered County tourism grant program is separate and distinct from the City's Community Tourism and Community Opportunity grant programs. Application to the County's program does not preclude application to the City's program.

Proposals are to be evaluated by the Parks and Recreation Advisory Board based on: likelihood of attracting visitors (20%); ability to generate overnight stays (20%); partnerships (15%); use of additional/leveraged funds (15%); self-sustaining growth and capacity-building potential (15%); and measurable ROI (15%).

For more information, contact Brian Stevenson, Recreation Coordinator, at 503-570-1523; [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

## Nominate a Community Enhancement Project By Jan. 16

The City of Wilsonville is still accepting suggestions from the public and sponsoring organizations for project ideas that enhance the Wilsonville community

with funding from the Wilsonville-Metro Community Enhancement Pro-

gram. The relatively new funding program provides the community an opportunity to advance a wide range of projects that might lack funding.

The funds may be used on a range of projects that benefit the community and seek to improve the environment, promote recycling, promote recreation, address safety concerns, serve youth, help seniors, assist low-income persons and/or underserved populations and many other potential areas public projects will also

qualify.

This past spring, City Council allocated \$79,500 in funds to five projects that include: relocating the **Memorial Park "Dog Park"**; implementing a **"Bee Stewards" Wilsonville Pollinator Improvement Project**; establishing a **Multifamily Waste-Reduction and Recycling Project**; completing **Frog Pond Church Campus Restoration Project**; implementing a **Fluorescent Mercury-Lamp Business Recycling Program**.

Metro estimates that in 2017 the City is expected to have approximately \$90,000 to allocate or roll-over into future funding rounds.

Anyone or any non-profit organizations may nominate a project for consideration that improves the community. Project nominations forms and guidelines are available online at [www.ci.wilsonville.or.us/CEP](http://www.ci.wilsonville.or.us/CEP) and nominations are due by Monday, Jan. 16.

For more information, contact Jon Gail, Community Relations Coordinator, at 503-570-1502; [gail@ci.wilsonville.or.us](mailto:gail@ci.wilsonville.or.us).

### Community Enhancement Program Project Submission Deadline

- Monday, Jan 16
- [www.ci.Wilsonville.or.us/CEP](http://www.ci.Wilsonville.or.us/CEP)



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

## Youth Programs Return Jan. 3

Winter youth programs start Tuesday, Jan. 3 with the return of Storytime, Baby Time, Toddler Time and Play Group.

Enjoy stories, songs, puppets and more that will help get children ready to read.

All programs are free and meet at the following times:

### Storytime

- Tuesday evenings, 6:30-7 pm
- Wednesdays, 10:30-11 am and 1-1:30 pm
- Thursday mornings, 10:30-11 am

### Baby Time:

- Tuesday mornings, 11-11:45 am

### Toddler Time:

- Tuesday mornings, 10-10:30 am

### Play Group:

- Monday mornings, 10-11:30 am

For more information, contact the Youth Services Department at [engelfried@wilsonvillelibrary.org](mailto:engelfried@wilsonvillelibrary.org) or 503-570-1592. Learn more at:

[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)



## Curiosity Cafe: Biking Across America with Terri Wortman

Have a cup of coffee and join us for our inaugural Curiosity Cafe. Enjoy monthly talks and workshops on a variety of topics to expand your world. These programs are free and in the Oak Room at 1 pm on the third Saturday of the month. This month, Youth Librarian Terri Wortman talks about her experience last summer riding her bike across the U.S.

**This free talk is on Saturday, Jan. 21, 1-3 pm, in the Oak Room.**



## Weekly Sunday Game Days

Board games, cards, and chess! Play one of our board games, or bring your own. Meet fellow gamers and have fun at this informal event that is open to all ages including children with a parent or caregiver.

Every Sunday, 1-6 pm. Game space available behind the Reference Desk. Learn more at: [www.WilsonvilleLibrary.org/games](http://www.WilsonvilleLibrary.org/games)

## Adult Winter Reading Program Starts Jan. 3

Read books and win prizes during the dark dreary winter months. The fun starts Tuesday, Jan. 3. Pick up forms at the Library, or download them from our website:

[www.WilsonvilleLibrary.org/AWRP](http://www.WilsonvilleLibrary.org/AWRP)



## Upcoming Classes and Events

### Library Closures

Sunday, Jan. 1, Monday, Jan. 2 and Monday, Jan. 16

### First Friday Film

Friday, Jan. 6, 6-8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

### Book Club

*Author Comparison: James A. Michener and "Founding Mothers" by Cokie Roberts*  
Thursday, Jan. 12, 6-8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

### Teen Event: TBA

Friday, Jan. 13, 6:30-8:30 pm  
[www.WilsonvilleLibrary.org/teens](http://www.WilsonvilleLibrary.org/teens)

### Book Notes Concert:

*Ensemble de Organographia*  
Ancient Greek music on period instruments  
Saturday, Jan. 14, 2-3 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

### K-2 Book Adventures

Thursday, Jan. 19, 4-4:45 pm  
[www.WilsonvilleLibrary.org/kids](http://www.WilsonvilleLibrary.org/kids)

### Sábado de Cine (Spanish Movie Saturday)

Fun, animated family film in Spanish with English subtitles  
Saturday, Jan. 28, noon-2 pm  
[www.WilsonvilleLibrary.org/kids](http://www.WilsonvilleLibrary.org/kids)

Visit our website for more events:  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

### County Seeks Volunteers for Victim Assistance Program

While attempting to heal wounds from abuse, overcome loss due to homicide, or repair damage from a home invasion, moving forward can be overwhelming for someone whose life has been changed by crime. To varying degrees, the life of a victim of crime is changed forever – they are never the same and now experience a ‘new normal.’

The Clackamas County District Attorney’s Office – Victim Assistance Program is seeking Volunteer Victim Advocates who are interested in assisting people who have become victims of crime.

A Volunteer Victim Advocate must have the ability to empathize and care about others, possess good listening skills, ability to rapid problem solve, be at least 18 years of age, have reliable transportation and no criminal background.

Victim Advocates provide crisis intervention, call out response to an Emergency Room, school or with

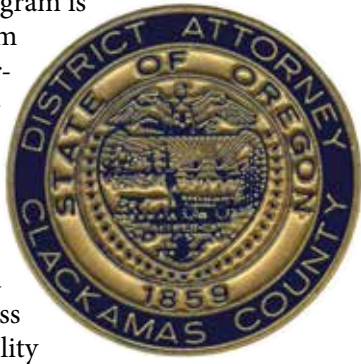
**Victim Assistance Program is seeking Volunteer Victim Advocates who are interested in assisting people who have become victims of crime.**

Law Enforcement, follow-up support, keep victims informed of their case status, accompany victims to court, encourage them

to exercise their statutory victim rights and making community referrals.

Applications are currently being accepted for the April 2017 class. All accepted applicants will receive training to prepare them to be a Volunteer Victim Advocate.

For more information or to download an application; visit [www.clackamas.us/da/victimassist.html](http://www.clackamas.us/da/victimassist.html) or contact Krysti Bellmore @ 503-655-8616 – [krystibel@co.clackamas.or.us](mailto:krystibel@co.clackamas.or.us).



### Chief Jeff Smith Thanks Community for Opportunity to Serve

It is with some regret that after three years serving as your Chief of Police here in Wilsonville, I must inform you all that I have been offered, and have accepted, a position as the Captain of the Detectives Division with the Clackamas County Sheriff’s Office.



Police Chief Jeff Smith

In my new position, I will be privileged to supervise a staff of 56 law enforcement professionals and, while I welcome that new challenge, I will miss working for the Wilsonville Police Department and with the citizens of this great community.

My time here has been immensely rewarding and the highlight of my career. I have met many of you, either while out on patrol or at a special event such as a summer BBQ, the Kiwanis Kids’ Fun Run, Fun in the Park, National Night Out, one of the Summer Rotary Concerts or any of the other events I generally attended throughout the year.

My departure will bring you a new Chief, whom I personally had a hand in selecting. I would like to take this opportunity to announce the appointment of Adam Phillips as your new Chief of Police. He comes to Wilsonville with more than 20 years of police experience after having served as a teacher with

the Centennial School District. His police experience includes patrol, motorcycle traffic enforcement, Public Information Officer, Detective, Sergeant and Shift Commander.



Outgoing Police Chief Jeff Smith (left) passing the Police Chief’s badge to incoming Police Chief Adam Phillips.

I hope you get a chance to meet Chief Phillips. We are fortunate to have him and I am sure you will find him to be the type of positive influence the Wilsonville Police Department has always strived to attract. Welcome aboard, Chief Phillips!

*Jeff Smith, Former Wilsonville Chief of Police*

### Whom to Call for Police Services

#### Dial: 9-1-1

- Life threatening emergencies
- Crimes in progress

#### Non-Emergency Dispatch: 503-655-8211

- A non-emergency situation, but you request the service of a police officer by phone or in person.



### Wilsonville Police Office: 503-682-1012

- Any resource information or questions
- Lost and found items
- Temporary OLCC permits and related questions

#### Other Useful Contacts:

- City Code Enforcement: 503-570-1572
- Abandoned Vehicles on private property: 503-682-4960; on public property: 503-655-8211
- Dog Control: 503-655-8628
- Municipal Traffic Court: 503-570-1587
- Parking Violations: 503-655-8211
- Traffic Complaints: 503-655-8211

## SMART

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29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

### Holiday Service Hours

**Martin Luther King Jr. Day**  
Monday, Jan. 16

**Regular Bus Service**  
SMART offices closed



**President’s Day**  
Monday, Feb. 20

**Regular Bus Service**  
SMART offices closed

### Walk Smart

Help us plan for 2017

Take our three-minute survey

[www.ridesmart.com/2017walks](http://www.ridesmart.com/2017walks)

### Get ETA SPOT App to Track SMART Bus Service and Alerts

When the weather turns cold many transit riders prefer to stay indoors as long as possible before going out to catch the SMART bus. To help time when to arrive SMART riders should download the ETA SPOT app or visit [www.ridesmart.com/bus](http://www.ridesmart.com/bus) from any computer. The service offers users real-time bus locations to see when the bus will arrive, rider alerts and potential closure announcements.

Another way to stay informed is by signing up to get text message or e-mail alerts sent directly to a phone by registering an e-mail address or mobile phone number online at [Ridesmart.com/notifyme](http://Ridesmart.com/notifyme).

To download the ETA SPOT app on a phone, simply visit the App Store or Google Play; search for ETA Spot; download the free app; and open the app and set SMART as the transit provider.

For more information visit [www.RideSMART.com](http://www.RideSMART.com).



### SMART Tips for a Heathy and Happy New Year

Perhaps you have set a goal to lose weight or become more active in 2017. There are many simple ways to incorporate physical activity into your day. Think Transportation Options! Here are a few SMART ideas to help you reach common goal:

**1) For shorter trips, walk or bike:** Riding your bicycle one hour per week could burn an extra 500 calories.



**2) Ride the bus or train:** Recent studies show that transit commuters tend to be more active than automobile commuters because they walk to and from their bus stops, adding more exercise to their daily routines.

**3) Walk at Lunch:** Go for a brisk 20-minute walk during your lunch break to recharge your batteries and burn a few extra calories. Walking helps rev-up



your metabolism and helps you feel more productive throughout your day.



Remember that every step counts. SMART wishes you a healthy and happy New Year!

# Wilsonville Celebrates the Holiday Season with Fun Festivities

## Community Tree Lighting

Families and friends gathered at the annual event on Nov. 30.

## Reindeer Romp and Bullwinkles Bash

Hundreds gathered on Dec. 3 to exercise and play at the Parks and Recreation's annual event.



29600 SW Park Place  
 Parks Programs: 503-783-7529  
 Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

### Registration Now Open!

Registration for the winter and spring session of classes and programs is now open. Classes begin the first part of January and many programs do fill to capacity so if you are interested, early registration at [www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com) is encouraged! Activities for all ages can be viewed and registered for online.



Reservations for park facilities will be accepted starting in March.

### Chalk Paint Projects

Wilsonville is proud to announce a new partnership with instructor Theresa Okamura. Theresa is a self-taught painter and refinisher of wood furniture who also creates signs out of aged fencing and pallets.



Three workshops are being offered this season with the first one being an Oregon sign (see example to right). The class takes place on Saturday, Jan. 28, 10 am–1 pm.

All supplies are provided and each class includes a finished project. More information and other class examples can be found on page 23 of the Activity Guide or on our website.

### New Classes Highlight 2017 Program Offerings

Wilsonville Parks and Recreation is excited to announce the addition of new programs for the winter/spring session.

For children, Soccer Shots, a Saturday morning soccer program for 2–8 year olds, moves indoors to the Community Center for the winter session. Additionally, two new spring break camps are being offered this year. Intro to Survival Camp is open to 6–9 year olds and Skyhawks Basketball is open to 7–12 year olds.



For adults, the department's wellness offerings have expanded to include Yin/Yang Yoga on Thursday nights and Slow Vinyasa Yoga on Saturday mornings. Don't worry, favorites like Healthy Bones and Balance, Tai Chi, as well as both Pilates and Body Sculpt are still part of the program offerings. Former customer favorites, like Line Dance and Everyone is an Artist—Exploring Watercolor, make a return for the winter/spring session.

Registration for all programs is available online at [WilsonvilleParksandRec.com](http://WilsonvilleParksandRec.com). Many programs fill to capacity so registering early is strongly advised!

### Daddy Daughter Dance - Hawaiian Luau

Dads and father figures, grab that special girl and come out for this year's Daddy Daughter Dance. The event takes place on Friday, Feb. 24 at the Wilsonville Community Center beginning at 7 pm. There are unlimited pictures in a photo booth and prizes are being given away all night.



This event sells out so register now at [WilsonvilleParksandRec.com/Dance](http://WilsonvilleParksandRec.com/Dance).

### Upcoming Classes and Events

#### Fit Girls!

Ages 11 and older  
 Mondays, Jan. 2–Feb. 20  
 7–8 pm  
 Community Center  
 \$59

#### Soccer Shots

Saturdays, Jan. 7–March 11  
 Community Center  
 \$170  
 Ages 2–3  
 9–9:30 am  
 Ages 3–4  
 9:40–10:15 am  
 Ages 4–5  
 10:25–11:00 am  
 Ages 5–8  
 11:15 am–12:00 pm

#### Slow Vinyasa Yoga

Ages 18 and older  
 Saturdays, Jan. 14–March 4  
 9–10 am  
 Tauchman House  
 \$59

#### Exploring Watercolor

Ages 18 and older  
 Mondays, Jan. 23–March 20  
 1–3 pm  
 Community Center  
 \$55

#### Zumba®

Ages 18 and older  
 Wednesdays, Jan. 25–March 15  
 5–5:45 pm  
 Community Center  
 \$55

### Our History: Walt Morey Feb. 3, 1907–Jan. 12, 1992

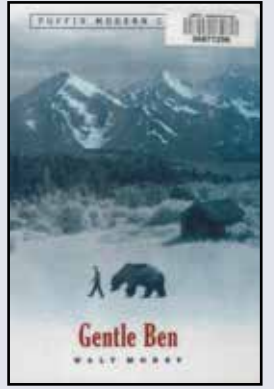


This Boones Ferry Historical Society photo shows Walt Morey and his wife in their home in Wilsonville. Morey wrote and published 18 books including *Gentle Ben* which in the late '60s was made into a movie and TV show. After Morey died on Jan. 12, 1992 the river-front land and home was sold and developed into Morey's Landing and Walt Morey Park.

### Books Written by Walt Morey

Walt Morey lived much of his life and wrote most of his books while living in Wilsonville until he died on Jan. 12, 1992. The following is a list of all 18 of the books that Morey published in his lifetime:

- No Cheers, No Glory (1945)
- Gentle Ben (1965)
- Kävik the Wolf Dog (1968)
- Angry Waters (1969)
- Runaway Stallion (1970)
- Gloomy Gus (1970)
- Deep Trouble (1971)
- The Bear of Friday Creek (1971), illustrated by Derek Collard
- Scrub Dog of Alaska (1971)
- Canyon Winter (1972)
- Home is the North (1973)
- Run Far, Run Fast (1974)
- Operation Blue Bear (1975)
- Year of the Black Pony (1976)
- Sandy and the Rock Star (1979)
- Hero (1980)
- The Lemon Meringue Dog (1980)
- Death Walk (1991)



In Morey's honor the Library has the Walt Morey Children's Room where visitors of all ages can learn about him and see his typewriter.

### TVF&R Encourages You to Make 2017 a Heart-Healthy Year

With the holiday season past and the new year upon us, January is a natural to be thinking about family and what the New Year may bring. Why not place heart health at the top of your new year's resolution list for 2017?

Chest pain, shortness of breath or discomfort in your arms, back or jaw can easily be attributed to the stress and strain of the season and not recognized as signs of an oncoming heart attack.

The winter holiday season can be a particularly dangerous time to ignore these signs and delay treatment.

An American Heart Association study stated, "The number of cardiac deaths is higher on Dec. 25 than on any other day of the year, second highest on Dec. 26, and third highest on Jan. 1." In the 2004 national study, researchers at the University of California, San Diego, and Tufts University School of Medicine studied 53 million U.S. death certificates from 1973 to 2001 and found an overall increase of 5 percent more heart-related deaths during the holiday season.

A number of factors can contribute to a higher rate of cardiac deaths during the holidays, but one thing is certain, delaying treatment and ignoring the signs and symptoms can be deadly.

Tualatin Valley Fire & Rescue encourages you to call 911 immediately if you or a family member experiences the following symptoms:

- Chest discomfort in the center of the chest that lasts for more than a few minutes or goes away and returns. It can feel like uncomfortable pressure, squeezing, fullness or pain.
- Upper body discomfort in one or both arms, the back, neck, jaw, or stomach.
- Shortness of breath before, during, or after other discomforts.
- Other signs, including breaking out in a cold sweat, nausea or light-headedness.

TVF&R's highly trained paramedics can diagnose if a heart is failing and start treatment on the scene. Calling 911 right away will connect you with trained call-takers who will immediately dispatch emergency responders. Visit [www.tvfr.com](http://www.tvfr.com) to learn more.

### Tips for a Heart-Healthy Life

TVF&R encourages you to make this a heart-healthy 2017. Here are some tips to living a heart-healthy life:

- Carve out time to take care of yourself — that includes a good night's rest and exercise.
- Start each day with a healthy breakfast.
- If you plan to eat a big meal, eat a lighter lunch and healthy snack before you go so that you don't arrive hungry.
- If you plan to attend a party or go out for a meal, challenge yourself to eat only healthy meals.
- Invite others to enjoy physical activities with you. Exercise is good for your heart and a great way to relieve stress. The American Heart Association infographic below provides weekly exercise recommendations for adults.

### Heart Healthy Foods from the American Heart Association

At the heart of a healthy lifestyle is good nutrition. Making smart food choices can help reduce your risk of heart disease and stroke. The good news is, eating right doesn't have to be hard or require a special diet.

Here are some simple tips to help you and your family follow an overall healthy dietary pattern.

#### INCLUDE

- Fruits and vegetables
- Whole grains
- Beans and legumes
- Nuts and seeds
- Fish (preferably oily fish high in omega-3 fatty acids), skinless poultry, and plant-based alternatives
- Fat-free and low-fat dairy products
- Healthier fats and non-tropical oils



#### LIMIT

- Sodium and salt
- Saturated fat
- Sweets and added sugars, including sugar-sweetened beverages
- Red meats – if you choose to eat red meat, select lean cuts

#### AVOID

- Trans fat and partially hydrogenated oils

#### TIPS

- Choose wisely, even with healthier foods. Ingredients and nutrient content can vary by brand and preparation.
- Compare nutrition information on package labels and select products with the lowest amounts of sodium, added sugars, saturated fat and trans fat, and no partially hydrogenated oils.
- Watch your calorie intake. To maintain weight, consume only as many calories as you use up through physical activity. If you want to lose weight, consume fewer calories or burn more calories.
- Eat reasonable portions. Often this is less than you are served.
- Eat a wide variety of foods to get all the nutrients your body needs.
- Prepare and eat healthier meals at home. You'll have more control over ingredients.
- Look for the Heart-Check mark to easily identify foods that can be part of an overall healthy diet.

Learn more about heart health from the American Heart Association at [www.heart.org](http://www.heart.org).



For Overall Cardiovascular Health:



OR



or a combination of the two

AND



For Lowering Blood Pressure and Cholesterol:





## Reports from City Boards and Commissions

### Council Tentatively Approves Changes to Franchise and Right-of-Way Rules and Welcomes New Police Chief

On Dec. 5, the Wilsonville City Council held a public hearing and first reading, tentatively approving an ordinance that amends the City's right-of-way and public-easement management section of City Code. The Council voted to keep the record open to provide additional time for public comments until the second reading of the ordinance at the City Council meeting on Monday, Dec. 19. The ordinance clarifies the method of determining the fees to be charged by the City for use of the public right-of-way by utility providers that may or may not provide service to Wilsonville customers. In particular the code amendments are needed to clarify how to calculate and assess the fees the City will charge for the four miles of 66-inch-diameter water pipeline being planned and constructed to pass through Wilsonville to provide water to Tualatin Valley Water District and City of Hillsboro customers.

The City Council also agreed to continue until a future meeting a resolution regarding adoption of an Intergovernmental Cooperative Agreement pertaining to the transfer of roadway authority from Clackamas County to the City for portions of Stafford and Advance Roads.

During the Communications portion of the meeting City Manager Bryan Cosgrove presented board members of Wilsonville Community Sharing a ceremonial check in the amount of \$5,000, representing the total

amount of funds raised by City staff and City-sponsored projects donated in 2016 to the local nonprofit food bank and social service agency.

Also during the Communications portion of the meeting departing Wilsonville Police Chief Jeff Smith introduced and then passed his badge to the newly-appointed Wilsonville Police Chief Adam Phillips, who has 20 years of professional law-enforcement experience with the Clackamas County Sheriff's Office.

On the Consent Agenda the Council adopted The Canvass of Votes for the Nov. 8, 2016, general election that certifies the final voting results provided to the City Recorder by the County Clerks of Clackamas and Washington counties.

During work session prior to the meeting, City Council heard reports and held discussions regarding the now-underway project that studies the potential development of a publicly-owned fiber optics network for potential commercial and residential use in Wilsonville. The Council also heard presentations from six different graphic designers about 15 logo concepts in order to identify a potential new logo for adoption by the City.

Community members are able to watch a replay of all or a portion of the City Council meetings on Comcast/Xfinity Ch. 30, Frontier Ch. 32 or on the City's video-on-demand service at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV).

### City Leaf Drop-Off Day Raises \$441 for Wilsonville Community Sharing



Board members from Wilsonville Community Sharing and City staff in safety vests (l to r): Michelle Ripple, Manny Ghiselline, Sean Byrne, Taft Mitchell and Ralph Thorp at the City and Republic Services sponsored Leaf-Drop Off Day which raised \$441 for Wilsonville Community Sharing, the local food bank and social service referral agency.

### Council Approves Contract for Outfall Project and Extends Deadline to Apply for Board and Commission Positions

At the Nov. 21, City Council meeting the Council approved a resolution authorizing the City Manager to enter into a contract with CH2M Hill Engineers, Inc., to provide final design and bid phase support for the Wastewater Treatment Plant Outfall Replacement Project. The purpose of the project is to replace a failing 40-year-old corrugated metal outfall pipe the runs from the treatment plant to the river. The project also includes the installation of a new diffuser to disburse and cool the treated effluent when being released into the river. Completion of the repair project is required by the Oregon Department of Environmental Quality and is expected to be done by next fall. The new outfall pipe and diffuser has a capacity of up to 7 million gallons a day to meet current needs and support future growth in the city.

The City Council also agreed to extend the deadline to apply for 11 different openings on various City boards and commissions until Friday, Dec. 30, 4:30 p.m. The extension provided community members additional time to submit applications for the open positions. There were openings to serve on the Budget Committee, Development Review Board (Panel A and Panel B), Parks and Recreation Advisory Board, the Planning Commission and Tourism Promotion Committee.

During work session prior to the meeting, City Council heard reports and held discussions regarding the Year 2000 (Eastside) Urban Renewal District and also on the Transit Master Plan which seeks to identify and recommend enhancements to the City's public transit system.

### Get Up to Five Free Trees

To promote healthy cool streams in Wilsonville, the City offers community members and local property owners up to five free native tree seedlings to plant on their property. The City encourages plantings along streams and other water bodies to reduce stream temperature and pollutants. The trees are obtained at a local nursery.



For more information and to get a tree coupon for five trees, contact Luke Bushman, Stormwater Coordinator, at 503-570-1552; [bushman@ci.wilsonville.or.us](mailto:bushman@ci.wilsonville.or.us).

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during January. Scheduling changes can and do occur; check for updated meeting information on the City's website, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Thursday, Jan. 5:** Liaison appointments; Town Center Plan task force; external committees; Frog Pond infrastructure funding; Memorial Park Dog Park/Community Garden parking lot; Swear In Newly Elected Mayor and Council Members; Martin Luther King Jr. Day proclamation; ground lease agreement; Water Treatment Plant master plan update; Residential Parking Program; employment contract for Judge Weinhouse; and road maintenance fees.

**Thursday, Jan. 19:** 2017-18 State Legislative agenda; Planning Permit fees; City Attorney's contract; Residential Parking Permit program second reading; road maintenance fees

### Planning Commission Update

The Planning Commission meeting on Wednesday, Dec. 14, was cancelled due to inclement weather and was rescheduled to Thursday, Dec. 22. At the meeting the Commission planned to hear reports and discuss the Wilsonville Town Center Plan Public Involvement plan, an update on the Frog Pond Master Plan and the Transit Master Plan.

### Development Review Board Update

On Dec. 12, DRB Panel A was cancelled due to inclement weather. The next meeting of DRB Panel A is planned for Monday, Jan. 9, 6:30 pm at City Hall.

The DRB Panel B meeting originally scheduled for Nov. 28 was cancelled due to a lake of agenda items. The next of DRB Panel B is planned for Monday, Jan. 23, 6:30 pm at City Hall.

### Library Board Update

The next regular meeting of the Library Board is scheduled for Wednesday, Jan. 25, 6:30 pm, at the Library.

### City Seeks Feedback on Updated Water Treatment Plant Master Plan

The City of Wilsonville, in partnership with the Willamette Water Supply Program (WWSP) is completing a master plan update for the Willamette River Water Treatment Plant. The master plan update identifies long-range water needs for the City and

**Planning Commission Hearing Water Treatment Plant Master Plan Update**  
• Weds, Feb 8, 6 pm  
City Hall, Council Chambers

WWSP, the treatment technologies to be used and the capital improvements needed to meet these future goals.

The Wilsonville Planning Commission is to receive a briefing on Jan. 11, and hold a formal public hearing and receive public comments on the master plan update at the commissions' meeting on Feb. 8, 2017, 6 p.m. at City Hall, 29799 SW Town Center Loop East.

The public is also invited to review and provide comments on the master plan update through an on-line virtual open house located at [www.ci.wilsonville.or.us/WTPMPupdate](http://www.ci.wilsonville.or.us/WTPMPupdate).

For information on the Willamette Water Supply Program, please visit [OurReliableWater.org](http://OurReliableWater.org). For more information on the master plan update, contact Eric Mende, PE, Capital Projects Engineering Manager, at 503-570-1538; [mende@ci.wilsonville.or.us](mailto:mende@ci.wilsonville.or.us).

Did you know...

You can ride the

**SMART**  
SOUTH METRO AREA REGIONAL TRANSIT

bus for free anywhere in

**Wilsonville!**

For more info, see [RideSmart.com](http://RideSmart.com)

### Rotary's 2017 Heart of Gold Celebration Honors Volunteers

The 16th annual Rotary Club of Wilsonville Heart of Gold Celebration is being held Saturday, Feb. 25, at the Holiday Inn Wilsonville, 25425 SW 95th Ave. Doors open and the silent auction begins at 5 pm, with the awards dinner and live auction following at 7 pm.



2015 First Citizens Sherine and Jaimy Beltran with Rotary Club past President Kyle Bunch.

This year, the club chose a Hawaiian theme for the event: "Mahalo to You." The Hawaiian word "mahalo" means "thanks," "gratitude" or "respect." "The theme recognizes how we feel about community volunteers as well as the club's supporters," Club President Richard Martens said. "People attending the event are encouraged to dress either formally or in Hawaiian/Aloha wear — or both, if they can pull that off!"

Tickets are \$60 per individual, \$120 per couple or \$480 per table of eight and are available for purchase online at [www.wilsonvilleheart.com](http://www.wilsonvilleheart.com).

Every year, the Heart of Gold Celebration recognizes local volunteers, including one individual or couple who is chosen for Wilsonville's highest volunteer honor — the First Citizen Award. The nomina-

tion process was held in December and finalists are to be announced in January.

The Heart of Gold Dinner and Auction celebrate Wilsonville's volunteers and the Rotary's many service projects:

- Wilsonville Chess Program for youth at local elementary schools help with analytical skills, attention and patience
- International service projects and End Polio Now are improving and saving lives across the globe
- Omelet Breakfasts at Wilsonville elementary and middle schools raise funds to support local education programs.
- Scholarships for local high school students provide college opportunities for our diverse students.
- Wilsonville Rotary Summer Concerts unite and entertain the community while also raising money for polio eradication.
- Through A Child's Eyes program helps Coffee Creek inmates be better parents both before and after release by hosting a summer event for the children of the inmates so they can bond and spend quality time with their children.

At the 2016 Heart of Gold Celebration, Sherine and Jaimy Beltran were honored with the First Citizen Award. Prior winners include Donna Bane, Kathryn Whittaker, Jay Puppo, Dick Spence, Debi Laue, Lonnie and Julie Gieber, Theonie Gilmore, Anne Easterly, Ron Anderson, Bob and Judy Woodle, Alan Kirk, Doris Wehler, Sue Ludlow, John Ludlow, Charlotte Lehan and Dr. Laura LaJoie Bishop.

For tickets to the event and more details visit [www.WilsonvilleHeart.com](http://www.WilsonvilleHeart.com).

### Oregon Tech Names Dr. Nagi Naganathan as 7th President

The Oregon Institute of Technology (Oregon Tech) Board of Trustees today named Dr. Nagi Naganathan as its seventh president, after conducting a national search. Dr. Naganathan is currently Dean of the College of Engineering at the University of Toledo, a position he has held since 2003. Naganathan succeeds Dr. Jay Kenton who has been Interim President since July 1 after former president Dr. Chris Maples chose not to renew his contract earlier this year.



Dr. Nagi Naganathan

Chair of the Oregon Tech Board of Trustees, Dr. Lisa Graham, said, "The Board is very excited about Dr. Naganathan leading Oregon Tech to its next level of innovation and service to students."

Dr. Naganathan has helped universities excel in some of the strategic opportunities we are beginning to engage in, like industry partnerships, additional market-driven degree offerings, diversifying our revenues and fundraising, and always seeking affordability and excellence for students."

Naganathan is expected to begin his tenure as president of Oregon Tech in the spring of 2017. Dr.

Naganathan said, "I'm excited about joining Oregon Tech and extend my thanks to the Board, Search Committee, faculty, staff and students for your trust and warm welcome. Oregon

**Oregon Tech has so many unique and high value assets and opportunities at this point in its history. Engaging with everyone to take the university to its next level of excellence is a venture I can't wait to begin**

Tech has so many unique and high value assets and opportunities at this point in its history. Engaging with everyone to take the university to its next level of excellence is a venture I can't wait to begin."

Dr. Naganathan has 22 years of academic leadership experience and currently serves as the Dean of Engineering at The University of Toledo (UT), a role he has had since 2003. Dr. Naganathan also served as Interim President of UT from 2014-15 as well as the chief administrator of its academic medical center (UTMC), which includes a hospital and many other positions in academia as a Interim Dean of Engineering; Professor and Founding Chair of Mechanical, Industrial & Manufacturing Engineering; Interim Chair of Mechanical Engineering; and Assistant/Associate Professor of Mechanical Engineering.

Dr. Naganathan received his Ph.D. from Oklahoma State University; his M.S. in Mechanical Engineering from Clarkson University; and his B.E. in Mechanical Engineering from the National Institute of Technology, Tiruchirappalli, University of Madras, India.

### City Studies Publicly-Owned Fiber Optics Network

The City of Wilsonville is seeking input from community members and local businesses on whether to invest in developing a publicly-owned fiber optics network for residential and commercial use. To gather input an online survey and open house are available at [www.ci.wilsonville.or.us/Fiber](http://www.ci.wilsonville.or.us/Fiber) until Jan. 31.

The goal of the assessment is to determine whether the community has a need for, and supports, City investment in development of a publicly-supported fiber optics network and if so, the plan intends to identify steps and strategies to fund and build such a network.

fiber optics infrastructure. These types of networks are created in order to provide community members and local businesses more competition, faster internet service, access to other cost-effective technology solutions and increased bandwidth. Such networks also serve as an economic development incentive to attract high-tech and other types of companies to a city or region.

CTC Technology and Energy (CTC), an independent technology-consulting firm with over 30 years of experience with public and non-profit entities, is leading and advising the City on the project.

City staff and consultants are also planning to hold discussions with local business representatives, City leaders and staff to gather further input.

For information, contact Andy Stone, Interim IS Manager, at 503-570-1532; [astone@ci.wilsonville.or.us](mailto:astone@ci.wilsonville.or.us), or visit [www.ci.wilsonville.or.us/Fiber](http://www.ci.wilsonville.or.us/Fiber).

**Fiber Optic Network Survey and Online Open House**  
• Survey, Ends Jan 31,  
[ci.wilsonville.or.us/fiber](http://ci.wilsonville.or.us/fiber)

### Patty Brescia Bids Farewell to Wilsonville and Community Center

Parks and Recreation Senior Program Manager Patty Brescia has accepted a new position with the Tualatin Hills Parks and Recreation District in Beaverton. Patty worked primarily in the Wilsonville Community Center and was responsible for managing, not only the Center, but also all many other programs offered in and outside of the building. In addition Patty was also responsible for community events such as the Reindeer Romp, WERK Day and the annual Harvest Festival.

Before departing Brescia shared, "I have been offered an opportunity for a new position at Tualatin Hills Parks and Recreation District, (in Beaverton, close to home). While my new job will offer me new opportunities, I am very grateful to have had the

privilege to know and work with you all for the past 12 years. I have learned a lot, and I thank you all for all the good that you do. I'm happy to know that our citizens are in very capable hands. I wish you all the very best."

Acting Parks and Recreation Director Jeanna Troha states, "Patty has been a great employee for the City and strong advocate for the needs and programs that serve not only the seniors in our community, but people of all ages. Patty has been instrumental in the delivery of our programs and we wish here nothing but success in her new role with THPRD."

Parks and Recreation Program Coordinator Brian Stevenson is serving as Interim Program Manager until a full-time replacement is identified for the role.



City staff gather with Patty Brescia (in center with red vest) to wish her farewell and good luck in her new position.

### End Hunger Locally by Donating to Wilsonville Community Sharing!

**Donate by check:**  
Wilsonville Community Sharing  
28925 SW Boberg Rd  
PO Box 205  
Wilsonville, OR 97070

**Sign up for a one-time or monthly donations at:**



[www.WilsonvilleCommunitySharing.org](http://www.WilsonvilleCommunitySharing.org)

Wilsonville Community Sharing is a non-profit social service referral agency that operates the community food bank and connects families in need with a variety of resources including financial assistance for those who need help with their water, gas, and electric bills.

### Scouts to Pick Up and Recycle Christmas Trees, Saturday, Jan. 7

Want to recycle your Christmas tree with the scouts? It's easy! On Saturday, Jan. 7, the Boy Scouts, Cub Scouts and Girl Scouts in the Wilsonville Area are recycling Christmas trees. If you live in a Wilsonville neighborhood, please have your tree on the curb no later than 8:30 am, Jan. 7 and the scouts will pick up your tree and collect your donation. Wilsonville residents may also drop off trees at the Wilsonville City Hall parking lot (behind Goodwill), 9 am–11:30 am. For more information contact Cathy Scoggins, Scouting for Trees coordinator, at 971-400-0998.



### WilsonvilleStage Prepares to Present The Laramie Project

WilsonvilleStage presents The Laramie Project, Feb. 16–March 4, which is a play about the 1998 brutal "hate murder" of Mathew Shepard in Laramie-Wyoming. Author-Artistic Director, Moises Kaufman, and members of NYC Tectonic Theater, went to Laramie to interview 200 citizens, thus creating this poignant, powerful play.



During Jan. 5–7, WilsonvilleStage is holding auditions for the play at the Wilsonville Campus of Clackamas Community College, 29353 SW Town Center Loop East, Room W108B. On Thursday, Jan. 5 and Friday, Jan. 6, the auditions are being held 6:30–9:30 pm and on Saturday, Jan. 7, 12–4 pm at the college.

To audition actors are asked to prepare and present a one-minute dramatic monologue. The local theater company seeks four men and four women who are at least 17 years old and capable of each portraying five to six characters in the play. For more information e-mail [wilsonvillestage@aol.com](mailto:wilsonvillestage@aol.com).

### Art Exhibit at CCC Wilsonville

Beginning Jan. 9 the Wilsonville campus of Clackamas Community College, 29353 Town Center Loop East, is featuring sculptures and paintings from artists Steve and Toni Tyree.

Steve, the sculptor, creates small steel animals, using the welding skills he had acquired while working at this brother's auto body shop.



Toni, a painter and mixed media artist, paints landscapes using vibrant and uplifting watercolors and oil.

The exhibit runs Jan. 9–March 17 in the college's gallery that is open Monday through Friday, 7 am–5 pm. For more information about the exhibit, contact Joyce Gabriel at 503-594-0950 or [joyceg@clackamas.edu](mailto:joyceg@clackamas.edu).

### Time to Register for Wilsonville Youth Lacrosse

Wilsonville Youth Lacrosse invites both boys and girls in grades 1–8 to register now for the upcoming 2017 youth lacrosse season. The club welcomes players of all abilities! Register your youth player as early as possible since space may be limited due to coach availability.

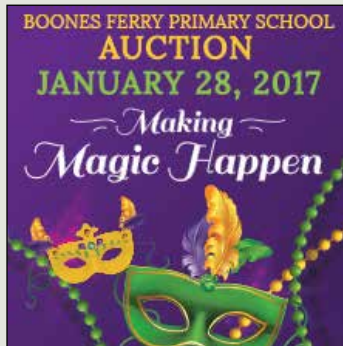


Registration closes Jan. 31, 2017, go to [www.WilsonvilleLacrosse.com](http://www.WilsonvilleLacrosse.com) to sign up.

For more information on both the girls and boys season e-mail Wilsonville Youth Boys Director Grady Nelson at [boys@wilsonvillelacrosse.com](mailto:boys@wilsonvillelacrosse.com).

### Auction Benefits Boones Ferry Primary School

The Boones Ferry Primary School Parents and Staff for Students is holding the 5th biennial fund-raiser and auction on Saturday, Jan. 28, 6 pm–1 am at Wilsonville's Holiday Inn, 25425 SW 95th Ave.



- The fun-filled event includes:
  - Wine and beer tasting
  - Silent auction with lots of great items
  - Tasty and filling hors d'oeuvres
  - Live auction full of amazing items
  - Dessert auction
  - Music, dancing and a photo booth
- All proceeds from the event benefit Boones Ferry Primary School students and programs. For tickets and information visit [www.boonesferrypps.org](http://www.boonesferrypps.org).

### Swearing-In Ceremony to be Held Jan. 5

Members of the public are invited to attend a Swearing-In Ceremony for Mayor Tim Knapp, City Councilor Susie Stevens and City Councilor-elect Kristin Akervall on Thursday, Jan. 5, 2017 at 7 p.m. at City Hall in the Council Chambers, 29799 SW Town Center Loop East in Wilsonville.

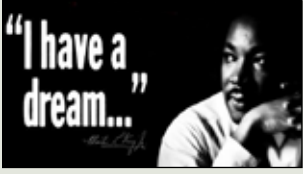


City Councilor-Elect Kristin Akervall

Following the ceremony and oath of office, led by the honorable Municipal Court Judge Michael Gleeson, the City Council is planning a brief recess to celebrate the occasion with cake and punch.

Mayor Tim Knapp and City Councilor Susie Stevens were both reelected in November to serve four-year terms of office that run through December 31, 2020. City Councilor-elect Kristin Akervall was elected in November to serve a four-year term that runs through Dec. 31, 2020 as well.

## City Calendar

| JANUARY  |   |   |
|----------|---|---|
| 1 Sun    | • New Year's Day  |   |
| 2 Mon    | • New Year Holiday, City Offices CLOSED   |   |
| 3 Tue    | • Municipal/Traffic Court, 5 pm, City Hall  |   |
| 5 Thurs  | • City Council, City Hall, 5 pm - Work Session<br>7 pm - Meeting  |   |
| 9 Mon    | • Wilsonville Community Seniors Inc., 1–3 pm, Wilsonville Community Center<br>• DRB Panel A, 6:30 pm, City Hall                     |   |
| 11 Wed   | • Planning Commission, 6 pm, City Hall  |   |
| 16 Mon   | • Martin Luther King Holiday, City Offices Closed   |  |
| 17 Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |   |
| 18 Wed   | • Citizens Academy, 6–9 pm, City Hall   |   |
| 19 Thurs | • City Council, City Hall, 5 pm Work Session, 7 pm Meeting  |   |
| 23 Mon   | • DRB Panel B, 6:30 pm, City Hall   |   |
| 25 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library February   |   |
| FEBRUARY |   |   |
| 6 Mon    | • City Council, 5 pm - Work Session, 7 pm - Meeting, City Hall  |   |
| 7 Tue    | • Municipal/Traffic Court, 5 pm, City Hall  |   |
| 8 Wed    | • Wilsonville Community Seniors Inc. Advisory Board, 1–3 pm, Wilsonville Community Center<br>• Planning Commission, 6 pm, City Hall |   |
| 13 Mon   | • DRB Panel A, 6:30 pm, City Hall   |   |
| 16 Thurs | • Citizens Academy, 6–9 pm, City Hall   |   |
| 20 Mon   | • Presidents Day Holiday, City Offices CLOSED   |   |
| 21 Tue   | • Municipal/Traffic Court, 5 pm, City Hall  |   |
| 22 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library  |   |
| 23 Thurs | • City Council, City Hall, 5 pm - Work Session, 7 pm - Meeting  |   |
| 24 Fri   | • Daddy Daughter Dance, 7 pm, Wilsonville Community Center  |   |
| 27 Mon   | • DRB Panel B, 6:30 pm, City Hall   |   |

All dates and times are tentative; check the City's online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

### Dads: Get Your Tickets for the Daddy Daughter Dance

Tickets for the ever-popular Daddy Daughter Dance on Friday, Feb. 24, 7 pm at the Wilsonville Community Center are available at [www.WilsonvilleParksandRec.com/Dance](http://www.WilsonvilleParksandRec.com/Dance).



## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and are replayed periodically. Meetings are also available to stream live and by video-on-demand at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.



Tim Knapp  
Mayor  
[knapp@ci.wilsonville.or.us](mailto:knapp@ci.wilsonville.or.us)



Scott Starr  
City Council President  
[scottstarr97070@gmail.com](mailto:scottstarr97070@gmail.com)



Susie Stevens  
City Councilor  
[stevens@ci.wilsonville.or.us](mailto:stevens@ci.wilsonville.or.us)



Charlotte Lehan  
City Councilor  
[lehan@ci.wilsonville.or.us](mailto:lehan@ci.wilsonville.or.us)



Kristin Akervall  
City Councilor  
[akervall@ci.wilsonville.or.us](mailto:akervall@ci.wilsonville.or.us)

### City Manager

Bryan Cosgrove  
503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)



### Wilsonville City Hall

29799 SW Town Center Loop E  
Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

City Council: 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)  
Police Non-Emergency Dispatch: 503-655-8211



## Mayor Knapp Presents '2017 State of the City' Address on March 6

Wilsonville Mayor Tim Knapp is preparing to present the "2017 State of the City Address" at the start of the City Council meeting on Monday, March 6, 7 pm, at Wilsonville City Hall, 29799 SW Town Center Loop East.

In general, Mayor Knapp will address recent and future projects and initiatives that contribute to main-

**2017 State of the City Address**  
 • Mon, March 6, 7 pm  
 City Hall, Council Chambers  
 29799 SW Town Center Loop East

taining Wilsonville's quality of life for. In particular, the address intends to highlight a broad range of major projects and initiatives the City recently completed or is beginning this year.

Community members are invited to attend the City Council meeting to hear the address or to watch a live broadcast either on the City's website at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV) or on Xfinity/Cast Ch. 30 or Frontier Ch. 32.

## French Prairie Bridge Project Task Force Holds First Meeting



The French Prairie Bridge Project task force met for the first time in late January to begin work on the proposed bicycle/pedestrian/emergency-access crossing of the Willamette River west of the Interstate 5 Boone Bridge in Wilsonville.

## City Names New Parks and Recreation Director

The City of Wilsonville announced the hiring of Michael "Mike" McCarty as the City's new Parks and Recreation Director. Mike has more than 27 years experience leading municipal parks and recreation departments and has worked for several different local governments.

Most recently Mike served as the Director of Parks and Community Services for the City of San Moreno Valley, Calif. Prior to this position over a 21-year period, 1986–2007, Mike also served in similar roles for the cities of Yuma, Ariz. and Napa Valley, San Leandro, and Ukiah, Calif. Mike's career in parks and recreation began as a Recreation Supervisor for the City of Paso Robles in 1979.

Mike studied at Humboldt State University in Arcata, Calif. where he earned a Bachelor of Arts degree in Public Administration and he also earned



Mike McCarty

a Masters Degree in Organizational Leadership from Cal Poly San Luis Obispo in San Luis Obispo.

Assistant City Manager Jeanna Troha states, "We are excited to have Mike join our team. He brings a wealth of experience in parks and recreation, a collaborative leadership style and passion for enhancing the quality of life for people that live and work in the community."

Mike explains, "I am very excited to be a part of the team in Wilsonville and see this as a great opportunity to make a difference in what is already a very nice town with exceptional parks."

In his free time Mike enjoys exercising, being outdoors, visiting baseball stadiums across the country and spending time with his children and grand children.

Mike McCarty, Parks and Recreation Director, may be reached at 503-570-1579; [mccarty@ci.wilsonville.or.us](mailto:mccarty@ci.wilsonville.or.us).

## New Transit Director Named for South Metro Area Regional Transit

The City of Wilsonville's public-transit agency, South Metro Area Regional Transit (SMART), announces the hiring of Dwight Brashear as the City's new Transit Director, assuming the agency's leadership position held by Stephan Lashbrook who retired on Feb. 28.

Dwight Brashear is a seasoned transportation professional with over 30 years of experience in the industry. Most recently he co-managed a large paratransit operation in Chicago with a fleet of 500 vehicles and a budget of \$53M focused on transportation services to medical appointments for people with disabilities.

Previously, Dwight held executive leadership positions with several transit organizations. He served for four years in executive-level positions in management and business development for passenger-transportation services firm Keolis Transit America in Las Vegas and Los Angeles. He also worked in management for a large transportation firm providing paratransit, fixed route, shuttle and commuter-bus transit services, as well as CEO of the public-transit system serving the Baton Rouge, La, metropolitan region.

Dwight began his career in public transit in 1985 as a part-time transit driver for San Diego Transit Corporation and over a 17-year period he worked his way up to the position of Manager of Transportation where he managed a fleet of 340 vehicles and a staff of 750 transit professionals.



Dwight Brashear

Dwight earned a Bachelor's degree from Azusa Pacific University in Azusa, Calif. with an emphasis in Business and Management. He has also earned numerous awards including a "Hero's" designation from then-Louisiana Governor Kathleen Blanco for his role as State Transportation Coordinator for the evacuation of the City of New Orleans following Hurricane Katrina, which struck the Gulf Coast in August 2005. In 2006, he was recognized by the American Public Transportation Association (APTA) when he was presented with APTA's first-ever Special Award for Extraordinary Leadership. Dwight is also a graduate of APTA's Elite Leadership Program.

Assistant City Manager Jeanna Troha states, "Dwight has significant experience in all aspects of public transportation. He has an innovative and collaborative leadership style and brings a passion for making a difference in a community."

Dwight explains, "I couldn't be more excited to be coming to Wilsonville and to be a part of a thriving community that really values high-quality public transit. I look forward to working with all stakeholders, including residents and businesses, to ensure that SMART lives up to its core values and provides safe, reliable and cost-effective transit services."

In his free time Dwight enjoys once-a-week date nights with his wife Synthia, classic cars, quiet time and he is an avid reader and writer.

Dwight Brashear, Transit Director, may be reached at 503-570-1576; [brashear@ridessmart.com](mailto:brashear@ridessmart.com).

### Mayor's Message

## Community Members Volunteer for City Positions

Every year City Council confirms the appointment of volunteer community members to fill vacancies on the City's various boards and commissions. Recently we had 12 positions to fill and we were fortunate to have 23 community members interview for the openings. Applicants included seven current board and commission members seeking reappointment and 16 more community members seeking new appointments. It's great to see such interest in public service.



Mayor Knapp

During December and January, City Councilors Susie Stevens and Kristin Akervall and I met with the applicants in order to become better acquainted and understand where volunteers might best serve. We were impressed with the level of interest, commitment, qualifications and passion of the applicants.

While we would have liked to have a position for everyone who applied, ultimately the City Council in February settled on the following appointments:

- **Budget Committee:** Sam Scull to a three-year-long term
- **Development Review Board:** Joanne Linville and Jennifer Willard appointed to two-year terms; James Frinell, Ronald Heberlein, Richard Martens, Shawn O'Neil and Aaron Woods reappointed to two-year terms
- **Parks and Recreation Advisory Board:** Jim Barns and Diana Cutaia to four-year terms, and Kate Johnson reappointed to a four-year term
- **Planning Commission:** Phyllis Millan reappointed to a four-year term

This process of appointing volunteers to serve on City boards affirms what a talented and caring community we have in our community. And one where the people's participation in local government provides the very foundation of our democracy to create a better place for all of us to live, work and play.

Thank you to everyone who applied for these positions and also to the many current and prior board and commission members who volunteer their time to serve Wilsonville — our community is better because of your participation. Feel free to let me know your thoughts; reach me at 503-896-0048 or [mayor@ci.wilsonville.or.us](mailto:mayor@ci.wilsonville.or.us).

Tim Knapp, Mayor

### In This Issue...

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City of Wilsonville  
 29799 SW Town Center Loop East  
 Wilsonville, OR 97070

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## Task Force Named to Guide Wilsonville Town Center Plan

The City of Wilsonville has named members of the public to serve on an advisory task force for the Wilsonville Town Center Plan. The 26-member task force is composed of a wide variety of local residents, business owners and stakeholders who are responsible for reviewing key information and ultimately providing recommendations to the Planning Commission and the City Council at key milestones of the Wilsonville Town Center Plan project.

Wilsonville City Councilor Kristin Akervall and chair of the task force states, "We know from the City's

***We know from the City's community surveys that Wilsonville residents desire to have a more identifiable down-town or main street to serve as a central gathering place for our community.***

community surveys that Wilsonville residents desire to have a more identifiable downtown or main street to serve as a central gathering place for our community. This planning process aims to identify strategies to do just that."

Task Force meetings are open to the public and will be held at Wil-

sonville City Hall. The City is hosting the first task force meeting for the

Wilsonville Town Center Plan on Tuesday, March 14, 6-9 pm, at City Hall, 29799 SW Town Center Loop East, and the public is welcome to attend.

The Wilsonville Town Center Plan Task Force, which plans to meet six times during the 18-month long planning process, includes:

- Chair **Kristin Akervall**, Wilsonville City Councilor
- Vice Chair (alternate) **Susie Stevens**, Wilsonville City Councilor
- **Marie Alaniz**, non-profit housing and representative of Northwest Housing Alternatives
- **Hilly Alexander**, local library advocate and resident
- **Ben Altman**, former Planning Commission chair and

past Chamber of Commerce president, professional planner and local resident

- **Kyle Bunch**, Town Center business owner of American Family Insurance and Wilsonville resident
- **Terrence Clark**, Town Center business owner of Wilsonville Dental Group
- **Paul Diller**, law professor and Wilsonville resident
- **Jon Dunn**, representative of the property-owner of the Shari's Café and Pies location
- **Bruce Eicher**, Town Center business owner of Wilsonville Diamond and Wilsonville resident
- **Kevin Ferrasci O'Malley**, CEO of the Wilsonville Area Chamber of Commerce
- **Darren Harmon**, Town Center business general manager of Wilsonville Family Fun Center and a member of the Tourism Promotion Committee
- **Ron Heberlein**, representative of Arbor Crossing Home Owners Association and Wilsonville resident
- **Eric Hoem**, a retired community college professor who lives in Wilsonville



The plan's study area includes Town Center Park and the 100-acre area within and adjacent to Town Center Loop.

- **Rosiland Hursh**, Town Center business owner of Eye to Eye Clinic
- **Sara Jantze**, local business representative of Owen Roe Winery and Wilsonville resident
- **Hank Jarboe**, Town Center business owner of Boston's Pub
- **Kate Johnson**, a local business owner, Park and

Recreation Advisory Board member and Wilsonville-Metro Community Enhancement Committee and a Wilsonville resident

- **Sophia Lochner**, Wilsonville resident and Wilsonville High School student
- **Lori Loen**, representative of Landover Home Owners Association and a Wilsonville resident
- **Kamran Mesbah**, Wilsonville Planning Commission member and local resident
- **Susan Myers**, an asset manager for Capital Realty commercial real-estate broker who helped develop the Town Center shopping center and nearby office building
- **Richard (Dick) Spence**, Wilsonville Community Sharing board member and a Wilsonville resident
- **Shelly Tracy**, Director of Clackamas Community College's Wilsonville campus
- **Scott Vosburg**, Town Center business owner of Wilsonville Lock and Security
- **Doris Wehler**, past president of the Wilsonville Area Chamber of Commerce and a local resident

The Wilsonville Town Center Plan seeks to be a guide for future development in Town Center to create a cohesive, unified district that enhances existing assets in the area and sets the stage for new development and investment. Potential strategies include new projects, programs, partnerships, or policies that foster an attractive and accessible place for visitors and residents of all ages to shop, eat, live, work, learn and play within the Town Center area.

The planning area, which covers approximately 100 acres and encompasses the properties north of Wilsonville Road, within and adjacent to Town Center Loop. The project also includes a project team that is completing a technical analysis to evaluate the economic, transportation, stormwater and land-use conditions within the study area. Funding for the long-range planning process comes from a Metro Community Planning and Development Grant with matching funds from the City's Urban Renewal Agency.

To sign up for project updates, be notified of upcoming events and to get more information visit [www.wilsonvilletowncenter.com](http://www.wilsonvilletowncenter.com) or contact Miranda Bateschell, Long-Range Planning Manager, at 503-570-1581, [bateschell@ci.wilsonville.or.us](mailto:bateschell@ci.wilsonville.or.us).



8200 SW Wilsonville Road  
503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

### Free Tax Help Available

A volunteer CPA will be at the library ready to answer your questions and/or help you fill out your tax forms. Drop in on Saturdays through April 15, 10 am-3 pm.

Please bring all of your necessary documentation and your 2015 tax records.

**Saturdays through April 15, 10 am-3 pm. Free.**  
[www.WilsonvilleLibrary.org/taxes](http://www.WilsonvilleLibrary.org/taxes)



### Teen Event: Game Night

We are bringing back one of our favorite, most requested events, but with a twist: think Summer Camp! There will be the usual tabletop and video games, along with some more active games. Capture the Flag (in the library), anyone? For students in grades 6-12.

**Friday, March 17, 6:30-8:30 pm.**  
[www.WilsonvilleLibrary.org/teens](http://www.WilsonvilleLibrary.org/teens)



### Curiosity Cafe: Estate Planning for Parents with Minor Children

Have a cup of coffee and join us for Curiosity Cafe. This month, get answers to your questions about planning your estate with your minor children in mind.

**This free event is on Saturday, March 18, 1-3 pm, in the Oak Room.**



### Book Notes Concert: Al Andalus

Explore the musical landscape of the Middle East, North Africa, and Europe with chamber ensemble An Andalus. Their soulful merging of classical, jazz, and contemporary music is infused with the musical traditions of Andalusian Spain.

**This free concert is on Saturday, Mar. 11, 2-3 pm in the Reference reading area.**



### Wilsonville-Boones Ferry Historical Society

Do you have an interest in Wilsonville history? Want to help out? The Wilsonville-Boones Ferry Historical Society wants you! Join an excited group of citizens looking to educate and raise awareness about the history of our fair city. Contact Patrick Duke for more information at [duke@wilsonvillelibrary.org](mailto:duke@wilsonvillelibrary.org) or 503-570-1590.



### Upcoming Classes and Events

**Local Artist of the Month: Kirsten Sneath**

**First Friday Film**

Friday, March 3, 6-8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

**Book Club**

**Author Comparison: James A. Michener**  
Thursday, March 9, 6-8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

**Book Notes Concert:**

**Al Andalus** (Middle Eastern fusion)  
Saturday, March 11, 2-3 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

**K-2 Book Adventures**

Friday, March 16, 4-5 pm  
[www.WilsonvilleLibrary.org/k2](http://www.WilsonvilleLibrary.org/k2)

**Genealogy Club**

Monday, March 20, 1-2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

**Great Books Discussion Group**

**"The Death of Ivan Ilyich" by Leo Tolstoy**  
Tuesday, March 21, 6-8 pm  
[www.WilsonvilleLibrary.org/gb](http://www.WilsonvilleLibrary.org/gb)

**History Pub**

Tuesday, March 28, 6:30-8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

**Visit our website for more events:**  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

## SMART Seeks Comments on Draft Transit Master Plan Prior to Potential Adoption In Late March

The City of Wilsonville's public transit agency, South Metro Area Regional Transit (SMART), is seeking public comment on the draft Transit Master Plan prior to potential City Council adoption. The purpose of the Transit Master Plan is to capture the community's priorities and needs and to guide the transit agency's future changes, investments and operations over the next five years. Copies of the plan are now available online at [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com) and at Wilsonville City Hall and at the local Library.



also welcomed and after which the public comment period is closed.

Transit Director Stephan Lashbrook said, "We are very thankful to all who have taken the time over the past year to provide SMART their thoughtful input

that has helped shape the current draft. Now we are asking interested community members to review the plan one more time to see if any further changes are needed before the Planning Commission and City Council consider the plan for adoption."

For more information contact, Michelle Marston, Transit Program Coordinator, 503-570-0332; [marston@ridesmart.com](mailto:marston@ridesmart.com).



Members of the public—including residents, employers, employees and visitors—can comment on the draft plan in various ways:

- **Online** at [www.plansmartwilsonville.com/tmpcomment](http://www.plansmartwilsonville.com/tmpcomment);
- **Email** to [marston@ridesmart.com](mailto:marston@ridesmart.com)
- **Call** 503-570-0332
- **Mail** to City of Wilsonville, SMART TMP Comments, 29799 SW Town Center Loop East, Wilsonville, OR 97070

The Wilsonville Planning Commission is scheduled to hold a public hearing on the draft Transit

**Public Hearings on TMP**  
 • **Wed, March 8, 6 pm, Planning Commission**  
 • **Mon, March 20, 7 pm, City Council**  
 City Hall, Council Chambers  
 29799 SW Town Center Loop East

Master Plan on Wednesday, March 8, 6 pm, at Wilsonville City Hall. A subsequent public hearing

is tentatively scheduled before the City Council on Monday, March 20, 7 pm, where public testimony is

Think SMART. Ride SMART. Plan SMART.

## Prevent Carbon Monoxide Poisoning in Your Home

Carbon monoxide (CO) is an invisible, odorless and colorless gas that can cause sudden illness and death. The odorless gas is created when fuels such as gasoline, wood, charcoal and petroleum products burn incompletely.

CO alarms alert you and your family when there are dangerous levels in your home. Alarms can be purchased at any home improvement store.

### Safeguard Your Home:

- Install and maintain CO alarms to provide early warning.
- CO alarms should be installed in a central location outside each sleeping area and on every level of the home.
- Choose a CO alarm that has the label of a recognized testing laboratory.



- CO alarms are not substitutes for smoke alarms.
- Know the difference between the sound of your smoke alarms and the sound of your CO alarms.

CO poisoning happens when you breathe too much of the gas. What makes CO so dangerous is that when you breathe it, the gas replaces the oxygen in your blood. Without oxygen, cells throughout the body die and the organs stop working.

Symptoms of CO poisoning include slight headache, dizziness, nausea, drowsiness, confusion, irritability and unconsciousness.

### Causes of CO Poisoning:

- Operating poorly maintained or unvented furnaces, boilers, and other fuel-burning equipment.
- Using gas stoves or ovens to heat the home.
- Clogged chimneys and heating exhaust vents.
- Running cars, generators or gas-powered tools in enclosed areas.

If your CO alarm sounds, immediately evacuate your home and call 911.

# SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
 28879 SW Boberg Road (office)  
 503-682-7790  
[smart@ridesmart.com](mailto:smart@ridesmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

## Rider Appreciation Week March 13-17

# SMART

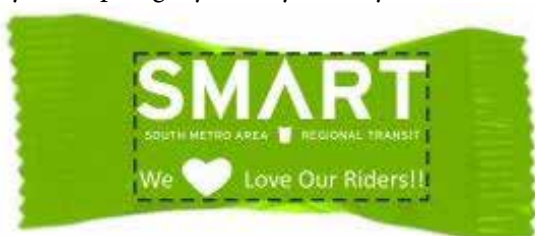
SOUTH METRO AREA REGIONAL TRANSIT

We ♥ Love Our Riders!!

Public transportation would not need to exist without passengers and we would like to take a moment and thank everyone for riding on SMART.

During the week beginning on Monday, March 13, bus drivers will be handing out little tokens of our appreciation as a simple way to say thank you for riding on Wilsonville's public transit system.

If you are new to the Wilsonville area or have been thinking about how ride the bus around town, please visit our website or give us a call and we will help you plan your trip to get you on your way.



## FREE Travel Training and Mobility Support Available

RideWise, through a partnership with SMART, is offering travel training and mobility support for older adults (60+) and people with disabilities in Wilsonville.

In addition to the one-on-one support offered, RideWise also offers group trips called the Riders Club. Past Riders Club groups have traveled to the Chinese Garden, Oregon Zoo, Farmers Markets, art festivals, and more. Last month, a group from the Wilsonville Community Center traveled to the Portland Art Museum for Art and Conversation, a monthly lecture series.

Our goal is to provide people an opportunity to get acquainted with public transit and get out and do something fun!

Please give us a call, or e-mail us if you're interested. 503.682.7790 x1438 or [sherbst@rideconnection.org](mailto:sherbst@rideconnection.org).



## Did you know? SMART has a Free App!

### GET SPOT MOBILE



- **Search ETA SPOT**
- **Choose SMART as your provider**
- **Receive System Alerts & Announcements**
- **Real-Time Status Updates of your route**

## Opportunities for Public to Comment on Transit Master Plan

The Draft Transit Master Plan is available for public comment online at [plansmartwilsonville.com](http://plansmartwilsonville.com).

The Draft TMP will also be discussed and open for public comment at the following meetings:

**Planning Commission, March 8, 6 pm,  
City Hall, Council Chambers**

**City Council, March 20, 7 pm  
City Hall, Council Chambers**

## Our History: Boones Ferry Crossing Connects Portland and Salem



This Boones Ferry Historical Society photo shows the Boones Ferry docked with three cars in Wilsonville on the north shore of Willamette River and in the center is the Tauchman House that is located today in Boones Ferry Park.

## Resolve to be Ready for a Power Outage in 2017

During stormy weather we can lose power. Sometimes the interruption lasts for only a few minutes, but other times you might have to go without electricity for a few hours or longer. Here are some basic tips for what to do before, during, and after a power outage.

### Before a Power Outage

- Restock your emergency preparedness kit, including a flashlight, batteries, cash and a first aid kit.
- Know where the release lever of your electric garage door opener is located and how to operate it.
- Keep your car's gas tank full - gas stations rely on electricity to power their pumps.

### During a Power Outage

- Only use flashlights for emergency lighting, candles can cause fires.
- Keep refrigerator and freezer doors closed. Most

food requiring refrigeration can be kept safely in a closed refrigerator for several hours. An unopened refrigerator keeps food cold for about 4 hours. A full freezer keeps the temperature for about 48 hours.

- Turn off or disconnect appliances and other equipment in case of a momentary power "surge" that can damage computers and other devices. Consider adding surge protectors.
- Put on layers of warm clothing if it is cold outside.
- Never burn charcoal for heating or cooking indoors.
- Never use your oven as a source of heat. If the power may be out for a prolonged period, plan to go to another location (the home of a relative or friend, or a public facility) that has heat to keep warm.

For more information visit [www.ready.gov](http://www.ready.gov).

## Free Microchipping, Rabies Vaccination for Licensed Dogs

Dog owners may now get free microchipping and a free rabies vaccine for their dogs at Clackamas County Dog Services with proof of a county dog license.

Microchipping/vaccination clinics are held 9–11 am on the fourth Saturday of every month at the county dog shelter, 13141 SE Highway 212 in Clackamas. The clinics, run through a

**Free Microchipping and Rabies Vaccinations for Dogs**  
• Sat, March 25, 9–11 am  
County Dog Shelter  
13141 SE Highway 212, Clackamas

partnership with Good Neighbor Vet, also offer other limited veterinary services.

Everyone is welcome and no advance appointments are needed. Upcoming clinic dates are March 25 and April 22.

To get a free microchip and rabies vaccination, owners of licensed dogs should verify current licenses with Dog Services before entering the mobile clinic in the parking lot. People without a current dog license may request an authorization form to purchase a license after their pet receives the microchip.

A dog who has a microchip is easier to find if they get lost or if they have to be left behind in an emergency. The process only takes about 20 seconds to insert a rice-grain-size microchip under the skin between the animal's shoulder blades. The chip can't be seen or felt by the dog, but can be detected by scanners used by veterinarians, shelters and rescue agencies.

To learn more about Clackamas County Dog Services visit [www.clackamas.us/dogs](http://www.clackamas.us/dogs) or call 503-655-8628. The county agency also offers information about licensing, pet adoption, reverse dog license search, low-income spay and neuter program and more.



29600 SW Park Place  
Parks Programs: 503-783-7529  
Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

## Park Rental Season Opens March 13 for City Residents

Reservations for Wilsonville residents and businesses will be accepted starting on March 13 for the 2017 season. The rental season runs April 8–Oct. 15. Reservations for non-residents will be accepted beginning March 20.

For more information on availability and fees please visit the Parks and Recreation website or contact Ahsamon at 503-570-1530.

## Dance, Dance, Dance!

Dancing is a great way to improve your physical fitness in a fun social environment. Dance can help with increased muscular strength, endurance and aerobic fitness. Dancing is also a great way to meet new friends! The Parks and Recreation Department offers two daytime dance programs at the Community Center.

**Tap Dance** is offered Wednesdays, 1:30–2:30 pm with the next session running April 5–June 7. The cost for 10 sessions is \$70, or just \$7 per class.

**Line Dance** is offered on Mondays from 2:30–3:30 pm with the next session running April 3 through May 22. The cost for 8 sessions is \$40 or just \$5 per class.

## Community Garden Sign Up

The Community Garden opens for registration on Thursday, April 6. Gardeners are asked to come to the Parks and Recreation Admin Office to register and pick their garden plot. Registration opens at 8 am. In-ground plots are \$22 for the season. Raised beds are \$25 for the season. Please visit the Parks and Recreation website or contact Brian Stevenson at 503-570-1523 for more information.

## Yoga for Mind and Body Health

Originating in India thousands of years ago, yoga has become an increasingly popular form of exercise for the body and mind. The American Osteopathic Association recognizes the following physical benefits of yoga: increased balance, flexibility and strength; improved respiration and heart health; and lowered blood pressure and weight. Mental benefits of yoga include stress relief, mental clarity, sharper concentration and greater self-awareness.



Over the past year, Wilsonville Parks and Recreation has expanded the department's yoga offerings to include a variety of teaching styles and class times. Gentle Hatha Yoga, which emphasizes relaxation and the use of chairs and props, and Hatha Yoga Basics, which focuses on the fundamentals, are offered on Tuesday and Thursday mornings, respectively. Evening Hatha Yoga offers a similar focus on alignment and relaxation techniques, and is great for unwinding after a long day of work. New this March, Yin/Yang Yoga, combines active poses to build strength in the body with restorative poses and guided meditation. Slow Vinyasa on Saturday mornings offers a dynamic practice with a strong focus on the breath. Instructors are trained to offer modifications to accommodate various levels of experience.

"[Yoga] changed my life profoundly and to not share it seems wrong," says Tracy Cohen, RYT. With decades of experience, she strives to offer yoga for everybody and, in addition to the adult evening Hatha Yoga classes for adults, will be offering Yoga Calm classes for youth this summer.

For more details and to register online, visit [WilsonvilleParksandRec.com](http://WilsonvilleParksandRec.com).

## Upcoming Classes and Events

### Oil Painting

Ages 12 and older  
Saturday, March 11  
10 am–2 pm  
Parks and Rec Admin Building  
\$50

### Intro to Survival Camp

Ages 6–9  
March 27–31  
9 am–1 pm  
Mary S. Young Park  
\$215

### Skyhawks Basketball Camp

Ages 7–12  
March 27–1  
9 am–3 pm  
Boones Ferry Primary School - Fauna Gym  
\$145

### Pilates Move for Life

Ages 18+  
Mondays, April 10–June 5  
9:30 am–10:30 am  
Community Center  
\$65

### Wilsonville Egg Hunt

Ages 1–11  
Saturday, April 15  
10 am  
Memorial Park  
No Charge

### Holistic Health Fair

Saturday, April 22  
10 am–1 pm  
Community Center  
No Charge



### Question of the Month

Every month throughout the 18-month-long planning initiative the Wilsonville Town Center Plan is asking community members a Question of the Month. The questions are designed to gather feedback on a wide-range of issues related to developing the plan that captures a community-driven vision and strategies to guide the development of Wilsonville’s Town Center.

#### Here is the Question of the Month:

**What, if anything, keeps you from spending more time in Town Center?**  
(Select all that apply.)

- a) There is not enough variety of stores.
- b) There is not enough variety of restaurants.
- c) Everything I need that is in Town Center, I can get closer to home.
- d) It’s difficult for me to get to Town Center.
- e) Once I’m in Town Center, it is difficult or uncomfortable to walk around.
- f) There are not enough social activities and events happening throughout the year.

To share your feedback and participate in the Question of the Month stop by City Hall, the Library or the Community Center or visit the project’s website below. To answer the Question of the Month, sign up for project updates and to get more information visit [www.WilsonvilleTownCenter.com](http://www.WilsonvilleTownCenter.com) or contact Miranda Bateschell, Long-Range Planning Manager, at [bateschell@ci.wilsonville.or.us](mailto:bateschell@ci.wilsonville.or.us) or 503-570-1581.

### CHIEF’S CORNER – TIPS FOR A ROAD TRIP

With spring break coming up at the end of the month many families are planning to take a road trip in late March. Before you go I wanted to share some tips on how to prepare before you hit the road.



Police Chief Adam Phillips

There are three main requirements to having a safe road trip and they include: having a well-maintained and good condition vehicle; planning your trip and knowing the road conditions you may likely face; and being an alert and safe driver.

#### Prepare Your Vehicle:

- Make sure your car is in good repair
- Check your tires to see they have a good tread and are properly inflated
- Check to make sure your battery is strong and keeping a charge
- Make sure you have an emergency kit in your car with water, blankets and extra food
- Make sure you have a full tank of gas before your trip.
- Check your spare tire and make sure you have the right tools to change a tire if needed

#### Plan Your Route and Know the Road:

- Visit [www.TripCheck.com](http://www.TripCheck.com) for up-to-date travel and road conditions
- If you are likely to hit snow during your trip: Carry proper traction devices and know how to put them on if needed; Clear all snow and ice from the vehicle windows and hood to improve visibility;

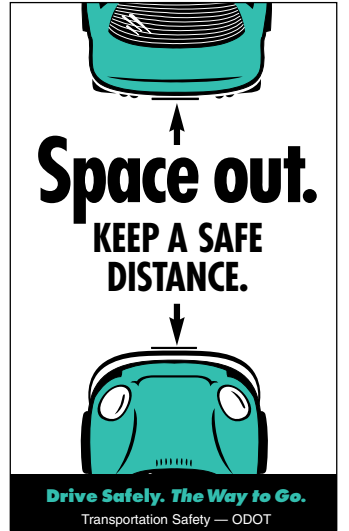


If your front wheels begin to slide during a turn, straighten the steering wheel slightly to allow them to regain rolling friction; If your back end begins to slide, do NOT use your brakes; this will shift weight to the front of the car and make the slide worse.

- Remember that driving in inclement weather isn’t necessarily difficult but stopping usually is - so just slow down.
- Leave early and allow yourself enough travel time.
- Avoid spinning or locking your wheels and accelerate and decelerate slowly, with or without chains

#### Be an Alert and Safe Driver:

- Get a good night sleep before your trip
- If driving in cold conditions be alert to and prepared for black ice
- If you have to pull over for any reason, pull over on the right-hand shoulder of the road and as far away from traffic as possible
- Always leave enough room between you and the car in front of you; leave at least one car length for every ten miles per hour you are traveling
- During nighttime driving, your vision may be impaired slightly so drive defensively and at a safe distance.
- Avoid texting or holding a mobile device while driving.



If you are planning a road trip any time soon, even if not for spring break, I hope these tips will help ensure you and your family have a safe trip wherever and whenever you are traveling on the road.

*Adam Phillips, Chief of Police*

### Local Partners Team Up to Promote Waste Reduction and Recycling in Multifamily Communities

Many who call Wilsonville home opt to live in apartment or condominium communities, known as ‘multifamily’ dwellings. While there are many conveniences to living in multifamily communities, disposal of bulky waste, electronic waste and recycling is not always that easy.



Bulky waste should not be placed near the recycling containers without approval from the property manager.

Recycling in multi-family communities can be a challenge for several reasons. Many communities have inadequate and inconvenient locations to collect garbage and recycling on site. Some residents feel proper recycling to be difficult or inconvenient depending on the distance to onsite recycling facilities. Property managers and tenants often move more frequently, making it harder to learn and adjust to each new property’s approach to recycling and waste disposal. Residents in multifamily properties must often depend on someone else such as a supportive property manager and owner, to make recycling work well.

In spite of these common challenges, supporting more and better recycling in residential and multifamily communities remains an important goal in Wilsonville. The City of Wilsonville, Clackamas County, along with Republic Services, the City’s

local waste and recycling collector, is conducting a six-month focused effort to raise awareness, increase recycling and decrease contamination in Wilsonville’s multifamily communities with funding from the City’s Community Enhancement Program.

Proper disposal of bulky waste, such as couches, TVs, bed frames and other large items is a common challenge in multifamily communities. Often these types of large items are placed outside or near a dumpster, but doing so can prevent other residents from accessing recycling and trash bins further discouraging recycling within the community.

Before attempting to deposit bulky waste onsite, tenants should first check with their onsite property manager to understand the applicable rules and limitations. Often property management policies, billing restrictions and space limits, prohibit onsite disposal of bulky waste in multifamily communities. During recent site visits in Wilsonville programs staff found that approximately fifty percent of the apartment sites

visited at the beginning of this joint outreach effort had bulky waste somewhere on the property.

In May each year the City, Metro and Republic Services sponsor a free Bulky Waste Day where local

**Bulky Waste Day**  
• Sat, May 20, 9 am–1 pm  
Republic Services, 10295 SW  
Ridder Road, Wilsonville

residents may drop off bulky waste such as couches, appliances,

and a wide-variety of other large sized objects including electronics. This year the event is being held on Saturday, May 20, 9 am–1 pm at Republic Services, 10295 SW Ridder Road in Wilsonville.

To help promote proper recycling throughout Wilsonville here are some important recycling rules to follow:

- Keep plastic bags out of the recycling. Most grocery stores such as Safeway and Fred Meyer take back plastic bags.
- Put only glass into the glass recycling bins; avoid mixing cardboard or plastic bags in the glass bins as these cause contamination.
- Recycle electronics instead of setting them by the dumpster. In Oregon it is illegal to dump computers and television sets. If the items still work, donation centers such as Goodwill and Salvation Army are a good alternatives.

Drop off electronic items at any business that participates in the Oregon E-Cycles program such as the Republic Services’ waste transfer station at 10295 SW Ridder Road in Wilsonville which



accepts laptops, monitors, printers, televisions, computers, mice and keyboards for free. For more information on multifamily recycling in Wilsonville contact Kayla at: 503-742-4482 or email: [KScheafer@clackamas.us](mailto:KScheafer@clackamas.us).



Cardboard should be broken down and flattened before being placed in the recycling bin to maximize space usage.



## Reports from City Boards and Commissions

### City Council Approves Parking Zone Program and Appoints 12 Community Members to City Boards and Commissions

On Feb. 6, the Wilsonville City Council gave final approval to implement a residential parking-zone program that takes effect in 30 days. The program allows community members that live in residential areas heavily congested with on-street parking to petition the City to create specific residential parking zones that are restricted to approved permit-holders and visitors with temporary permits during specified hours. The approved ordinance also governs administration and enforcement of the program. The new program seeks to decrease parking congestion and address safety concerns, reduce parking violations and improve neighborhood access for emergency, waste-management and postal-service vehicles.

The City Council agreed to continue until Feb. 23 two ordinances related to the water-treatment plant and the proposed raw-water pipeline. The first ordinance authorizes a ground-lease agreement for the raw-water pipeline with Tualatin Valley Water District and the City of Hillsboro and the second ordinance amends an agreement related to ownership and management of the Willamette River Water Treatment Plant in Wilsonville.

Under Mayor's Business, the City Council discussed and approved the appointment of 12 community volunteers to fill open positions on the City's various boards and commissions. See article on page 1 for details.

During the communications portion of the meeting Bob Gibbs, a national expert on retail market analysis and member of the Wilsonville Town Center Plan team, presented initial findings from his draft retail market study for Wilsonville. Initially the study has found that Wilsonville and the surrounding area can support more retail offerings in Wilsonville Town Center study area. A copy retail market analysis plan is posted at [www.WilsonvilleTownCenter.com](http://www.WilsonvilleTownCenter.com).

On the consent agenda the City Council approved a contract for the City to obtain meter reading services.

During work session prior to the meeting, the City Council also heard reports and discussed the Transit Master Plan and the Frog Pond Master Plan, both of which are scheduled for public hearings at the March 8 Planning Commission meeting; public hearings are tentatively planned before the City Council March 23 for the Transit Master Plan and April 4 for the Frog Pond Master Plan.



Retail expert Bob Gibbs presents initial findings from a retail market analysis his firm is conducting for Wilsonville at an event co-sponsored with the Wilsonville Area Chambers of Commerce.

Community members are able to watch a replay of all or a portion of the City Council meetings on Comcast/Xfinity Ch. 30, Frontier Ch. 32 or on the City's video-on-demand service at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV).

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council during March. Scheduling changes can and do occur; check for updated meeting information at the City's website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, March 6:** State of the City Address by Mayor Knapp; Frog Pond infrastructure financing; Water Treatment Plant Master Plan and WWSP coordination; Equitable Housing Strategic Plan update; Boones Ferry Road to Kinsman Connector design contract; Wastewater Collection System Master Plan project list amendment; Water Distribution Master Plan project list amendment; Letter of intent and first right of refusal to purchase raw water facility.

**Monday, March 20:** Year 2000 Urban Renewal Master Plan; cell tower lease; Charbonneau phase two construction contract award; low-income housing property tax exemption; Transit Master Plan (potential adoption); supplemental budget adjustment; and new planning fee schedule.

### Planning Commission Update

At the Planning Commission meeting on Jan. 18, the Commission elected Jerry Greenfield as Chair and Eric Postma as Vice-Chair. The commission also reviewed and provided staff feedback on a draft of the Frog Pond West Master Plan which is also scheduled to have a public hearing at the upcoming Planning Commission meeting on March 8. The Planning Commission has also scheduled a public hearing for the Transit Master Plan for the March 8 meeting.

### Development Review Board Update

The regularly scheduled DRB Panel A meeting for Monday, Feb. 13, was cancelled due to a lack of agenda items. The next DRB Panel A meeting is scheduled for Monday, March 13.

The next regularly scheduled DRB Panel B meeting is scheduled for Monday, March 27.

### Library Board Report for January

During the Library Board meeting on Wednesday, Jan. 25, the following reports were given:

- In December and January the library hosted two teen events—Game Day and Winter Party.
- The Library is moving forward with RFID tagging and once the equipment is received staff and volunteers will be trained. To avoid unnecessary tagging staff is weeding out old and unused items.
- The Library is moving forward with the renovation project and met with the architect in January.
- The Library's proposed budget for next year has been submitted and is primarily a status quo budget except for a request to fund a four-hour-per-week Outreach Librarian using Foundation funds.
- The Library celebrated its 35th anniversary on Feb. 14, that included a birthday cake and fee waivers.
- Dolly Parton's Imagination Library celebrated its 5th year of operation in Wilsonville and has just over 700 kids enrolled in the program with 650 graduates. Findlay Dodge Chrysler held a celebration for the program naming it their charity of the month.
- The Library is again planning elementary school Family Nights with all three elementary schools in Wilsonville.

The next regular Library Board meeting is scheduled for Wednesday, March 22, 6:30 pm, at the Library.

### Local Firm Donates \$1045 to Wilsonville Community Sharing



Next Level Chiropractic in Wilsonville donated \$1045 to Wilsonville Community Sharing to support the local food bank and social service referral agency (l to r): Wilsonville Community Sharing (WCS) board member Dick Spence, Next Level staff person Melissa Gonzalez, WCS staff Leigh Crosby and Next Level's Chiropractor Dr. Ryan McCormic.

### City Council Approves Parking Zone Program

On Jan. 19, the Wilsonville City Council held a public hearing and approved an ordinance to create a residential parking-zone program see the above article for details.

The City Council agreed to continue until Feb. 6 two ordinances related to the water-treatment plant and the proposed raw-water pipeline. The first ordinance authorizes a ground-lease agreement for the raw-water pipeline with Tualatin Valley Water District and the City of Hillsboro and the second ordinance amends an agreement related to ownership and management of the Willamette River Water Treatment Plant in Wilsonville.

Under Mayor's Business, the City Council discussed the process to appoint community volunteers to fill

12 open positions on the City's various boards and commissions. Mayor Tim Knapp and City Councilors Stevens and Akervall conducted interviews and the Council expects to make appointments at the next City Council meeting on Feb. 6. A total of 23 community members have applied to fill 12 board and commission vacancies.

During work session prior to the meeting, the City Council also heard a report and discussed the City's 2017 State Legislative Agenda for general public-policy priorities that guides how the City reacts to specific legislative proposals that may arise in the upcoming Oregon Legislative Assembly session that convened on Wednesday, Feb. 1, 2017.



On Feb. 14, 2017 the Library celebrated 35 years of operation with a birthday party and cake for attendees.

## Winter Weather Wreaks Havoc on Wilsonville

The many snow and ice storms this winter in Wilsonville have taxed local resources and raised a number of questions regarding how the City responds to these types of inclement weather events.

The following frequently asked questions (FAQs) address how, and why, the City responds to inclement weather events:

### Why doesn't the City plow more streets?

This year's inclement weather events are more frequent and prolonged than years past, leaving many local residents asking why the City does not do more to respond to these types of events.

The simple answer is that the City does not budget for plowing and sanding every residential street in Wilsonville. Based on the City's current budget and level of service, the City plows and sands heavily used arterials and major roads after each major event. In addition, City public works and parks department personnel are called into duty to clear snow and ice from sidewalks and parking lots for all City buildings, facilities and parks. City crews also remain on-call, 24 hours a day seven days a week, to respond to high-priority calls such as frozen water service lines, debris removal, clearing downed trees blocking roadways and a host of other high-priority calls the City typically receives during these types of inclement weather events.

Plowing and sanding every street in Wilsonville represents a large increase in the City's current level of service and would take significantly more resources to accomplish. If community members desire for the City to shift resources to plow and sand more or all of the streets in Wilsonville there would need to be strong support from the community and City Council to do so.

**Why doesn't the City use de-icing agents?**  
The City does not use salt and other de-icing chemicals on Wilsonville's roads due to the negative impacts the agents have on roads, vehicles, City equipment and the environment. Up until very recently the Oregon Department of Transportation (ODOT) did not consider the use of these agents to be a best

practice, but that may be changing as ODOT is now considering and experimenting with their use. The City is open also open to evaluating their use and other changes to City policies after undergoing an after-action review related to this most recent event. The City can, should, and does evaluate City policies related to our response to these kinds of events. There is always room for improvement in how the City responds and prepare for inclement weather.

### Why do Wilsonville schools sometimes close even when the weather conditions are not that bad?

Understandably many local residents and educators are frustrated by the amount of school students have missed this year due to school closures. While the City has no say over school closures or delays, the West Linn Wilsonville School District must base the decision on the weather conditions throughout the district and not just on conditions in Wilsonville. Many parts of West Linn and the areas surrounding Wilsonville are very hilly and often experience more severe weather conditions due to being at much higher elevations. As a result of the district's geographic diversity, the district must exercise extreme caution in order to protect the safety of all students, staff and parents as they travel to and from school. Due to a variety

of procedural, contractual and reporting requirements, school districts must apply school closure decisions uniformly throughout the district and are not able to close schools in one town and open schools in another if they are in the same district.

### Why don't more property owners shovel the snow and ice from the sidewalks in front of their homes and businesses?

City code requires all Wilsonville property owners to clear snow and ice from the sidewalks in front of and adjacent to the property they own. Unfortunately the majority of residents and businesses fail to do so and violate City code. The infraction also exposes the property owner to potential legal liability and damages if someone were to fall and injure themselves.

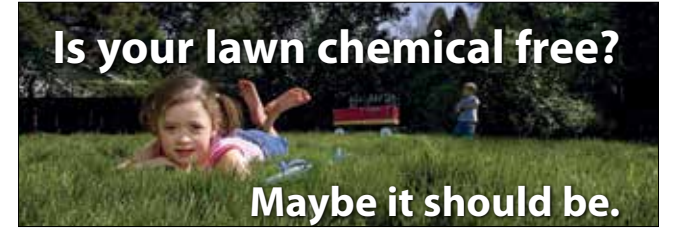


City staff clears snow from the sidewalk at City Hall.

## Limit Pesticide Use to Protect the Environment and Loved Ones

The most commonly applied pesticides are insecticides (to kill insects), herbicides (to kill weeds), rodenticides (to kill rodents) and fungicides (to control fungi, mold and mildew). Harmful environmental impacts of pesticide use include: loss of biodiversity and elimination of key species (e.g., pollinator bees), water pollution, soil contamination, and pest resistance that leads to an increase in the application of pesticides. Studies directly link the use of many pesticides with human health problems and chronic conditions.

In your own backyard you can do your part to eliminate or reduce the use of pesticides that might end up in streams, soil, food chains and on children's skin. In many cases, the only "active ingredient" you'll need is some elbow grease; hand-pulling weeds, for instance, can eliminate the use of herbicides and the risks they pose to the broader environment.



Is your lawn chemical free?

Maybe it should be.

Other tips for eliminating or reducing pesticide use and other unintended effects include:

- Be aware that pesticides applied in gardens can move from your intended application area by **drifting in the air or washing off into storm drains or streams.**
- **Be aware of the weather** and do not apply pesticides prior to rainfall or during windy conditions.
- **Avoid applying pesticides to surfaces like sidewalks or driveways**, where they can easily be washed off.
- **Check pesticide labels** for warnings regarding their appropriate use and limitations.
- **Never dispose of pesticides in storm drains, sinks or toilets.**
- **Under no circumstances should pest control equipment be cleaned where rinse water could flow into gutters, storm drains or open waterways.**
- **Never exceed application rates** listed on labels.
- **Purchase only enough pesticides for immediate needs**, which eliminates pesticide storage.
- **Keep pesticides in their original containers.**

Unwanted pesticides, even diluted ones, should be taken to an approved waste facility, such as the Metro Household Hazardous Waste Disposal Facility, open Monday - Saturday, 9 am - 4 pm, and located at 2001 Washington St., Oregon City (503-234-3000).

Unused rinse liquid is also considered a hazardous waste and must be disposed of properly at a hazardous waste facility. For more information, contact Kerry Rappold, Natural Resources Manager, at 503-570-1570: rappold@ci.wilsonville.or.us.

## Final Draft of Frog Pond West Master Plan to be Reviewed by Planning Commission and City Council

In March and April the City is providing the public two different opportunities to comment on the Frog Pond West Master Plan. The first is at the Planning Commission meeting on March 8 and then at City Council meeting on April 17.

**Planning Commission Public Hearing—Frog Pond Master Plan**  
• Wed, March 8, 6 pm  
City Hall, Council Chambers  
29799 SW Town Center Loop East

The public is invited to share feedback about the plan at the public hearings and also is invited to submit comments prior to the hearings.

The Frog Pond West Master Plan implements a "Residential Neighborhood" comprehensive plan map designation and creates a zoning designation for the approximately 180-acre area located generally west of Stafford Road and north of Boeckman Road within the Urban Growth Boundary. The Plan identifies land uses, infrastructure funding, comprehensive plan and development code language, community design, a multi-modal transportation network, parks and open space, street trees, signage, gateways and implementation for the area. The Plan also establishes the overall vision for the neighborhood; illustrates and defines neighborhood specific plans and requirements for land use, streets, pedestrian connections, bike routes, parks, open spaces

and natural resource areas; describes and illustrates the City's expectations for high-quality architectural and community design; serves as the guide for coordinating individual developments and public realm improvements into a cohesive whole; and provides implementation strategies for land use regulation and infrastructure funding.

The Wilsonville Planning Commission is scheduled to hold a public hearing on the Plan on Wednesday, March 8, at 6 pm. At the meeting the Planning Commission

is considering whether to recommend adoption of the plan with or without changes to the City Council.

The Wilsonville City Council is then scheduled to hold a public hearing on the plan on Monday, April 17, 7 pm, after which the City Council may approve the plan. Both hearings take place at Wilsonville City Hall, 29799 SW Town Center Loop East.



An artistic rendering of the proposed Frog Pond West neighborhood.

The draft plan and other relevant information, including the staff report, findings, and recommendations, are available for viewing seven days prior to each public hearing at Wilsonville City Hall, at the Wilsonville Public Library and on the City's website at www.ci.wilsonville.or.us/FrogPond.

Written comments regarding the proposal are also welcome prior to the public hearings and will be submitted into the public hearing record. Direct comments or testimony to:

Chris Neamtzu, AICP, Planning Division,  
29799 SW Town Center Loop East,  
Wilsonville, Oregon, 97070  
neamtzu@ci.wilsonville.or.us,  
503-682-4960

### Grace Chapel Rummage Sale to Benefit Community Groups

The annual Community Rummage Sale sponsored by Grace Chapel is set for Friday, March 10, 5 -9 pm, and Saturday, March 11, 9 am–noon, when all merchandise is half off. Proceeds from the sale are provided to community organizations that in the past have included school-support groups, Random Kindness, Wilsonville Community Sharing and others.

**Community Rummage Sale**  
 • Fri, March 10, 5–9 pm  
 • Sat, March 11, 9 am–noon  
 Grace Chapel, 9600 SW Boeckman Rd

Donations of clothing, merchandise and appliances are welcome, except for mattresses, car seats and ripped or broken furniture and maybe dropped-off at

Grace Chapel, 9600 SW Boeckman Road on Sunday, March 5, Monday, March 6 or Tuesday, March 7.



For more information, visit [www.WilsonvilleRummageSale.com](http://www.WilsonvilleRummageSale.com) email [wilsonvillecrs@gmail.com](mailto:wilsonvillecrs@gmail.com).

### Refuse to Be a Victim Class to Held on Saturday, March 25

The Clackamas County Sheriff's Office Public Safety Training Center is holding a "Refuse To Be A Victim" class on Saturday, March 25, 2–4:30 pm, at the 12700 SE 82nd Ave in Clackamas.

The single most important step toward ensuring your own safety is having a personal safety strategy in place before you need it. "Refuse To Be A Victim" is designed to teach that mindset. It is NOT a firearms class. The seminar will help you improve your personal safety strategies and provide you with valuable information you can apply to every area of your life.



Key topics covered during the session include:

- Mental preparedness
- How to make your home more secure
- Improving your physical security in various environments
- Safety precautions when driving
- Safeguarding yourself and items when traveling
- What to consider when choosing physical self-defense training
- Personal protection devices
- Workplace safety
- Safety for college students

The cost for the class is \$35 and advanced registration is required. To sign up, call 503-794-8023.

### Free Filing Software for Taxpayers Earning \$64K or Less

The Internal Revenue Service (IRS) is partnering with the Free File Alliance, a group of brand-name tax software providers to offer "Free File" which makes available free federal tax return products to taxpayers whose adjusted gross income was \$64,000 or less during 2016. For taxpayers who earned more than \$64,000, the IRS is offering Free File fillable forms, the electronic version of IRS paper forms. To access the free software visit [www.IRS.gov/FreeFile](http://www.IRS.gov/FreeFile).

Some of the software providers are offering both free federal and state tax return preparation for states with income tax requirements, while others charge a fee for offering state tax return preparation. **The tax filing deadline is Monday, April 18, 2017.**

### Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and are replayed periodically. Meetings are also available to stream live and by video-on-demand at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.

|   |  |  |  |   |
|---|--|--|--|---|
|                       |                                       |                                       |                                    |    |
| <b>Tim Knapp</b><br>Mayor<br><a href="mailto:knapp@ci.wilsonville.or.us">knapp@ci.wilsonville.or.us</a> | <b>Scott Starr</b><br>City Council President<br><a href="mailto:scottstarr97070@gmail.com">scottstarr97070@gmail.com</a> | <b>Susie Stevens</b><br>City Councilor<br><a href="mailto:stevens@ci.wilsonville.or.us">stevens@ci.wilsonville.or.us</a> | <b>Charlotte Lehan</b><br>City Councilor<br><a href="mailto:lehan@ci.wilsonville.or.us">lehan@ci.wilsonville.or.us</a> | <b>Kristin Akervall</b><br>City Councilor<br><a href="mailto:akervall@ci.wilsonville.or.us">akervall@ci.wilsonville.or.us</a> |

### WilsonvilleSTAGE Presents THE LARAMIE PROJECT

WilsonvilleSTAGE is presenting THE LARAMIE PROJECT by Moises Kaufman and the Tectonic Theater Directed by Terry D. Kester.

The play is being performed on March 2, 3, and 4 at 7:30 pm on the Wilsonville Campus of Clackamas Community College, 29353 SW Town Loop Center East.

Tickets are \$15 and \$12 for seniors and students. For information and to purchase tickets visit [www.WilsonvilleStage.org](http://www.WilsonvilleStage.org), the local Fred Meyer store or at the Wilsonville branch of Clackamas Community College



The play is based on hundreds of interviews conducted in Laramie, Wyoming after the 1998 murder of University of Wyoming gay student Matthew Shepard. The play includes how 60 different characters reacted to the hate crime that spurred many states to enact new hate crime laws.

Throughout 2017 the Wilsonville branch of Clackamas Community College is hosting all of WilsonvilleSTAGE's performances and providing the local non-profit theater company low-cost space for performances, auditions and rehearsals.



### Volunteer Pollinator Planting Event on Saturday, March 18

On Saturday, March 18, 9 am–1 pm, join Friends of Trees and the City of Wilsonville in planting native trees and shrubs to create more pollinator habitat at the Willamette River Water Treatment Plant, 10350 Arrowhead Creek Lane in Wilsonville. Please arrive around 8:45 to be registered and assigned to a crew. Breakfast snacks and hot chocolate/coffee, as well as gloves, tools and guidance are provided. Dress appropriately for weather conditions and all ages are welcome. Additional volunteer pollinator planting events are planned in Wilsonville on Saturday, April 1 and April 15, 9 am–1 pm. Contact Friends of Trees at 503-595-0213 or visit [www.FriendsofTrees.org](http://www.FriendsofTrees.org) for more information about volunteer opportunities in Wilsonville and the region.

**Pollinator Planting Event**  
 • Sat, March 18, 9 am–1 pm  
 Water Treatment Plant  
 10350 Arrowhead Creek Ln.

## City Calendar

| MARCH    |   |
|----------|---|
| 6 Mon    | • City Council, City Hall 5 pm–Work Session 7 pm–Meeting  |
| 7 Tue    | • Municipal/Traffic Court, 5 pm, City Hall  |
| 8 Wed    | • Wilsonville Community Seniors Inc. Advisory Board, 1–3 pm, Wilsonville Community Center<br>• Planning Commission, 6 pm, City Hall |
| 9 Thurs  | • Park and Rec Advisory Board Meeting, 4:30 pm, Park and Rec Admin Offices  |
| 13 Mon   | • DRB Panel A, 6:30 pm, City Hall   |
| 14 Tues  | • Wilsonville Town Center Master Plan Task Force Meeting, 6–9 pm, City Hall   |
| 15 Wed   | • Budget Committee Meeting, 6–9 pm, City Hall   |
| 16 Thurs | • Citizens Academy, 6–9 pm, City Hall   |
| 18 Sat   | • Volunteer Pollinator Planting Event, 9 am–1 pm, Willamette River Water Treatment Plant, 10350 SW Arrowhead Creek Lane.            |
| 20 Mon   | • City Council, City Hall 5 pm–Work Session, 7 pm–Meeting   |
| 21 Tue   | • Tourism Promotion Committee Meeting, 10 am–12 pm, City Hall<br>• Municipal/Traffic Court, 5 pm, City Hall                         |
| 22 Wed   | • Tourism Promotion Committee Meeting, 6 pm, City Hall<br>• Library Board, 6:30 pm Wilsonville Public Library                       |
| 27 Mon   | • DRB Panel B, 6:30 pm, City Hall   |
| APRIL    |   |
| 1 Sat    | • Volunteer Pollinator Planting Event, 9 am–1 pm, Memorial Park–Murase Plaza, 8300 SW Memorial Drive.                               |
| 3 Mon    | • City Council, City Hall, 5 pm–Work Session, 7 pm–Meeting, City Hall   |
| 4 Tue    | • Municipal/Traffic Court, 5 pm, City Hall  |
| 10 Mon   | • DRB Panel A, 6:30 pm, City Hall   |
| 11 Tue   | • Tourism Promotion Committee Meeting, 10 am–12 pm, City Hall   |
| 12 Wed   | • Wilsonville Community Seniors Inc. Advisory Board, 1–3 pm, Wilsonville Community Center<br>• Planning Commission 6 pm, City Hall  |
| 13 Thurs | • Parks and Recreation Advisory Board Meeting, 4:30 pm, Parks and Rec Administration Building                                       |
| 15 Sat   | • Volunteer Pollinator Planting Event, 9 am–1 pm, Wilsonville High School, 6800 SW Wilsonville Road                                 |
| 17 Mon   | • City Council, City Hall, 5 pm–Work Session, 7 pm–Meeting  |
| 18 Tue   | • Tourism Promotion Committee Meeting, 10 am–12 pm, City Hall<br>• Municipal/Traffic Court, 5 pm, City Hall                         |
| 20 Thurs | • Citizens Academy, 6–9 pm, CH2M Hill and Veolia  |
| 24 Mon   | • DRB Panel B, 6:30 pm, City Hall   |
| 26 Wed   | • Library Board, 6:30 pm, Wilsonville Public Library  |
| 27 Thurs | • Tourism Promotion Committee Meeting, 10 am–12 pm, City Hall   |

All dates and times are tentative; check the City's online calendar for schedule changes at [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**City Manager**  
 Bryan Cosgrove  
 503-570-1504  
[cosgrove@ci.wilsonville.or.us](mailto:cosgrove@ci.wilsonville.or.us)

**Wilsonville City Hall**  
 29799 SW Town Center Loop E  
 Wilsonville, OR 97070  
 Phone: 503-682-1011 Fax: 503-682-1015  
 Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
 E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)  
**Police Non-Emergency Dispatch:** 503-655-8211





## Council Honors High School Boys Basketball Team for Back-to-Back Championships



Members of the State Champion 2016-17 Wilsonville High School Boys Varsity Basketball team and Wilsonville City Council pose for photo after the Mayor declared the week of April 17-23 as "Wilsonville Wildcat Week" to honor the team.

In April the City Council honored the Wilsonville High School Varsity Boys Basketball Team for winning the 5A High School Boys Basketball State Championship in March. The Mayor and Council acknowledged the team's success by declaring the week of April 17-23 as "Wilsonville Wildcats Week."

In issuing the proclamation Mayor Tim Knapp declared, "The success of the Wilsonville Wildcats Boys Varsity Basketball team is indicative of the High School's overall commitment to athletic and academic excellence and sportsmanship."

The City Council also commended the team for their academic accomplishments for placing fourth in the 5A Boys Basketball Academic All-State Awards with an average GPA of 3.56.

In addition to acknowledged the team for winning the state championship, the Council also recognized the team for winning the Northwest Oregon Confer-

ence with an undefeated conference record of 14-0, and an overall season record of 27-1.

### 2016-17 Wilsonville High School Boys Varsity Basketball Team

**Players:** AJ Burkhead, Draco Bynum, Caleb Larsen, Ryan Napoli, Nathan Overholt, Dakota Reber, Gabe Reichle, Zach Reichle, Jack Roche, Zac Rossetti, Keegan Shivers, Harrison Steiger, Sam Westing, Kyle Willett and Zac Woodworth

**Head Coach:** Chris Roche

**Assistant Coaches:** Davis Martin, John Roche, Michael MacKelvie, Mike Weston and Tom Haaga

**Manager:** Ashley Mory and Makenzie Clancy

**Athletic Director:** Dennis Burke

**Principal:** Dan Schumaker

### Mayor's Message

## Thanks for Volunteering and Giving Back to our Community

The Wilsonville City Council recognizes and appreciates the hundreds of community members of all ages and backgrounds, including residents and employees of local businesses, who donate their time to help improve our community.

There are many examples of how volunteers make Wilsonville a better community. For example, in March and April hundreds of residents pitched in to plant trees and shrubs at the Water Treatment Plant and at Memorial Park in partnership with Friends of Trees and the Bee Stewards program. The City also hosted in April for boards and commission and other volunteers an appreciation event.

Every month more than two dozen residents actively participate in the City's Citizens Academy. This six-month-long program provides participants a foundation in local-government civics and is preparing community members to serve in positions of leadership on volunteer City boards and commissions or other local organizations.

Every year volunteers donate their time to the Library in order to perform a wide variety of tasks that help improve the quality and expand the range of services. Volunteers help at the Community Center to serve meals, deliver meals to homebound seniors, help prepare income taxes and assist seniors getting to medical appointments.

The upcoming May 13 WERK Day—Wilsonville Environmental Resource Keepers—attracts hundreds of community members every year for the annual

*Throughout our community, day in and day out, volunteers contribute in countless ways to help Wilsonville be the place we want it to be.*

park clean-up event to spread bark, rake-up debris and remove invasive plants. And Wilsonville Rotary volunteers with business support to donate a free warm-up breakfast.

During the first week-end of June, the 18th annual Wilsonville Festival of Arts is being held in Town Center Park thanks to substantial contributions from a team of hard-working volunteers and art enthusiasts. And really, this is true with all of our major community events — volunteers are the underpinning of success.

Throughout our community, day in and day out, volunteers contribute in countless ways to help Wilsonville be the place we want it to be, and carry forward a vision of a positive future. Please join me in showing our appreciation for all of our volunteers who so freely give their time to strengthen and improve our community!

*Tim Knapp, Mayor*

City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

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Permit No. 104

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## Time to Perk Up the Parks on WERK Day on Saturday, May 13

The City's 17th annual community park improvement and clean-up event, Wilsonville Environmental Resources Keepers (WERK) Day, is on Saturday, May 13, 8 am–12 pm.

Registration and a free omelet breakfast sponsored by the Wilsonville Rotary Club runs 8–9 am in the

Wilsonville Community Center, 7965 SW Wilsonville Road.

During the event participants

primarily focus on improving and maintaining Memorial Park in Wilsonville. WERK day projects include painting the soccer goals, placing bark chips on trails, rebuilding areas of trails that have eroded, removing ivy, holly, blackberry, and other invasive species, sanding and painting picnic tables and a variety of other assignments.

Participants should bring work gloves, water bottles, sun screen and be dressed appropriately for weeding, raking and shoveling. Participants of all ages are welcome and encouraged to attend, but children under the age of 14 must be accompanied by an adult.

Participants in the rain-or-shine event receive coupons from Wilsonville Lanes and Family Fun Center and a free WERK Day t-shirt while supplies last.



On WERK Day a wide variety of groups and individuals come together to spruce up Wilsonville's parks.

Participants and volunteer groups are encouraged to register in advance on the City's Parks and Recreation website at [www.WilsonvilleParksandRec.com/WERK](http://www.WilsonvilleParksandRec.com/WERK) day where they can download and sign the WERK Day volunteer liability release form.

To sign up a group, please contact Brian Stevenson, Recreation Program Manager, at 503-570-1523, [stevenson@ci.wilsonville.or.us](mailto:stevenson@ci.wilsonville.or.us).

## Al's Garden Center Expands in Wilsonville on Parkway Avenue



In early April Al's Garden Center held a ribbon-cutting ceremony with members of the City Council and the Wilsonville Area Chamber of Commerce at the location of the former Wilsonville Garden Center.



## May's Question of the Month

Every month throughout the 18-month-long planning initiative the Wilsonville Town Center Plan is asking community members a Question of the Month. The questions are designed to gather feedback on a wide-range of issues related to developing the plan that captures a community-driven vision and strategies to guide the development of Wilsonville's Town Center.

### Here is May's Question of the Month:

**What type of stores do you feel would be good additions to the Town Center? (Select all that apply.)**

- a) Small, local, independent shops
- b) Large format national retail chains with a variety of merchandise
- c) Medium or small-size specialty stores focused on specific merchandise
- d) Flexible sized spaces with shared amenities to attract emerging and growing businesses
- e) Marketplace with multiple vendors / shops within one building

To share your feedback and participate in the Question of the Month stop by City Hall, the Library or the Community Center or visit the project's website below. To sign up for project updates and to get more information visit [www.WilsonvilleTownCenter.com](http://www.WilsonvilleTownCenter.com) or contact Miranda Bateschell, Long-Range Planning Manager, at [bateschell@ci.wilsonville.or.us](mailto:bateschell@ci.wilsonville.or.us) or 503-570-1581.

## City Partners with Clackamas Community College to Host Workforce Solutions Roundtable for Local Employers

The City of Wilsonville and Clackamas Community College are joining forces to host a breakfast session on May 10 with local employers to address a critical topic for many businesses—workforce. From recruiting to leadership coaching to customized

**Business & Workforce Solutions Roundtable**  
 • Wed, May 10, 7:30–9 am,  
 Clackamas Comm. College–Wilsonville  
 29353 SW Town Center Loop E, RSVP at  
[wilsonvilleworkforce.eventbrite.com](http://wilsonvilleworkforce.eventbrite.com)

employee training, this roundtable event is aimed at connecting

businesses with the workforce resources they need to grow and thrive. The event is being held at Clackamas Community College's Wilsonville campus, 29353 SW Town Center Loop East in Room W108B.

"When meeting with local employers and learning about their key challenges to success, workforce issues are consistently on the top of the list. Many Wilsonville employers don't realize the robust workforce resources that exist in their backyard, offered by Clackamas Community College and others. We are very excited to help remove barriers to growth for local employers through some of these resources," said Jordan Vance, Economic Development Manager for the City of Wilsonville.

With 50 years of quality job training experience, Clackamas Community College works with employers to find creative solutions to their business needs and some coursework even qualifies for college credit. From training and professional development opportunities for staff; to finding a skilled workforce that fits business demand; or helping create strategies for growth; Clackamas Community College is a trusted partner that can help find a custom solution for local employers.

"Clackamas Community College's Connections with Business and Industry (CBI) team makes

it easy to get expert assistance to start or expand your business, or to help you navigate challenges such as downsizing or dealing with market changes. CBI is a one-stop business support network, and we can connect you with the tools, staffing, training and resources you need to prosper. We are here to help you grow and succeed," said Lisa Davidson, Executive Director of Connections with Business and Industry.

To learn more about Clackamas Community College's programs, visit: [www.clackamas.edu/cbi](http://www.clackamas.edu/cbi).

To RSVP for the Business & Workforce Solutions Roundtable on May 10, 7:30 am–9 am, please register at [wilsonvilleworkforce.eventbrite.com](http://wilsonvilleworkforce.eventbrite.com) with the name(s) of those that will be attending or call 503-594-3201 to register over the phone. Free breakfast is provided.



The upcoming business roundtable event is focused on helping local employers learn about and access the resources provided by the City and Clackamas Community College.



8200 SW Wilsonville Road  
 503-682-2744  
[www.WilsonvilleLibrary.org](http://www.WilsonvilleLibrary.org)

## "Finding Your Ancestral Gold" Genealogy Workshop

Everyone has an amazing family history—but most have no clue how truly remarkable it is. This workshop, led by a genealogist, inspires you to embark on a quest for your ancestral heritage and guides you through the steps to get you started.



Saturday, May 6, 1–3 pm, in the Oak Room. Free.  
[www.WilsonvilleLibrary.org/GC](http://www.WilsonvilleLibrary.org/GC)

## Garden Club Holds Spring Plant Sale

The Wilsonville Garden Club has a large selection of sun and shade hanging baskets, blooming annuals and perennials, plus cutting and divisions grown by the Club's members. All profits are used to support local horticulture education and scholarships.



Saturday, May 13, 9 am–2 pm, in the Oak Room.

## Teen Event: Karaoke & Quiz Night

Sing-a-long or play along . . . your choice. Enjoy a lounge-style atmosphere with some friends and a drink in your hand while we sing a few of our favorite sing-a-long songs, and do team quiz pop trivia games.



For students in grades 6–12.  
 Friday, May 19, 6:30–8:30 pm. Free.

## Curiosity Cafe: Hubble Telescope

Former Hubble design team engineer Tom Stczynski talks about the design, build, and testing of the Hubble Space Telescope with a focus on the challenges of servicing in space, particularly the first servicing mission that addressed the problem with the optics.



Saturday, May 20, 1–3 pm, in the Oak Room. Free.

## Summer Reading Program Coming in June

Get ready for a "constructive" summer (in a variety of meanings) with "Build a Better World". All ages can participate.

Sign up starting Monday, June 12.

[www.WilsonvilleLibrary.org/SRP](http://www.WilsonvilleLibrary.org/SRP)



## Upcoming Classes and Events

\*Library closed Monday, May 29\*

**Local Artist of the Month:**  
 Priya Tripathi

**First Friday Film**

Friday, May 5, 6–8 pm  
[www.WilsonvilleLibrary.org/films](http://www.WilsonvilleLibrary.org/films)

**Book Notes Concert**

*Indalo Wind*

Father-son duo plays folk, classical, & jazz  
 Saturday, May 13, 2–3 pm  
[www.WilsonvilleLibrary.org/concerts](http://www.WilsonvilleLibrary.org/concerts)

**K-2 Book Adventures**

Thursday, April 13, 4–4:45 pm  
[www.WilsonvilleLibrary.org/k2](http://www.WilsonvilleLibrary.org/k2)

**Book Club**

**Author Comparison: Stephen Ambrose**  
 Thursday, May 11, 6–8 pm  
[www.WilsonvilleLibrary.org/bookclub](http://www.WilsonvilleLibrary.org/bookclub)

**Genealogy Club**

Monday, May 15, 1–2:30 pm  
[www.WilsonvilleLibrary.org/gc](http://www.WilsonvilleLibrary.org/gc)

**Great Books Discussion Group**

*"Medea" by Euripides*  
 Tuesday, May 16, 6–8 pm  
[www.WilsonvilleLibrary.org/greatbooks](http://www.WilsonvilleLibrary.org/greatbooks)

**History Pub at Wilsonville McMenamins**

Screening of OPB's documentary "Vanport"  
 Tuesday, May 30, 6:30–8 pm  
[www.WilsonvilleLibrary.org/hp](http://www.WilsonvilleLibrary.org/hp)

## Planning Commission Holds Hearing on Transit Master Plan

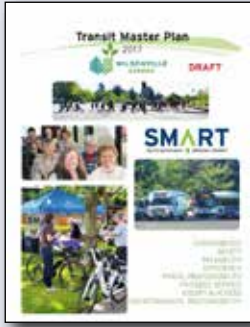
The City of Wilsonville's public transit agency, South Metro Area Regional Transit (SMART), is presenting the draft Transit Master Plan at public hearing before the Planning Commission on Wednesday, May 10, 6 pm at City Hall.

A revised Draft 2017 Transit Master Plan (TMP) is now available for review and comment at [www.plansmartwilsonville.com/tmpcomment](http://www.plansmartwilsonville.com/tmpcomment).

Based on recent feedback, the draft plan has been reorganized and updated with new information.

SMART welcomes comments on the revised draft plan and has extended the public comment period until May 24, 2017. There are many ways for the community to comment on the plan:

- **Online** at [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)
- **Email:** [marston@ridessmart.com](mailto:marston@ridessmart.com)
- **Phone:** 503-570-0332
- **Mail:** SMART TMP  
ATTN: Michelle Marston,  
29799 SW Town Center  
Loop East, Wilsonville,  
OR 97070



Copies of the Plan are available to review at:

- Wilsonville Public Library Reference Desk, 8200 SW Wilsonville Road
- Wilsonville City Hall Reception, 29799 Town Center Loop East
- SMART Offices Reception, 28879 SW Boberg Road

In addition to the May 10 Planning Commission meeting, the plan is also tentatively scheduled for a public hearing at the Wilsonville City Council Meeting on Monday, June 5, 7 pm, at City Hall.



## Tips from TVF&R to Limit Hazards and Prevent Fall Injuries

As people grow older, the consequences of a fall grow more serious.

Unintentional falls are the leading cause of injury deaths for adults ages 65 and older, according to the Centers for Disease Control and Prevention. They are the most common cause of traumatic brain injuries and account for more than 95 percent of hip fractures. In addition, fractures from falls are a leading cause of disability and often result in entering a nursing home.

Tualatin Valley Fire & Rescue crews responded to 4,863 reports of falls within the district's service area in 2016.

"Ground-level falls are one of the most concerning type of medical calls our firefighter paramedics respond to because we know many of our elders struggle to recover from them," states Cassandra Ulven with TVF&R. "Most falls happen in the home and can be avoided.

"Factors such as poor lighting, narrow stairs, and slippery surfaces can increase the chances of a fall. In addition, problems with glare, depth perception, tiredness or dizziness from prescription medications contribute to falls."

**Research conducted by Yale University showed that older adults are also most at risk of falling when they fail to use handrails on stairs, reach for objects overhead, and climb on chairs or ladders.**

objects overhead, and climb on chairs or ladders.

Tualatin Valley Fire & Rescue reminds seniors and children of older adults to take the following precautions to help prevent a fall injury:

### Take Care of Yourself

Get regular medical check-ups and talk to your doctor to ensure appropriate levels and types of medication. Have your hearing and vision checked. Exercise regularly, and eat nutritious meals.

### Remove Existing Hazards

Fasten throw rugs to the floor with double-backed

tape. Keep cords out of pathways, and clean up spills as soon as they happen. Also, arrange furniture to minimize obstructions, and clear clutter from your living areas.

### Add Protection

Wear shoes with non-skid soles. Turn on lights when walking through dark rooms or hallways. Use nightlights in all rooms. Install grab bars in the bathtub, shower, and toilet areas, and add handrails on stairwells. Place phones in multiple rooms or keep a cellphone with you in case you have to call for emergency assistance.

For more life safety information, visit [www.tvfr.com](http://www.tvfr.com).

## Healthy Bones and Balance Class Begins Week of May 15

In May, the City of Wilsonville's Parks and Recreation department is offering a Healthy Bones and Balance Class to help active adults maintain and improve their strength and balance in part to help participants prevent fall accidents and other injuries.

The City's fitness instructor Brad Moore explains, "In this class participants learn exercises to build strength and improve mobility that are both important keys to prevent falling and other accidents."

The course is held at the Wilsonville Community Center, 7965 SW Wilsonville Road, on Mondays, Wednesdays and Fridays, 8:30-9:20 am and the standard charge for the seven-week class is \$59. The instructor also uses evidence-based exercise strategies with the goal of improving bone density and decreasing the likelihood of a fall or an injury.

For more information contact Fitness Specialist Brad Moore, 503-570-1522; [moore@ci.wilsonville.or.us](mailto:moore@ci.wilsonville.or.us) or register at [WilsonvilleParksandRec.com](http://WilsonvilleParksandRec.com).



## SMART

SOUTH METRO AREA REGIONAL TRANSIT

29799 SW Town Center Loop E. (mail)  
28879 SW Boberg Road (office)  
503-682-7790  
[smart@ridessmart.com](mailto:smart@ridessmart.com)  
[www.RideSMART.com](http://www.RideSMART.com)

## Market Shuttle Returns May 25

Wilsonville Farmers Market shuttle now serves Parkway Avenue near Portera and Jory Trail neighborhoods. You can also catch the shuttle at Town Center Park, SMART Central and at Barber/Yosemite St. in Villebois each Thursday.

| Town Cntr Park | SMART Central | Barber@ Yosemite | Farmers Market |
|----------------|---------------|------------------|----------------|
| 3:45 pm        | 3:55 pm       | 3:56 pm          | 4:00 pm        |
| 4:25           | 4:35          | 4:36             | 4:40           |
| 5:05           | 5:15          | 5:16             | 5:20           |
| 5:45           | 5:55          | 5:56             | 6:00           |
| 6:25           | 6:35          | 6:36             | 6:40           |
| 7:05           | 7:15          | 7:16             | 7:20           |

| Farmers Market | Barber@ Yosemite | SMART Central | Town Cntr Park |
|----------------|------------------|---------------|----------------|
| 4:00 pm        | 4:02 pm          | 4:05 pm       | 4:15 pm        |
| 4:40           | 4:42             | 4:45          | 4:55           |
| 5:20           | 5:22             | 5:25          | 5:35           |
| 6:00           | 6:02             | 6:05          | 6:15           |
| 6:40           | 6:42             | 6:45          | 6:55           |
| 7:20           | 7:22             | 7:25          | 7:35           |
| 8:05           | 8:07             | 8:10          | 8:20           |



## Get Walking on Wednesdays Throughout May

Join Walk Smart at noon each Wednesday in May at various locations for walks that begin promptly at 12:10 pm.

A full schedule of walks is available at [www.ridessmart.com/walksmart](http://www.ridessmart.com/walksmart). You can receive notifications

sent to your email by registering your email at [www.ridessmart.com/enotify](http://www.ridessmart.com/enotify).

Meet new friends and do some lunchtime networking. Remember to bring your walking shoes to work with you! Check out our May partners listed below for rewards. See you there!



| Date   | Meeting Location         |
|--------|--------------------------|
| May 3  | Community Center, front  |
| May 10 | Convergys, front         |
| May 17 | Mentor Campus, rear      |
| May 24 | Rockwell Collins, front  |
| May 31 | Charbonneau Country Club |

## Join the 2017 Bike More Challenge

The 2017 Bike More Challenge is kicking off on May 1! This free Challenge is designed to engage businesses across Oregon in a friendly competition to see which business can get the most people to bike during the month of May (which is National Bike Month).



Each year over 10,000 individuals participate in the Challenge in which workplace teams log their cycling trips (both work and non-work related) between May 1-31. A points model is used to determine winners across various business size-categories. Points are awarded for logged rides, as well as for the recruitment of new riders. Prizes are earned not only for challenge winners, but also for reaching various benchmarks along the way! A celebration is held at the end of the challenge to applaud everyone's participation and commitment throughout the month. The Bike More Challenge is fun, free and a great way to get people riding!

If your business is not yet registered, you can register at [www.lovetoride.net](http://www.lovetoride.net) as an individual and register your business at the same time!

Happy National Biking Month!

### Our History: Boat Race Draws Big Crowd in May 1913



This Boones Ferry Historical Society photo taken on May 25, 1913, shows people lining the north bank shore of the Willamette River in Wilsonville to watch a boat race with the houses of Jake Peters and the Tauchman’s in the background.

### Sister City Seeks Host Families for Fall Visit of Japanese Students

This fall, the Wilsonville Sister City Association is hosting a delegation of 20 students and two chaperones from Kitakata, Japan, beginning on Saturday, Oct. 28, through Tuesday, Nov. 7. The students, who range 13–15 in age, visit schools, do local sightseeing, participate in group activities and take part in Halloween events that are uniquely American and often a highlight for the Japanese visitors.

Wilsonville Sister City Association coordinates the visit and is looking for families to host members of the delegation. Host families don’t need to speak Japanese or have a high school student at home. If people can’t host, but are interested in becoming more involved, organizations or individuals may also provide assistance by helping to plan

daily excursions, sponsoring a group activity, serving as volunteer tour guides for a day, helping with the welcome or farewell party, or by donating souvenirs.

For more information, contact Bev Schalk, Wilsonville Sister City Association President, at Wilsonville.SisterCity@gmail.com.



### CREST Offers Summer Camps

#### CREST Camps and Programs

#### EnviroCamp - for students entering grades 1-5 in the fall

- Session 1: June 26–30, CREST Wilsonville
- Session 2: July 31–Aug. 4, Mary S. Young Park, West Linn
- Session 2: Aug. 7–11, CREST, Wilsonville

#### Middle School Adventure Camp - for students entering grades 6-8 in the fall

- Session 1: July 17–21, location varies
- Session 2: July 24–28, location varies
- Session 3: Aug. 14–18, location varies



#### Teen Leadership Training-for students entering grades 8–10 in the fall

- June 28-29, CREST, Wilsonville

#### Farm Internship-for students entering grades 8–10 in the fall

- Session 1: June 26–July 13, CREST Farm, Wilsonville
  - Session 2: July 17–Aug. 3, CREST Farm, Wilsonville
  - Session 3: Aug. 7-24, CREST Farm, Wilsonville
- Information and registration for the West Linn - Wilsonville School District’s CREST Summer Camps and Programs are available at [www2.crest.wlww.k12.or.us/register/](http://www2.crest.wlww.k12.or.us/register/)



29600 SW Park Place  
Parks Programs: 503-783-7529  
Community Center: 503-682-3727  
[www.WilsonvilleParksandRec.com](http://www.WilsonvilleParksandRec.com)

### Spring and Summer Registration Now Open!

Registration is now open for all spring and summer Parks and Recreation programs. This summer there are a number of new program offerings, including a multi-sport camp that includes dodgeball, disc golf and ultimate frisbee, a Stealth, Parkour and Circus Skills Movement Camp, and a youth “Yoga Calm” class is being offered on Saturday mornings. All programs can be viewed and registered for online.



### Partner Yoga Workshop

The power of relationship is profoundly helpful in Yoga practice. This workshop allows you to practice a variety of partner yoga poses to build strength and improve flexibility while connecting with your friend or loved one. Beginners welcome. The workshop takes place on Saturday, June 17 from 2–3:30 pm at the Tauchman House. Cost is \$15 per person.

### Mother’s Day Brunch

On Monday, May 15, the Community Center is hosting a special Mother’s Day brunch to celebrate (and commemorate) all moms. Lunch is being served at noon and all moms and grandmothers are getting a special treat.

### Medicare 101: Extra Help

United Healthcare presents Medicare 101 on the second Tuesday of each month, 10:30 am–11:30 am at the Community Center. The sessions will cover the basics of Medicare, including what to expect when turning 65 or qualifying on the basis of disability, Medicare Advantage/Medicare Supplement Overview and Extra Help with prescription drugs and other available programs. This is a drop-in program and so no appointments are necessary.

# W. E. R. K. DAY

SAT. MAY 13  
8:00 AM - NOON

MEET AT COMMUNITY CENTER



WERK Day is an annual event to assist with a variety of park projects.

Each year the projects differ but are typically focused within Memorial Park.

[WilsonvilleParksandRec.com/WERKDay](http://WilsonvilleParksandRec.com/WERKDay)

### Camps, Camps, Camps!

Wilsonville Parks and Recreation offers a variety of summer camps for children of all ages. In June, the program offerings get started with a number of camp options. Full list of camp offerings and online registration available at: [WilsonvilleParksandRec.com/SummerCamps](http://WilsonvilleParksandRec.com/SummerCamps)

#### Manners Matter

Ages 6–10  
6/3 and 6/10  
9–10:30 am  
Tauchman House  
\$60

#### Coach Nics Hoop Camp

Grades 1–3  
6/26–6/29  
9 am–noon  
Memorial Park  
\$69

#### Babysitters Bootcamp

Ages 11–15  
6/26 and 6/27  
9 am–2:30 pm  
Tauchman House  
\$225

#### Intro to Survival Camp

Ages 6–9  
6/26–6/30  
9 am–1 pm  
Mary S Young Park  
\$215

#### Skyhawks Mini Hawk Camp

Ages 4–7  
6/26–6/30  
9 am–noon  
Memorial Park  
\$109

#### Skyhawks Multi - Sport Camp

Ages 6–12  
6/26–6/30  
9 am–3 pm  
Memorial Park  
\$149

# Happy National Biking Month Wilsonville!

## SMART Offers Free Bike Checks

SMART is offering free bike checks to local residents at the Wilsonville Festival of Arts on Saturday, June 3, 10 am–4 pm, and also at the Fun In the Park event on Saturday, August 5, 10 am–4 pm, in Town Center Park, 29600 SW Park Place. For more information contact Michelle Marston, Transit Outreach Program Coordinator, marston@ridessmart.com; 503-570-0332.



## French Prairie Bridge Plans Safer Way of Crossing River

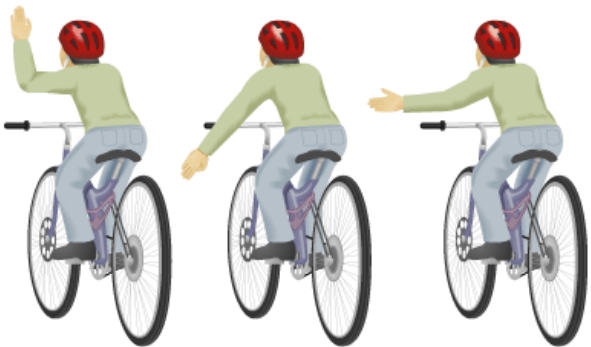
The City of Wilsonville is leading a project to plan and develop preliminary designs for the French Prairie Bridge, which is a proposed bicycle/pedestrian/emergency-access crossing of the Willamette River. The project aims to address the following questions:

- Where are the preferred landing points for the bridge?
- What is the preferred bridge type?
- What is the estimated cost of the preferred bridge and how might its construction be funded?

The bridge location under consideration is at the approximate site of the historic Boones Ferry, located between the I-5 Boone Bridge and the existing railroad bridge to the west.

The bridge will connect regional trail systems and recreation areas, such as the Ice Age Tonquin Trail, Willamette Valley Scenic Bikeway, Graham Oaks Nature Park, and Champoeg State Park through Wilsonville. In addition, the bridge will be built to seismic standards and will provide redundant emergency vehicle access in the event of a major earthquake, as well as provide a secondary emergency response route in instances where there is an accident or other traffic disruption on I-5 can impacting emergency response time and secondary responders working to clear the road. To learn more about the project and to provide feedback visit [www.frenchprairiebridgeproject.org](http://www.frenchprairiebridgeproject.org).

## Basic Hand Signals for Bicyclist



Right Turn

Stop

Left Turn

## Ride Report Wilsonville

COUNT EVERY RIDE, AUTOMATICALLY.

Track your miles, map your routes, and earn ride streaks for your daily errands and commutes.

The best part? You don't have to do a thing. Just hop on your bike and Ride Report will automatically log your ride and take care of the rest.

Get the Free App at [RideReportApp.com](http://RideReportApp.com)

## CHIEF'S CORNER – HAPPY BIKE MONTH

Biking is a fun and healthy activity and Wilsonville is fortunate to have so many great bike paths, lanes and routes to get around town. With May being National Bike Month, this is the perfect time to share some bike-riding safety tips to ensure you and your family have safe and enjoyable bike rides throughout the spring and summer.



Police Chief Adam Phillips

For your bike-riding safety and enjoyment remember to:

- Follow the **rules of the road**
- **Ride with traffic** and obey the same laws as motorists
- Use the **furthestmost right lane** that heads in the direction that you are traveling
- **Obey all traffic control devices**, such as stop signs, lights and lane markings
- Always look back and **use hand and arm signals** to indicate your intention to stop, turn or merge
- Be visible, **predictable**, alert and anticipate conflicts
- Ride where **drivers can see you**
- Wear **brightly colored clothing** at all times
- At night, use a **white front light** and **red rear light or reflector** and wear **reflective tape or clothing**
- Ride in a **straight line** and don't swerve between parked cars
- Make **eye contact** with motorists so they see you
- Be **aware of traffic** around you
- Be prepared to take evasive action
- **Practice braking and turning techniques** and do not ride on the sidewalk

### More Bike Safety Tips

- Be **extra alert at intersections**
- Make sure that the **helmet fits** on top of your head, not tipped back or forward
- If you crash or have any impact that affects your helmet, visible or not, **replace it**
- Riders younger than 16 must wear a helmet and everyone should wear a helmet regardless of their age

As the weather improves I am excited to resume the Police Department's bike patrol. Having officers on bikes throughout the spring and summer allows our officers improved opportunities to engage with community members and to patrol areas that are otherwise less accessible.

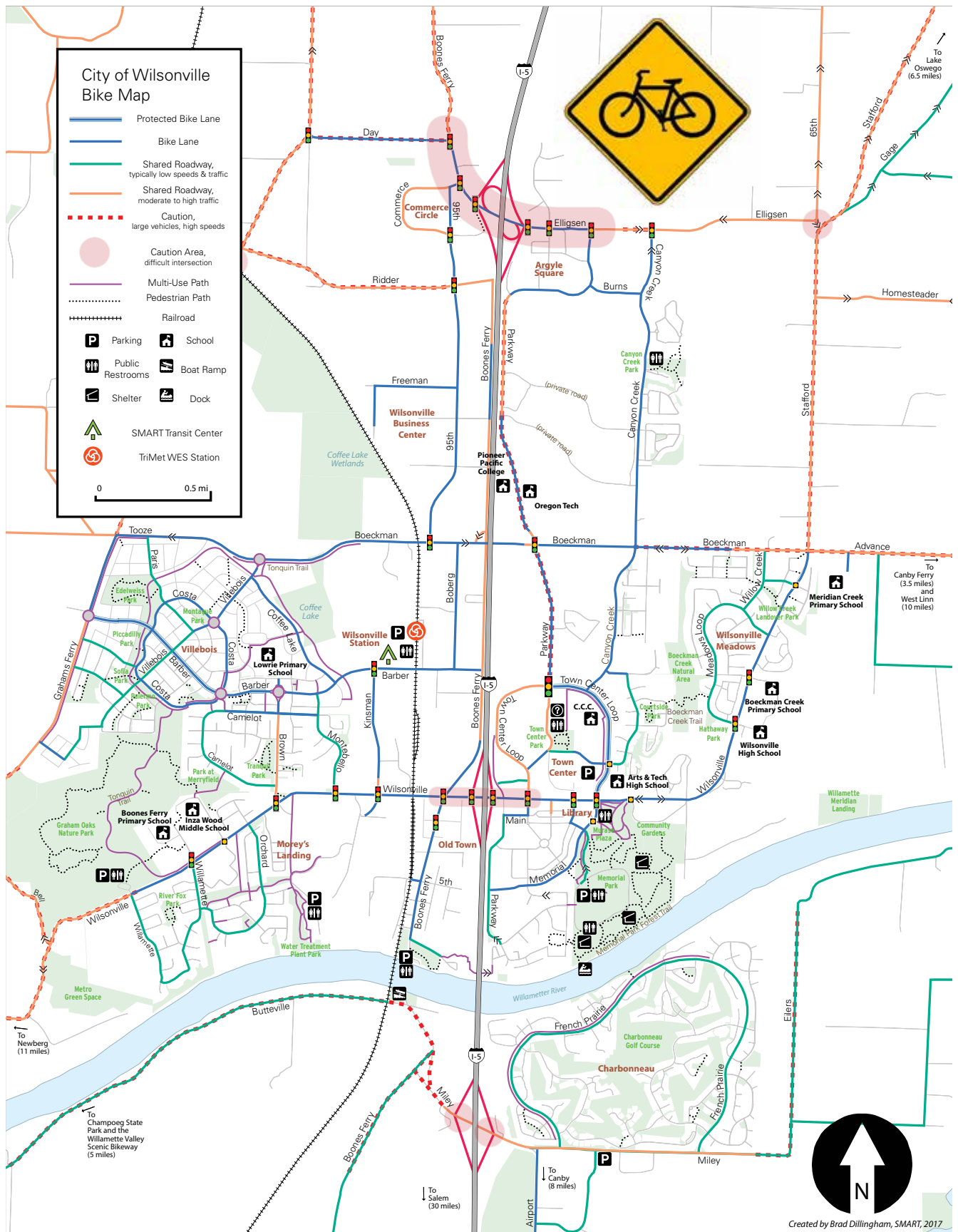
I hope these tips help ensure that you and your family have many great bike rides throughout the month of May and all year long. Happy National Biking Month Wilsonville!

*Adam Phillips, Chief of Police*



The Police Department is resuming bike patrols in May that last until fall.

## Biking and Walking Map of Wilsonville





## Reports from City Boards and Commissions

### City Council Adopts New Planning and Development Fee Schedule, Affordable Housing Property-Tax Exemptions and Council Goals

At the March 20, Wilsonville City Council meeting, the City Council adopted an updated fee schedule for land-use development and planning-review fees to better align the City's costs to review and administer various land-use, planning and development applications with the actual fees charged. The fee adjustment is estimated to improve the City's cost-recovery to deliver these services from 45 percent up to 84 percent of actual costs. Previously some fees, such as tree permit fees, were purposefully subsidized to reduce the burden of taking out certain types of permits.

During the meeting the Council also held a public hearing and authorized a Supplemental Budget Adjustment to amend the current adopted budget to move expenditures from one category to another within a specific fund but that does not increase the overall budget that was approved during the annual budget process last June. The adjustments included moving program funds from one department to another, increasing the anticipated utility revenues, and various adjustments to the capital projects budget.

A public hearing and ordinance to adopt the 2017 Transit Master Plan update was continued to April 17. Prior to holding a public hearing at a City Council meeting, the plan is first scheduled to have a public hearing on Wednesday, May 10, before the Planning Commission.

The Council adopted the 2017–19 City Council goals that guide and prioritize the City's efforts and investments over the next two-year period. City Council members also approved an updated 2017 Council Protocol Manual that guides City Council processes and operations.

Under the Consent Agenda City Council adopted nine different resolutions, five of which grant property-tax exemptions to five different affordable housing developments in Wilsonville. The Council authorized the City Manager to appoint an audit firm and to enter into a construction contract to complete the second

phase of the Charbonneau High Priority Utility Repair Project for stormwater pipe repairs. The Council also added the 5th Street/Kinsman Road Extension project to the current wastewater collection system and the water system master plans.

Under Citizen Input and Community Announcements numerous community members encouraged the City Council to adopt an inclusivity resolution to promote Wilsonville as a city that supports and promotes diversity.

During work session prior to the meeting, the City Council heard reports and discussed the Year 2000 Urban Renewal Plan, Wastewater Collection System Master Plan project list amendment, Water distribution master plan project list amendment, Basalt Creek Concept Plan update, Equitable Housing Strategic Plan update, and acceptance of the audit request for proposals.

### City Celebrates Volunteers at Appreciation Event



In April the City held a Volunteer Appreciation event honoring City Board and Commission members and other volunteers with dinner along with entertainment provided by Marylhurst University's Guitar Ensemble.

### City Council Adopts 2017–19 Goals for the City and Community

The Wilsonville City Council has adopted the goals to guide and prioritize the City's efforts and investments over the next two years. The 2017–19 City Council goals to be completed by Dec. 31, 2018, include the following 18 goals:

- Complete the **French Prairie Bridge Project** to plan and develop preliminary designs.
- **Install interpretive signage** for Beauty and the Bridge and on Murase architectural features; inventory all public art with interpretive recognition.
- **Organize Library archives;** capture history as it happens and before it changes, including coordinating photography.
- Explore the establishment of an **Arts and Culture Commission**, based on the results of the Arts and Culture Commission Study, and develop a strategy to reinstitute the sculpture program.
- **Conduct a study of the Arrowhead Creek Area** considering river access options, transportation, and land use issues.
- **Complete the Parks Master Plan** and, subsequently, master planning for Boones Ferry Park.
- Secure funding to complete design work for the

#### I-5 Pedestrian & Bikeway Bridge.

- Continue to negotiate with TriMet to **adjust service boundaries.**
- Complete the preliminary work necessary to begin soliciting bids on Phase I of the **Boones Ferry/Brown Road project.**
- Evaluate the results of the **housing affordability study and begin policy development**, including addressing housing mix.
- Hold **educational town hall/summit meetings** regarding traffic challenges, affordable housing, building elevation, and density policy trade-offs.
- **Complete form-based code** work currently underway.
- **Complete the fiber business plan.**
- **Become a bee city.**
- Develop and implement a **street tree replacement program.**
- **Improve Wilsonville's Walk Score.**
- **Complete the Town Center Master Plan**, including an International Square.
- **Develop a wayfinding program.**

### 2017 Wilsonville Citizens Academy Visits the State Capitol



Members of the 2017 Citizens Academy visited the State Capitol and met with Rep. Rich Vial and Sen. Kim Thatcher.

### Upcoming on the Council Agenda

The following issues are tentatively scheduled for consideration and or discussion by the City Council in May. Scheduling changes can and do occur; check for updated meeting information at the City's website home-page, [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us).

**Monday, May 1:** red light camera; adult diversion program; community engagement; Meadows Loop on street parking (across from HS); French Prairie Bridge evaluation criteria; transit funding; snow and ice control; Basalt Creek Concept Plan; Republic Services report; Willamette Falls Locks update by West Linn Mayor Russell Axelrod; and inclusion resolution.

**Monday, May 15:** Joint session with Planning Commission on Town Center Plan; National Public Works Week; Metro Update by Councilor Craig Dirksen; and CCSO (Wilsonville Police) annual report.

### Planning Commission Update

At the Planning Commission meeting on May 10, 6:00 pm at City Hall the Commission is holding a public hearing regarding the Transit Master Plan (see related article on pg. 3).

On May 15 the Commission is holding a joint meeting with City Council to discuss the Wilsonville Town Center Plan.

At the Commission meeting in April the Commission heard updates and reviewed a draft of the Transit Master Plan, 2106 Housing Report, Basalt Creek Concept Plan and recent City Council actions.

### Development Review Board Update

The regularly scheduled DRB Panel A meeting for Monday, April 10 was cancelled due to a lack of agenda items. The regularly scheduled DRB Panel B meetings scheduled for Monday March 27 and Monday, April 24 were also cancelled due to a lack of agenda items.

### Library Board Report for March

During the Library Board meeting on Wednesday, March 22, the following reports were given:

- The Library held a Teen Game Night in March that revolved around Kahoot, a new quiz game system online that can be used for both fun and education.
- The board reviewed and discussed the RFID tagging project that is currently underway.
- The Board discussed the Library's add-packages for the budget and the budget approval process.
- The Inclement Weather policy was reviewed and the Board discussed the library's role in serving community members in need during inclement weather and at other times.
- The Board reviewed the new 2017–2019 City Council goals that calls on the Library to "organize Library archives; capture history as it happens and before it changes, including coordinating photography."
- The Board reviewed the Library's Photo Policy, Community Bulletin Board Policy, Exhibitor Guidelines and Agreement, and Policies and Procedures and plans to have further discussion about the policies at the next meeting.
- Caroline Berry's first term on the Board expires on June 30 and she is eligible for and desires to be reappointed to a second term.
- The Library Foundation is holding a spring fundraising luncheon in Charbonneau.

The next regular Library Board meeting is scheduled for Wednesday, May 24, 6:30 pm, at the Library.

### Budget Committee Meets in May and June

The Budget Committee is scheduled to hear the City Manager's proposed budget for Fiscal Year 2017–18 on May 18, June 1 and if necessary, on June

#### Budget Committee Meetings

- **Thurs, May 18, 6–9 pm**
  - **Thurs, June 1, 6–9 pm**
  - **Thurs, June 8\*, 6–9 pm**
- City Hall, 29799 SW Town Center Loop East, \*If needed

8. These meetings are open to the public and public comment is encouraged. Proposed budget documents are expected to be available to the public in early May and will be posted online at [www.ci.wilsonville.or.us/budget](http://www.ci.wilsonville.or.us/budget).

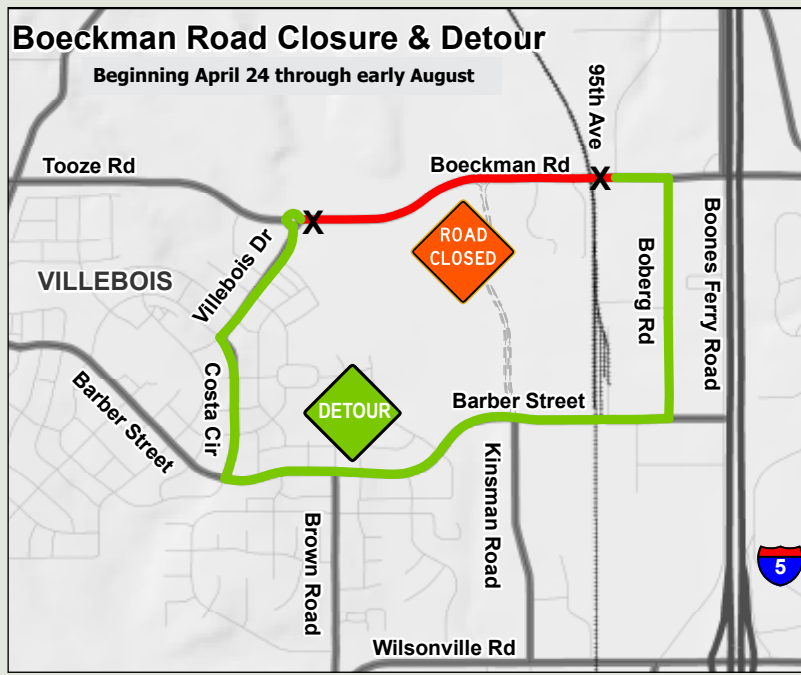
## Boeckman Road Near Villebois Closed Until Early August

Construction of the Kinsman Road Extension project that connects SW Boeckman Road and Barber Street requires a three-month-long closure of Boeckman Road until early August. The project builds a concrete roundabout at the new intersection of Boeckman and Kinsman.

Until early August, Boeckman Road between Villebois Drive and the railroad tracks west of SW 95th Avenue is to be closed to through traffic, with access to local businesses maintained. A detour route along Boberg Road, Barber Street, Costa Circle and Villebois Drive is planned.

The project extends Kinsman Road one mile from Barber Street near the Barber Street Bridge and the SMART/WES Commuter Rail Transit Center to connect with Boeckman Road to the north.

For more information, contact Zachary Weigel, PE, Civil Engineer, at 503 570-1565, [weigel@ci.wilsonville.or.us](mailto:weigel@ci.wilsonville.or.us).



Boeckman Road (west of 95th Ave.) is closed until early August to construct a new roundabout/intersection at Kinsman Road.

## Advance Road at 60th Ave Closed until May 19

Advance Road is closed to all through-traffic between Wilsonville Road and 60th Avenue for a three-week period, lasting until Friday, May 19. The intersection at Wilsonville Road and Stafford Road remains open.

Local access to homeowners is being maintained, but short delays are to be expected.

The closure is needed to allow complete reconstruction of a section of the roadway in association with the new Meridian Creek Middle School being constructed

by the West Linn–Wilsonville School District.

A detour route using Mountain and Homesteader roads will be in effect. The City and School District apologize for any inconvenience this may cause citizens and businesses.

For more information, contact Steve Adams, PE, Development Engineering Manager, at 503-682-4960; [adams@ci.wilsonville.or.us](mailto:adams@ci.wilsonville.or.us) or Tony Vandenberg with the School District at 503-673-7000 x7990.

## City Provides High School Journalism Club Video Equipment

The City of Wilsonville and Wilsonville High School are partnering together to provide the High School's journalism club, Wilsonville Broadcast Network, approximately \$20,000 worth of video equipment to film and produce a wide range of content to be shown on the City's government access channel which airs on Comcast/Xfinity Ch. 30 and Frontier Ch. 28.

The City purchased the equipment using Public, Education and Government (PEG) access program funds, which originate from a \$1 monthly fee paid by local cable television subscribers. The City is loaning the video equipment to the High School at no charge.

City Manager Bryan Cosgrove explains, "Through this partnership we hope to not only help our local high school kids learn about video production and journalism, but also the City is excited to be getting

new, original and local content to air on the City's government channel."

Wilsonville Broadcast Network seeks to produce and air live events, weekly news shows, informational news stories and public service announcements of interest to local community members.

Technical assistance is being provided to the club and the High School by Willamette Falls Media Center (WFMC), which is a local community media center located in the Historic McLoughlin District of Oregon City. WFMC currently manages five local cable channels, including the City of Wilsonville's channel. The nonprofit organization also offers classes and training. For more information visit [www.wfmc-studios.org](http://www.wfmc-studios.org).

To learn more about the high school's journalism club visit [WilsonvilleBroadcastNetwork.com](http://WilsonvilleBroadcastNetwork.com).



Local high school students and staff along with Melody Ashford, Executive Director of Willamette Falls Media Center (third from right), pose with new video equipment donated by the City of Wilsonville.

## City Accepting Applications to Serve on Library Board

The City of Wilsonville is now accepting applications for a Library Board member for a term that begins on July 1, 2017.

The Board supports and assists the Library Director in all library planning, and advises the City of budget-

ary and policy considerations relating to the Library's operation and development. Board members assist in monitoring library operations and services to the



public; encouraging cost effectiveness; and periodically reviewing the Library's policies, procedures, goals and objectives.

Persons interested in serving on the Library Board may pick up an application from Sandra King, City Recorder, at City Hall, 29799 SW Town Center Loop in Wilsonville, or may obtain an application online at [www.ci.wilsonville.or.us/apply](http://www.ci.wilsonville.or.us/apply).

The application deadline is May 31, 2017.

For more information contact Sandra King, City Recorder, at 503-570-1506; [king@ci.wilsonville.or.us](mailto:king@ci.wilsonville.or.us).

## City Council and Planning Commission Hold Initial Joint Meeting on Town Center Plan

On Monday, May 15, 5–7 pm, at City Hall, the City Council and Planning Commission are holding a joint public meeting to discuss the Town Center Plan's

**City Council and Planning Commission Meeting  
Wilsonville Town Center Plan**  
• Wed, May 15, 5–7 pm  
City Hall, Willamette River Room  
29799 SW Town Center Loop East

vision and goals, existing conditions, opportunities, and a summary of the public comments received to-date.

The Wilsonville Town Center Plan focuses on creating a community-driven vision for the Town Center and a plan that will guide future development.

In the past few months the project has achieved several notable milestones. On Feb. 28, City staff held a public kickoff event at City Hall and received input from a diverse group of residents, employers, businesses, workers, and youth. Furthermore, an online survey that ran throughout March collected 989 responses from wide variety of community members and stakeholders as well. Community input gathered through the event and survey is being used to help shape a vision, strategies, and actions for the future of Town Center.

Throughout the 18-month-long project the public is invited to submit feedback, share ideas and provide input that has and will continue to be recorded and considered during the plan development process. Visit [www.wilsonvilletowncenter.com](http://www.wilsonvilletowncenter.com) for more information about the project, to review the existing conditions, see results of the kickoff event and to sign up for project updates and other opportunities to stay involved.



Community members provide input on the Wilsonville Town Center Plan at the Feb. 28 Community Kickoff Event.

For more information contact Miranda Bateschell, Long-Range Planning Manager, at 503-570-1581; [bateschell@ci.wilsonville.or.us](mailto:bateschell@ci.wilsonville.or.us).

## Wilsonville Welcomes New Oregon Tech Leader



Mayor Tim Knapp greets Dr. Nagi Naganathan (Naw-ga-naw-thun), Oregon Tech's new president, and his wife, Dr. Kasturi Naganathan, along with SMART Transit Director Dwight Breshear at an April reception held at the Wilsonville campus. While starting officially last month, President Naganathan has visited the Klamath Falls and Wilsonville campuses several times since his November appointment by Oregon Tech's board of trustees after a nationwide search. Prior to joining Oregon Tech, Dr. Naganathan was dean of engineering at the University of Toledo, served as UT's interim president, and was the founding chairperson of the Mechanical, Industrial, and Manufacturing Engineering Department.

### Garden Club Holds Annual Plant Sale

The Wilsonville Garden Club is holding an annual plant sale on Saturday, May 13, at the Wilsonville Public Library, 8200 SW Wilsonville Road.

The sale begins at 9 am and lasts until all the plants are sold, which is usually before 2 pm.

The club's sale offers large selection of the very popular sun and shade hanging baskets, blooming annuals, perennials plus cuttings and divisions from club member's gardens and an assortment of Garden Gallery yard art. All of the proceeds from the event go to support local horticulture education and scholarships.



### Memorial Day Observance

The Korean War Veterans Association (KWVA) is holding a free, public Memorial Day Observance Ceremony on Monday, May 29, 11 am, in Town Center Park, located at 29600 SW Park Place, Wilsonville. The event features the national anthem, presentation of colors, wreath laying ceremony, various speakers, POW/MIA recitation, closing prayer, TAPS and retiring of the colors. Veterans of all wars and their families are encouraged to attend. For more information, contact Bob Cassidy, President, KWVA Oregon Trail Chapter, at 503-476-9012, IrishBob7@gmail.com.

**Memorial Day Observance Ceremony**  
 • Mon, May 29, 11 am  
 Town Center Park,  
 29600 SW Park Place

### Bulky Waste Day Provides Free Disposal of Large Waste Items

In partnership with Metro and Republic Services, the City of Wilsonville is holding Bulky Waste Day on Saturday, May 20, 9 am–1 pm, at 10295 SW Ridder Road in Wilsonville.

**Bulky Waste Day**  
 • Sat, May 20, 9 am –1 pm  
 Republic Services  
 10295 SW Ridder Road

Community members, with proof of Wilsonville residency and toiletries to donate, are invited to drop off large, bulky items that don't fit into standard-sized garbage cans. The types of waste that **will be accepted** include: clean untreated wood, dishwashers, televisions, refrigerators, computers, monitors, stoves, dryers, water heaters, couches, mattresses, scrap metal, tables and chairs.

The following are types of items that **will not be accepted** include: construction debris of any type; propane bottles/canisters; paint; batteries; solvents; thin-

ners; household garbage; tires; and other related items.

The event is free, but participating households will be asked to donate toiletries such as shampoo, soap and deodorant to benefit Wilsonville Community Sharing's Food Bank and the low-income households they serve.



For information contact Matt Baker, Public Works Supervisor, at mbaker@ci.wilsonville.or.us; 503-570-1548.

### Wilsonville Festival of Arts is Set for the First Weekend in June

The Wilsonville Arts & Culture Council presents the 18th annual Wilsonville Festival of Arts. This free community event is on Saturday, June 3, and Sunday, June 4, at Town Center Park. The festival begins at 10 am both days

and lasts until 6 pm on Saturday and 5 pm on Sunday. The festival includes original art from over 50 professional artists and 200 West Linn-Wilsonville School District students. The Authors Pavilion features over 20 authors from throughout the region. The festival features the following live music and dance performances:

- Saturday, June 3**
- 10 am–2 pm – Polaris Dance Theatre (roaming performers)
  - 10 am–Edge Movement Arts
  - 11 am–Bug Toast (folk)
  - 12 pm–Soul'd Out (a cappella)
  - 1 pm–There is No Mountain (folk/pop)
  - 2 pm–Portland Youth Jazz Orchestra
  - 3 pm–Mexica Tiahui (Aztec dance)
  - 4 pm–The Brassroots Movement (New Orleans brass band)
  - 5 pm–Saeeda Wright (R&B / Soul)



Kim Nickens' art is just one of over 50 artists being featured at this year's event.

- Sunday, June 4**
- 11 am–4 pm – Edge Movement Arts (roaming)
  - 10 am–Steal the Sun Tai Chi Studio
  - 11 am–The Green Note (a cappella)
  - 12 pm–Library Book Adventures
  - 1 pm–Youth Music Project
  - 3 pm–Those Willows (pop / folk)
  - 4 pm–Salsanova (Latin music)
- On Saturday, South Metro Area Regional Transit (SMART) is providing free trolley tours of public art throughout Wilsonville and also tours of historic places around Wilsonville. New this year will be interactive art installations by architect and artist Aaron Whelton, and fiber artist Jessica Hickey. A variety of food vendors are also going to be at the festival including Jr. Scoop Ice Cream, Lupita's Restaurant, Top O'Hill and TNT Kettle Corn.

The Wilsonville Festival of Arts is supported by grants from the City of Wilsonville Community Tourism Grant Program, as well as donations from Fred Meyer, Argyle Square, Pamplin Media Group, SMART, NW Natural, Findlay Wilsonville, Luby/Daraee Law Group, Academy Mortgage and The Laue Real Estate Team. For more information about the Wilsonville Festival of the Arts visit [www.WilsonvilleArts.org](http://www.WilsonvilleArts.org).

## City Calendar

| MAY  |  |
|--|--|
| 1 Mon  | • City Council, City Hall 5 pm–Work Session 7 pm–Meeting   |
| 2 Tue  | • Municipal/Traffic Court, 5 pm, City Hall   |
| 8 Mon  | • DRB Panel A, 6:30 pm, City Hall  |
| 9 Thurs  | • Park and Rec Advisory Board Meeting 4:30 pm, Park and Rec Admin Offices  |
| 10 Wed   | • Wilsonville Community Seniors Inc., 1–3 pm, Wilsonville Community Center • Planning Commission, 6 pm, City Hall                                      |
| 13 Sat   | • WERK Day, 8 am–12 pm Community Center  |
| 15 Mon   | • City Council, City Hall 5 pm–Work Session (extended) 7:30 pm–Meeting   |
| 16 Tue   | • Municipal/Traffic Court, 5 pm, City Hall   |
| 18 Thurs   | • Budget Committee Meeting, 6–9 pm, City Hall • Citizens Academy, 6–9 pm, City Hall  |
| 20 Sat   | • Bulky Waste Day, 9 am–1 pm 10295 SW Ridder Road, Wilsonville   |
| 22 Mon   | • DRB Panel B, 6:30 pm, City Hall  |
| 24 Wed   | • Library Board, 6:30 pm Wilsonville Public Library  |
| 29 Mon   | • City Offices Closed–Memorial Day • Memorial Day Ceremony–Korean War Veterans Association, 11 am–12 pm, Town Center Park                              |
| JUNE   |  |
| 1 Thurs  | • Budget Committee Meeting 6–9 pm, City Hall   |
| 3 Sat  | • Wilsonville Arts Festival 10 am–6 pm, Town Center Park   |
| 4 Sun  | • Wilsonville Arts Festival 10 am–5 pm, Town Center Park   |
| 5 Mon  | • City Council, City Hall 5 pm–Work Session 7 pm–Meeting   |
| 6 Tue  | • Municipal/Traffic Court, 5 pm, City Hall   |
| 8 Thurs  | • Parks and Recreation Advisory Board Meeting, 4:30 pm, Parks and Rec Administration Building • Budget Committee Meeting 6–9 pm, City Hall             |
| 10 Sat   | • Hazardous Waste Collection Event, 9 am–1 pm, City Hall Parking Lot   |
| 12 Mon   | • DRB Panel A, 6:30 pm, City Hall  |
| 14 Wed   | • Wilsonville Community Seniors Inc., 1–3 pm, Wilsonville Community Center • Planning Commission, 6 pm, City Hall                                      |
| 19 Mon   | • City Council, City Hall 5 pm–Work Session 7 pm–Meeting • Wilsonville Citizens Academy Graduation, 7 pm–During City Council Meeting, Council Chambers |
| 20 Tue   | • Municipal/Traffic Court, 5 pm, City Hall   |
| 24 Sat   | • Korean War Memorial Remembrance Ceremony, 10 am–12 pm Town Center Park   |
| 26 Mon   | • DRB Panel B, 6:30 pm, City Hall  |
| 28 Wed   | • Library Board, 6:30 pm Wilsonville Public Library  |
| All dates and times are tentative; check the City's online calendar for schedule changes at <a href="http://www.ci.wilsonville.or.us">www.ci.wilsonville.or.us</a> . |  |

## Wilsonville City Council

The City Council usually convenes on the first and third Monday of the month at City Hall, with work session generally starting at 5 pm and meeting at 7 pm. Meetings are broadcast live on Comcast/Xfinity Ch. 30 and Frontier Ch. 32 and are replayed periodically. Meetings are also available to stream live and by video-on-demand at [www.ci.wilsonville.or.us/WilsonvilleTV](http://www.ci.wilsonville.or.us/WilsonvilleTV). Public comment is welcome at City Council meetings.



**Tim Knapp**  
Mayor  
knapp@ci.wilsonville.or.us



**Scott Starr**  
City Council President  
scottstarr97070@gmail.com



**Susie Stevens**  
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Wilsonville, OR 97070  
Phone: 503-682-1011 Fax: 503-682-1015  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)  
E-mail: [webmaster@ci.wilsonville.or.us](mailto:webmaster@ci.wilsonville.or.us)

**City Council:** 503-570-1501; [council@ci.wilsonville.or.us](mailto:council@ci.wilsonville.or.us)  
**Police Non-Emergency Dispatch:** 503-655-8211





## Wilsonville Launches the Transit Master Plan Update

Welcome to the process of updating Wilsonville's Transit Master Plan (TMP). Over the next eight months, you will receive periodic e-News about the planning process and requests for your opinions.

### What's the TMP?

The purpose of the TMP is to guide the City's public transit agency, South Metro Area Regional Transit (SMART), over the next five years.

This update addresses the following two key questions:

- How should the City prioritize available transit resources?
- What improvements are needed in the next five years?

### What's your role?

Wilsonville is projected to grow over the next 20 years. This means SMART and the City Council need to make some strategic choices about future transit investments.

### SMART seeks your input on how Wilsonville adapts to:

- A growing community
- An aging population
- An increase in commuters who are traveling daily to jobs throughout the region
- Addressing the state and region's Climate Smart goals

**A community-wide survey will be available beginning March 1. Please look next week for a Wilsonville Transit Master Plan e-News with survey link.**

Much good progress has been made in Wilsonville since the Transit Master Plan was last updated in 2008:

- SMART serves more of Wilsonville with better service
- Commuters have improved transit connections to rail
- A new administrative and fleet facility opened
- SMART Transit Center and WES Rail Station provide greater connectivity
- A fleet of alternative fuel buses is in service
- A new Compressed Natural Gas (CNG) station opened for more environmentally-sound service

Please help us keep getting better together.  
For more information about the project, please visit:

**[www.PlanSMARTWilsonville.com](http://www.PlanSMARTWilsonville.com)**



To unsubscribe from this newsletter, please use the link at the bottom of this email.

Wilsonville Transit Master Plan | [plansmartwilsonville.com](http://plansmartwilsonville.com)

STAY CONNECTED:





## Wilsonville Transit Master Plan Update

### SURVEY NOW AVAILABLE

#### What are your priorities when you can't have it all?

Wilsonville needs to hear what you care about the most to update the Transit Master Plan to best serve our community. Give just five minutes to help shape the transportation choices for Wilsonville over the next five years.

**Take the survey now  
and let us know what's important to you.**

[Click here](#)

Keep an eye out too for convenient survey stations at Wilsonville City Hall, the Library, local businesses, events, and community meetings.



#### What's next?

SMART transit planners will study your opinions and explore the service tradeoffs. Then in summer, a 2nd survey will be available with draft service options for your review.

In autumn, the draft Wilsonville Transit Master Plan will be available for review and comment prior to City Council's discussion and decision about how to allocate SMART resources.

To sign up for future e-updates, please use this link:

[Sign Up for E-Updates](#)

To unsubscribe from this newsletter, please use the link at the bottom of this email.

To learn more about the Transit Master Plan, please visit:

[www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

**Think SMART. Ride SMART. Plan SMART.**



## Wilsonville Transit Master Plan Update

### How should Wilsonville balance intercity and local service?

More bus service for trips out of Wilsonville or more trips within Wilsonville?

**Tell us here.**

(takes only a few minutes-really!)

Please forward to others.

Survey also available at electronic kiosks and in paper (English and Spanish) at City Hall and Wilsonville Public Library beginning August 17th.



### What we heard from community input last spring:

- \* Expand Saturday and evening service
- \* Faster east/west cross town service
- \* Increase service to Villebois
- \* Expand service to Salem
- \* Better and more frequent connections to the north
- \* Improve timing of connections to Canby Transit for Oregon City

### Within a limited budget for transit, what are your priorities when you can't have it all?

To read more about the Wilsonville Transit Master Plan's Public Input, [Click here to see the Summary.](#)

### What's next?

SMART transit planners will study your opinions and refine the plan. In autumn, the draft Wilsonville Transit Master Plan will be available for review and comment prior to City Council's discussion and decision about how to allocate SMART resources.

To learn more about the Transit Master Plan, please visit:

[www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

To receive future project updates, please use this link:

[Sign Up for E-Updates](#)

**Think SMART. Ride SMART. Plan SMART.**



## Wilsonville Transit Master Plan Update

**SURVEY OPEN UNTIL THIS FRIDAY, OCT. 21**

**If you haven't had a chance to take it, please do so now.**

**How should Wilsonville balance intercity and local service?**

More bus service for trips out of Wilsonville or more trips within Wilsonville?

**Tell us here.**

(takes only a few minutes-really!)

**Please forward to others.**

Survey also available at an electronic kiosk and in paper (English and Spanish) at City Hall.

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[www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

To receive future project updates, please use this link:

[Sign Up for E-Updates](#)

**Think SMART. Ride SMART. Plan SMART.**

Wilsonville Transit Master Plan | [plansmartwilsonville.com](http://plansmartwilsonville.com)



## Wilsonville Transit Master Plan Update

The Draft 2017 Transit Master Plan for the City of Wilsonville is **now available for review and comment.**

[Click here](#)



**SMART**  
SOUTH METRO AREA REGIONAL TRANSIT

CONVENIENCE  
SAFETY  
RELIABILITY  
EFFICIENCY  
FISCAL RESPONSIBILITY  
FRIENDLY SERVICE  
EQUITY & ACCESS  
ENVIRONMENTAL RESPONSIBILITY



Sincere thanks to all who took the time to provide thoughtful input on the type of transit system and supportive transportation options required to meet Wilsonville's changing mobility needs.

SMART welcomes comments on the draft Plan. **If you want to have your comments evaluated prior to the first public hearing, they need to be received by SMART staff by March 5, 2017.** The official record will remain open until the close of the City Council's public hearing (tentatively scheduled for March 20, 2017).

**There are many ways to comment:**

Hard copies of the Plan will be available at the Wilsonville Public Library after February 1st, 2017.



To make online comments, please visit  
[www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

EXHIBIT D

**Email**

**Phone:** 503-570-0332

**Mail:**

City of Wilsonville  
SMART TMP Comments  
ATTN: Michelle Marston  
29799 SW Town Center Loop E.  
Wilsonville, OR 97070

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**Please mark your calendars for upcoming TMP meetings:**

**City Council Work Session/TMP Discussion**

February 6, 2017, 5:00 PM - 7:00 PM @ Wilsonville City Hall -  
Willamette River Rooms (No public testimony)

**Planning Commission Meeting/TMP Public Hearing**

March 8, 2017, 6:00 PM @ Wilsonville City Hall - Council  
Chambers (Public testimony)

**City Council Meeting/TMP Public Hearing**

March 20, 2017, 7:00 PM @ Wilsonville City Hall - Council  
Chambers (Public testimony)

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To learn more about the Transit Master Plan, please visit:

[www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

To receive future project updates, please use this link:

[Sign Up for E-Updates](#)

**Think SMART. Ride SMART. Plan SMART.**

Wilsonville Transit Master Plan | [plansmartwilsonville.com](http://plansmartwilsonville.com)

STAY CONNECTED:





## Wilsonville Transit Master Plan Revised Meeting Schedule

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New information has been added to the [draft TMP](#). In order to provide additional public review, the TMP public review meetings have been revised.

### New Schedule:

#### Wilsonville City Council TMP Work Session

March 6

5:00 - 7:00 p.m.

Wilsonville City Hall

(No public testimony)

#### Wilsonville Planning Commission TMP Public Hearing

April 12

6:00 - 8:00 p.m.

Wilsonville City Hall

(Public testimony)

#### Wilsonville City Council TMP Public Hearing

TBD

(Public testimony)

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Think SMART. Ride SMART. Plan SMART.



## Wilsonville Draft Transit Master Plan

A *revised* Draft 2017 Transit Master Plan (TMP) for the City of Wilsonville is **now available for review and comment**. [Click here.](#)

Based on feedback received, the draft plan has been reorganized and updated with additional information.

SMART welcomes comments on the revised draft plan. The public comment period is open until **Wednesday, May 24, 2017**.



**There are many ways to comment:**

- To make online comments, please visit [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)
- **Email**
- Phone: 503-570-0332
- Mail:

City of Wilsonville  
 SMART TMP Comments  
 ATTN: Michelle Marston  
 29799 SW Town Center Loop E.  
 Wilsonville, OR 97070

Hard copies of the Plan are available at [Wilsonville Public Library Reference Desk, 8200 SW Wilsonville Rd.](#)  
[Wilsonville City Hall Reception, 29799 Town Center Loop East](#)

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**PLEASE NOTE:**  
**New schedule for upcoming TMP public meetings**

**Wilsonville Planning Commission Meeting  
TMP Work Session**

**April 12**

**6:00 - 8:00 p.m.**

Wilsonville City Hall - Council Chambers  
(Public testimony)

**Wilsonville Planning Commission Meeting  
TMP Public Hearing**

**May 10**

**6:00 - 8:00 p.m.**

Wilsonville City Hall - Council Chambers  
(Public testimony)

**Wilsonville City Council Meeting  
TMP Public Hearing**

**June 5**

**7:00 p.m.**

Wilsonville City Hall - Council Chambers  
(Public testimony)

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For more information about the Transit Master Plan, please visit:

[www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

To receive future project updates, please use this link:

[Sign Up for E-Updates](#)

**Think SMART. Ride SMART. Plan SMART.**

Wilsonville Transit Master Plan | [plansmartwilsonville.com](http://plansmartwilsonville.com)

STAY CONNECTED:



**NOTICE OF LEGISLATIVE PUBLIC HEARING  
BEFORE THE PLANNING COMMISSION:  
TRANSIT MASTER PLAN**

**Planning Commission:**

On **Wednesday, March 8, 2017, beginning at 6:00 p.m.**, the Wilsonville Planning Commission will hold a public hearing regarding adoption of the Transit Master Plan (Case File # LP17-0003). The Planning Commission will consider whether to recommend adoption of the amendment to the City Council. No additional mailed notice will be sent to you unless you either:

- Submit testimony or sign in at the Planning Commission hearing, or
- Submit a request, in writing or by telephone, to the Planning Division.

**City Council:**

The Wilsonville City Council is scheduled to hold a public hearing on the proposal on March 20, 2017, **at 7:00 p.m.** after which it may make the final decision.

The hearings will take place at **Wilsonville City Hall, 29799 SW Town Center Loop East, Wilsonville, Oregon**. A complete copy of the relevant file information, including the staff report, findings, and recommendations, will be available for viewing seven days prior to each public hearing at Wilsonville City Hall, at the Wilsonville Public Library and on the City's web site.

**Summary of Proposal:** Transit Master Plan

The new Transit Master Plan, upon adoption, will replace the current Transit Master Plan, adopted in 2008. As a sub-set of the City's Transportation System Plan, the Transit Master Plan is intended to guide future planning and investment in Wilsonville's public transit system — South Metro Area Regional Transit.

**How to Comment**

Oral or written testimony may be presented at the public hearing. Written comment on the proposal to be submitted into the public hearing record is welcome prior to the public hearings. To have your written comments or testimony distributed to the Planning Commission before the meeting, it must be received by 2 pm on Tuesday, February 28, 2017. Direct such written comments or testimony to:

**Chris Neamtzu, Planning Director**  
29799 SW Town Center Loop East,  
Wilsonville, Oregon, 97070;  
[Neamtzu@ci.wilsonville.or.us](mailto:Neamtzu@ci.wilsonville.or.us) or 503.570.1574

Copies of the full draft plan is available from the Wilsonville Planning Department at the above address and at the project website: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)

*Note: Assistive Listening Devices (ALD) are available for persons with impaired hearing and can be scheduled for this meeting. The City will also endeavor to provide qualified sign language interpreters and/or bilingual interpreters, without cost, if requested at least 48 hours prior to the meeting. To obtain such services, please call Linda Straessle, Planning Administrative Assistant at (503) 682-4960.*

**Transit Master Plan Notices in Wilsonville Spokesman**

Five notices have been listed in the Wilsonville Spokesman regarding Transit Master Plan public meetings for the hearing dates of:

- May 10, 2017
- March 20, 2017
- March 8, 2017

Those notices can be found at:

[http://publicnotices.portlandtribune.com/?s=transit%20master%20plan&category\\_name=](http://publicnotices.portlandtribune.com/?s=transit%20master%20plan&category_name=)

# WILSONVILLE

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## AREA CHAMBER OF COMMERCE

*A South Metro Business Advocate*

February 3, 2017

Mayor Knapp  
Council President Starr  
Councilor Stevens  
Councilor Lehan  
Councilor Akervall

Dear Mayor and Councilors:

The Wilsonville Area Chamber of Commerce has been an active and engaged Community partner throughout SMART's Master Transit Plan process. We appreciate the opportunity to participate, and we commend city staff for the inclusiveness, transparency, and professionalism of this process.

We support the overwhelming majority of the recommendations found in the City of Wilsonville's 2017 Transit Master Plan Draft. There are, however, two areas of concern which we would like to share with you:

1) Since the very beginning of this Master Transit Plan process and at every public input meeting, one key question has been raised by the Chamber (as well as by employers, employees, and even residents): "How do we arrive at sound transit recommendations withouts having a discussion of revenue and costs?" The answer provided by the consultants and SMART staff to this question was always the same, "Costs and revenues are a very critical component, but we're not there yet. We will get there by the end of the process."

In the 2017 Master Transit Plan Draft documents you will be discussing on February 6th, there are many substantive and consequential recommendations about a Transit Plan for our City of Wilsonville, but there is one key missing ingredient: a financial recommendation. In other words, while we are seemingly at the end of the process we have not yet had the costs and revenues discussion.

**Therefore, we would ask City Council before adopting this Master Transit Plan to require city staff and consultants to conduct a robust financial conversation with stakeholders.** The goal would be to make recommendations to the Council about how best to meet balanced budget needs, given increased service levels. Our Chamber's goal is to have frank,

February 3, 2017

Page Two

in-depth discussions about important trade-offs and how to create sustainable, fair funding source(s) for SMART moving forward.

We are very concerned that with our current transit budget consisting of approximately 79% payroll taxes, 20% grants, and 1% rider/citizen contributions, SMART faces a very uncertain funding future. If, as is expected, that grant funding will be seriously reduced, then that will leave the only option to be another increase in the payroll tax rate, which will negatively impact jobs.

Appendix F mentions the Corvallis Fee Model. This is one potential solution for a more sustainable and broad based equitable funding mechanism, and it deserves further study along with other options. Moreover, as part of a robust financial discussion there should also be an analysis of and recommendations about costs. For example, the Draft Transit Plan mentions the very high expense of Dial a Ride and that it should be studied. Why not perform the study/review now?

**2) The second area of concern is related to the deletion of the 2X route to the Barbur Transit Center and its realignment instead to the Tigard Transit Center.** In reviewing the survey results, there are a large number of rider comments that this move would mean these riders would no longer be using SMART (i.e., due to the addition of a lot more time for a trip from Wilsonville to Portland). Since this represents a major change, would it be possible to forecast how many riders we would lose? It has been 3+ years since the last rider survey was conducted. Does it make sense to conduct a rider survey now before wrapping up the Master Transit Plan so that we at least have up to date information?

Again, our thanks to the Mayor, City Councilors, and staff for your leadership and stewardship of this 2017 Transit Master Plan. In the coming months, we look forward to partnering with you to help finalize this plan. I would welcome the opportunity to answer any of your questions. My cell phone (503-577-7111) is the fastest way to connect.

Sincerely,



Kevin Ferrasci O'Malley, CEO  
Wilsonville Chamber of Commerce

8565 SW Salish Lane, Suite 150  
Wilsonville, OR 97070  
503-682-0411



**WILSONVILLE LEADERSHIP ACADEMY INPUT****November 18, 2017 2014**

Participants in the Wilsonville Leadership Academy group were asked to describe their positive vision for four areas (transportation, land use, education, housing) and ideas for any other topics. Their responses included:

**1. Transportation**

- a. Improve frequency of service on WES
  - i. Expand beyond commute hours including weekends
  - ii. 30 minutes or less increments
  - iii. Better bike paths and walking paths to WES
  - iv. Better pedestrian connections over I-5
  - v. Safer bike and pedestrian paths on Wilsonville Road
  - vi. Improve bike and pedestrian paths on Boeckman Road
  - vii. Partner with Mentor Graphics to improve paths
- b. Build bike bridge across Willamette River
- c. Better maintenance and debris removal on bike paths
- d. Safe and reliable corridors connecting parts of the city and metro area
- e. Multi-modal system (pedestrian, bike, transit, personal vehicle)
- f. Strategic timing of traffic lights
- g. Faster SMART between east and west
- h. Better connections between SMART and WES
- i. Balanced traffic flow

**2. Housing / Land Use**

- a. Decrease density / mixed density
- b. Affordable but integrated duplexes, housing, etc.
- c. Increase the percentage of homes
- d. No additional multi-family housing
- e. Controlled growth
- f. Preserve open spaces for parks
- g. Avoid residential development along I-5
- h. Industrial access to freeway without neighborhood shortcuts – neighborhoods protected from industrial traffic
- i. Balanced growth – commercial, light industrial, residential, open space
- j. A housing mix with neighborhood pride
- k. No yards smaller than 5000 sq ft
- l. Sidewalks in all neighborhoods
- m. Increased greenspace and trees
- n. Balanced open space vs. development
- o. Balanced high density vs. low density

**Transit Master Plan Public Comments of Draft Plan - 36 Total Comments**

*(note the draft continues to be revised to incorporate public feedback)*

| Date Submitted | Comment  | Format Received | Context of Comment            |
|----------------|--|-----------------|-------------------------------|
| 24-Apr-17      | Hi there, my name is Rob W., and I am a resident in Charbonneau. I work at the Wilsonville Fred Meyer and rely on the SMART bus service and the kindness of others to get me to and from work. I also happen to qualify for Dial-A-Ride service. I am writing this with a concern that others in the area may also share with me, and that concern is weekend bus service. More specifically, the lack thereof. The SMART bus system has limited service hours and routes on the weekend. The limited service excludes the entire Charbonneau area. I am also unable to get Dial-A-Ride to schedule a ride for me on a weekend day. There is no service at all on Sundays for anyone in Wilsonville. I have also not been successful finding reliable taxi service. The transportation options here are extremely limited for someone in my situation. It seems extremely likely to me that I am not the only person in Charbonneau who would like to go places on the weekend. And, not only would people like to go places but, actually need to get places like work and appointments. I find it consistently frustrating that I am not able to ride the bus all the times that I need to go to and from work. It seems fair to me that there should be service to the Charbonneau area during all the times there is service to other parts of Wilsonville. I have heard that things may change regarding the service in this area and it sounds like it will be more restricted than it is now. This worries me that the service is already so limited and to think that it will be reduced from what it is now is hard for me to imagine. I do not know if the new changes in service will even be enough for me on the weekdays. I may only be able to take dial-a-ride, which is helpful at times but, is not a guarantee I can get home. There are those times my manager requires me to work beyond a scheduled time and then I would not be able to get my dial-aside bus and be stuck at work if there are no fixed route buses I could take. I hope that you all will consider the needs of this part of the Wilsonville community.   | email           | Charbonneau - weekend service |
| 19-Apr-17      | I received feedback today regarding SMART and wanted to pass it along to you. This resident said she would appreciate bus service hours to be extended later in the summer. This would allow her to attend things like Movies in the Park and other events that end after the sun sets. She did not request follow up, so I am just passing along the suggestion   | in person       | Route 4 - later service hours |
| 11-Apr-17      | Thanks for at least acknowledging our inputs to retain Bus Route 6 service to the Mentor Graphics/FLIR Systems/Rockwell-Collins parking lots. A large number of Route 6 riders use these stops. Thanks again.  | website         | Route 6                       |
| 8-Apr-17       | I am disappointed and frustrated that SMART management ignore the safety of industrial employees of OT, Rockwell, Flir and Mentor by forcing a crossing of Parkway to board buses on the afternoon commute, additionally imposing a significantly longer access walk for low-mobility customers.   | website         | Route 6                       |
| 3-Apr-17       | I had wanted to chat briefly about WES and connections to the MAX system. You've got a tough spot there. Most everyone who opts not to use WES points out that it takes forever (2 hours) to get to where they want to go on the system. That may or may not be a convenient excuse to just use the car, but I can attest to the fact that it does take a while. A couple of years back I decided that I was going to use public transportation to get from here to a business on North Lombard where I was taking a short course of 8 weeks. It was an every Friday thing. WES to the main MAX line, to the Yellow line and onto a bus to my drop was about 1.75 hours at that time. But that was because I was headed to the very far side of the system. We have an opportunity to explore though, that might shorten that connect. Since that timeframe, METRO has added a line that terminates in Milwaukie on McLoughlin Blvd/Park Ave. That is the Orange line which is a straight run up into town. We should probably explore (I bet you already are) bus service direct to that transit center. We previously had a route that went to Oregon City but that got dropped for lack of ridership. (I had used that a few times while I was living in Clackamas.) That would position WES as the artery for the west side commuters and the bus to Milwaukie as the artery for in-town and east-side. And it might also open the possibility of bus service to Clackamas Town Center (MAX Green Line) for further east access and out into Gresham/East County. When the Oregon City route was dropped, there were only about 6000 residents in town, and the business base was smaller. Traffic was lighter as well, though still ugly at times. We're pushing close to 5X the residents now and our business base is larger. Traffic is consistently ugly. It might be enough to create ridership to support a line. Third, you are left with a legacy situation with regards to WES. Virtually everyone, myself included, was quite disappointed when it was disclosed very late in that project that service would ONLY be during commute hours on weekdays, and not any time else. You will still hear folks questioning why we can't have evening service, mid-day service, weekend service, etc... I know the reason. "We" don't own the tracks and we negotiated for available time. In the negotiations, weekends were taken completely off the table - as in "that will NEVER happen." I do not know if that is an intractable situation or not. I certainly can understand why heavy rail traffic might be interested in limiting the inconvenience of sharing the rails with light-rail traffic. I can also see the other side of that argument for folks who do want additional service times, but I doubt the ridership would ever amount to enough to justify it. The whole development of the WES line would make a great Harvard Business Case Study in how projects can snowball in critically bad ways. You might want to see if you can drag up a review of what occurred just to get history on it. Thank you for taking the time to read this. Welcome to Wilsonville and the Pacific Northwest. The best of wishes for your new position! | email           | WES - midday service          |
| 9-Mar-17       | Regarding the Draft TMP, Page 122, Bus Route 6 Canyon Creek Route, I have been using SMART for 6 years now, and the #6 bus is part of my daily commute. I am a FLIR employee, and use the Mentor Graphics stop. Elimination of the MG and OIT/FLIR stops makes no sense to me. This "parking lot" route also serves Rockwell Collins. Nothing with these companies and OIT has changed versus Xerox leaving town. I strongly encourage keeping Route 6 intact as currently run. thank you for your continued great service. J. Mrogenski FLIR Surveillance, Inc. Regular Route #6 Patron   | website         | Route 6                       |

|           |   |         |                                    |
|-----------|---|---------|------------------------------------|
| 8-Mar-17  | SMART is a great asset for Wilsonville. To increase SMART ridership it is necessary to understand the difficulty of the general public using the service. Someone who designed the routes, etc. has no idea of the maze presented. Route #'s and direction mean nothing if you don't know where the route goes. Switching buses to go 3 blocks makes no sense nor is it cost or time effective. Shouldn't a red line go one direction and a green line another with a map showing this? Don't empty buses mean basic changes need to be made? SMART is just too difficult to use.   | website | Ease of using/understanding system |
| 7-Mar-17  | Recently I have learned that SMART transit is considering changing the pickup and drop-off routes such that they would only do this on Parkway Ave not in the parking lot as currently providing. This is not acceptable to us who use the bus regularly. In addition to the inconvenience of extra distance and time to go to Parkway Ave it is also a safety hazard. There are no crosswalks on Parkway except in front of the medical training school. Can you imagine numerous people getting on/off and trying to cross in traffic?? Smart transit is financially supported by the local businesses. As such Flir should have some influential input as to preventing this schedule change. Please submit some kind of letter addressing this issue. Thanks.   | email   | Route 6                            |
| 7-Mar-17  | I'm hoping that SMART continue to route #6 and 2x into Flir and Rockwell Collins stops. I've been taking SMART for approximately 6 years now, and appreciate the pickup points off of the street, specially in inclement weather. In addition, it would be extremely dangerous for us to cross over the street to catch the shuttle during rush hours and being in a hurry ourselves. Please reconsider and continue to have the stops within the campus loop. Thank you for your time.   | website | Route 6 and 2X                     |
| 6-Mar-17  | Hello. I am a new resident to Wilsonville living here only two months. The entire reason to live here is because of the available transit system. I find it very difficult to use, even reading the schedule is difficult for me. I would prefer to see a loop used in town, so that no matter where I get on it stays on the same loop. I can get on and off at various locations to be able to access the library, Fred Meyer, and home again on Parkway. I don't want to change buses period, would prefer to stay on one but to get to my locations within Wilsonville. Why does the bus stop midday at City Hall? And then go nowhere?? That doesn't help me, I don't want to switch buses at City Hall. Please take this into consideration that some folks have no bus experience and need it to be as simple as possible.   | email   | Ease of using/understanding system |
| 3-Mar-17  | Smart the can not due transit bus system. They can not solve the problems they have now . They sure do not need more. They can not take care of their riders and seniors now. They will not eventry. This told to me buy operation officer. Hope they have fun.   | email   | complaint                          |
| 2-Mar-17  | The cost (over 6 million annually) to service this small community is outrageous. I do not see the value provided, and it is not covering costs with the fees to local businesses. Increasing the fees to businesses and the community is not the answer. Learn how to work with what you have like the rest of us do! Either decrease routes, frequency, etc. to meet your budget.   | website | Budget/spending                    |
| 1-Mar-17  | Dear City Council of Wilsonville and SMART transit, I am alarmed by the 2x SMART route proposal changes. I depend both on the regularity of the 2x, and its connection to Barbra Transit Center. The WES is not an option for me either. A change like this would likely mean relocation outside of Wilsonville. Please consider my concerns. Sincerely, J. Soul  | website | 2X                                 |
| 1-Mar-17  | Changes for the #6 Canyon Creek coming up in September Will they be providing shelter? How about people with disabilities? crossing Parkway Ave during rush hour?   | website | Route 6                            |
| 1-Mar-17  | We here at Rockwell Collins ride both #2x into work from Barbur and I/we ride 2x back to barbur. With that said eliminating Barbur you eliminate fare being paid IE generated and passes being paid for,also if you eliminate the stops going to WES at Rockwell Collins and Flir and Mentor graphics you have all these people potentially using the same crosswalk and the bus having to stop on Parkway(very bad options for SAFETY) People will most likely be running across a very busy road to catch bus. also if you move #2x to roads behind our businesses it would also be unsafe to walk or ride along Parkway to just get a bus to 96 stop. I say keep these 2 routes the same for us fare buying companies/People and we all will remain safe. Thankyou for letting us have a say in the up coming moves to better serve your riders of SMART (You all are much more efficient than TRI-MET) Thanks for listening J. Manser | website | Route 2X                           |
| 28-Feb-17 | On page 99, you propose changing the route of route 6 to eliminate the stops at Mentor Graphics, Rockwell Collins, Flir, etc. Patrons would have to walk out to the stop on Parkway Ave. Very bad idea! It doesn't seem like it would save much time or fuel. Additionally, it would put your ridership at risk during winter when the roadways and parking lots are icy and dangerous. It put patrons at risk in all seasons because they would have to cross Parkway Ave. to get to the southbound bus stop. Parkway is a very busy street and dangerous to cross. It would be worse in the winter months when it is dark outside and cars cannot see pedestrians crossing the street   | website | Route 6                            |
| 28-Feb-17 | I understand, even if I don't like, the desire to eliminate service to the Barbour transit center. This will make my commute from/to Portland longer. However, I do not understand why the new 2 route does not proceed down Parkway Ave. similar to the rerouted 2X. This forces many commuters to take the WES/MAX to Portland because the TriMet 96 option is now inaccessible to business along Parkway. It seems if you are attempting to support commuters, you would route the 2 to increase Portland travel options, not decrease them.   | website | Route 2X                           |
| 28-Feb-17 | Please keep rout 6 as is. It is not that much out of the way for them to come in the parking lots of Rockwell Collins, and Mentor Graphics and Flir. People would not have to cross the busy Parkway Ave in the evening to get to the WES. Also with peoples busy schedules, some work right up till the time when they have to run out the door to catch the bus. PLEASE keep it as it is.   | website | Route 6                            |

|           |  |                      |          |
|-----------|--|----------------------|----------|
| 28-Feb-17 | I ride the #6 nearly every work day. While I understand the reasoning behind the proposal to remove the run through the parking lot of the OIT/Rockwell, FLIR and Mentor Graphics, I feel it will reduce ridership. There is no crosswalk on Parkway at the OIT/Rockwell site. The nearest crosswalk is on Parkway and Boeckman Rd (0.5 mi). The riders in our area will have to walk a long distance in varying weather to make a safe crossing, or try to cross at a non-marked portion of Parkway Ave. It is an unsafe resolution and needs to be addressed. Will there be a crosswalk added on Parkway? What is the input by the City of Wilsonville on adding a crosswalk? The speed in front of our driveway entrance is 45 mph, and traffic is very heavy at times. I hope that all involved will be required to walk to the proposed stops from the front doors of Rockwell Collins, FLIR or Mentor Graphics before a decision is made.  | website              | Route 6  |
| 28-Feb-17 | Good Morning, My name is G. Smith, and I work for Rockwell Collins. I have been using SMART system buses (along with Tri-Met Buses, MAX and WES) every workday for the last 4 years. It is my understanding that SMART System management is considering re-routing their 6 Bus away from the main service road that connects Rockwell, OIT, FLIR, Mentor Graphics, et al., and instead keeping the buses on Parkway Drive, forcing riders who use that bus to cross a dangerous street without crosswalks, and hike as much as a quarter mile to their places of employment. It is my understanding that SMART service is funded in part by our employers - I am quite sure the intent was to provide reasonable service that would help ALL employees, including their own, to safely and quickly utilize public transit as an adjunct to their own efforts to create green communities. I feel that this kind of change runs counter to the intent of that funding, which in part was to assure all that transit is available and USABLE for their own employees. What ever benefit you might see in reducing the reach of the 6 bus will be cancelled out by the level of discomfort and difficulty riders will experience, and which will surely cause some riders to change their minds about using transit altogether. Convenience is key for us. Please, find some other solution. Keep the interior stops for the 6 Bus intact. Regards, G. Smith Rockwell Collins   | website              | Route 6  |
| 28-Feb-17 | Rerouting bus route 6 away from the OIT, Rockwell Collins, FLIR and Mentor Graphics parking lots does not make sense. All the riders from the above listed companies catch the bus in the parking lot area. I do not recall anybody catching the bus on Parkway Avenue. There is also a safety factor involved. Parkway Avenue is a narrow (two lane) 45 MPH road without shoulders. A bus stopping on this road will cause traffic to slow abruptly resulting in potential rear end collisions. Additionally, there are no crosswalks (marked or lighted) to ensure the safety of riders who must cross Parkway Avenue in the evening to catch the bus bound for the Wilsonville transit center. Rerouting the #6 bus is a disservice to the majority of the ridership on this route and creates a hazardous situation on parkway avenue, for riders/pedestrians, buses and other vehicles. R. Dean Rockwell Collins Employee #6 Bus rider (5+ years)   | website              | Route 6  |
| 28-Feb-17 | For some customer feedback from Rockwell Collins - we would vote to have the bus stop locations remain as is. We have 72 employees utilizing our bus fare program, and it is a great customer service to have a bus stop located in our parking lot.   | email                | Route 6  |
| 25-Feb-17 | I just want to say that a person who regularly rides 2X to Barbur Transit Center in order to transfer to either lines 12 or 94 to get to Downtown Portland, it may be a headache trying to catch those at Tigard TC especially when it adds additional times which can tax my time for getting to work or school. I know the community positioned this proposal to make better connections with Lines 45 (Garden Home-Portland), 76 (Beaverton-Tualatin), 78 (Beaverton-Lake Oswego), 93 (Sherwood), and 94 (Sherwood/Pacific Hwy-Express to Portland), but I don't like the change. If you have to make a change, at least make 2X an actual express bus; the fact it has a score or more of stops in town should just be delegated to either routes 4 or 6, because it takes that dang 2X bus 25 minutes to get out of town!   | website              | Route 2X |
| 25-Feb-17 | I have been a regular route 6 rider for the past year, and do not want to see the diversion from the current Mentor route. If there is a need to drop the second stop along Boeckman that may be appropriate, but having a protected (on-campus) stop on the west end has been a big aide to use of transit. Moving the stop to only Parkway would introduce the hazard of additional traffic in crossing Parkway, and leave the stop without any meaningful sidewalk for waiting at. I'm not even sure erecting a shelter would mitigate this risk, and likely would lead to a drop in ridership from the businesses. (In comment form, phone number should note dashes not be supplied.)   | website              | Route 6  |
| 24-Feb-17 | As an employee of Mentor Graphics who regularly relies on a combination of WES and SMART to get to work, I am opposed to the proposal to change the Route 6 to bypass the parking lots of Rockwell Collins, FLIR and Mentor Graphics for the following reasons: 1. There is no shelter on the Parkway Ave stops, and the stop at Mentor Graphics allows passengers to wait under the shelter of a Mentor Graphics building while waiting for a southbound trip. As any passenger who has had to wait in the rain can tell you, shelter is a necessity. 2. There is no crosswalk near the stop for Mentor Graphics/FLIR or Rockwell Collins that does not involve a lot of backtracking up and down Parkway. This is especially true for southbound trips to SMART central, which would mean either extra time required to reach the stop, or jaywalking across a very busy road. 3. The vast majority of riders I have met work at Rockwell Collins, FLIR, or Mentor Graphics. As our companies are providing funding to SMART as part of the payroll tax, we do not appreciate hampered service to companies whose employees regularly use Route 6. To summarize, I am opposed to the changes to Route 6 because they hinder the ability of employees of Mentor Graphics, FLIR, and Rockwell Collins to utilize Route 6 to get to and from work. Unless shelters are added at the stops, and pedestrian crosswalks are added to Parkway near each stop to accommodate southbound trips, I will continue to be opposed to these changes. | website              | Route 6  |
| 23-Feb-17 | Please don't change the bus route 6. I work at Mentor Graphics and the current stop is very convenient. I ride bus 6 everyday. Thank you.  | Ask the City Request | Route 6  |

|           |  |         |   |
|-----------|--|---------|---|
| 14-Feb-17 | Ms. Herman, CAT (City of Canby) does not operate the Wilsonville bus to Canby. SMART (City of Wilsonville) operates that service. We are aware that folks would like more midday service between Canby and Wilsonville. The City of Canby is in the midst of a transit planning process. Please check our webpage <a href="http://www.canbyoregon.gov/transportation/transitmasterplan.htm">http://www.canbyoregon.gov/transportation/transitmasterplan.htm</a> for more information. I will forward your comment to the plan email CATMasterplan@gmail.com so it can be included in the comments that are being collected about CAT services. I have also copied Stephan Lashbrook the Director of SMART so he will have your comments as well. Thank you for taking the time to let us know your service preferences. Your comments are very important to us. Regards, J. Wehling From: B. Herman Sent: Monday, February 13, 2017 5:49 AM To: J. Wehling Subject: Canby bus schedule Hello Julie, I take the 6:30 am bus daily to Canby. I finish work at 2:30 and then wait until 4pm to take the first return bus to Wilsonville. I have spoken with several other individuals who would be very interested in having a 3pm return bus from Canby to Wilsonville. I just wanted to let you know that there is an interest and a need for a 3pm bus and hopefully the schedule can be amended to make that happen. Sincerely, B. Herman   | email   | Route 3X  |
| 13-Feb-17 | Subject: Weekend bus service Hello, Stephan, I'm writing to support weekend bus service, especially to the Barbur Transit Center and to Charbonneau. Wilsonville is wonderful to its seniors, with many options available during the work week, and for that I am grateful. However, our lives are not suspended on weekends - we still need to get around (and for some, the weekends is just the time to do that). Funding is always an issue for public services, so I'm sure it's a consideration in this case, too. Please consider an initiative to fund weekend transit service - I believe you would find plenty of public support. And if there's anything I can do to further this cause, please don't hesitate to let me know. Regards, L.  | email   | Route 2X and Charbonneau - weekend service                    |
| 10-Feb-17 | Attn: Stephan Lashbrook, Transit Director Bryan Cosgrove, City Manager We have read that you are currently working on the Transit Master Plan for Wilsonville. Please consider the following ideas in your plans. Charbonneau is across the river from many services for our residents, and our area offers several activities for residents of Wilsonville to enjoy. We view the transit service as a great success for the area, however we believe that our area sometimes is not receiving the full benefits of the service. We need to have regular bus service on the weekends when some of our residents may want to attend church services or go shopping at the stores or go to a movie or have lunch. Also, some of our residents are employed at companies that are open on the weekend. Without a car they are unable to keep their work schedules unless they have access to the SMART bus service on the weekends. I have a few elderly friends who would appreciate access to the SMART bus at easily accessible stops in various places at Charbonneau so that they can walk to the bus from their home, get onboard and go to "the city". It could be a shuttle bus that is regularly scheduled a few times a day—both going to the city and returning to the closest bus stop to their home. The Country Club at Charbonneau could even be one of the stops close to Charbonneau's center and could bring residents on the "city side" of Wilsonville over to Charbonneau for golf or to visit a friend or to enjoy our village center dining and services. Thank you for all you are doing to improve our city. K. and A. Miller Wilsonville (and Charbonneau) residents for 23 years | email   | Charbonneau - weekend service                                 |
| 6-Feb-17  | Thank you for asking. This is an overwhelming amount of information. I waded through the first 50 pages... the relevant nub in summary format would be helpful. Unable to make any useful comment as I was unable to complete reading or fully digest the content.   | website | TMP format- too long  |
| 6-Feb-17  | With a quick scan, I did not see any provisions to increase bike lanes. With the addition of a middle, then an elementary school on Advance Road; there should be a bike lane on Advance Road to enhance student safety going to and from school. If I missed this in the plan, please tell me the pages where it is described. Thank you! L. Smith  | website | Bike - increase lanes/safety                                  |
| 2-Feb-17  | Hello ! I do not use the SMART for local rides as we are located in very close proximity to the Wilsonville road Loop. But the last time I explored the option to take a combination of bus and train to the PDX, I gave up and just drove to the airport and parked at the economy long term parking. The hassle of the traffic on I-205 is less than the more than two hours needed to navigate the SMART system to get to the airport. This assumes the bus is on schedule. I am sure the other southern suburbs have the same issues. But maybe Wilsonville will be the first to address this and really find a solution. Looking down a 3 to 5- year time frame, do you think we will be able to get to the airport by public transport like the one-hr Max Red Line ride from Beaverton to PDX ? In short, is there enough volume to justify adding a Red Line that cuts through Lake Oswego, Tualatin, Wilsonville and perhaps also Canby ? Jimmy Lee Brenchley Estates - North Wilsonville, OR   | email   | MAX service   |
| 31-Jan-17 | If a segment of Route 2 went down Canyon Creek Road to TC Loop East then to Parkway (or vice versa) it would include the apartments and senior centers along Vlahos and the Post Office and come closer to Clackamas Community College. A segment of Route 5 or Route 6 should consider using the new Kinsman Road extension between Barber and Boeckman, then down Boeckman to 95th. With Frog Pond getting close to adoption, shouldn't the MP include a possible service route to this area?  | website | Route 2X, 5, 6  |
| 30-Jan-17 | It would be helpful if there was a 1x shuttle some time around noon Monday through Friday. I don't think a full size bus would be required. I have sense of being "stuck" in Salem with the current 1x schedule.   | website | Route 1X - midday service (shuttle)                           |
| 30-Jan-17 | Encourage advocate weekend bus service in Charbonneau for myself and several of my neighbors.  | website | Charbonneau - weekend service                                 |
| 27-Jan-17 | Expanding the Villebois Shopping Shuttles hours, even from 8am to 5pm, would be a great help. I take this bus, when I can, to work and to shop but it doesn't always fit into the hours needed. I would also love to see it service more of the north side of Villebois. I live off of Boeckman Creek and Villebois Dr. N and the walk to the nearest stop is almost a mile away. Hard when you have an arm load of groceries. I love Wilsonville and SMART is one of the perks that makes this a great place to live. Thanks for all your hard work. J. Willeford   | website | Villebois Shopper Shuttle - extend hours and northern service |



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# TRANSIT MASTER PLAN

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## PUBLIC OUTREACH SUMMARY

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Winter 2017  
(Revised 2/6/17)



## Public Outreach Summary

### INTRODUCTION

In January 2016, South Metro Area Regional Transit (SMART) formally began updating the 2008 Wilsonville Transit Master Plan (TMP). The TMP is a broad look ahead to the type of transit system and supportive transportation options required to meet Wilsonville's mobility needs.

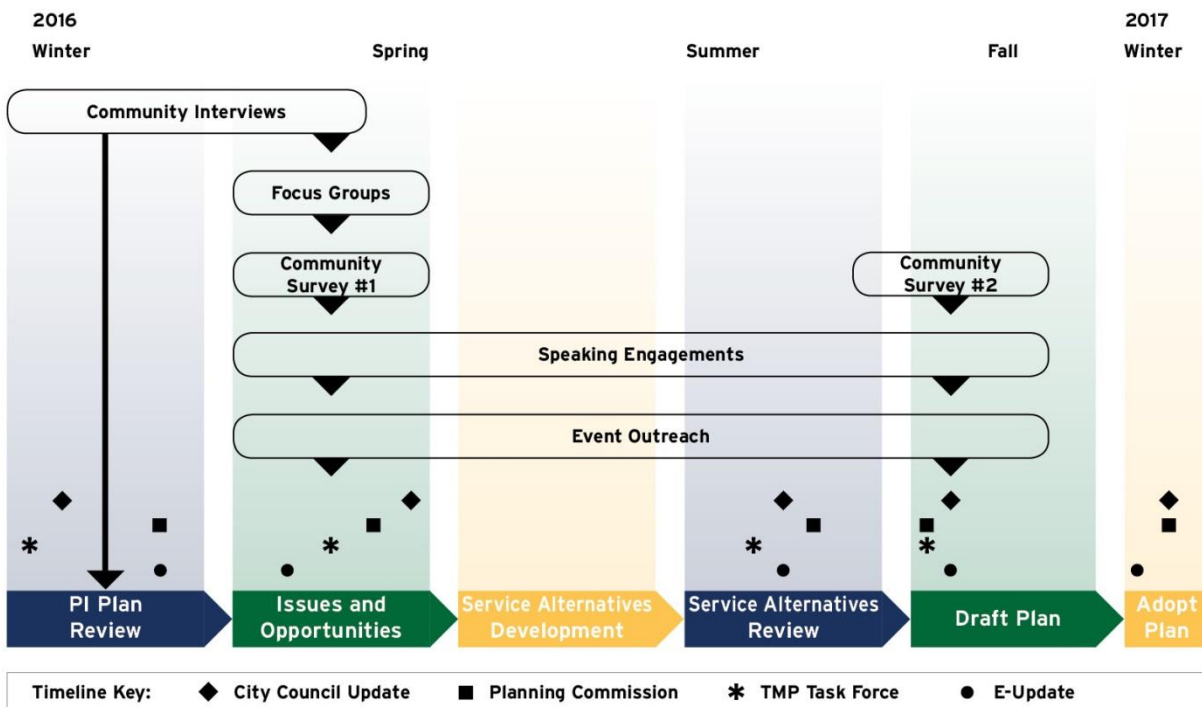
From the outset, SMART directed a process to involve a diverse and broad spectrum of existing and potential system users, and the businesses that support transit through payroll taxes.



*Outreach Photos*

## Three key phases:

- 1) **Issues and Opportunities Identification** *Winter 2016*  
 Informed community partners and stakeholders of the project purpose and framework, and listened to their perspectives on where to focus SMART resources.
- 2) **Service Alternatives Development** *Spring 2016*  
 Based on input from the Issues and Opportunities phase, SMART service planners and Jarrett Walker and Associates developed draft service alternatives.
- 3) **Service Alternatives Review** *Summer 2016*  
 Review by community partners and stakeholders via multiple and accessible outreach methods of the proposed service alternatives and potential funding strategies.



## Values guide involvement:

**Realistic:** be clear about the project constraints, objectives, and parameters

**Inclusive:** reach out to everyone, including those who don't use computers or face other barriers

**Meaningful:** provide timely information that is accurate and easily accessible

**Transparent:** make decisions public with key project materials available



## To identify issues and opportunities SMART asked:

### *What are your priorities when you can't have it all?*

This frame helped SMART stakeholders to understand the trade-offs SMART faces in delivering transit services with available resources. Public responses grounded and illuminated the issues and opportunities facing Wilsonville and set the stage for transit service alternatives development. These options will be available for community consideration in summer and fall 2016.

## OUTREACH APPROACH

- Consistent, reliable, accessible information with identified SMART contact person
- Thorough stakeholder analysis
- Draft Public Involvement Plan vetted with key stakeholders
- Representative stakeholders interviewed from throughout Wilsonville
- Small issue-focused meetings held
- Non-traditional outreach targeted to those with low-income, people with disabilities, young and elderly people
- E-updates to Interested Parties List
- Timely response to all submitted comments and questions
- Electronic kiosks with information and survey placed at high visibility locations
- Speaking engagements at civic organizations
- Updates to relevant committees and City Council
- Transit Master Plan website with comment and survey functions
- Media (print, TV)

## OUTREACH METHODS

Beginning in spring 2016, staff used a series of methods to adhere to SMART's value of inclusion.

### Project Website Development

A bright, inviting, and accessible website provides community members a one-stop location to learn about the project, see upcoming events, project documents, available input opportunities, and option to sign up for project updates.



## Community Survey #1

An informative and brief survey posed questions on the key tradeoffs for SMART to consider in service alternatives development including:

- **Which types of trips do you currently use transit for?**
- **Which types of trips would you want to use transit for, but currently cannot?**
- **Are short or long transit trips more important to you?**
- **At what times should SMART primarily target services?**
- **Transit can be spread out, to get close to everyone, or it can be concentrated into frequent routes. Which would you rather do?**

Launched on March 1, 2016 with pro-active outreach efforts, the survey received **over 800 responses** via a variety of methods including on-line, mobile kiosks, and in-person paper surveys.

## Community Survey #2

SMART launched a second survey on August 8, 2016 to illustrate these service tradeoffs and ask about a spectrum of potential service alternatives to gauge the public's priorities:

- **How to balance ridership and coverage goals?**
- **How to balance rush-hour service with service other times of day and week?**
- **How to balance local and intercity (out of town) services?**

Over 500 respondents provided feedback. See Appendix A for a complete list of locations and community partners instrumental in a successful survey response rate.

The image displays two screenshots of the SMART Community Survey #1 interface. Both screenshots show the survey title, a question about priorities, and a list of trip types. The top screenshot shows the survey title, a question about priorities, and a list of trip types. The bottom screenshot shows the same survey but with a list of trip types and an 'Other' field.

Home » Participate

Take five minutes for the SMART Community Survey #1

SMART

*What are your priorities when you can't have it all?*

Wilsonville needs to hear what you care about the most to update the Transit Master Plan to best serve our community. Please take a few minutes and provide your thoughts on the following questions to assist in the development of service design options. What we learn from this survey will be available for community review this summer. This important community feedback will inform and help decision-makers understand the choices for SMART resources.

Thank you!

For more information or to do this survey at another time, please go to: [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

Think SMART. Ride SMART. Plan SMART.

Q1: Please tell us about yourself. Which types of trips do you currently use transit for?  
Please click all that apply:

- Work commute

Home » Participate

Take five minutes for the SMART Community Survey #1

SMART

*What are your priorities when you can't have it all?*

Wilsonville needs to hear what you care about the most to update the Transit Master Plan to best serve our community. Please take a few minutes and provide your thoughts on the following questions to assist in the development of service design options. What we learn from this survey will be available for community review this summer. This important community feedback will inform and help decision-makers understand the choices for SMART resources.

Thank you!

For more information or to do this survey at another time, please go to: [www.plansmartwilsonville.com](http://www.plansmartwilsonville.com)

Think SMART. Ride SMART. Plan SMART.

Q1: Please tell us about yourself. Which types of trips do you currently use transit for?  
Please click all that apply:

- Work commute
- School commute
- Recreation
- Errands (shopping), meetings, or events
- Medical
- Regional travel

Other

If using the mobile kiosk, please click the bottom right keyboard icon to return to the survey.

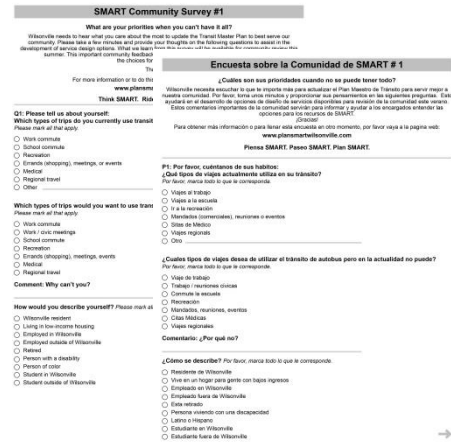
## Mobile Kiosks

Electronic kiosks placed at high visibility locations such as Wilsonville City Hall, Wilsonville Public Library, Oregon Institute of Technology, and Wilsonville Community Center made input easy for stakeholders on the move.



## Paper Surveys

Bi-lingual surveys accompanied the mobile kiosk stands and were distributed to large employers, schools, higher education institutions, large housing complexes, and in partnership with community organizations.



## Informational Bookmark Tear-Offs

In partnership with local businesses, community organizations, and city services, over 8,000 tear-offs were distributed throughout the community and on SMART buses. Brightly colored and easily accessible, these bookmarks connected mobile devices to the survey via code scan.



## Display Outreach

Project staff conducted survey outreach at retail outlets, WES Station, SMART Central Station, employee break rooms, educational institution lobbies, and more.

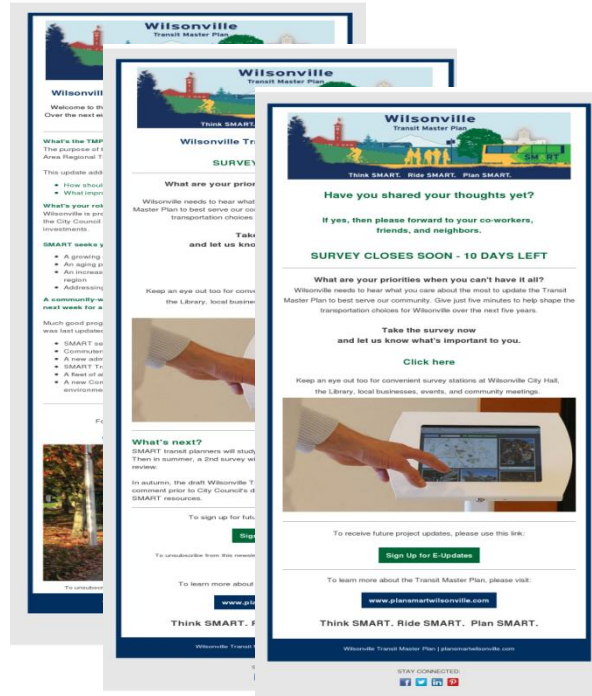


## E-Updates

With the TMP's Interested Parties List of **over 1,300**, the project sent E-Updates announcing:

- Project Overview
- Survey Launch #1
- Survey Reminder
- Survey Launch #2
- Survey Reminder
- Draft TMP Announcement & Public Hearings Notification
- City Council Action (to be sent late March 2017)

City departments, community businesses, faith-based organizations and other partners assisted in forwarding notices to their networks of residents, employees, students, and congregants.



## Media

The Wilsonville Spokesman reported twice about the project launch and announced the on-line survey.

Using the city wide monthly newspaper, Boones Ferry Messenger, SMART announced and reminded residents of survey availability.

## Stakeholder Interviews

Project staff met one-on-one to interview and explore further the tradeoffs facing SMART. Participants included:

- Businesses
- Elected Officials
- City Staff
- Community Groups
- Educational Institutions
- Home Owner Associations

## Workshop and Targeted Focus Discussion Groups:

(See Appendix B for summaries.)



## Community Workshop

Project Team and consultants held a large stakeholder workshop with community representatives to provide the necessary understanding of transit planning for well-informed decision-making and a community supportive Transit Master Plan for Wilsonville.

## Title VI/ Environmental Justice (EJ) Populations

To ensure that the impacts and benefits of the Wilsonville Transit Master Plan are equitably experienced regardless of race, national origin, gender, disabilities, English language proficiency or being low-income, project staff sought initial input on outreach methods with community organizations and conducted two targeted discussion groups:

- Title VI Focus Group (Low-income and Spanish Speaking)
- Older Adults and People with Disabilities



## Business Discussion Groups

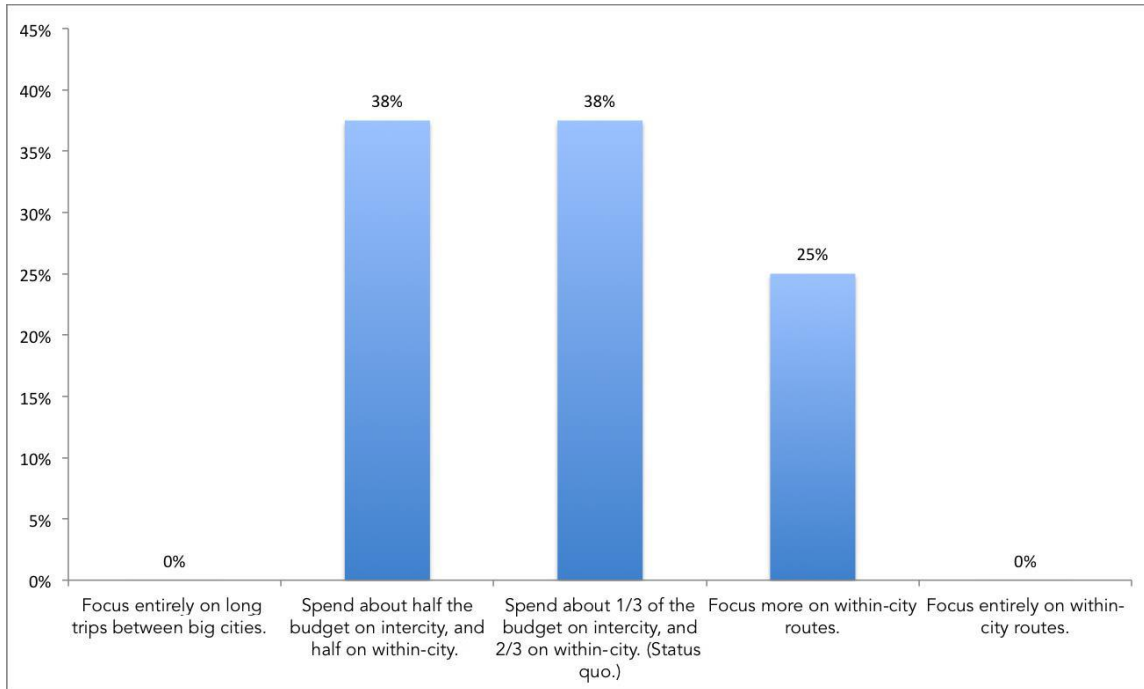
Coordinated in partnership with the Greater Wilsonville Chamber of Commerce, two groups of business representatives met with project staff to discuss trade-offs, issues, and opportunities.

## SUMMARY OF COMMUNITY INPUT

Project Team and consultants created a variety of venues for interaction with diverse stakeholders, who were polled on some of the trade-offs addressed in the online survey. What follows is a summary of input collected from the Wilsonville TMP Community Workshop, community-wide survey, stakeholder interviews, and targeted focus groups.

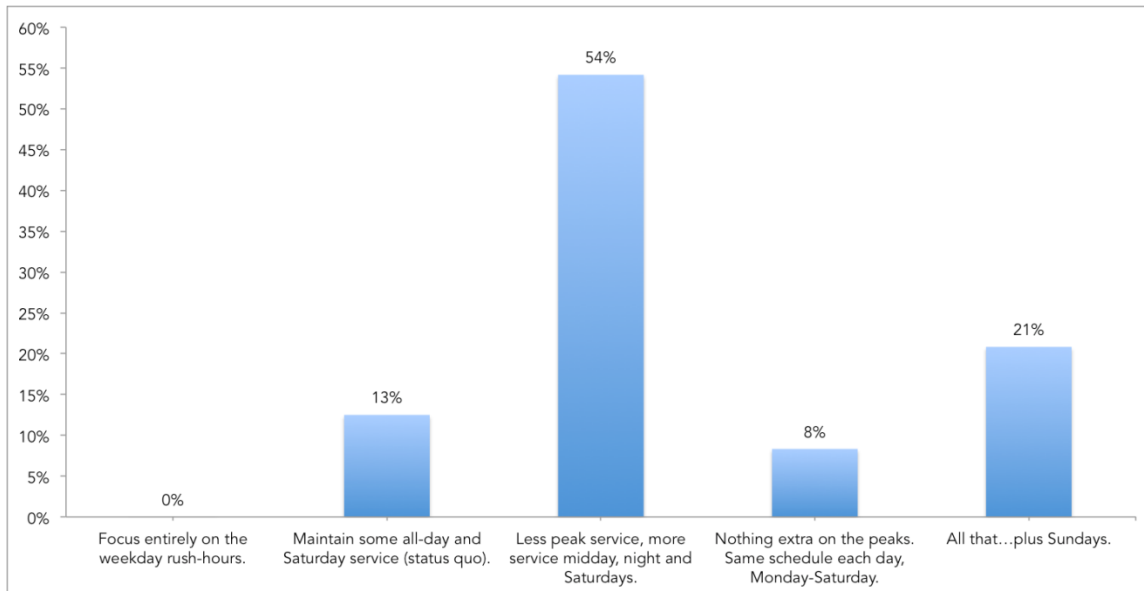
## Community Workshop Results

***Should SMART focus on inter-city services, or within-city services?  
(Similar to Q2 from the survey.)***



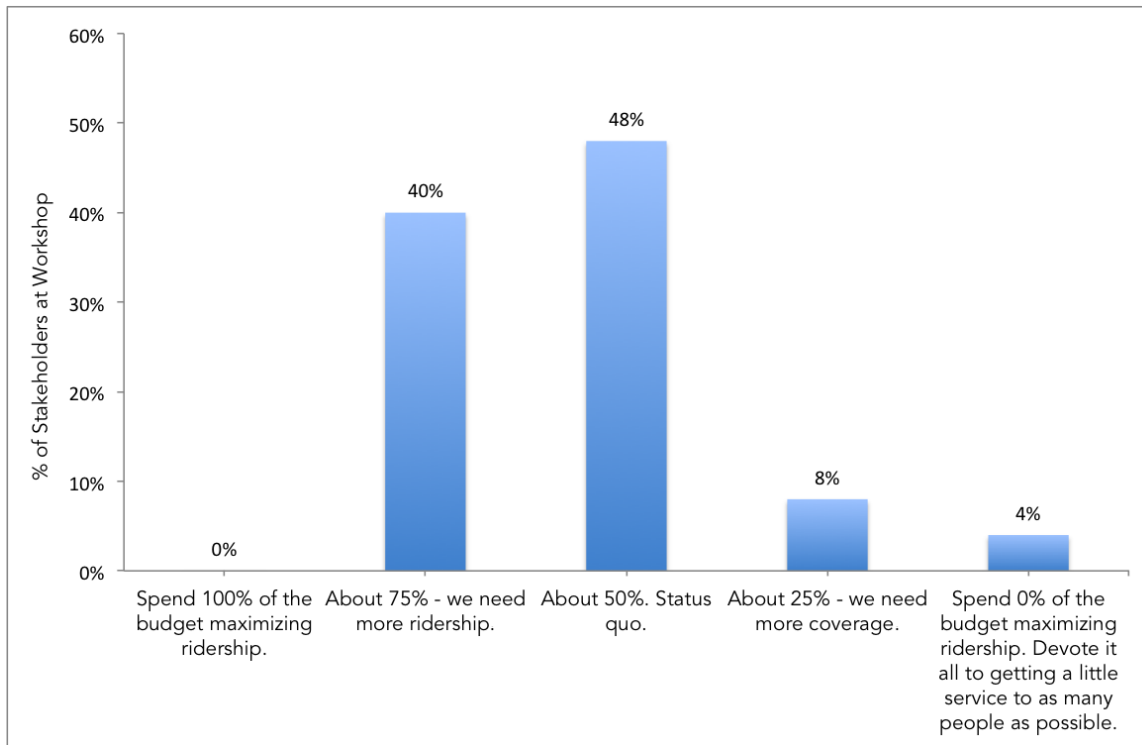
Stakeholders almost equally split on whether there should be more intercity or more local service emphasis. However, 38% were happy with the status-quo balance.

***Should SMART focus more on rush-hours or more on all-day all-week service? (Similar to Q3.)***



Stakeholders expressed fairly strong support for a move away from rush-hour service and towards flatter, all-day all-week schedules.

**How should SMART balance maximizing ridership and providing coverage?  
(In practice, this means balancing frequency with walking distance, similar to Q4.)**



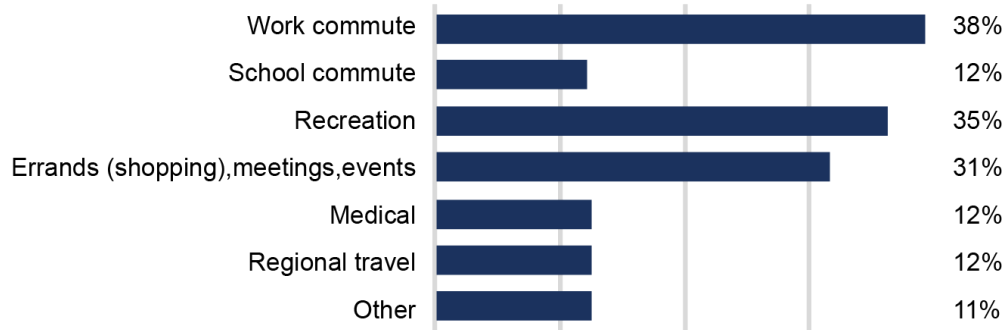
After a long conversation about what leads to high ridership and the trade-off between having few frequent routes or many low-frequency routes, the stakeholders were polled on how to balance these competing goals.

While nearly half of stakeholders expressed support for the current balance of maximizing ridership vs. providing coverage, 40% want to see more focus on ridership, and therefore less coverage.

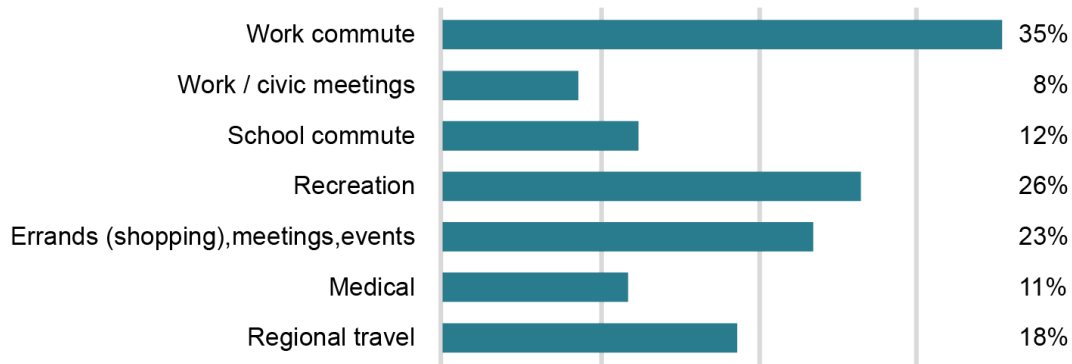
## Community Survey #1 Results

During a period of six weeks, the TMP project staff received 772 survey responses through the website and bilingual paper surveys.

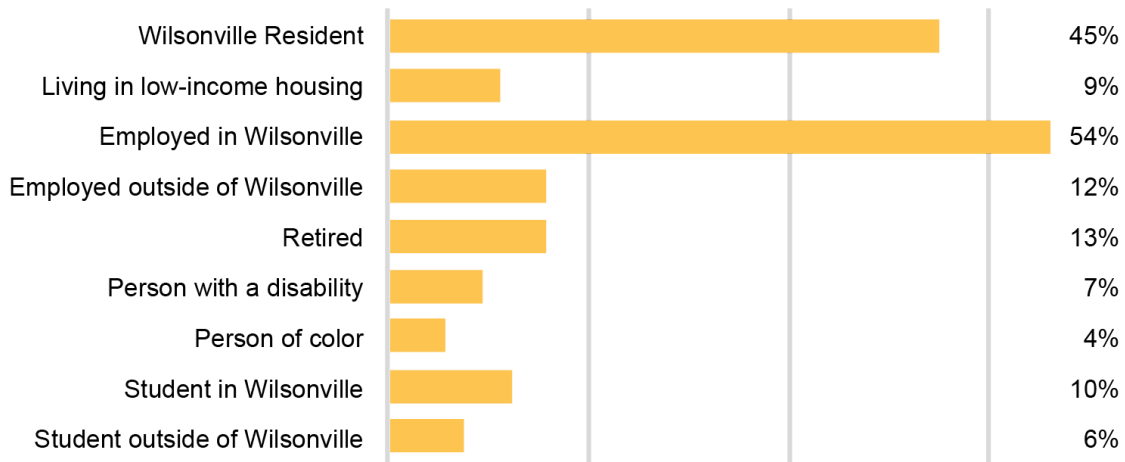
### Q1a) Which types of trips do you currently use transit for?



### Q1b) Which types of trips do you want to use transit for but currently cannot?

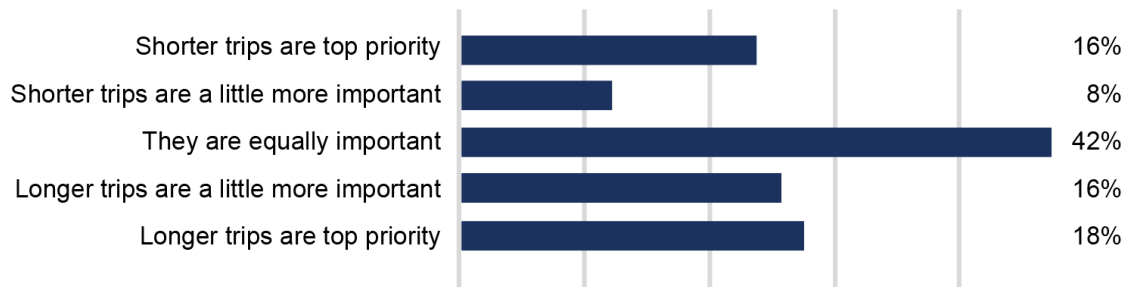


### Q1c) How would you describe yourself?



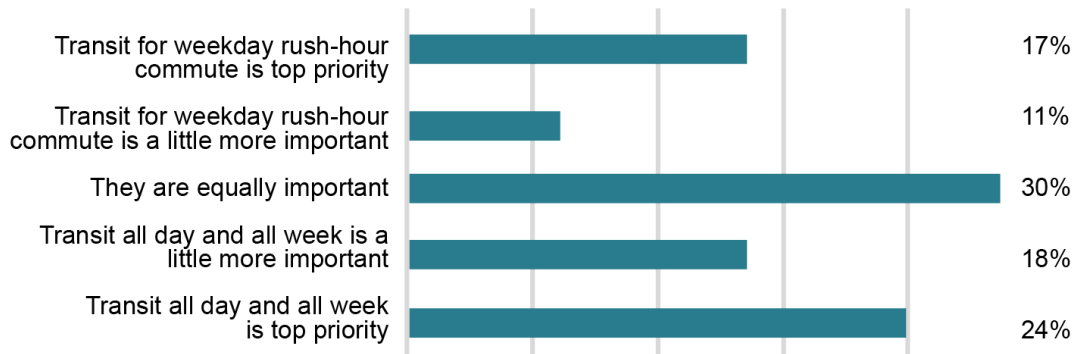


**Q2) People make both short transit trips in town and longer transit trips to other cities in the region. Which is more important to you?**



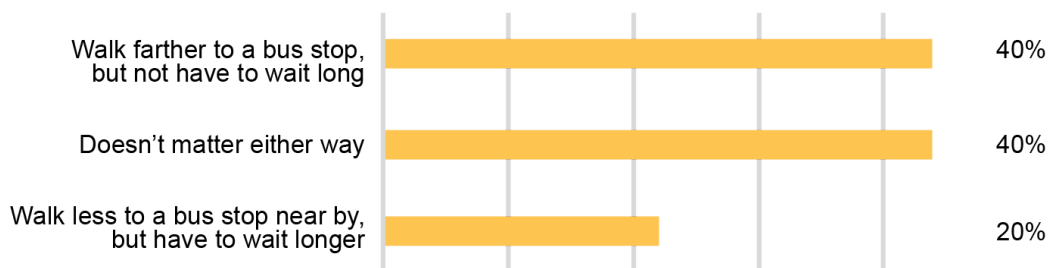
Slightly more survey respondents said that they think longer trips are more important to serve with transit than short trips.

**Q3) Some people commute to work during rush-hour for 9-5 jobs, while others work different schedules. People could use transit for other kinds of trips besides their work commute, as well as during evenings and weekends. At what times should SMART primarily target services?**



More survey respondents expressed support for a shift towards all-day and all-week service than for maintaining SMART's current focus on weekday rush hours.

**Q4) Transit can be spread out, to get a little close to everyone, or it can be concentrated into frequent routes. Help us understand your preference.**



Short waits are only possible, within a fixed budget, if an agency provides less coverage and therefore more frequency. For local transit, frequency is key to high ridership. This survey

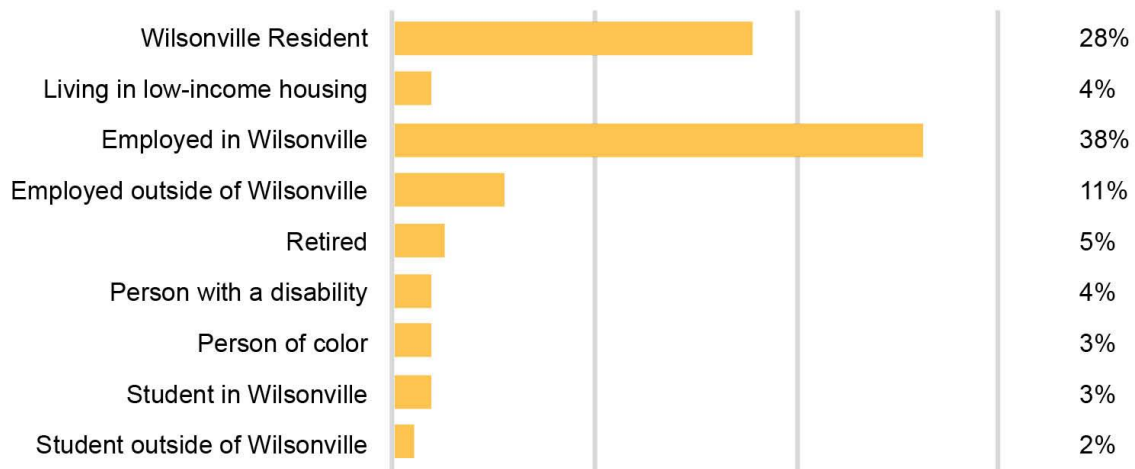
question is about the *individual* experience of a high ridership transit system versus a high coverage transit system: in a high ridership system, waits are short but walks are longer.

## Community Survey #2 Results

Note: Questions #4 & #6 begin on page 36

The Service Alternatives Survey administered on line, via electronic kiosks, and at community events received 550 responses. The key question aimed to understand the community's balance between intercity focused and locally-focused service alternatives. The response data shows the majority is employed in Wilsonville and suggests a favoring towards more intercity service.

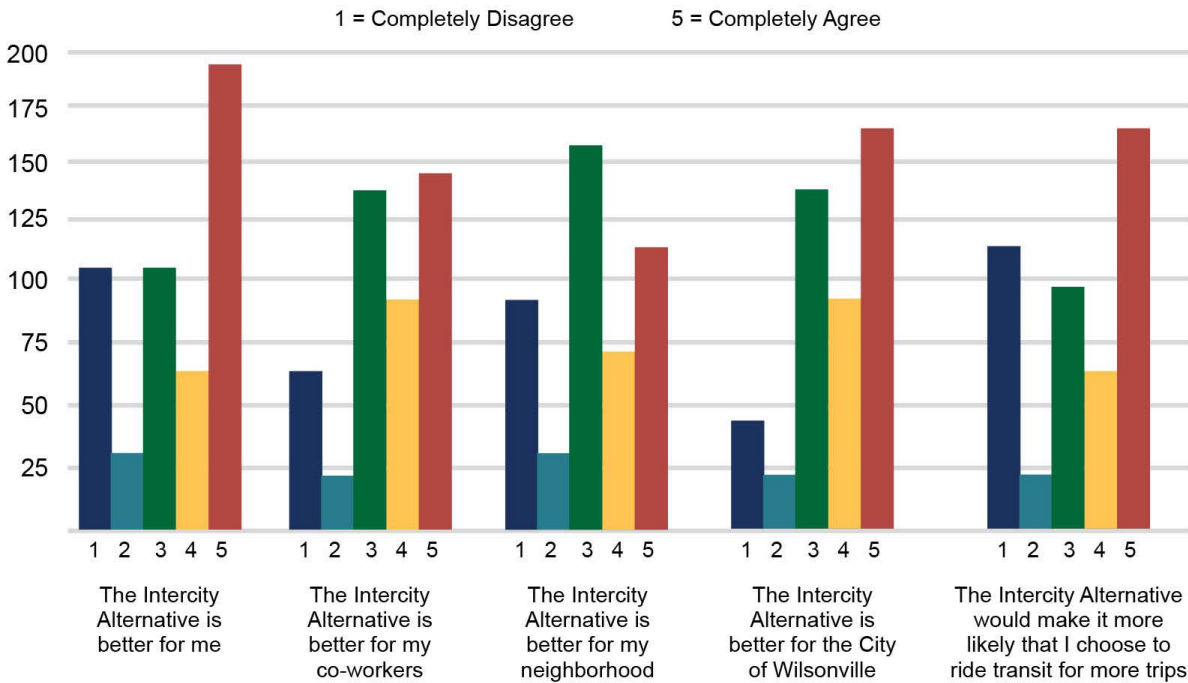
### Q1) How would you describe yourself?



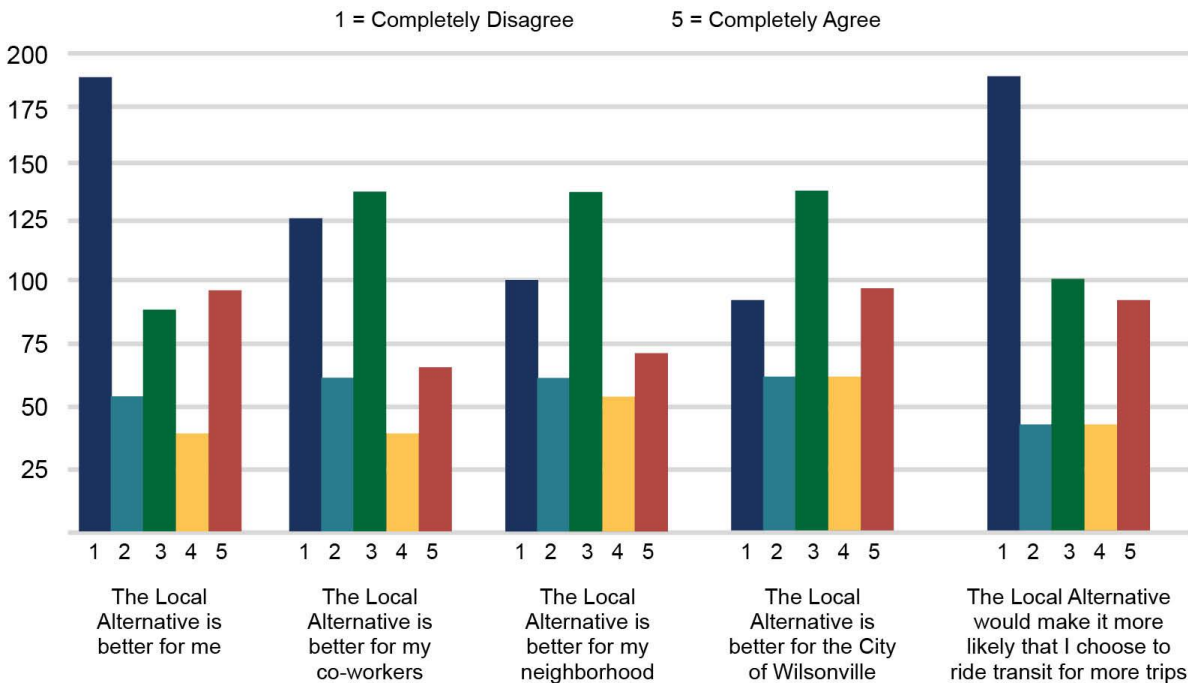
### Q2) Did you participate in the first survey?



**Q3) For the Intercity-Focused Alternative, please rank the questions below on a scale of 1-5.**



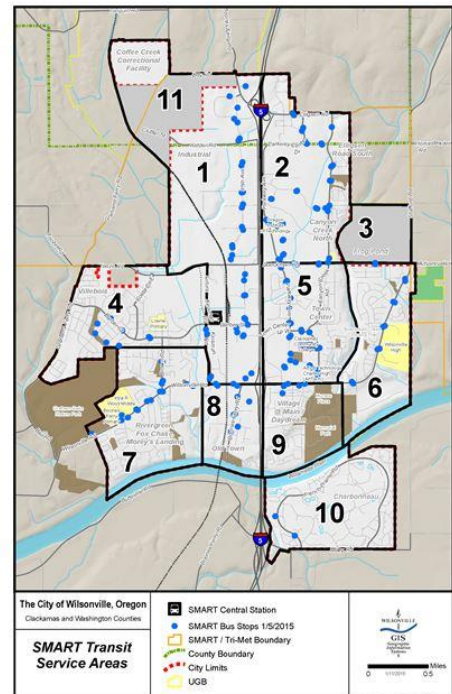
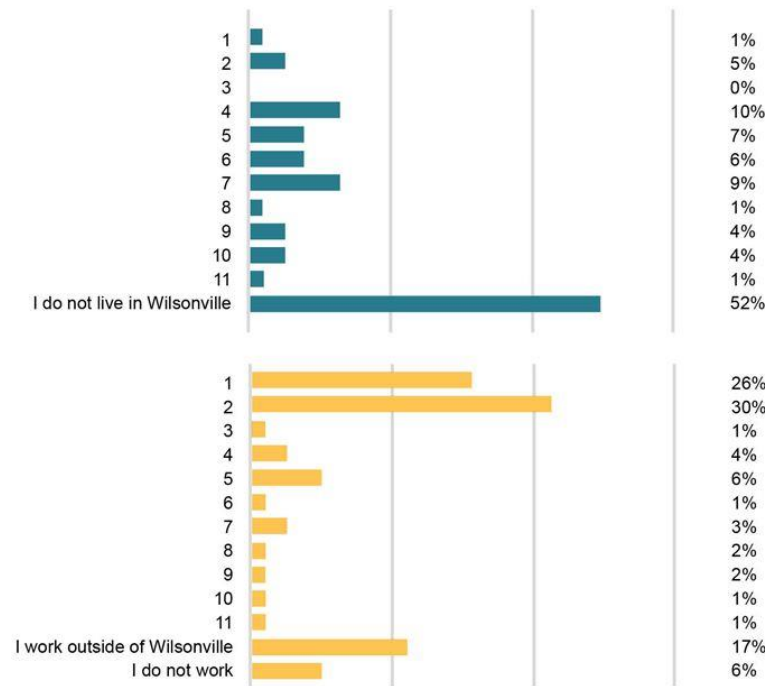
**Q5) For the Locally-Focused Alternative, please rank the questions below on a scale of 1-5.**



**Q7: Which of the below options comes closest to the balance between Intercity-Focus and Local-Focus that you prefer?**



**Q8: Using the service map, tell us where you live and work:**



## Stakeholder Interviews and Focused Discussion Results

### Issues and Opportunities Key Themes:

#### Support the economy

- Tourism could be served well with more routes on weekends to Salem and PDX
- More access from airports and other cities important to Wilsonville
- SMART'S role is to provide Wilsonville with a work force
- Important to serve workers in and out of community

#### Enhance connectivity

- Proximity of stops near schools for new middle school
- Oregon City is critical location for social services
- Plan for transit in new developing areas
- Target high density neighborhood and growth in industrial areas
- Stronger connections to Portland
- More WES service
- East to west within Wilsonville take much too long

#### Expand service times

- Traditional 9-5 no longer the norm for businesses with multiple shifts
- Need later service for evening shifts
- More times to address after-school needs
- Weekend service
- Quick service between transit and higher education facilities
- Priority should be given to the shift times with the highest number of workers

#### Provide additional transportation options

- Promote ridesharing (vanpooling/carpooling)
- Explore additional car sharing opportunities
- Explore bike sharing

#### Get the word out

- Lack of awareness that service is free within Wilsonville by English and non English speakers
- Schools, students, and families are not aware of free service
- Travel training needed
- Need to work with employment agencies to reach daily in-flux of temporary workers

#### Create partnerships

- Use back-to-school information to highlight SMART services to parents and students
- Include in admission and orientation packets to higher education facilities
- Explore increases in vanpooling

- Create stronger partnerships with Tri-Met and SAMTD for more transportation options

### Explore additional funding mechanisms

- Fix the transit districts' boundaries to match location of businesses
- Businesses are dealing with minimum wage increases
- People wanting more service causes businesses concern about financing

Focus group participants named a number of transit outcomes that relate to either maximizing ridership (target high density and move large numbers of workers) or to providing coverage regardless of ridership (provide access to social services and new developing areas). In the focus group setting, we did not ask them to resolve this conflict. Insight into how people want SMART to resolve these conflicting goals may be better gleaned from the online survey and the stakeholder workshop, where respondents were asked to make a trade-off.

## Considerations for Service Alternatives Development

The input of the online survey respondents, stakeholder workshop attendees, stakeholder interviewees, and focus groups participants suggest that SMART could shift its priorities slightly, on two key trade-offs, with public support:

- **The importance of rush-hour, relative to the rest of the day.** Only 28% of online survey respondents said that service during the peaks is more important than the rest of the day and week. Similarly, of the polled stakeholders, only about 13% supported the status-quo level of emphasis on peak service; the rest wanted to see a small or major move towards flatter, all-day all-week schedules. People who participated in focus groups expressed a great deal of interest in service outside of rush hours.
- **How to balance providing frequency to maximize ridership, and providing low-frequency coverage services to more places.** A large minority of stakeholders at the workshop asked for a shift towards higher frequencies and/or higher ridership. Among survey respondents, about twice as many people prefer short waits than short walks, suggesting higher support for a more frequent network than an infrequent network that gets close to everyone.

On the matter of *local vs. intercity trips*, only a slightly larger number of survey respondents and stakeholders expressed support for longer, intercity trips than short, local trips.

### Service Alternatives Key themes:

#### Preference for more intercity service

- Connections to Oregon City remain critical

- Proposed frequencies may be challenging for higher education students
- Focus on rush hour does not apply to many of Wilsonville employer shifts

### **Transportation Options**

- Explore additional options like ridesharing, car and vanpooling, bike sharing, car sharing, and ridesourcing to augment SMART services
- Involve Chamber of Commerce in this conversation

### **Considerations for local service**

- Need to continue to serve low-income apartment complexes
- More service for arts and culture sites

## **APPENDICES**

### **Appendix A**

#### **Outreach Overview**

Outlined are the stakeholder groups reached, by what methods, and verbatim survey comments received as part of the Wilsonville Transit Master Plan’s Public Involvement.

#### **Mobile Kiosk Placement with bilingual paper surveys**

City Hall  
 Wilsonville Public Library  
 Boones Ferry Elementary School  
 Clackamas Community College

#### **Display Outreach (\* indicates multiple sessions)**

Wilsonville Community Center\*  
 SMART Central Station\*  
 Thriftway  
 Safeway  
 Oregon Technical Institute\*  
 Town Park Center  
 Wilsonville Farmers Market\*  
 Charbonneau Country Club  
 Montague Park  
 Wilsonville Community Sharing\*  
 Sofia Park  
 Autumn Park  
 Oregon Technology\*  
 Costco

Creekside Apartments  
Microsoft  
Fred Meyer  
Mentor Graphics\*  
Pioneer Pacific College  
Vision Plastics  
Flir  
Target  
Edge Family Fitness  
Bullwinkle's Family Fun Center

### **Paper Survey Outreach**

Convergys  
Vision Plastic  
Gillespie Graphics  
Xerox  
Mentor Graphics  
Clackamas Community College  
Autumn Park (Spanish survey distribution)  
Valley Christian Church  
Saint Cyril Catholic Church  
SMART Central  
Shopper Shuttle

### **Informational Bookmark Tear-Offs**

Over 10,000 distributed throughout Wilsonville at key locations including:

SMART buses  
Wilsonville Public Library (as bookmarks)  
Fred Meyer  
Qdoba (stapled to all "to go" orders)  
Large and small employers

### **Community Workshop**

Convergys  
Xerox  
Mentor Graphics  
Coffee Creek Correctional Facility  
City of Wilsonville Community Development  
City of Wilsonville Community Planning  
City of Wilsonville Planning Commission  
SMART Transit Mater Plan Task Force  
Oregon Institute of Technology  
Clackamas Community College  
Wilsonville Chamber of Commerce  
Wilsonville Public Library Foundation



Charbonneau Home Owners Association  
Charbonneau Country Club  
Mayor Knapp  
Councilor Stevens

**Issues and Opportunities Small Group Discussions:**

Business Community

Republic Services  
Hasson Company Realtor  
Pacific Residential  
IBEW Local 48  
Centrex Construction  
Columbia Bank  
Langdon Farms  
Bittner & Hans  
Crowley Landscaping Management  
Washington County Planning Commission

Title VI & Environmental Justice (Low income/Limited English Proficiency)

Lowrie Elementary School  
West Linn Wilsonville School District  
Northwest Housing Alliance Coordinator  
Wilsonville Public Library Spanish Outreach Coordinator  
SMART Bilingual Driver  
Clackamas County Social Services Developmental Disabilities Coordinator  
Clackamas County Social Services Children, Youth, & Families  
Creekside Woods

Older Adults / ADA

Clackamas County Social Services Volunteer Connections  
Creekside Woods Senior Housing  
SMART Paratransit  
City of Wilsonville ADA  
Wilsonville Community Center  
Wilsonville Community Sharing

**Service Alternatives Small Group Discussions:**

Note: These groups were combined in order to foster understanding of competing transit needs.

Convergys  
Wilsonville City Councilor  
City of Wilsonville Planning Department  
Xerox Corporation  
Oregon Institute of Technology

Landgon Farms Golfcourse  
Clackamas County Children and Youth Services  
Clackamas Community College  
Transit Master Plan Task Force  
Wilsonville Planning Commission  
Wilsonville Art and Culture Council  
Greater Wilsonville Chamber of Commerce  
Wilsonville Development Review Board Committee  
Wilsonville School District Community Services  
Wilsonville Library Foundation  
Wilsonville Parks and Recreation  
Wilsonville Community Sharing

### **Stakeholder Interviews**

Wilsonville Youth Sports  
City of Wilsonville Councilor Lehan  
City of Wilsonville City Manager  
City of Wilsonville Public Works  
West Linn Wilsonville School District  
Pioneer Pacific College  
Microsoft  
Fry's Electronics  
Wilsonville Holiday Inn  
World of Speed  
Employment Express / Tualatin Chamber of Commerce  
American Family Insurance

## **Appendix B**

### **Issues and Opportunities Small Group Discussion Notes**

Title VI #1 Low Income and Limited English Proficiency  
March 29, 2016

#### **1. What type of trips do you and the community you know currently use SMART for?**

Medical appointments within and outside Wilsonville.  
Errands or shopping.  
School for non-driving teenagers, but walking is quicker than the bus. Takes an hour.  
Services are on the Red Soils Campus in Oregon City, energy assistance, WIC appointments, behavioral health, housing.  
Lowrie Primary School- Very little, but occasional to clinic in Woodburn, some library (mainly people don't know about SMART) Serve bi-lingual for all Wilsonville and parents cannot get there to participate.  
Library staff uses it for work commuting.

As a driver---mostly errands and shopping, a few kids ride for school (mainly younger who can't walk), going to college or work out of town.

## **2. What type of trips do you want but currently cannot use?**

Recreation.

Not convenient, takes too long.

Doesn't run late on Friday nights (example after high school football games).

Spanish speaking families don't know about SMART.

Need a BUS to Oregon City-direct—not commuters—but to services at Red Soil campus.

Folks do not have access to reliable cars.

Increase service to Canby where you can switch to Canby to Oregon City. I can't do that because the appointments are in the off hours—not commute hours. SMART to Canby Transit to Oregon City, but too many transfers and time to get up hill. The social services I go to are in Canby. Very difficult to get to Oregon City.

Language barriers—not just Spanish. Don't know about service or that it is free.

Safety for kids. Are drivers trained for dealing with kids? Is it a safe walk to and from the bus?

Teachers want to educate about city services. In the past, teachers have taken students on the bus to see their city. Kids taught how to read the signs, visit a bakery. Should be teaching young people how to use transit and about their city.

School boundaries are being redrawn. Lowrie is full but demand for dual language is increasing. New middle school being built at Frog Pond.

Want to go to green spaces.

This Clackamas Community College (CCC) can't get to other campus. Even harder with disability.

Wilsonville doesn't have many social services—Oregon City has more programs and services.

Takes too long to use transit. Stops running too early. Have it run longer at least Friday evening so kids can participate. East to west is very hard.

Getting to the clinic in Woodburn.

## **3. Short or long trips?**

Depends on the population. Longer trips may be more important to get to medical appointments but shorter trips to grocery store.

Freedom and mobility makes them equally important to seniors.

Equally important to seniors and families.

Yes it takes longer but I don't have to drive.

More community building value with short trips that you can get around town, even though I commute from Portland.

Shorter trips here in town. More people would use transit if it were frequent enough.

Folks going in and out of town have set schedules and can plan ahead for that as opposed to folks within town where trips are fewer.

For undocumented workers, if it were more convenient and more frequent. People are terrified to get behind the wheel but have to go to grocery or get kids.

If we could get easily to multiple locations in Wilsonville that would make us not use our cars.

Short trips in town for youth. Challenge to get across town across freeway.

Getting the word out to Spanish speakers is really important—how it works, how to use it. Really important to do this education.

With advent of Wes Station (SMART Central) you can get long trips. Less focus on Portland more to Salem.

What about ride ambassadors like Tri-Met does?

#### **4. What times are most important to serve?**

Students—weekdays long trips, but area residents all day.

Services to Oregon City-more frequent during work hours to get to services.

Like the way it is but also Saturday and Sundays, especially if Wilsonville holding an activity.

Needs to go later to 8:30.

School hours, but then sick kids can't get home during day. Kids can't stay for enrichment programs. Can't participate in music in evening.

Service later to get to community events.

Later and increased weekend especially during concerts and festivals.

#### **5. Transit spread out/less frequent or more concentrated and more frequent? Walk or wait?**

Walk less due to mobility issues, including small children.

#### **6. Overall SMART experience?**

Fabulous.

Have heard only great things.

One problem only—forcing us to use paratransit but if we had accessibility we could use the big bus. Put a ramp on big bus. Increase our freedom.

I didn't even know about SMART for a whole year. It is tough to help my Spanish speaking families to figure out how to access. Need to do more Spanish outreach.

More direct routes later in the evening for kids between east and west between high school and middle schools.

People complain about how long it takes to get east to west—can walk faster.

Not a stigma. Buses are clean and beautiful. Just not convenient.

We want to help you reach the Spanish speaking community.

It's going to take a while of inviting people to get them to show up.

I look forward to improvements and I love living here.

We are happy to print and deliver paper surveys.

Stay involved so you can advocate for your position.

Title VI #2

Older People and People with Disabilities Focus Group

March 30, 2016

**1. What type of trips do you and the community you know currently use SMART for?**

Errands, medical mostly in town, though some treatments are out of town.  
Medical in town and out of town. Come to our lunch program or senior trips.  
Working for Wilsonville Community Center users.  
We supply transportation and serving rural areas to the south.  
Currently have 52 dialysis riders. We have a disconnect in Wilsonville.  
Can't get volunteers. Volunteer Connection is part of how we deliver service.  
We use volunteers from Canby since we can't get connections with Wilsonville.  
Have a lot of people coming in and out for work. Traffic is a problem.  
It would be good to get people off I-5. Population growth.  
Work, medical, errands.

**2. What type of trips do you want but currently cannot use?**

People to accompany people to medical appointments.  
Our volunteer drivers do help people unload groceries but they aren't supposed to.  
Need service that helps person with purpose of their trips (help shopping, doctors, etc.).  
Weekend trips.  
Wish we could get to church or evening things like movies or plays.  
Weekends.  
Out of area medical trips. Not enough capacity. Always booked a week in advance.  
Need to be able to pick up prescriptions, but doesn't qualify because not a medical appt at a medical facility.  
Some people want to go south or west of Wilsonville but not in area. It's occasional.

**3. Short or long trips?**

Shorter trips for medical. Always filled and can't meet demand. Not everyone has cognitive ability to plan two-weeks in advance to make reservation.  
Shorter-errands and medical..  
Equally important. Some have to go to the Veterans Administration.  
Shopping is a short trip.  
The more you try to be all things to all people the worse it will be. Showing funding source means that it should be getting to serve folks getting to work and not necessarily going through all neighborhoods.

**4. What times are most important to serve?**

All day and all week-more holistic. Not just serving employers. What about small business, a little coffee shop or other retail?  
They are two different kinds of service. Depends on goals.  
Need travel trainer program. Transit is obstacles for seniors who lack strength or people with disabilities.  
All day all week. People appreciate not having to have a car.

Seems like all day all week will not appeal to businesses that are paying.  
Does the City have the authority to raise that payroll rate?

### **5. Transit spread out/less frequent or more concentrated and more frequent? Walk or wait?**

People coming to Food Bank would say whatever is easiest, not walk.  
Personally walk not wait. Most people in the general population will say this.  
Most people we see cannot walk.  
If you have to wait, you need amenities (benches, shelters, lighting).  
Elderly can use bus but cannot walk much and especially for return trip when they have a package. People are disabled even though they say they are not.  
Creekside residents are happy with service.

### **6. Overall SMART experience?**

Positive. I market it with my property. A big win for Wilsonville in the senior community.  
I never hear about it.  
Positive. Wish there was more of it.  
I never hear anything.

### **7. Future Vision - Five years**

It's everywhere with short walks and short waits seven days a week.  
Efficient so I can use it.  
Paratransit is ideal to balance the other transportation needs. Need both.  
Asking business to support commerce but also for social program needs.  
Think about the growth in aging population. We are going to have increased demand.  
Energy efficiency increases affordability.  
Baby boomers want to go to exercise programs that aren't covered.  
What about new aquatic center for kids, if we get that? How are they going to get there?  
Seniors share cars and rides a lot.  
Rideshare is not applicable to poor people or truly disabled.  
Stronger partnerships with Tri-Met and Salem.  
Carsharing vanpooling should be on the table for commuters.  
Capacity for ADA rides has a buffer, but out of town dial-a-ride is past capacity and needs to be increased.  
We have a 38% turn down rate since we don't have capacity (Volunteer Connection).

### **Ideas to expand resource base**

You are going to need a secondary source of funding. Business will not support all the social service you are talking about.  
Could charge or have a residential tax.  
A user fee would be counterproductive.  
Who is the customer? Who is the funder?

Business Discussions  
June 2, 2016

Suggest SMART think about what happens if all of our grants went away. If that is approximately 20% of the budget, perhaps the community (not businesses) could begin to pay their share by paying 20% of the SMART budget.

Participants would like to see the Dial-A-Ride (DAR) numbers (ridership and costs) split to show the different categories – out of town vs. in-town vs. general DAR, ADA eligible, etc. There are different tiers of service, they would like to know eligibility and see that broken out.

Multiple participants say that getting around regionally is not just a cost issue anymore. They want more weekend service, more WES service, and more frequent bus service.

Increase efforts in serving the Latino/Hispanic community.

Restaurants in Wilsonville have expressed interest in later and weekend service for employees and customers to use.

The business community could possibly put pressure to help the conversation with TriMet near Coffee Creek and new planned development out in that area.

Is SMART doing a ridership survey? Who rides SMART? Would be good to update rider profiles and see who is currently riding.

Increasing cost of living in Wilsonville, some employees are moving further away.

Any planned service to Woodburn?

The Chamber would like to be part of the discussion about who pays for what and if it is community service, should the business community carry the entire burden or are there other creative ways to fund SMART.

Should the residential community help pay for SMART?

### **Service Alternatives Small Group Discussion Notes:**

September 28, 2016 12-1:30 p.m.

#### **Power Point Presentation**

SMART has small and have to make hard choices; don't currently have enough bus drivers. Have 30 buses and can have 18 buses on the road at a time. Wilsonville has a commuter culture. Paid for by payroll taxes on business. Mostly focused on commuters at rush hour now.

Is the #4 shifting slightly?

Two big changes: a turnaround at the school, not Mentor Graphics. Dotted line is Wilsonville Rd to SMART Central during peak times, but not going when WES is not running. Mid-day don't have the bus sit at SMART Central when there is no connection to make.

## **Locally Focused Alternative Implications**

Villebois has a higher level of service.

Potentially positive as there are a lot of residents there and directly connects them to the Transit Center. I like that it connects them to commercial and Town Center.

More local trips possible using transit to parks, library. Lots of youth live there.

Tabling at Villebois, we learned that their main thing is that it would get kids to after school activities and the library.

We have a core of students who live in Wilsonville, and they use the bus, but 83% of our students live around the Portland area. We want students to use the bus, but out of 800 students, with 400 responses, we heard that 50 students use the bus Portland to Wilsonville. They use the Barbur Station.

## **Who do you think the locally-focused alternative is best for? Implications?**

This is a disaster for businesses, since there is no mid-day link to Portland.

Big implication to not provide service to the region and serve as a hub to get elsewhere in the region.

Existing plan seems to marry these two. Why or how are people using the service now? If you were a local resident, maybe you'd prefer that. If you were the business paying for this service, I'd want it to represent intercity focus.

We aren't 9-5, but it is morning and evening in chunks. We start some at 5 am and end some at 10 pm. But we aren't having people come or go in the middle of the day.

Main thing we hear is can't use transit on the weekends, since WES doesn't run, though we are a 7-day/week operation. Portland, Salem and Beaverton are main places employees live.

## **Who do you think the intercity-focused alternative is best for? Implications?**

I ride the bus from Portland to Wilsonville every day and I don't want the journey extended. If you lose connection to Tualatin Park and Ride and the Barbur Transit Center, it takes two trains to get to Portland, so don't do that.

My employees don't use the bus. I don't pay taxes since we are outside the city. If I were, I'd want it to meet my needs if I am paying for it.

I have 800 employees and 105 employees living in Wilsonville and I want them to get to work on time, too. We have a hard time getting people to take the bus. Slower.

The 5, 6 decrease frequency getting people east west of I-5. They are going to have to get in their car and drive, and those crossings are getting more and more congested. Keeping that convenient and frequent, more people could take the bus and control that congestion.

The 7 connecting all the way across from Villebois is important also.

Where routes go isn't that important. What is the purpose of SMART—getting a teenager to Taco Bell for a taco or to get workers to use public transportation to get to their jobs? As a business, the funding for the system purely comes from business, it should serve business.

If you service more intercity and have more congestion internal to Wilsonville, it impacts people who work here.

Let's design a system around employer needs.



It is not that easy. 1/3 of people who live in Wilsonville take the SMART bus, and almost all that don't, use WES. People won't take the long distance bus. Takes too long, and concerned about ability to do errands and pick up kids.

Car is more convenient. But if you make bus more convenient and cheaper, than make that plan and it will serve.

I am the Equity Services Coordinator and I think about our clients who need to get to WIC services in Canby. Until undocumented people can get drivers' licenses, people need to take public transit. Local access is very important to families that are not allowed to drive.

We do need to pay attention to that 14 year old. They get driven back and forth to afterschool activities. Beauty and the Bridge was to make walking under I-5 better and more inviting. 10 -16 year olds need to be able to get around with out a parent chauffeur. Generally speaking, I could support the intercity-focused one, except limited service to

Villebois. It's dense and young people live there. We have a fragile population there also, and every 60 minutes is cutting them off.

We do have a grant-funded service to take people shopping from Villebois and it is well used! About to start a travel-training service to teach people how to use the service.

### **What is the best balance?**

I like the current system better than the alternatives, except I would have it run on Saturdays. I'd combine the 5 & 6 and maybe the 6 and 2X to pay for the Saturday service.

The 5 & 6 loop will take longer.

### **What Transportation Options would best benefit Wilsonville? Who would use them?**

#### **Would your employees use vanpooling?**

City of Wilsonville employees –yes.

So many people from so many different areas with so many schedules.

#### **Bike Sharing? Ride Sharing?**

County has emergency ride home service.

Uber/Lyft era makes that possible, since we don't have taxi service.

#### **Who would use services like that?**

I've thought about it, and I don't really see how that would work here.

I drive every day from Portland. I can't work late if I take transit. Car2Go can't really help me with that. It doesn't work so well for the out of town travel commuters, which is 90%. It would have to be guaranteed.

Emergency ride home is a problem with so much distance.

Worth exploring? Ok, but challenge is that everything is dynamic. Shifts start all over the place. Classic rush hour is expanded.

## **Final Comments**

When it is decided what to be, it needs to be promoted a lot!

Fairness to business is important since they are paying. If you change funding model, then you can change the focus.

When I dig in, it gets more complicated.

A mix of both probably, but as a resident, I think it is not fair to demand service when you aren't paying for it.

September 28, 2016 5:30-7:00

## **Power Point Presentation**

### **Locally-focused alternative questions during presentation**

Is the assumption about what is happening with WES something that can change?

TriMet says they will expand WES service to mid-day and Saturday. If that happens, when they get the money, we would have to respond to that. On the more local front, the new road the City Council is considering connects Brown Rd., Kinsman Rd with Boones Ferry Rd. If that is there, we will use it.

On the #4, you are not going to stop at the primary school and go to Graham Oaks?

Correct

Who is left out with the 2X change?

We don't know. We do have overall ridership numbers.

### **Intercity-focused alternative questions during presentation**

Is TriMet likely to have a concern about changing to Tigard from Barbur?

They are busy at Tigard, so we might hear that.

Is there any middle ground?

Yes.

People who take WES can't get back in the middle of the day. Would this serve that problem? Will people use it?

Tigard has options in the middle of the day. Should be a shorter trip. Use WES during commute times and SMART during mid-day.

Talk about Route #3 to Oregon City.

Stays the same. We'd be connecting with Canby. Need access to Clackamas Community College and social services.

We hear it takes 3 ½ hours to get to Oregon City from Wilsonville. We serve vulnerable people—veterans, and people with developmental disabilities, and people who need mental health services. It is a challenge for people in Wilsonville to get to Oregon City. Wilsonville has great parks, and people from elsewhere would like to come here, too.

### **What implications can you see for locally-focused alternatives?**

Need to get out of town. Need the 2X. Concerned about combining 5,6 making too long a loop.

Later evening service would be helpful to students. With Inter-city more people could get here.

I didn't use the bus because it took too long to get home, even within Wilsonville. Need more frequency to make it timely.

I don't use the bus and I don't go north much, but losing 2X would have a huge impact. I don't know from a business standpoint.

Later service would be great so students can do evening activities. But people need to be able to get in and out of town. Few trips that go later would be beneficial.

Get an understanding if 2X is not there we have to know whom that will impact.

What number of people who work at what employers? If you cut ability to get people from the north here, businesses that pay 80% of the budget do not support that.

It is a hike to the school, so I am glad if it goes to Graham Oaks. I support frequency on a main road. I would like to see bike racks along the route.

Is there any analysis that ridership will increase with either of these?

Increase on 1X, we heard that people want it to run all day due to varying work schedule.

It seems since we live near the 4, it's good to see it go to Graham Oaks. What about bus sitting for a long period of time at end of route? I don't like to see them idling.

Could you get more runs in by cutting wait time?

Question about #7, we have big population in Villebois and serve them at rush hours but not for mid-day shopping.

Locally focused alternative addresses that.

I live in Villebois and I used to use the 1X to go to Salem. My husband uses TriMet and the 2X connecting in Tigard takes too much time to get to Portland. The times we are using buses is to get outside of Wilsonville. Bus doesn't go to shopping centers.

When you see #7 increased, do you think we would get increase ridership from Villebois?

I'd think so, but don't currently use it now.

### **What implications can you see for intercity-focused alternatives?**

What did Oregon Institute of Technology say?

Mixed message due to where people live.

Heard people need to get north. Commuters don't use local service and want more options for getting in and out of Wilsonville.

Took too long to use transit to get in and out of city, but maybe that was a long time ago. I want the service to work quickly. Takes two hours to get to the airport. West side of Portland is so congested. More ridership doesn't give you more money.

How long will it take to get from Tigard to Oregon City? It is a huge issue. Services are in Oregon City. I live here on Wilsonville Rd. Getting people who don't drive to social services is so important.

North and south are both important, designed to get people to work. Gas prices go up and more people will take transit.

We have so much work here and not affordable housing so people have to live somewhere else to work here. They have to be able to get to their jobs. Canby, Woodburn. Lots of people coming from there.

Molalla, McMinnville, Woodburn—farther out for affordable housing.

Why switch to Tigard?

More options.

If light rail comes down, then it will go to Tigard.

Tualatin, Beaverton have lots of people coming here to work.

Employers are funding this and we have to keep that in mind.

Possible to align anything we timing of classes?

Intercity alternative better as more connections possible.

People spend a lot of time waiting. Optimize connections.

### **What is the best balance point?**

Can you tell who is saying what? Local residents may not be using the bus and not filling out the survey. Funded by payroll tax makes us respond more to intercity, but high school kids are taking SMART and they aren't filling out the survey. Leary of the veracity of surveys.

Lots of outreach to broader than businesses. And it is a barometer reading.

### **Transportation Options**

Vanpooling for late night shifts or from Woodburn?

How implemented? A subsidy to provider? When I was working at Xerox 12 years ago, I didn't understand why no connection to Sherwood. A van could work from specific companies to specific communities.

Why not to social services in Oregon City?

I think the director could be interested.

People come to Villebois for public transportation, but Graham Oaks maybe has a different point of view and want to drive their cars.

### **Car sharing?**

That might work in bad weather. Kids walk and ride until rain.

Fantastic idea. Worth exploration for sure.

### **Emergency ride home**

City code says the business needs to encourage non-car use.

We don't have zip car or Car2Go here yet. Uber very expensive.

### **Final Comments**

Senior citizens want to see SMART provide services to arts and culture sites—library Saturday afternoon for example, St Francis concerts, etc. Most people at Creekside apartments don't have cars.

Senior Citizens welcome transportation to art & culture venues in Wilsonville ; and would like to see SMART consider/ plan to provide transportation to Wilsonville Arts & Culture Venues for events and regularly scheduled art and culture activities in or near Wilsonville.

SITES presenting arts & culture activities

Library – Saturday afternoon BookNotes concerts; and other art and culture activities

St. Francis of Assis Church Concerts

Charbonneau Arts Association October Arts Festival

Butteville and Champoeg

McMenamins - History night

Trudy's Furniture – First Thursday art happening

Holiday Inn

Al Kader Shriners – ballroom dancing -last Sunday – 4 pm to 7 pm

Findley Auto Dealer- Monthly Art Exhibit

Frogpond Church – Strawberry Festival/Fall/Jazz Concert

Clackamas College Bi-monthly Art Exhibit

Wilsonville Community Center art and culture activities

Events in Town Center Park:

Wilsonville Festival of Arts—First weekend in June

Fun in the Park – First Saturday in August

Other art and culture activities in Wilsonville parks:

Music and Arts programs in Wilsonville schools:

Boones Ferry Elementary School

Lowrie Elementary School

Inzra Wood Middle School

Wilsonville High School

September 29, 2016 8:00-9:30 a.m.

### **Power Point Presentation**

Because of our population, it would be nice to know where the transfer points are to come into our community. I need to know where the connection points are to see if it works like a network that would serve students coming to OIT.

#96 to Commerce Circle. All other connections are at SMART Central.

## **Whom do you think the locally-focused alternative is best for? Implications?**

The 5,6 is only running every 60 minutes and that will eliminate ridership of our students who are on fixed time. Hour long delays won't work for OIT.

How about 30 minute wait?

Better. Make it more viable for local, but I am concerned.

One thing I struggle with is rush hour works for classic schedules, but library doesn't open until 9 am. Fewer options to get to story times, for example, we have a lot of part time workers and they don't start at 8 am and they have short shifts, so they have to drive due to lack of options. Another concern, apartment complexes of lower income people, are we serving them well enough?

I live in the Maryfield neighborhood. I have a car and don't use SMART. I work at library and my choice is to walk faster than taking SMART due to the giant loop.

If you ask businesses if locally-focused is important they will say we need both. Convergys needs inter-city connection, but they don't have 8-5 schedule. We do have rush hour dependent businesses, but our retail businesses need local mid-day service. We need balance.

Need more frequent northbound service.

The hour transit wait doesn't really work. Villebois will be a walking boundary and we have dual language program at Boones. The neighborhoods that Hilly was discussing are important.

Serving lower income apartments are really important to have bus service because they have little kids and can't walk great distances. Lower income neighborhoods may need shorter distances to walk. As far as Community Sharing is concerned, we need service to Oregon City!! As a business owner, we have to have connections to Portland. You can't cut that out. It is not OK to have only commute hours. I love cutting out going to WES when WES isn't running. My kids quit taking it because it took so long.

Dial a ride for seniors? Any impact? Want expanded in town weekend service for seniors. Oregon City is critical for getting to services. Side note, I like options to Portland and Salem because that's where transient populations need to get from here to service centers.

From a business point of view. Need to go and come from the north. If I understand, are the students more at rush hours?

Mostly rush hours, then evening and weekend where they are commuting from work to school and then we have trouble getting them back home at night after classes. Timely route into Wilsonville is a barrier and then worse to get out after classes.

I like local focus, except needs route north.

Not covering Canyon Creek Apartments though and that is a problem.

It is close.

Too many different shifts on 95<sup>th</sup>.

Rush hour focus has really changed. And use of temps also.

I do like #4 changes. How does someone on #4 connect to Target?

Mid-day there are stops in town; rush hour at Transit Center.

### **Whom do you think the intercity-focused alternative is best for? Implications?**

How often does WES go through Tualatin and Tigard?

Eight times in the morning and eight times in the evening.

We would only go to WES mid-day, 9:30-3:30.

WES doesn't serve Portland. It serves Beaverton. It is not good for transit time.

Too abstract without seeing what it means to someone.

What is the cost difference of these, since this is so important to low-income?

Same fare for connections to TriMet.

Need to see some tests to see how long it would take.

25 minute wait for the #96 needs to be cut. We need buses waiting for the connection to make it work.

What's the WES ridership compared with SMART ridership to the north?

1800-1900 passengers a day. 40-45% are boarding or getting off in Wilsonville.

2X is about 260-270 and we don't know how many are going north of town. We have 3 year old data.

Cherriots has morning and evening and SMART would run mid-day.

Who should be paying for 1X service and things like it? It should be a state function since it is thirty miles with no stops.

Need parents to escort young children.

Mid-day to Canby is important.

Would it help if the 2X went through the campuses and headed north?

It is walkable to get to Parkway and Beckman. Frequency is the issue for us.

We prefer this type of approach, but don't want to cut mid-day for 5,6,7, because it is retail customers.

I like the Tigard Transit Center shift, but would like to pick up #96 people too. Can that work? Can 2X pick up and increase north connection?

### **What is the best balance?**

Wilsonville is only two miles wide, and we need the inter-city connection. Can sacrifice some local focus to have inter-city connection. Kids need to get to Portland for after school activities and it serves low-income people who do not have alternatives. But be mindful to keep shorter walking distances for lower-income neighborhoods.

As the commute gets more congested, we need more connection. I like what Michelle said. Get people to walk and ride their bikes.

Can you do special buses for community functions?

Yes. We have a dial-a-ride that became shopper shuttle from Villebois every hour.

Don't have to call for it.

### **What Transportation Options would best benefit Wilsonville? Who would use them?**

We've advocated for dial-a-ride to coordinate with larger medical facilities to create more predictable schedule/shuttle style. We were shocked to understand how much dial-a-ride we were using and want to increase that efficiency.

Coordinating medical appointments is too hard. Low-income and elders have it hard to get appointments at all.

Dialysis and chemo trips should be able to be coordinated.  
At some point we need medical facilities to partner with us. More efficient.  
Are there any incentives for businesses to do this?  
Employee retention.  
Best way to airport? How many people go daily to PDX? Shuttle?  
If businesses had shuttles, and students knew about empty seats, we could connect, especially off shift vanpools.  
Use vans that are used for rush hour shifts for other purposes.  
Technology is impacting our transportation choices.  
We've made technology upgrades at SMART. We have app for your phone. SPOT.  
Car sharing, here most people have cars.  
I own two cars and use Car2Go all the time. If I miss the bus I use Car2Go. Have to locate them well. It would be great if you could get a connection to Portland from Wilsonville. So convenient and great adjunct to transportation. No meters. Costs nothing to join. 35 cents a minute. No fueling. Don't have to bring it back to where you started.  
Yes, if it is feasible.  
Do any of the larger employers have incentives for people to walk, bike or ride the bus?  
Some do for carpool that adds money to paycheck for carpooling, bike locks, etc. to meet Trip Reduction Plan. No fines for not doing it.  
Coordination with major employers in Wilsonville, but what about out of area, like Hillsboro? Are we talking to large employers out of Wilsonville? If we want Wilsonville resident to use WES, we need to talk to big employers where our residents are commuting. Intel, for example.

### **Final Comments**

Are there any more groups about cost of these options?  
Not groups scheduled, but opportunities for input on draft plan.

## **Issues and Opportunities Community Survey #1 Comments**

The below comments are categorized geographically. Some comments may apply to multiple categories. Comments are verbatim with the exception of personnel names removed. No spelling or punctuation errors are corrected.

### **Q.1a Which types of trips do you use transit for? Other:**

I used to use Smart transit but realized that using my own vehicle is much easier and preferred.

I use Trimet to downtown Portland.

Charbonneau warrants and deserves more service, considerably. Examples include more frequent availability, (ideally 24-hours/7-days,(,) service) with more stops in Charbonneau,



between Charbonneau and the rest of Wilsonville, such as S. M. A. R. T. Central Wilsonville Station, please. Expanded service, please.

I do not use transit currently. However it could be important for work commute for our employees.

None. I drive when the weather is bad and after dark. Bike or walk otherwise.

To get to PDX for business and personal air travel.

do not use transit

don't use transit

Airport

PDX

We are having a hard time filling our Swing and Graveyard shifts with staff who ride the bus system. I believe the last stop outside of our facility (Vision Plastics, Inc.) is at 10:00 p.m. We would love to see the bus run until 11:45 p.m. for drop off/pick up to help our bus riding staff.

I don't use it, but our employees use it for work commute.

"You really need additional options for all these questions ... N/A, don't use public transit ever, not interested in public transit issues because I have no intention of ever using it and resent my tax dollars subsidizing those who do, etc.

Your bias is showing."

I don't use it now because I work downtown 7-4. I moved here as retirement plan and plan to retire in 5 years. One of factors that was positive in my city selection was the ability to get around when I got older (post-retirement) and did not have as much need to drive. Currently, I do most my errands in W-ville on way home from work so I don't need short rides either. But will want to when I retire!

I have not used the transit service

Commute from Villebois. Current schedule seems to over serve those working in Portland and underserved those working in Wilsonville. Routes also made sense 4 years ago, but not today.

To the Senior Center

Senior Center

We have used Dial a ride for dr. In this Wilsonville area it was good.

Could not use it because I don't' have the time to commit to work and use SMART shopping

Church

I drive now but I am 82, so in time will need.

don't currently use it

None currently

I don't use transit now, but have in the past. I used to ride SMART to Oregon City several days a week, and also to events in Wilsonville when parking might be a problem.

connections to other transit systems, eg.- Tri-Met, Amtrak, etc.

I currently use my car for transportation, and there are private, free-enterprise, non-tax-supported methods of getting around that do not require someone with a badge and a gun to coerce wealth from others to provide something that is a service like any other service.

I usually don't use transit but it's nice to know that it is there if needed.

I do not use transit

I have never used transit

Not currently a user of Smart Transit

Airport connection

Don't use Smart at all.

I don't use it.

If my car is out of service.

Other

I don't

If I run out of gas.

Don't use

Emergency work commute

Airport

I don't use the public transit right now. I don't know enough about it or the routes.

Used three times in 10 years.

None at this time

when I can

The SMART is not all day, I live in Canby, would like all day service and late into evening to be able to get home from Class at OT

do not use

I don't use the SMART system at all. I occasionally use Tri-Met (mostly MAX) for trips to the downtown area from my home.

i do not use any transit today

None, there is not a bus that stops at The City of Woodburn

I have lived in Wilsonville almost 10 years. I used the 1X for about 10 years (since before I moved to Wilsonville) back when it was free. I didn't have a car for about 2 years and used the SMART bus to move around town during that time. I changed jobs about 1 year ago and stop using transportation because it didn't make sense. I wish it did because I rather read a book than seat in traffic.

Does not have access to newberg

Anytime I have to go to downtown Portland

EVENTS

None, but would like to

Occasional trips downtown on holidays when I know its going to be crowded

None yet, but I'm starting to make plans

Don't use

Currently don't

None currently.

None, I like my car

rarely

No weekends service in Charbonneau

Live 25 miles away from (out of area) no need.

none because of schedule

not on the transit line

### **Q.1b Which types of trips would you like to use transit for but currently cannot? Why can't you?**

I work in Portland, and am not sure how well it would work for me to ride transit that far. I would ride transit around the Wilsonville area if I knew more about where the system connects. I moved to Wilsonville in December, so am still new to the city.

None. I don't have any use for public transportation at this point.

There's no light rail connection to Salem (work) and to Portland during the hours I'd use it for recreation.

I would like to do a lot MORE of these activities with transit, but it simply doesn't run frequently enough to Portland (never on weekends, and very indirect during week) or Salem.

WES doesn't run on the weekends or at times that work for me. We would love to take it in to Portland on the weekends but can't.

The routes within town take much too long due to the route patterns.

I would like to be able to use it to travel to Portland outside commuting hours. Sometimes I have to work late and so I can't take SMART to work. there are no convenient connections later at night or on weekends.

I would like to be able to get to downtown Portland on WES

Live in the new section of Villebois and still doesn't have services. They did recently start a shopping shuttle which I love. [NAME] is a sweetheart and loves her clients. I look forward to seeing this service expand to a greater portion of the area and extended times.

Limited weekend service, limited evening/night service, limited connection points with tri-met.

Direct from SMART park and ride to downtown Portland.

I love the 2X. I use it every workday. I just wish it ran at least one late night trip to Barbur (or Downtown Portland) and back. I know ridership demand must be there. Currently I have no way to get home from work if I don't catch that last southbound 96 at 7:35 PM from downtown Portland.

Currently there is no direct service to Portland downtown.

I'd like to head to Portland on the weekend, but can't get there on public transportation. Doesn't have to run all day or all the time, but something would be helpful.

Forced to use multiple bus systems between Oregon City and Wilsonville that isn't a realistic time option

schedule doesn't run often enough, not enough detail for stops

Poor connections between different transit providers.

poor availability of weekend service.

Service is not frequent enough or hours of operation are too short. Also, west side of town service is too long for efficient use.

Does not connect to the greater metro area late enough, often enough, or at all on Sundays.

Smart does not run on the weekends

The bus doesn't serve my work

Mainly related to the perceived lack of flexibility, but having not tried to use the transit system for work and errands, it is hard to say that it is good or not.

Charbonneau warrants and deserves more service, considerably. Examples include more frequent availability, (ideally 24-hours/7-days,(,) service) with more stops in Charbonneau,

between Charbonneau and the rest of Wilsonville, such as S. M. A. R. T. Central Wilsonville Station, please.

Expanded service, please.

Can be hard to get to Newberg on bus from Wilsonville.

The trains have a limited round trip run time.

Inconvenient pick up locations.

Takes way longer and is more expensive than other transportation options. the commuter rail does not go straight to downtown Portland and it is faster given transfers to drive. It is about \$200 more per month to drive and park but it saves time and is more convenient. I do not want to transfer and wait for the transfer in our weather.

Doesn't run late enough or connect with the transit options going in / out of town often enough.

WES hours are restrictive, encourage expanding WES hours.

Train does not run on weekends.

Inconvenient/unavailable routes

I work in Vancouver.

The trip time is too long, and busses do not run early enough for me to use on my work commute.

Getting out of town on the weekend is problematic and without many options - zero on Sunday. The trip to the providence medical area is long with some bus hopping when it's possible at all - it's easier to drive

Limited WES service (e.g., it is difficult to take the train to go to dinner downtown due to limited service hours).

Either not enough available in the area or too inconvenient with large family/ small children.

Because the hours are not helpful to me

No one works nearby with same shift

Getting stuff from 6 different stores takes too long

I commute from Newberg and varying schedules

start at 4am

live in Newberg

need my car for work

Commute from Sherwood

no service where I live.

outer edge of Wilsonville Sherwood

live outside of Wilsonville

no routes

start time 4am

Downtown Portland, bad schedule

Insufficient service to Villebois

Because it doesn't come at the right times

I cannot take evening classes in Portland because there is no bus service to Wilsonville at night.

I go to school at PCC Sylvania and the smart shuttles don't go that far. I could take TriMet but their fares are way too high to make it really worth it.

No direct transit to Woodburn or PDX. Smart train doesn't run on weekends.

I would like to bicycle but not use I-5

The current schedule is totally aimed at commuters. It does not offer any option for mid-day to or return from downtown Portland

No efficient travel from Wilsonville to downtown

"Does not run on Saturday or Sundays.

And only on the hour during the day."

No weekend trips...

I can't get into Portland and back before the WES stops for the night, and there's no WES on the weekend. Since I-5 is a parking lot and I-205 can be worse, I want an alternative so I can load my e-bike onto mass transit and get into the city rather than drive.

Limited Salem transit.

hours of use limitations

Currently the hours for transit do not accommodate all scheduling needs for our employees' work schedules.

On weekends I'm practically trapped in Wilsonville sure Smart runs the 2x but its a limited service and only gets me a few miles outside downtown. Portland Metro has lots of fun activates on the weekends such as the carnival and the weekend market. The thought of having to catch the 2x then the 12 and pay two different transit providers just to have a nice trip out to the city on the weekend really sucks :( on top of the limited time frames the 2x runs on the weekends.

You don't offer more weekend schedules

Transit lines do not run late enough, or on weekends.

Conflicting hours, and long travel waits and times.

WES does not operate on weekends

Service to my area is almost non-existent. The WES should be extended all the way to Salem with a limited number of stops (one per town? South Wilsonville, Donald, Brooks, Keizer, Salem) along the existing rail tracks, with a park and ride at each stop. It also needs to run more frequently and with more extended hours, similar to the MAX.

Service isn't frequent enough to accomdate personal scheduling needs.

It would be great if the SMART Route towards Portland/Barbur Transit Center was more direct. I was hoping it would save me time, like a commuter bus would, or "bus rapid transit," compared to taking WES and transferring to a Trimet bus. That's just my personal experience.

Need sat service and Sunday with cherriots

I have no intention of ever using public transit.



"If there were a direct SMART to downtown Portland instead of Barbur Blvd, I would consider even changing my work hours to use SMART.

Right now I do all my errands as part of my commute home from work and I have a long commute to downtown Portland in the morning 7-4. I retire in 5 years and will want to use SMART more at that point to go from Charbonneau to Wilsonville ."

Not enough morning shuttles to villebois

Would use more regular and convenient service on Saturday to get to shopping and to destinations in the region.

Due to the extremely limited service to Villebois.

The canby/char bus doesn't run enough

No Max lines/ public transits presence

transit not available in evening

"Lack of transportation options either:

(a) at certain times of the day,

(b) on certain days of the week, and/or

(c) no direct service between points X and Y

A good example is when I need to work (in Wilsonville) at the weekend. Getting to work from Portland is difficult on a Saturday, and impossible on a Sunday. Why? Well, TriMet does not run the 96 Bus or WES train except at certain times on Mondays to Fridays. And SMART's 2X does not go beyond Tualatin on Saturdays or late at night. So, to get from Portland to Wilsonville involves the 12 and 76 TriMet buses, and the 2X bus, and 2 bus fares. It is pathetic. Look at how proper cities such as NYC do transit and learn!"

No weekend service on WES.

Need service from Villebois

Takes too long need better service in Villebois. Shuttle-train-bus or 1/2 hour walk to bus stop

There are not enough routes, and the ones available are not convenient at all. Especially buses running to West Linn High School area from Arts and Technology High School in Wilsonville. There are a LOT of students who would use this! Especially in the morning or after school, up to 5:00 so they can stay after for after school activities/tutoring.

Easier transportation to World of Speed

There is no smart bus 5 if I want to leave during lunch time.

The bus doesn't go to West Linn where I work or Oregon City where friends are.

I cannot commute from home to work on the weekends because I live in Beaverton and work in Wilsonville.

no easy connection from Wilsonville to Clackamas Community College

The max system times are less frequent to & from Wilsonville than all other stations it seems, making it difficult to use as a reliable form of transportation to portland

Don't need them.

No evening or weekend trips

To Newberg hospital our dr. is there. When our car has to go, we will need to Newberg to our family dr. 50\$ for a taxi ride is too much. We only live on Social Security.

Need service to Newberg

I don't' want to take up the space.

No weekend service to downtown Portland and beyond

Buses do not run late enough at night or to Newberg

I have a car and use it.

I drive now but I am 82, so in time will need.

Need cross walks at every bus top. Would be nice but cost would be out of sight.

Doesn't run on weekends

The morning/evening weekday schedule is too restrictive. I would like to take WES to evening events in Portland, as well as to be able to go shopping in Portland for a few hours. I also would use it to connect with more flights at PDX.

I am just using Smart for my first week and I just moved out here, I work at Intel and I also use MAX and the Wes, I love the route of number 4 lot's of shops and stores to go to and I do not drive for reasons of my own, as I quizzed the drivers they have told me that the bus service don't run through the town very often on the weekends, I haven't been here for 9 years...wow the growth!! can't wait to see the downtown part on foot.

"Meridian Park Hospital complex  
anywhere in tualatin, tigard, beaverton, clackamas"

would love to use it for work, but the timing does not work for my commute and tri-met's schedule

Not adequate service between Tigard and Wilsonville.

It would take too long

Too lazy to figure out the schedules

When taking TriMet from Commerce Circle, there is no connection from Commerce Circle to the transit/train station outside of very specific hours. This makes it too difficult to travel outside of Wilsonville because it is too far to walk. Would like to see rush hour connection between Commerce Circle and Wilsonville transit station.

Due to the limited hours and days.

Inconsistent times, sometimes it is too slow.

None of the above.

I live in the Villebois neighborhood and work at the Centerpointe offices on the corner of 1-5/217/Kruse Way in Lake Oswego. SMART does not do a daily (Mon-Fri) scheduled drop off and pick up there.

Medical - need it to Kaiser Tualatin. Errands are generally lumped together so travel by car is much simpler.

Cannot reach work to be able to clock in by 6am, which is required for any initial training when starting a new position, or the flexibility to show up at a 6am shift which is a work shift in dire need of employees.

"No easy connections to downtown- it takes so much longer than driving. I still would have to drive 20minutes to get to train station.

I wish I could take the train to Eugene and downtown Portland, and connect to the Seattle train."

I can get where I'm going faster in my car than on the bus.

I don't work

Need service later in evening. Need service to Church on Sunday Please!

Wes only runs on a rush hour schedule and Smart buses run too infrequently. No Saturday service on Wes.

#### TO TUALATIN EXIT 290 FOR SHOPPING/EATING WITH FRIENDS

A 15 minute trip would take over an hour by transit with transfers. Also, buses aren't available if I want to leave work at 2pm or 3pm

Doesn't run on weekends

No easy and fast transit from the Wilsonville TC to downtown Portland

"events- some of the local events are in the PM- plays, concerts, etc.or on weekends when Smart is not available.

regional travel- eg.- can't get to Lake O, West Linn,Sherwood, Oregon City or Canby without taking a roundabout trip & several buses.

not enough connections to Salem to use their connections to the Coast, Amtrak, etc."

Live in Charbonneau and the SMART does not make enough trips to the area for my work schedule and only stop 1 place a mile and a half away. I can ride my bike but there is a lot of inclement weather.

The commute is too long from Portland to Wilsonville.

no weekend WES service/time frame ends too soon

If I wanted to take myself and my community on a one-way trip to poverty and slavery I would encourage tax-supported provision of this service that punishes productive people, provides a disincentive to non-productive people to become more productive and inhibits opportunity for productive people who would like to serve others in the area of transit, but will never be able to compete with the inefficient providers that can only provide the service if others are forced to pay for it whether they want to or not.

No service on Sundays nor Saturday evenings.

Connections to other regional transit (Trimet) are not timely or efficient. It is easier to just battle traffic and drive.

Service is local

Trains do not run after commute times or on week-ends.

Too far, in Tigard. It takes too long.

The route is longer on the bus than to bike - although in bad weather would prefer to ride the bus, but instead I use my car.

Service too far from home for my kids

To go downtown Portland for dinner and entertainment is not possible, as the return bus back stops at 8:30

Living in Sherwood, we would have to commute to WES or have a very long ride to go to a transit center to come back south to work.

I take the Bus to Canby in the morning but I am off work at 2:30 and the first bus back to Wilsonville doesn't leave until 4pm.

No easy way to Wodburn or Eugene. Transit needs to be easier as fast as driving. Or at least close

I live outside the free bus zone.

Because the train doesn't go into downtown Portland, and I can't spend all day walking from station to station.

The transit doesn't come to my town.

bus does not run through Newberg and doesn't run early enough for me to get here.

WES does not run Sundays or in the middle of the day.

none- I have a car I DRIVE!

Inconvenient access

Bus does not run between Wood Middle School and high school for middle school kids who need to get to high school for some classes

The train does not run during the day - only during the commuter hours.

Doesn't go to my work area. And neither does trimet

No bus/transit system in my area.

There are not enough transit options.

I work too early.

WES does not run weekends, making it hard to leave Wilsonville.

The only effective ways into Portland are during rush hour.

Later shifts take too long to get home.

None

N/A If I used it, I'd use it for recreation.

Canby has limited routes

I don't use it at all.

Cost effectiveness when also using TriMet and availability on Sundays.

Only a few buses come through Charbonneau.

I work at 4am.

Not available.

Not enough transit options or buses.

Transit is not in Canby.

Bus to train is too early

It is too far, and would take too long.

I do not live in Wilsonville

No availability in my area for my hours

It is a hassle to ride transit from North Portland to Wilsonville. 2-plus hours.

The bus and WES don't start early enough for me to make it to work on time

Unable to leave on weekends/bus to Woodburn

Because of timing and location.

Time. We start at 6am

Earlier working shifts/later working shifts

Hours from Salem are not expansive enough. We need later returns and earlier arrivals

Shift change ends at 7pm, last bus to Salem leaves at 6:30pm.

No Sunday transit.

buses do not go far enough

Hours of operation

Does not run on lunch hours or weekends

Transit does not go near my home and it does not run late enough

Hours are limited (SMART hours)

I live in Woodburn

Buses do not run before my shift starts

My friends live too far away and I cannot afford the out of town trips.

Time

WES is not available on weekends and buses are not frequent when trains are off.

No buses to Wilsonville on the weekends, the 4x doesn't run on Sundays and the 2x is on a not so convenient schedule

Nothing goes to Wilsonville on Sundays

Trimet does not link with SMART

I live in Hillsboro, commute to Wilsonville. I work too early to ride SMART

Trimet does not enforce animal policy which affects my asthma

WES doesn't run on Saturday or Sundays

Transit ends too early for evening outings

Transit does not run late enough or in the areas I would like to go to

Does not run on weekends

Current bus system doesn't go that far

Does not allow for weekend travel

SMART Wilsonville is SEVERELY limited and region restrictive. Evenings and weekends are nearly impossible!

The places I want to go do not have a bus stop within two miles.

Weekend commuter

Work is the only time I use Smart Transit. (I live in Beaverton.)

Weekend

Too much time, no links to trimet on weekends

Transit does not go where I need to travel.

At present, half hour increments (as opposed to 15 minute intervals) make scheduling clunky and inefficient for my current errand needs. Additionally, not having service to Portland on Saturdays make using SMART for recreation impractical.

Most of my errands take place on the weekends, when I'm free to run them; The WES doesn't run on weekends - which means I can't get to the airport, or Portland proper until the weekdays.

The pick up locations are too far away and the pick up times too far apart to warrant the time and effort needed to use transit for these activities if I'm using SMART. Also, some of my work commute ends after the last pick up using the WES so I would have to rely on SMART and the drop off locations late at night are not convenient at all. I wouldn't even be able to get back to my home via MAX because the MAX doesn't run that late to all locations. Also, the cost of having a year long MAX pass far exceeds the cost of using my personal vehicle for transportation. I would take advantage of a MAX pass if it were subsidized by OIT.

Work location or need to attend meetings at other locations does not allow commuting. I use a personal vehicle for company business and expense mileage.

"Getting into Portland or to the airport from Wilsonville is very difficult and time consuming when the WES is not running. Thus it is not useful for weekend or night trips, which constitute most of my driving since I am able to use transit and to walk most work



days.

I likely would not use transit for grocery or other shopping, no matter how available -- lugging bags on a bus is really inconvenient; I've done it many times."

WES only runs during peak travel times M-F and there is no alternative offered during non-peak times to get back into Wilsonville. If I get sick or have an emergency, I have no way to get home in the middle of the day from work. I am not going to get stuck and then have to take a cab. My kids' college classes do not follow a 8-5 schedule at PSU. No one in our household wants to pay to park downtown - or even drive downtown. Furthermore, the i-5 traffic is terrible on the weekends and the cost for parking downtown has recently increased; yet, there is no way for me to get out of Wilsonville to a transit center to utilize public transportation on the weekends or non-peak hours. Perhaps in the past, people who lived as far as Wilsonville did not travel to downtown Portland each day. Those days no longer exist. People commute farther on a daily basis. I moved to Wilsonville from Milwaukie four years ago. In Milwaukie everyone in my family was daily bus riders. Since moving to Wilsonville I haven't been on a bus. I like living in Wilsonville, but I do not like the limited public transportation options that I am stuck with out here. Help me get out of my car and out of traffic!

The route is available for my commute but only during limited times. If I get sick at work during the day or want to stay late I can't.

Some buses do not reach or take too long to get to places I need to go.

Just not convenient locations

My work schedule is irregular and I often start and/or finish work before/ after the bus route runs. I work in Eugene and live in Salem and it makes more sense for me (time and money-wise) to drive rather than take the bus.

The times that the specific routes are offered are sparse and I wouldn't be able to rely on consistent service to and from the area

Faster to drive.

times

I need to know more about it! It would be cool if there were more ways to find out information - like a map of routes in the newspaper or the library.

Too indirect; not frequent enough. The 4 is ridiculously circuitous.

I don't want more transit. I want more roads.

WES's schedule only works for work-day commuting.

The services don't run late so I can't visit Portland for any shows.

I live and work in Hillsboro, the commute to Wilsonville is too time consuming.

Currently no direct connect between Clackamas Community College Wilsonville Campus and Oregon City Campus.

I currently live too far away.

The WES does not run on the weekend.

WES does not run on the weekends.

It would be nice to take shopping trips further outside of Wilsonville and WES outside of commuter times.

I work in Woodburn.

The times are not convenient

Route not available

Need to be in Wilsonville by 8 am and new availability to get to salem at 10 pm

Not connected well enough to Portland transit

I use my car

Lack of planning to coordinate and end up driving

Does not go everywhere

Too difficult to ride from Oregon City campus to Wilsonville

Too far

Too far

I like to travel to Portland on the weekends. While SMART connects with Trimet on Saturdays, the available operating hours are limited and not even available on Sundays. It would be nice to have a direct route to Portland on Saturdays and Sundays.

I live in Oregon City and commute to Wilsonville for work. It's just not feasible to use

transit to get to work.

Doesn't run at times I need

Routes to my workplace in the morning are too infrequent to be useful.

No service on the weekends.

More trips to Tualatin Park and Ride on the weekend, and, of course, WES on Saturday!

Driving to the Tigard transit station from my house in Tigard takes almost as much time as it does to just drive to Wilsonville, where I work.

"1 - nearly doubles my commute time if I drive to a train stop; more than triples it if I try to use all public transit

2 - doesn't operate outside ""traditional"" office hours; I work a different schedule precisely so I avoid rush hour traffic."

takes too long not convenient

need Newburg -Wilsonville connection

Too long

"1. WES only runs M - F during the morning and evening commutes. I'd want to use it to go downtown Portland for evening and evening events

2. Currently, SMART doesn't run in my neighborhood (Park at Merryfield). In order to use for my work commute, I'd need closer bus system."

There is not a bus that stops in my home town

I use transit for everything it's currently possible for me to do without a car, so there's nothing listed I couldn't do if I wanted to and it was feasible.

I change jobs from Salem to downtown Portland. My commute would be almost double if I use public transportation than if I drive. I rather use public transportation but it doesn't make sense as it is right now. Also, the fact that the WES it's only in service during commute hours makes it impossible for me to use it for recreational purposes. For example I would love taking the kids to the zoo in the WES on Saturday but that is not possible.

Blazer games and late night airport drop off

Doesn't extend to Newberg

There is no time effective means to go to Clackamas Community College in OR City from Wilsonville.

Live in Salem with limited service.

Transit from Beaverton (where I live) down to Oregon Tech (Wilsonville) is lengthy (about 1.5 hours) and largely depends on the schedule of the WES, which is normally limited to commuting hours. Classes at Oregon Tech, however, can start at inter-WES operation hours such as 12pm or 3pm, possibly leading to a ton of waiting time. Also, my car is sufficiently fuel efficient as to be the better economical option. ~\$20 for about 10-12 round trips as opposed to \$50 for 10 round trips via TriMet then SMART.

There is no commuter bus or train in my area from Happy Valley region to serve as transportation for the needed time to commute.

I live outside of Newberg and work in Wilsonville. Plus have kids to drop at school.

Bus schedules and transfers do not line up.

There is no direct route or an express route from my area in SW Portland; so commute would be over 1.5 hrs with transfers.

Work commute from Beaverton would significant walking, multiple transfers and way too much time.

I live in SE Portland and commute to Wilsonville. Connecting all the legs of existing transit takes too much time to be worthwhile. Also, I have some pain/fatigue problems that make me anxious about the possibility of having to stand while waiting at a stop, or on crowded transit.

Because it would take way too long with at least 3 connections.

SMART does not have a stop that is close enough to my place of work to make it practical.

Saturdays there is no WES and no SMART from Tigard/ SW Portland

Scheduling and time it takes for bus

Time constraints

I have a car, but want to save more gas

I live in Salem and my schedule is weird

connection aren't good enough or not there

Need Service from Keizer to Wilsonville

Smart does not have direct service to Portland

Max does not travel south of I5 corridor. , I live in north Portland

Would take too much time

Would have to commute approx. same amount of time to reach Tigard transit station

Trips within town take too long.

work schedule differs from route schedule

Live outside good transit areas/ Rural Marion County

Not offered in my location

Does not fit my schedule

too slow to get from home to work and back

Wish there was a direct bus to downtown Portland

WES schedules no trains during the middle of the day and none on weekends.

Because there is nothing close by that is available.

The schedule doesn't show transit late enough in the day for me, though I do see a bus that runs past Mentor Graphics at 10:00pm. Without knowing if I can catch it in time I can't rely on it.

I'd be open to using transit if there was a "direct route" from home to work. (West Linn to Wilsonville - Mentor Graphics Corporation)

This question is not applicable to me. I drive to Wilsonville to commute to Salem.

Don't work. I usually have too much to carry with errands.

Currently drive our car

No weekend service in Charbonneau

Still able to drive myself

WES not running on weekends~

Your scheduled hours for the Villeboi bus route to Mentor Graphics is too infrequent for my extended working & flex time hours.

"Errands - Have to much to carry if I go shopping and have several errands to run.

Medical - Takes to long to get there by bus since I live in Wilsonville and my Doctor is off of Carmen

Drive, I would have to take more than one bus."

The hours are not available to leave wilsonville during the weekend

No bus to Oregon City campus from Wilsonville.

I would like to be able to get to downtown Portland more easily

Don't have information on times, routes, etc.

no bus from sherwood to oregon city

Unfortunately, SMART doesn't run to Sherwood, and TriMet has infrequent and inconvenient bus times. If there were a bus that ran from Sherwood to Wilsonville, I'd be more than happy to take public transportation. As it is, driving is much faster.

It's not that I cannot, but I object to the "all in one" fee structure of WES. I live in Tualatin, and the \$5.00 daily fee is silly, to me, for that short distance. I'd ride it every day if there were a more reasonable fare. My very efficient compact car does not consume \$25 in gas per week. I get that it may be an administrative problem to come up w/ another ticket, but couldn't they work w/ local companies to create a special monthly pass that one could purchase? I've also thought about using SMART bus, but by the time I drive to the park & ride, I may as well have just driven in to work. As for errands, meetings/events, and medical, bus rides - which is the only commuter transit available in my area - are just not practical.

The schedule for the bus 3 that goes to Canby only has a certain routes hours per day. "I live in Wilsonville and go to Clackamas community college and there is no fast or easy way to get and come back home there without a car. I don't have a car. I try to take the canby area transit and the smart transit whenever I can and whenever the schedules align (takes 1-2 hours). If they don't align I have to go through Portland which takes 2.5 or more hours. I don't have the time to spend 4-6 hours a day on a bus especially when it only takes

30 minutes to drive there.

My orthodontic clinic is in canby and sometimes it's hard to get appointments when there is no bus to canby after like 9 am.

The main issue here for me and many other students I know is the lack of transportation options for CCC students who live in Wilsonville. Which is strange considering we have a CCC campus. There should be a shuttle that leaves people on the Wilsonville campus at the very least."

I don't use transit services

I live in the Dales, work in Tigard, go to school in Wilsonville

I live 20miles away, work in Washington.

time

too far from bus

too far away

I don't understand the bus routes area well enough

Don't know the routes

I don't ride to takes too long

time 2 hours, vs. 30 min. drive

Don't know enough about it

Its just not available yet, and it would cost too much money to do so without public transportation

No good and easy service to Wilsonville.

There is no link to CCC main campus

I live in Beaverton

night classes

Currently I cannot get to work on the weekends, from Charbonneau. Additional service on the weekends would be helpful.

Lack of available service at some times to and from Charbonneau.

No rural bus stops where I live.

not available

Buses do not come into woodburn

Buses don't leave early enough / stop too early in the day in Dundee

Earlier route for #6, maybe has to match with WES Schedule?

live in the boonies

drop off for night to early, need 10:50 pm drop off

I can but I don't

The hours and route not service 2

Church on Sunday

Commerce Circle to Portland later at night

live too far away

WES train only operates week days at peak hours.

too far and no bus

I don't think about it

No weekend services or midday services

## **General Comments**

I'm not convinced its worth spending millions of dollars to make an attempt to reduce pollution a small amount. I don't feel pollution is a problem. I would rather see taxes and fees reduced and cut back on public transportation.

We need a less circuitous 4 to go across town. We need more direct service to Portland and more mid-day weekday service to Salem.



SMART is far superior to what TriMet provides so overall I am happy. It would be better to have some Wilsonville-Portland transit options later on weekdays and Saturdays and Sundays as well. I think you would find more ridership in non-commuting hours. Perhaps you can do a few trial routes?

"I feel SMART is a valuable component to our town. Do not allow TriMet bully SMART. Adding neighboring communities for economy of scale is fine as long as there is NO negative impact to Wilsonville service or additional cost to our businesses.

TriMet needs to fund SMART service to the prison..."

I love would to see move service in the Villebois area. I live on Villebois DR N near Boeckman Road in the new construction and it is very difficult to navigate through the construction mess to get to bus stops. I have loved the new shopping shuttle service but would like to see it expanded. Renee is amazing and a real asset to the SMART brand.

"Subsidized taxi for medical transports, not SMART.

Have direct route from SMART park and ride to downtown Portland.

Discontinue WES. Don't start WES to Salem."

Would like to see Tonquin Trail between Sherwood and Wilsonville completed. I commute regularly by bike between Sherwood and Wilsonville.

I wish there was a very limited "holiday schedule" for those times when the 2X or other work related buses could run a few times first thing and then again at the end of the normal scheduled day. Even if it is very limited, like four trips first thing in the morning and four trips at the end of the day, it would allow some transit to TriMet and work on those days for those whose only other option is to carpool or take a taxi to work.

Doesn't have to be frequent, but early and late options help getting to and from work. I live and work in Wilsonville.

Connecting students to high school and higher education

The hub and spoke system has complicated the issue of having a reliable, consistent and convenient knowledge of how and when one can use SMART. It seems that an average trip now takes longer than it did previously when running the circular route that was around every 30 or 60 minutes. Perhaps there is a hybrid approach. Or, could you use the circular approach with 2 buses after peak hours and on weekends to provide broader coverage with fewer units running?

"My perception is that the fleet is under utilized with larger buses that are not typically anywhere near capacity. It would be nice to see that data to understand how well the

services are being utilized.

If the fleet is under utilized, would it make more sense to have smaller vehicles run more routes, or more frequently on the existing routes."

Why are you asking if applicants are a person of color? Why would that matter in this survey? Shame on you.

Charbonneau warrants and deserves more service, considerably. Examples include more frequent availability, (ideally 24-hours/7-days,(,) service) with more stops in Charbonneau, between Charbonneau and the rest of Wilsonville, such as S. M. A. R. T. Central Wilsonville Station, please.

Expanded service, please.

We need more trains running round trip during the weekend.

Get your schedule out so it can be picked up by @transitapp

Many of the 7 are equally important, so most people's answers are probably not that significant.

Connecting transit with bicycle and walking paths is important to me as these are the methods I use most often to get to bus/train stops.

Public transit with children makes the trip less likely. Wish they were more family friendly I would love to have the opportunity to take the WES to Beaverton to catch the MAX evenings and weekends as well as midday during the week.

"Some suggestions:

1. Increase frequency and/or extend #7 bus route (Villebois) to east or north Wilsonville (e.g. combine with #6 or #5 routes).
2. Routes to Sherwood or Newberg.
3. Airport shuttle!"

A lifeline to trimet max line - would be so nice to be able to take transit into Portland for Timbers games - concerts, Blazer games, convention center events, symphony, opera, theatre but it is incredibly difficult to work times around returning to Wilsonville after an event - even an early show

This thing is fun!

I would love to see a better Viilabos route from the transit station. A more direct route that could even serve the people of Park at Meryfield

[Name] is my favorite driver. His friendliness is a+

Have poor height on bike racks. Not all the same. Poor driver leaving riders out in the weather while they go inside, How many breaks do drivers get??

She was kidding

For the "How would you describe yourself?" question, why would you have "person of color" as a choice? Does it matter what one looks like when riding a bus?

Getting over the river(other than Canby Ferry) in a safe way would be very nice plus an alternate bike/walk route.

I live in Beaverton and Wilsonville. It is difficult to continue the commute with limited transportation options.

Much as I dislike it, I believe I will be forced to move out of Wilsonville because of the lack of useful transit.

Expanding the WES to Salem/Keizer would be fantastic. I currently commute via my own car mostly, and avoiding congestion around the Boone Bridge would be a relief. I also take the WES after work into Portland. It sure beats fighting 217 and I-5 traffic for any weekday night events I would attend.

SMART has gotten a lot better in the past couple years. I haven't had any complaints about buses not showing up on schedule in well over a year, so I'm very pleased! I only wish there was a way for the 4X to run just a little more frequently, although I realize that's probably an unrealistic request.

I'll be honest I have a car, so most of the times when I use transit its because I enjoy the experience of it. Sometimes after work I catch the WES for an afternoon walk in Portland and I used to catch the 2X to PCC. So on weekends I'd love to catch the bus up to Portland and enjoy the day but with the limited route times and the fact it only goes so far out before I have to cough up some money to trimet. I really just think it'd be better to use my car. Same with Salem on weekends. Being in Wilsonville on the weekend is suffering sometimes, with limited transportation from SMART it does change a lot of my plans about just using my car.

Love the bus! I wish it stopped closer to central Portland, but that would be tough?

Consistent hours of operation are important and I have had no problems with Smart not being punctual.

Waiting for the bus after work can be a scheduling problem and I have to stand outside in the rain or I can stand back across the parking lot in the building and watch for the bus but have missed it a few times doing that (Xerox). Sometimes I work late and the latest Smart

bus does not go to Barbur and I big fiasco to get to Barbur through Trimet in the evening and will take 2 hours extra. I need to pick up the bus at Barbur but that requires me to take back roads going the opposite way and too far to walk and too much time to ride on Trimet there . If I go to Tualatin P&R I might as well as ride all the way in to work because I am on the Freeway by then from Beaverton. Bus transfers chew up too much time and are not cost effective that is why I like Smart. Smart drops me off next to my work Straight from Barbur and back (that part is good) I still have to drive 3 miles from my house though and riding a bike is too dangerous at night when I come home and I have to climb a steep hill to get to Barbur in the morning. The Trimet light rail (1 mile from my house) does not operate late enough for me to use and the cost is as much as the gas so that is why SMART works. It is relaxing to ride as no hectic bus transfers. I think my plight is typical for a lot of bus riders but a few things that make the trip attractive are . No bus transfers that chew up a lot of time waiting. Close to work building delivery as people are usually wearing nice clothes to work and walking very far is just not very attractive or safe for some especially at night. Cheap bus fares are important as if it gets too close to gas prices it is a tough argument to take extra time and energy to take the bus.

I don't understand the option "allowing growth and mobility beyond what congestion would allow" - do you mean dealing with rush hour traffic, or allowing housing developments to spring up in new undeveloped areas of land? Everything else is clear but that one option.

I have no intention of ever using public transit.

I feel badly ranking "Providing transportation for people with limited physical mobility" so low. In fact, I think that, as a society, we should absolutely be providing that. But, the question said "valuable to you," so I ranked it low since I don't have limited physical mobility.

More commuter shuttles to villebois. Go over the new bridge!!!!!!!!!!!!!! Include travelers from verdun loop

"All the SMART staff I've interfaced with have been fantastic. Kudos to you all on that.

My only complaint at all is the limited service to the Villebois neighborhood. I ride my bike or walk to the transit center, so it's not a huge problem, but there are days when it would be very convenient, say for running errands on the weekends or evenings. These are things I used to do when I lived at Canyon Creek, but don't have the option to do now at Villebois.

Aside from that, no complaints. Keep doing what you do!"

Please don't just focus on low-income people, people of color, and old people. We should all be riding the train and bus to work/school every day.

We need WES to provide mid day and weekend services. Would like WES to Salem as well.

Villebois has a huge population and no regular bus service!

I would like Smart to provide transport from Portland to Wilsonville to and from work as I am willing to pay monthly fees to get work in time and back home. If you know of one, please let me know.

Bus to West Linn and Oregon City would be so helpful!

Bus to West Linn and Oregon City would be so helpful!

Having additional transportation options to Portland would be extremely helpful. Wilsonville becomes completely disconnected to Portland a lot of the time eliminating my ability to utilize public transportation. This also prohibits Portland residents and visitors to easily access all the wonderful things that Wilsonville has to offer.

I would like easy direct transit to downtown Portland. Also - there really is not way to get between Wilsonville and where I live in Canby - we need more options. I would like MAX to Wilsonville and Canby - then I would ride it often. Right now, the buses and MAX do not go where I need to go.

Some type of Sunday service would be good. Having a bus that goes East into Oregon City would be good. Since Oregon City is the county seat there are many county services that need to be accessed there. Also, a plan could be worked out with Clackamas Community College to have some type of connection service. CCC has a ton of shuttle services but nothing that would get me from Wilsonville on campus.

Dial a ride on the weekends for transportation to CHURCH for those that don't drive.

Keep up the good work.

Need service to Newberg Hospital please add for future service. regular service to Tualatin would be handy.

SMART does a wonderful JOB

Would like bus to Newberg, for dr. visits and to visit those in hospital.

More hour and days for a reasonable fee. need Newberg service

need suburb access

keep free service

Sunday service is needed

I use SMART for shopping and to get my hair done. It works well. I moved here 2 years ago and feel lucky to have SMART here

Provide transportation to downtown Portland on Saturday and Sundays- ALL DAY

I dream that all buses have the low floor ramps the new buses have. I can walk but can't handle any stairs. Nothing against Dial a ride, but the regular bus is freedom to me. I can make impulse decisions to travel if the bus had the low floors on the regular routes

Need bus service to Portland on the weekends.

Husband used DAR for years and I rode along with him. He had to stay at the hospital, for a length of time before passing. I wanted to ride DAR to visit him, as I was upset and couldn't drive safely. I was told I couldn't use DAR because I was "able". Is it possible for long term caregivers to use DAR when still caring for the DAR patient? Long term Caregiver to DAR rider needs assistance to visit them.

Mentally struggling with grief driver does not need to be on the road.

Need service to Newberg. Also need service to Day Dream Ranch area.

I want someone SMART to invent and develop and inexpensive JET JACKET for my transportation, I am only 85.

Currently live just outside area but assume in time we will be incorporated. Butteville area.

Looking forward to help with transportation even though I'm just a few miles from Wilsonville.

Connection to Tualatin, Portland, Beaverton is important to me because that truly reduces traffic and significantly expands mobility. Providing mid-day and evening connections to Portland and providing some service on weekends would also allow access to Portland for major events.

"Keep in town rides free!

Review your population demographics so that the small buses are not over crowded.

Do not allow drivers to skip stops during rush hour.

Tell drivers to go over speed bumps more gently please.

More ways to connect to TriMet."

I work at the World of Speed Motorsports Museum. We have heard from guests that they would like the option to use transit to visit the Museum.

It would be great to have more frequent, and regular, bus service to places like World of Speed.

Spend the money currently used on buses on adding more lanes to reduce traffic in town.

"Minimal transportation" seems like "inadequate transportation" . It might be of much higher importance to me if I understood what it would be like. Schedules are important to me as I take classes at the community center, but drive rather than getting there way early or having to wait long after class to get home. Better affordable connections to train depot and airport would be wonderful. It would have taken me 1 1/2 hrs to get to WV transit center from train depot ... and I didn't even look to see how much longer from transit center to home. (Or \$55 by taxi).

Having a line 2 that leaves Barbur and arrives at the main Wilsonville Transit Center by 5:50am or so would be helpful.

the worst is running errands on the wilsonville rd route but having to stop at the transit center. please make more routes during the day skipping the transit center. also, why isnt there a bus stop in front of the library?

I would like better access to the Beaverton transit center to ride the MAX line to downtown Ptd. There are many activities in Portland in the summer and we can't get there from here. I still drive for local trips but no longer drive downtown. After living on the east side, where I had access to MAX and rode it often, I was disappointed in the limited availability to downtown. I'm not interested in the commuter train when I would have to stay all day. And it doesn't run on weekends.

the previous question was difficult to do.

the previous question was difficult to do.

the previous question was difficult to do.

[NAME] is the BEST driver!

I think we need some weekend shuttle service, and maybe remove the window signs we we can see out of the bus. Also your best driver is [NAME].

[NAME] is an excellent driver and very kind and considerate to al. It is hard to for people with disabilities to be comfortable with those who are not.

[NAME] is a great driver and good person. We need more weekend bus trips.

I would like bus service on the weekends.

Transportation offered on weekends in the tri-counties and local for city churches on Sunday..

Excellent service, especially the new Villebois shuttle.

I am very grateful for the SMART bus.

Could you add a Saturday service to Villebois, like the weekday service?

Have weekend service in Villebois like daily weekday service. Keep [NAME] as a driver, she is gret!

EXPAND DIAL-A-RIDE SERVICES, EVEN OUT OF AREA FOR NON-MEDICAL. WITH A FARE CHARGE EACH WAY. AND CONSIDER WEEKEND SERVICES; ESPECIALLY FOR DISABLED AND SENIORS WHO DO NOT DRIVE.

Would use transit if available eves and weekends. Do not commute 9-5.

"1 Allowing growth and mobility beyond what congestion would allow: Free-enterprise, non-tax-supported transit options will be more efficient and cost effective at allowing growth and mobility.

In fact, Free-enterprise, non-tax-supported transit options will provide more accessible and cheaper transportation mobility to young people and families, low-income people, people with limited physical mobility, and to everyone, regardless of where they live. Trying to coerce and steal, and erecting barriers-to-entry in the transportation vocations can never make it better."

MY NUMBER ONE ISSUE IS WITH WES TRAIN NOT SMART, THAT IS IT STOPS RUNNING IN THE MIDLE OF THE DAY BOTH WAYS,

I want to attend sporting events and cultural events in downtown Portland by using public transportation. I also want to shop downtown without having to worry about parking.

I would like to see transit service along I-205 between the OC transit station and/or Clackamas Town Center Max station to Wilsonville.

If possible, I'd like more covered bus stops.

I do not use the transit system but understand that it can assist in increasing my customer and employee base.



Please do not attempt to use transit to have the City grow beyond its congestion capacity.

I live out by the prison and would love for my kids to have a way to get around.

Provide service for smaller routes outside the big cities later than 7-8pm and provide weekends always.

It would be nice to have the option to ride a bus to work so it save on the environment and also to help with parking.

I would use it to get to the airport, if it was more convenient.

I would like to see seats or benches at ALL bus stops in Wilsonville. I can't use the bus because there isn't a seat at my bus stop on Boberg Road.

"Regarding Q4~I would rather walk less and wait less.

Smaller areas, like Hubbard, need more transit options."

More options and more trips in and out of Wilsonville especially after 10pm and weekends.

I use SMART for all of my transportation needs as I am a poor driver, so am grateful for the service.

I drive, but I might use public transport to save money.

None.

More coverage all week and weekends is VERY important!

Ranking the options on Q5 is impossible. These are all important. This survey is poorly written and does not inquire about weekend service which is needed.

Not really...But you should really do 7 days a week.

The WES should start earlier, around 4am.

Please allow us the ability to move about 7 days a week.

My actual answer for Q4 is walk less and wait less.

Possibly having a bus around as early like 4am

More trips to and from Salem

We need service to Woodburn!

I really wish the buses/WES would run on weekends

Great service!

Limited weekend transit options prevents employment options for those looking to work in Wilsonville

As a person who has to drive to work in Wilsonville from Hillsboro because the bus does not run to get here by 5am I feel like this is not very relevant.

I wish you and Trimet shared fares.

The company needs to enforce their policy and stop trying to be politically correct.

We need WES service on weekends!

WES should run on weekends

Open up the WES all hours and all days. Thank you.

Thank you my local Smart Bus services.

Thank you for SMART. It makes commuting from Beaverton to Wilsonville (via MAX, WES) pleasant instead of a gridlocked nightmare.

Please add an option from the east side of Portland to Wilsonville that takes less than 2 hours. Please please please!

It would be very nice to have the SMART bus go from Wilsonville to Tualatin along Boones Ferry with stops along the way instead of going directly to Bridgeport along the freeway.

Free in-town bus travel in Wilsonville is a huge convenience. I wouldn't object to paying for the ride, but the convenience of not having to find money or dig out a pass is worth a lot to me. I use the bus to commute to work about 4 days each week.

The traffic is getting worse and worse. It seems no matter what day or what time, I-5 is backed up around Wilsonville. I-5 becomes so backed up that the traffic then backs up onto Wilsonville Rd. Offering transportation from the Wilsonville TC to get out of Wilsonville at a variety of times, and not add to the congestion, should be a goal.

I think that our top priority should be to address the limited north/south river crossing issue. The fact that we have one bridge crossing is a current problem because it is a traffic bottleneck during peak hours. It is also a future liability because of the aging infrastructure and the likelihood that our primary bridge will collapse during an earthquake. We need to

immediately take action to strengthen and upgrade our existing structure. We also need to add a secondary bridge within close proximity, perhaps at the location of the current Canby Ferry crossing. Once we have this in place we need to look at expanding high speed north/south transportation options along the I-5 corridor.

We need more 1x service to Salem and more DIRECT service to downtown Portland.

Go to Milwaukie, meet up with the Orange line, please!

Smart is fairly well run compared to trimet (if you ignore WES debacle), but it doesn't need to do any more than it is doing already and should services should not be increased as a justification for pushing higher densities or ignoring the need to build more roads. Our traffic congestion problems are the result of planning failures, not an under-investment in transit.

I ride the new Villabois (sp?) to get seniors to the stores.. I LOVE the new bus line.. expanding to Target and that end of shopping would be great!!

2x route is excessively long for those trying to leave town from the transit center. The 2x route needs to be consistent regarding the service at commerce circle, mentor, Rockwell and OIT stops. Either provide service to those stops at all times north and south trips or don't service them at all. Makes it difficult to track when there is service at those locations, especially for new passengers. Possibly consider making Tualatin P&R a drop off only when leaving town. Hardly anyone heads north from there. Those who do ride can use trimet to get to Barbur.

I would recommend providing even more room on trains for bicycles. Now that electric bicycles are going to be very common place, the few bicycle spots currently on trains are going to get use up quickly.

I think SMART is awesome! I can choose to walk or ride the bus for free!

I depend upon SMART Transportation

Would be nice to have an express bus from clackamas town center to oit

I wish the Wes train ran on the weekends because I would use it for errands I do around the Washington Square area.

This is such an important topic for Wilsonville. I am glad we have SMART in our community. I feel very fortunate. The SMART drivers are usually helpful and friendly. I really enjoyed riding with some of them. Wilsonville's in town traffic is getting worse and with all the house development in planning (or in construction) it will be important to improve our transit system to relieve the traffic in the city. Also, the support for the seniors should be kept (Dial-a-ride).

"It seems crazy that there is no good way to access the county community college from Wilsonville without taking a 2-3 hour bus ride, each way, through Portland. (And don't tell me to use the Wilsonville college extension - they do not offer the classes required.)

Also, it sure would be nice to be able to use public transit to get to & from the airport from Wilsonville. Right now, it takes 2 hours to get there in the morning and 12-14 hours to return in the evening. Obviously, that is a situation that does not work.

I consider the public transit system in Wilsonville to be working well for local travel, but fundamentally broken for non-local travel. For example, why should we have to take a train to Beaverton to go to Portland? I wish we had real, practical public transportation alternatives that would allow me to replace my car. But we don't."

"Their needs to be more buses to Salem from Wilsonville plus more buses running from Salem to Wilsonville from 10 AM to 3pm."

I often see Jehovah Witnesses proselytizing outside the ramp where everybody exits the WES in Wilsonville. I am very offended by this, and request that religious proselytizing be prohibited. It's offensive to human intelligence and makes SMART look dumb.

"Wilsonville is just like other small cities that tucked in between 2 big cities and will not be able to escape the explosion of population in the future. Therefore, public transportation will be immensely important to reduce the congestion of traffic for commuters.

It will be great to see a bullet train running from North to South at the state borders and have station stop along cities en route with the support of community public transportation."

better connection to visit downtown Portland

"I am impressed that SMART is free in-town.

I'd like to see an easier connection to Oregon City, where social services mostly are."

"We would like stops that service 95th Avenue and line up with the WES. Currently, if our staff use the WES or a rapid transit bus line, there is either a 1.5 mile walk or significant wait and indirect route for using the bus.

We plan to move a significant amount of staff from our Tualatin office to Wilsonville and without reasonable access to public transportation, our staff will likely not use it.

-Pacific Foods"

"8-5 makes sense and should be top priority, but find other off times, that would be

frequent such as for night classes is a priority as well.

Having direct service from PCC Sylvan and probably Rock creek as well as a few others from Portland to main areas in Wilsonville would help- OIT, businesses such as Xerox, Mentor etc."

Thank you for SMART

My mass transit concerns are more focused on the nightmare that is the Columbia crossing

I would like to use WES for commuting if I could be closer to access. I would also like to see a max line south along I5 from Tigard to Wilsonville

Before I had a vehicle I depended completely on SMART- not having service on weekends was extremely inconvenient. I would still use WES on weekends if it were available.

Bike lanes are sorely needed on Advance Road between Wilsonville Road and Mountain Road. Not just for the many adult riders, but children who will be attending the new middle and elementary schools.

Expand service to weekends in Charbonneau

I would like to go to Portland on weekends to attend movies, concerts - all that cities offer. Would like WES to run more than commute.

"Your current service to Villebois does not make sense to most residence. I could not answer the following questions from a visiting family member.

Is there a regularly scheduled bus/shuttle or do we just call for pick up? Where do we wait? What are the scheduled service hours?"

I wish there could be more buses like the 1X which works very well for State Employee's working around the Capitol Mall area, however other state offices not in the Capitol area are difficult to get to by Bus or not accessible at all.

I think Transit options are great and much appreciated. I have used Transit in the past when I didn't have a car, and it allowed me to get to work and home. I live in Sherwood but work in Wilsonville, so I don't know if there is Transit service for me to get to and from work. However, I like the idea of using it for shopping and recreation.

All of the listed options are important, but I some things won't change, having a bus that goes where people need it for an affordable cost will encourage people to use their cars less and having a bus as an option automatically reduces green house gasses in the atmosphere. I think we should focus on giving people transportation first, then focus on the environmental aspect second.

One is always the most important not 7, confusingly worded questions.

Bi Hourly routes through the middle of the day as well as morning and evening

In the afternoon offer half hour pick ups vs. every hour

I would love to see direct bus service from Brown Road directly to Eastside mayor employers. Current commute time would be 35 min vs. 7 minutes by car. I would be nice if it could be less than 20 minutes.

If I worked outside of Wilsonville , like I may In the future, I think direct routes to Portland would be valuable.

Wilsonville transportation is very helpful. having a way to get to oregon city would be awesome.

IF I didn't have a car then I would use this.

There needs to be service to Charbonneau on the weekends. We have places we need to get to on the weekends just like everyone else!

Needs to be weekend service to Charbonneau.

Additional Charbonneau service please. Weekend service to and from Charbonneau is of top priority. Continuation of weekday service to from Charbonneau is top priority.

Have a dedicated area for buses to pick up passengers off travel lanes: When buses stop in travel lane, increases congestion higher risk of rear end collision, difficult travel by for bikes less reduction of CO2 with more cars driving behind bus.

24-7

Add more pick up and drop off times 24/7

Need more drop offs and pick ups 10:45pm-12:15 am for people working night shifts

I used to use SMART everyday. I know a lot of people that would use the extra service.

Do not live in Wilsonville. Too expensive.

Is it possible to adapt transportation times to accommodate people who work graveyard?

Transit isn't much use for people who work graveyard

Thank you so much for getting me to work every day. Maryanne rules!

Have the train run on weekends with connections to downtown Portland.

I don't use transit but thinks its great

Seriously, no one can have it all. If the city turns its back on affordable housing and refuses to address the crisis in the rental market the quality of living will be impacted in the most negative manor. Plan on seeing more evictions, more homelessness, more crime, and, worst of all, a decrease in quality of local schools.

The Bus system as it is makes this a wonderful place to live.

Thank you

We appreciate the pick ups for our preschool strips in Town. Valley Christian Church.

## **Service Alternatives Community Survey #2 Comments**

The below comments are categorized geographically. Some comments may apply to multiple categories. Comments are verbatim with the exception of personnel names removed. No spelling or punctuation errors are corrected.

### **Q. 4 Intercity-Focused Alternative**

#### **What specifically do you like or dislike about this alternative?**

##### **GENERAL COMMENTS**

Intercity travel is key to dealing with the fact that such a high percentage of people live in a different city from where they work. Having intercity connectivity on weekends not only helps workers, but helps retired people participate in activities throughout the region. It is important to provide alternatives to individually-driven automobiles.

I live downtown Portland and work at Oregon Tech. I teach evening courses that end around 9 pm. The intercity-focused alternative, while somewhat more limited than the current service, provides service at the times I need.

Without knowing what TriMet connections will be available when, I cannot say if I will ever be able to use this.

I like the ability to go out of town to see family.

It's awesome.

Like the availability.

More Flexibility.

Will reach farther into my neighborhood.

Easier to get to work on flexible schedule.

Utilizing public transit outside of Wilsonville during non-commute hours is challenging. WES is our only option.

Completely neutral - My transit involves the 6 and WES, which do not appear to be affected by this proposition.

I like how you make it free for residents of Wilsonville.

How much farther would I have to walk to get more frequent trips and will there be parking at these alternative stops?

I appreciate that students have options if they don't own a car.

Looks like it runs through the hot spots of the city.

The current service meets my commuting needs well. I don't need better intercity service.

I commend SMART and the City for trying to do the best with limited resources. The workshop exploring trade offs for public transportation was very helpful.

Expanded service.

Dislike lack of specifics.

It reduces traffic and parking issues created by individual driving. It is more environmentally friendly. It is cost effective for the commuters.

Need more buses. Like connections.

My teenage children could take the bus to get to different places. I could take the bus to get to my volunteer commitments.

I drive everyday, and my route varies depending on what I need to do after work. If my route was consistent, then this works well for me.

Neutral on this alternative. Variable start and leave times do not make transit viable alternative.

Will not apply to me, as I will not be taking a bus to these neighboring cities.



There isn't any bus from Lake Oswego to Wilsonville.

I live in Lake Oswego. It does not change the situation that I am in.

I don't currently use public transit to get to/from work. My commute is too far to consider this as an alternative for me. There is never an easy way to get down 205 from Milwaukie area.

I live in West Linn. Taking transit to Wilsonville requires connecting in Portland. That's not feasible due to time constraints (~90 min each way). Neither option will help make transit more available for me.

I live in Sherwood and this alternative doesn't do anything for me.

I don't use mass transit. I live near Mt. Hood.

Que es una muy importante para mi familia (That is a big importance for my family).

Provides a viable alternative to WES when WES does not run. It would be best if the Trimet 96 could run to WES Wilsonville but, in the absence of this possibility, this is the best alternative.

Making it easier for me to get to places

I ride the WES and the 6. This does not effect me.

Tigard tc is a good transfer point for trimet

It's an improvement but it will not impact whether I take transit or not because when I drive I have more control over when and where. If there's a problem I can get to my destination faster than transit. I'd be more inclined to use transit to get to Portland if I can come home without waiting 6 hours.

I use SMART primarily to get to and from work on weekdays (Route 4 - 8:52am from Boulder Creek stop to Mentor Graphics stop) - this still requires quite a bit of walking, and the timing isn't perfect, but I like to use public transport. And secondly I use the system to connect with my husband when he has our car, taking the 2x to Barbur TC. Tigard TC will work, even if it is less convenient.

Better connectivity among neighboring cities and taking advantage of current TriMet infrastructure especially managing the service gap when WES is not in service

The dark green on the legend still indicates that Wes will only run during rush hours. That is still not helpful.

Better connections to TriMet

We only use the 1x and the 95 on weekdays. All I can say is that SMART has the BEST drivers. My wife and I think that they are great. It is wonderful to let them drive.

I like the concept of improved intercity connections.

I dislike that service to Barbur TC is being terminated.

It increases my commute time from ~40 minute to over an hour. This is a show stopper for me.

I dislike that SMART service (5/6) intervals at my place of work is reduced to hourly at rush hours.

## **WILSONVILLE TO PORTLAND**

I can't believe you are getting rid of rush hour 2X.

It always seems well used at the times I use it.

I think this connection makes Wilsonville seem part of Portland-area society, instead of an isolated island.

Still not enough service to ride from Portland to Wilsonville in timely manner.

Dislike when you get sick can't go home by WES.

I'd like more pdx.

Better alternative to driving to and from work when there is more frequent service to TriMet.

Like easier access to TriMet.

Better connections to TriMet.

More connections to TriMet or every half hour to Tigard is great.

Like the improved connectivity with TriMet.

More frequent trip opportunity to the broader Portland area.

Connections to TriMet need to be improved. Several co-workers who live in Portland would never take SMART because the transit time takes too long, and too many connections.

One of the hardest things about trying to live a low-car lifestyle in Wilsonville is the ability to get to other places outside of commute hours. Wilsonville is small enough to bike or walk around for most trips, despite being a car-first town. Getting outside of Wilsonville is where

traffic is worst and connections are hard.

At the very least, you should incorporate 95th & Commerce Circe on the 2X so those of us connecting to the 96 to go to work for the morning commute still have a chance to get to work. For example, if the current morning bus stopped at 95th & Commerce circle before going north o 1-5, I could catch the 96 there.

Better access for those of us who only work in Convergys.

Like the every half hour when WES isn't running.

Like more frequent routes, unsure why WES can't do them, much faster, less new buses.

More frequent access to MAX.

Like being able to connect to services that connect to Portland.

Better access to downtown Portland and PDX.

It does not help meet potential riders from the East side of Portland metro area.

I dislike about this alternative that it lacks a fast connection to Portland and that big shopping malls like Bridgeport or Washington Square Mall are not considered. Rather I would like more, if the planned Terminus is instead Beaverton TC (with a fast MAX connection to downtown) and stops provided at Tualatin TC and Washington Square Mall.

I like that there would be an alternative to connect with MAX, when WES is not running. I would use transit a LOT more if WES offered service on the weekends, and at regular intervals throughout the day. I have been stranded more than once, because I missed my last WES connection back to Wilsonville. :(

I no longer live in Wilsonville, so I don't use the bus system anymore. Increasing trips is always appreciated.

Intercity Alternative proposed suggests I now take 3 busses to get from home (Montebello Dr) to Work (Parkway Ave).

I would like to have TriMet bus(s) closer to SMART transit center.

Most of my work is in the greater Portland area and having a timely way to reach this area would greatly increase my use of the transit system.

The #4 is running down the exact roads that get BACK UPED during peak hours. The problem is I-5. Everyone gets off I-5 at Tigard thinking the "back roads" are faster. Kinsman, Wilsonville road, Boones Ferry road become congested clog messes, such that it

takes 45 mins to get from the Nissan dealership to the Fred Meyer! This is insane! You have no transit options on WES during the day. Try increasing that! You can't get to Portland by transit in anything less than 2-3 hours when the WES train isn't running. Trying improving the WES running times and get the Maxrail down here!

TriMet should do their part to improve intercity connections to the North. SMART has a limited budget and as such, should concentrate on getting passengers in and around Wilsonville with a hub at the transit center as there is now. With improved TriMet service this would be a very good system.

I didn't take the first survey, but my primary concerns are for those who can't afford cars, or can't drive due to age, to get around. I selected this alternative as it connects more to other transportation, which I think would be better for working adults that don't have cars, and to get cars off the road. However, I personally don't ride, so I whichever routes are used the most, should be continued. I do ride WES to get to Portland and the airport, so from Charbonneau to WES would be great, but not a priority for me.

You have increased the journey time between Portland and Wilsonville. It can take an eternity for the Tri-Met 12 or 94 bus to get from Barbur TC to Tigard TC on Highway 99. Having the 2X serve Tualatin P&R and Barbur is a much better idea than this new plan. Having the 2X run to downtown Portland when the Tri-Met 96 is not running is an even better plan.

Like the weekday service to Tigard Transit Center every hour when WES does not run.

It goes to Tigard.

Better connectivity to Tigard.

Like Tigard access.

I do like that the 2x will travel to the Tigard transit center, I dislike that the 2x will no longer be stopping at the Tualatin transit center on the way.

I like the idea of the weekday service to the Tigard Transit center, because that means it's a more direct route and I could get there faster than I can with the current schedule. This would affect me in the way that when I take the train to work, I could get closer to my home (in Beaverton) sooner with Smart Transit, if I get sick in an emergency or need an urgent ride home to get to my family. This would benefit my transportation needs. The last time I got sick at work, I had to take the #2 Smart bus to the Tualatin Transit Center and take the TriMet bus to Beaverton and it took me 2 hours. It was miserable. Unfortunately, the idea to discontinue service to the Tualatin Transit Center and the Barber Transit Center affects quite a few riders. I have always seen riders get on and off at these stops. I have coworkers that take the #2 bus to Barber everyday. This would be a major change. I like the idea of having improved bus connections with TriMet, but I am not certain this will fix the issue.

Catching the downtown connection to TriMet in Tigard adds 20 more min. to the commute.

I like that it goes to Tigard on non-rush hours. I don't live in W-ville so it's unlikely that any alternative is useful to me except as emergency backup--it's 2 miles from my home to the nearest tri-met stop--but if I had to get to work by bus I need to get there from hwy 99 Tigard-King City area. My coworkers I know who bus it in are coming in from Portland, NOT Beaverton.

I like that these routes seem a lot more efficient than current routes. I also like that the 2X will go to Tigard TC since there are a lot more TriMet connections. No complaints. :)

There needs to be a way to not have to pay SMART and TriMet if I want to go home to Tigard when WES is not running or even a discount.

I live in Tigard so it doesn't affect me much, but would benefit employees that live in Wilsonville.

Connecting to Tigard transit center would make it possible for me to get home when shift runs over, has parking there.

Having the 2X go to Tigard only when the WES is not running kind of makes sense, but it would be better if the 2X and WES ran simultaneously, as they do today.

The elimination of 2x will make it impossible for those who start work in Wilsonville prior to 8:00 a.m. to arrive on time. For example, if someone is commuting from Vancouver WA there is no public transportation from Vancouver to downtown Portland that could get the person to WES in a timely fashion.

You have also removed the half-hourly connection between the 2X and Tri-Met 96 at Commerce Circle. So I might be able to take an express bus from Portland to Wilsonville, or vice-versa, but then have to wait an eternity at Commerce Circle, or have to walk over the freeway (which is no fun, believe me).

I guess the wait at Commerce Circle may still only be 30 minutes, but only if you time the clockwise and anti-clockwise buses just right, and even then, every other trip will take much longer to get to Xerox / Mentor / OIT etc.

I like that it connects to TriMet better and more often. I like it because Convergys can retain more employees who rely on public transit and don't have cars. WES is a terrible option for hundreds of us who work evenings/weekends.

I will appreciate if there is Smart going to Gateway transit center, as it will cut down my commute time by 30 minutes or so.

I cannot stress highly enough how inconvenient this new plan makes it for people traveling between Portland and Wilsonville, just so that the "Beaverton people" get a slightly improved service. Today, I can get from downtown Portland to the office in Wilsonville in at little as 37 minutes. But to do this, I need a little luck, and, more importantly, I need the 96 - 2X connection at Tualatin. I can jump off the 96, and immediately board the 2X, and although it costs me an extra \$1.25 or \$1.50, I get to the office 30 minutes sooner than staying on the 96 and going to Commerce Circle, where I would catch the next 2X.

Need to connect outside of Wilsonville. TriMet and Canby transit not adequate.

And by the way, Tri-met tends to run the 76s and 78s between Beaverton TC and Tigard TC at the same time, so you're not really doing those "Beaverton people" a great service. What I mean is that, when you're at Tigard waiting for a Bus, you'll get the 76 and 78 coming along at the same time more often than not.

In summary, this proposed Intercity plan does make some improvements, but it's a disaster for folks commuting between Wilsonville and Portland. I will have to buy a car if this goes ahead.

I dislike cutting Portland out of the picture. If you want to get to downtown Portland, transferring at Tigard is much less convenient.

I like that it improves access to Canby/Oregon City, and that it improves access to Tri-Met when WES isn't running. But none of the inter-city routes benefit me or my family personally. Every 30 minutes seems awfully frequent, but if the ridership is there to support it then I think it should be supported if it can be. I would have liked an "unsure" option here, I settled on '3' as the closest.

I like to see an option to get out of Wilsonville into the bigger world on the weekend.

Anything that will better connect commuters to TriMet's network and limit the amount of travel times is good.

Like that the bus runs when the WES does not

NO late night. 12:00 to 1:00 am rides from Portland to Wilsonville to go to events. Expand later.

I would like the 2X to continue to go to Barbur Transit Center. The WES train is much less convenient, since I work near on the 2X line and not the WES line. And, the Tigard Transit Center is farther from home than the Barbur Transit Center.

I attend classes in Wilsonville Monday thru Thursday from 7:50am to 1:50pm and I need the 2X for those times from the Barbur Transit center, or at the least from the Tigard transit center. Eliminating the morning connections on this route would end up forcing me to take an UBER and the cost would be prohibitive. Please do not change anything until at least the

first of the year. I depend on the 2X during these times.

This doesn't accommodate my transit needs. I currently ride the 2X from Barbur Transit and my total commute is 50 - 60 minutes. Going to Tigard and making the connection to WEs, then to another SMART but to get to Rockwell Collins would add approx. 45 minutes each way, making my daily commute 3 - 3.5hrs.

No Barbur TC connection. A longer commute for me.

No Barbur drop off.

Dislike stopping Barbur TC connection, Like increased frequency when WES not running.

Don't like the Barbur Transit Center cut.

Dislike stopping service to Barbur.

Alternatives are identical for rush hour travel to/from Zone 2.

I think you'll be sorry switching from Barbur to Tigard transit center. The routes on and off the highway at Tigard are horrible. Non-highway routes to Tigard TC go through central Tualatin, which is very inefficient.

You will have less headache if you stick with Barbur transit center, especially during non-rush hour when I-5 is fine.

I don't agree with removing Barbur transit station from the route.

Dislike. Service to Barbur being cut.

No Barbur transit stop, although Tigard is a reasonable alternative.

The removal of the Barbur Transit option would mean a longer trip for me and my co-workers, who take the 2X to Xerox Campus and Commerce Circle.

No Service to Barber Transit.

Dislike: Losing the Barbur Transit Center stop.

DISLIKE: Dropped service to Barbur Transit Center

Barbur stop is near PCC Sylvania, which is where I also take classes.

Elimination of the Barbur TC will make my commute 1 1/2 to 1 3/4 hours each way at this time its approx 1 hour each way.

El servicio no andara a los centros de transitos de Barbur o Tualatin, pero las conexiones a TriMet seran mejordes No es importante para mi familia. (The service will not go to

Transit Centers at Barbur or Tualatin, but the connections to TriMet will be improved. It's not important for my family.)

Dislike no service to Barbur, like service to Tigard TC.

I dislike that it would discontinue transit to Barbur

I don't like that you will eliminate the Barber stop.

Specifically, 2X re-routing makes it unusable for me personally, as well as quite a few people whom I see on my commute and need to get to either Tualatin Park & Ride or Barbur. I also don't understand what issue re-routing 2X to Tigard TC is trying to accomplish. Right now, Tigard TC is already served by WES from Wilsonville, and the WES takes a lot less time to get there. A major connection from Barbur is the #12, which people can take to get to place along Barbur Blvd, including Hillsdale, OHSU, downtown, and NE Portland. Taking the #12 from Barbur as opposed to from Tigard TC is a lot more time efficient.

I would miss the access to Barbur but expanding service to connect with other cities would be very helpful.

Reducing the number of stops served helps no one.

Cutting out Barbur makes sense. Folks trying to get from Wilsonville to Barbur have alternatives, which I see this plan makes better (connecting with the 12 at Tigard Transit Center instead of at Barbur) and eliminates the chronic issue of rush hour dragging down the 2X, but keeping the 2X with what currently is the Saturday route, would also remove much of the 1-5 congestion, which occurs between Barbur and Tualatin Park and Ride.

It appears this alternative does not provide good connections to downtown Portland (i.e. TriMet route 12 from Barbur or TriMet route 96 from Commerce Circle. This makes transit useless for me and several of my co-workers.

What specifically I do not like: Service will no longer be provided to the Barbur or Tualatin Transit Centers.

For people commuting between Portland and Wilsonville, transiting in Barbur might be more convenient and could save more time. It may take another 20 mins for TriMet 12 to get to Barbur from Tigard. It is also easier to get to connect to TriMet express 94 if it goes to Barbur. Tualatin transit center is close to Bridge Port, and it connects to TriMet 96, which is an express to downtown Portland. Going to Tigard Transit Center might be a good choice for Weekends.

It doesn't say where the 2 will go when WES is operating - so if the only option(s) to get to and from Wilsonville while the WES is running is to take or the 96, then it sucks.



I like that they are taking some stops out to increase frequency.

I would rather go to Barbur Blvd Transit Center than Tigard (Barbur is closer to home).

Would need more info on moving to Tigard TC and eliminating Barbur TC.

It gives a way to get in and out of Wilsonville, when the WES train is not running. Usually I travel during rush hour, but sometimes things come up.

The increase service to Tigard.

I live in Tigard so caters to me better.

Direct connection to Tigard TC during mid-day will be great for those who ride WES normally.

Seems like a long way to Tigard.

I like travelling from Tigard and back, even without a WES.

Really like the ability to meet up with TriMet in Tigard during non-WES hours. Would totally change how often I commute by transit - right now if it looks like I may work late (past being able to catch the last WES) I need to drive so I don't get "stuck". Getting to Tigard TC w/ 30 min 2x running late really opens things up. Really opens things up in terms of hitting a show or game in PDX too without needing to drive!

Like: Weekday service to Tigard Transit Center every half hour at times when WES is not running and hourly service on Saturdays.

Better connections to Tigard Transit Center during WES non-revenue hours is the high point for me.

As a person who works in Wilsonville and lives in Tigard, this would help.

I don't like that it would take me longer to get to work and classes (and vice versa) should the 2x terminate at Tigard TC. This means a lengthier commute to the heart of Portland.

Like: Service to Tigard when WES is not running.

2x connections to Tigard while WES isn't running (like).

Rush hours for 2x are cut, preventing late buses. Connections to TriMet are improved.

I like the connections to Tigard and times when WES isn't running.

I would prefer WES alternatives to Beaverton not just Tigard.

I live in Tigard, so there's a better chance I'd use this if it came to Tigard more often.

I like the trips to Tigard, but getting rid of Barbur & Tualatin would be terrible.

I live near Barbur transit center

With non-commute time options to the Tigard transit Center, I can work in the morning and take public transportation to the airport instead of having to drive to work and then drive to the airport.

More frequent service connections to TriMet. I prefer to use WES for my commute to downtown Portland, but often the last train in the morning is still too early. On those days, I drive to Barbur TC. Having a frequent connection to Tigard TC would likely work better for me than the existing options.

I live in SW Portland and am not convenient to WES or Tigard.

There is none for the WES going into BTC.

Weekday service to Tigard Transit Center every half hour at times when WES.

I travel to school in Wilsonville/ OIT from Hillsboro. Class runs late after the WES stops running. Usual class time is 6pm- 10 pm.

Would like route 5 bus to go to Commerce Center earlier in the day to meet TriMet 96 there.

There need to be at least one mid-day trip on WES to Beaverton Transit Center and return to make the system

We need more commuter options at different times. This would make a huge impact for my family!

Much better.

No service to Downtown Portland or airport.

I like being able to walk, bike and bus/train in and out of town. I think the bus should be up Boones Ferry then 72nd then Durham and hall to Tigard, rather than 217. Probably as fast as I5, 217 99w.

Rush hour to/from Area 2 is exactly the same: route 2 every 30 min, routes 5&6 every 60 minutes. For me the options are the same.

The routes today allow me a predictable travel time to work; I ride the 2X. More intercity

alternatives might increase/change my ride time.

## TUALATIN

I have doctor appointments in Tualatin, and this program does not include trips to Tualatin. That is a problem for me as a Senior.

I think the connections to Tualatin & Barbur Transit Centers should remain.

Getting rid of Barbur & Tualatin would be terrible.

I still think taking Tualatin Park & Ride out of the route is a HORRIBLE idea.

You have apparently removed the 2X stop at Tualatin P&R. This stop is convenient for people riding on Tri-Met's 36, 37, 38 and 96 buses. The Tri-Met 44 will soon be running all day between Portland and Tualatin P&R too, but your bus will not be there to meet it. This is a really bad idea.

I don't like that you will eliminate the Barber and Tualatin TC stops.

Like the new route in Tualatin.

Dislike no connection to Tualatin Transit Center.

Dislike not being able to get to Tualatin and Barbur Blvd to transfer to appointment. Providence at Tualatin is my source for ongoing blood lab work.

No more service to Tualatin TC, no good.  
From where I live in Tualatin it is shorter to drive.

It would ruin being able to get to work.

This doesn't not go to Tigard or Tualatin at all.

Dislike: Losing the morning route to Tualatin, as that is my primary route to and from work.

Dislike no service to Tualatin Transit Center.

No service to Tualatin transit Center I very much dislike.

DISLIKE : Discontinued service to Tualatin Transit Center.

Will miss the Tualatin Xfer connection

I go to transit centers in Tualatin and on Barbur. I know many others do too. Why take them out?!

Dropping service to Barbur and Tualatin will not be good. When I lived in Wilsonville, I took the bus to Tualatin Saturdays quite often.

I just think a lot of people use the Tualatin Transit Center so that would be a loss for the people of Wilsonville. I like that the routes would be a bit faster though.

I use the 2x frequently to Tualatin- to Whole Foods & other nearby shopping/restaurants (not available in Wilsonville); the 76 in both directions to access different parts of Tualatin for activities & to other locations such as east Tualatin, Tigard & Beaverton; I use the 36 & 37 buses to Lake Oswego.

Eliminating the Tualatin stop would eliminate all these options for me. Please consider keeping this stop, even if it's less frequent.

I am confused, it reads as if, even the "Intercity-Focused Alternative," we are losing connections to the 96 at Tualatin Transit Center. I can see, personally, eliminating the 2X part to and from Barbur Transit, but to and from Tualatin Park and Ride is what I see folks doing on my 2X to get on the 96, like I do, to get to downtown Portland. I also use the 2X bus to go to Bridgeport and I see others doing the same, some who work in the Bridgeport Village area. This is also how I go to see my doctor at Providence, which is just down the street from Tualatin Park and Ride. Also, TriMet makes it sound like they are proposing a MAX line (years away, I know.) to connect to Tualatin Park and Ride. At this point, if I can catch the 96 in Commerce Circle, than I guess I would, but only a subset of 96 buses go to Commerce Circle. Some stop at Mohawk Park and Ride. If I had to go south from my house on the 2X to the WES, and then make a transfer to a MAX, that is a problem for me getting to work on time. Right now I am 96 and 2X. How is 2X, WES, MAX (or heaven help me), 2X to WES, to the 12 a better option for me to get to work? This would be horrible. We're a one-car family right now, this kind of change may push us to being a two car family again.

Live in Beaverton could take a few hours to get to work/price.

I live in Beaverton and it is very convenient and properly timed. I think it's a blessing for me.

I use Smart to get to Tualatin and Lake Oswego. Need to be able to travel mid day to those areas too. It sounds like those areas will be stopped and I need them to be enhanced.

## **CLACKAMAS/OREGON CITY**

My children could access CLACKAMAS community college. Or at least I believe so given the Canby to OREGON city connection. Am I right?

In order to ride public transit to work I have to get to Clackamas and it takes too long even

with better service to Canby and Oregon City.

Need better-timed connection to Canby, Transit from Wilsonville.

I like that it improves access to Canby/Oregon City, and that it improves access to Tri-Met when WES isn't running. But none of the inter-city routes benefit me or my family personally. Every 30 minutes seems awfully frequent, but if the ridership is there to support it then I think it should be supported if it can be. I would have liked an "unsure" option here, I settled on '3' as the closest.

I like the potential connection to West Linn (via Oregon City).

Would like to see an earlier afternoon service between Canby and Wilsonville. A 3:00 return to Wilsonville would be good for those who are off work by early afternoon, instead of having to wait an additional hour to return to Wilsonville.

I like the better-timed connections to Canby transit for trips to Oregon City and the service to Tigard when WES is not running.

Like connections to Oregon City.

Stops to Oregon City would be helpful, that is where I commute from.

Easier access to Oregon City.

I like the better connection to Canby Transit. This will help for when I have Jury Duty or anything like this in Oregon City.

It would be helpful to increase the frequency of the service to Canby. And it would be helpful to pursue new service to Oregon City, which currently takes over an hour between SMART and CAT.

What specifically I like: Better timed connections to Canby Transit to get to Oregon City.

I drive to work from Oregon city and traffic can be a little hectic at times.

Changing the 3x schedule to time connections with Canby Transit would harm the timing of connections between the 3x and WES. Many residents and employees in Canby and Villebois depend on making a connection between WES and the 3x to get to work.

## **WOODBURN**

One thing that is needed (and not included in this plan) is a new bus serving the Woodburn Park and Ride lot at I-5 & OR214. This could be a partnership with the City of Woodburn much like SMART partners with Cherriots to provide service to/from Salem. At least five

round trips per day would make it useful to many people. Currently, the only way to get to Wilsonville from Woodburn is to take the Canby Area Transit bus from the Woodburn Bi-Mart and transfer to the SMART bus in Canby, but this is inefficient and doesn't allow people to park and ride as easily.

Need Woodburn to Willamette transit.

I wish there would be service to Woodburn.

Seems like there would be less service to the mentor graphics area mid day.

No access to Woodburn transit.

I live near Woodburn, and the better timing for the Canby route would make it possible for me to get to Wilsonville via Canby more reliably.

## **VANCOUVER**

I wish there was a straight route from Vancouver WA early morning and to Vancouver in the afternoon. I currently leave Vancouver around 5:-5:30 am and leave Wilsonville from Rockwell Collins 3:00-3:30 pm.

I know there are other coworkers coming from Vancouver area. One that takes the bus has to go through 3 transits to get to and from work, which turns out 2-3 hours each way.

I am able to save time driving my own car instead of taking the bus. If there were a straight route between Wilsonville and Vancouver that would be in time specified above (before traffic starts really picking up) then I would seriously consider taking the bus instead of driving my car. There may not be enough of us to make a business case at this point, but wanted to bring this up.

## **BRIDGEPORT**

Provides the same access to the 94/12 but no access to Bridgeport. Bridgeport is an important destination for shopping, entertainment.

I dislike about this alternative that it lacks a fast connection to Portland and that big shopping malls like Bridgeport or Washington Square Mall are not considered. Rather I would like more, if the planned Terminus is instead Beaverton TC (with a fast MAX connection to downtown) and stops provided at Tualatin TC and Washington Square Mall.

I also use the 2X bus to go to Bridgeport and I see others doing the same, some who work in the Bridgeport Village area.

## **SALEM**

The 1x works fine for us. However I have a coworker that is not happy with the changes about barbur center. She takes it in the morning and evening commute

I like the increased frequency to Salem.

I take the 1X and there are days when I drive because there is no service during mid-day - this alternative would be great!!!!

Increased 1X service to/from Salem.

Mid-Day service to and from Salem.

Greater Salem frequency.

One of us works here, and one of in Salem so we truly depend on the 1X service to Salem daily.

Trips to Salem might be helpful.

I could go to / from Salem during midday.

Like Salem access.

With more midday trips (after 8am) to Salem, makes 1X trips there much more feasible and attractive.

Having more Salem buses is also a good thing. If you ever rode the 2X during the day, which nobody at SMART seems to do, then you would know that many of the 2X riders getting on at Barbur are actually trying to get to Salem. They get frustrated when they find out the next bus is in the middle of the afternoon though.

I work in Salem, and live in Wilsonville. On days when I have to be back earlier than the bus can get me home, I have to drive. I would drive less if I could count on mid-day service back to Wilsonville on the 1X.

I live in Portland but work and go to school in Salem. More routes between the two are helpful, especially added service when the Wes is not running. I think more convenient travel to and from Wilsonville to Portland and Salem would incentivize people to utilize public transport and consider living in Wilsonville.

More service to Salem.

Mid-day service to/from Salem may increase ridership when folks have other appointments or when unplanned activities arise and riders need to return to Wilsonville.

I like the possibility of another 1X in the morning between the first and second departures from Salem.

Salem service increase.

LIKE: Midday/weekend trips to Salem.

I like the increased frequency to/from Salem.

(Like) Mid day runs on 1X

I ride the 1X from Market & Hawthorne park and ride to Wilsonville. Great Schedule.

The possibility of mid-day trips to Tigard and Salem means it would be possible for me to get home early if needed. Also, it appears that the 2X would be faster going through Wilsonville, which is now the biggest deterrent to not take the bus from Barbur TC on my way to catch the 1X to Salem. I hope SMART will not get rid of 20 min. frequency between 6:30 and 7:30 am going to Salem. 20 min. frequency is also needed in the afternoon between 4:00 and 5:00 returning from Salem.

I don't use SMART for Intercity travel and I don't know anyone who does. So, I don't have a preference one-way or the other. But I think there needs to be an option to connect to TriMet and Canby and Salem.

Commute to Salem is a big plus for Wilsonville residents that work for state agencies.

Like midday service to Salem.

Like the mid day service to Salem.

I only ride the 1x bus so I shouldn't comment on the other transit alternatives, however, I do like the idea of more 1x buses running during the day. I work in Salem and live in Tigard. If I came to work in Salem in the morning and got sick or had some other emergency, the next bus home isn't until 3:35 p.m. so you feel like you're trapped! A few extra buses during the day would be nice; it would probably work better for some of the travelers too (ones with suitcases and large backpacks!) who sometimes ride the rush hour buses. I also like the idea of service to Tigard Transit center when WES is not running.

More service to Salem would be useful, and would provide a more affordable alternative to Amtrak or NorthWest Point for people travelling to and from Portland.

What specifically I like: Midday service on weekdays, to Salem.

Like that it could relieve crowding on 1X buses. Also like that it would provide more



flexibility in riders' schedules and could reduce traffic on I-5 by providing service for people working only part of the day.

I live in Salem and commute to Wilsonville 4 days and for school.

El numero 3 nesositamos. Servicio de medio dia durante la semana a Salem muy bien. (We need #3. Midday service to Salem during the week is very good.)

Dislike: It does not increase frequency of weekday service to/from Salem (e.g., there is no 6pm Salem-Wilsonville).

No increased 1X service to Salem.

It keeps the limited 1x service as is.

Not interested in Salem.

## **HOURS OF OPERATION**

Later buses means I can attend events that are too lste for current times

Adding more Saturday service is a good thing. Also, it will be slightly easier to get from Portland to Wilsonville on a Saturday. Today, you need to take the Tri-Met 12 to Tigard, then the Tri-Met 76 to Tualatin P&R, then the 2X. This new plan means the 12 connects directly to the 2X on Saturdays.

I can utilize public transit in the middle of the day without the fear of being stranded. It also provides me Saturday transit options without having to take my car.

I'd really like to see weekday evenings and weekends served by the 2x. It's currently very difficult to get into Portland outside of commuting hours without a car.

Expanded Saturday service.

Later bus service and reduced times.

I like that it runs later.

Like more Sat service.

More frequent service.

Like the frequent transit times plus the extended hours for the buses.

More service on weekends and when WES isn't running is an excellent idea.

I don't like how it only runs 1 hour on weekends up to Tigard.

Easier and more frequent access in and out of Wilsonville, better flexibility for times.

Mas servicios de transporte al medio dia. (More mid-day service.)

I don't like that the bus don't run on Sundays and during the week, last bus is at 7:00 p.m.

Still no regular service in the middle of day to allow people to travel easily around the city on public transport.

We need more commuter options at different times. This would make a huge impact for my family!

This makes taking transit into Portland more reasonable on weekends, which is when I would most likely use it.

Like: hourly service on Saturdays.

Need more options for weekend in town.

I'd like to see more trips to Tigard on Sat evenings and Sunday.

More access on weekends and evenings.

Being able to visit people on weekends and be able to work on Saturdays.

Weekend and midday service to Tigard TC.

Weekend travel would be nice. It needs to be easier to get into Portland on the weekends with fewer connections.

What about Sunday? People do get around Sunday as well.

What I want is to be able to take the bus to/from work on route 7 + 5/6 then take intercity during the weekend.

More Service on weekdays.

The most challenging issue is time. Between work full time and school adding additional hours for transport is not acceptable. There is no fast way to commute between Milwaukie and Wilsonville all hours.

## **TRANSPORTATION OPTIONS**

Not related to the above, a bike lane on Advance Road is really necessary, given the amount of bicycle traffic. With new middle school and coming elementary school, it is a necessary safety issue. Young people like to bicycle to school.

### **GENERAL PRO INTER-CITY ALTERNATIVE**

I live outside of Wilsonville and generally when I travel its outside the city.

I go to school in Wilsonville at OIT and ride the bus. Currently I go to school 5 hours before class. This would allow me more time for other things.

I like the fact that is has better options.

Estoy de acuerdo con la idea Alternativa. (I am in agreement with the alternative idea.)

Gets cars off I-5.

It allows people to travel to and from work when they do not have a 9-5 shift. It also allows travel outside the city without being stranded for half a day to get back.

If people need to use public transportation it would be better to have service to places outside Wilsonville. Can always walk within town but not farther away.

Connecting outside Wilsonville via TriMet and MAX makes entertainment, shopping, medical, airport more available without driving.

I don't live in Wilsonville, but work here. This would be helpful.

Inter city access all of the time.

Better options for people to get to work.

I personally don't use intercity routes, but I do believe this new alternative would prove to be very convenient for others who do use it.

It goes to TriMet, better options.

More flexibility to the hours my employer needs to cover.

The intercity Alternative is the only viable option. As you already know, most people who live in Wilsonville work elsewhere, and most people who work in Wilsonville live elsewhere. It is essential that your buses connect to the Portland metro area. Remember, you are funded by Wilsonville businesses!

More people have access to Wilsonville business and people and Wilsonville have access to other cities with more jobs. Wilsonville has very few jobs to support its new communities.

I want intercity services. I can ride my bike locally.

Better times that improve connections of going somewhere else.

Tri met improved.

What specifically I like: Overall connections to TriMet will be improved.

I don't live in Wilsonville and only use Smart for work commuting.

It allows for a smarter way of transportation and less wear and tear for our roads and personal vehicles, and gas consumption.

I like that neighboring town residents can reach Wilsonville.

This option would actually allow for me to use public transit for work.

Seems more convenient.

LIKE: Better connection to a wider range of places.

It seems like a viable solution!

It seems to aim to help riders be more reliable and stay in budget.

I believe for maximum affect, more connections with other cities and more express routes to them are more impactful for Wilsonville residents than adding more stops internally. For instance more people in Wilsonville probably go to Salem, Portland and suburbs in Portland than Wilsonville for a number of reasons from work, to commerce, to recreation, etc.

It gives people better options to travel out of the city limits and to work.

I don't use SMART much, and have never used it locally, but I would be more likely to use it as an intercity resource. I can't speak for my neighborhood or the city, and don't think it would have much affect on my co-workers, so have left those lines blank.

## **WITHIN WILSONVILLE**

We live in Charbonneau and only have one car. We would make more trips to Wilsonville on Smart for shopping if the service was more frequent throughout the day.

I'm interested in SMART Transit within Wilsonville for my kids (ages 14 and 12) to take across town, not to other destinations outside Wilsonville (but they may in the future.)

All of it is good but the priority should be to take care of local peoples needs first.

Right now I would be limited by my inability to handle stairs. I do not own a car and would love the freedom.

Need more options for weekend in town.

I would like to be able to move between areas easier.

The only way this would benefit me, is if SMART ran in my neighborhood more than just rush hour.

Outside transit are more likely to cause disruption rather than spending valuable time in our lovely city, dislike=bad tradeoff.

Does not reach all areas.

I will only use SMART locally.

Want service to Villebois.

Saves money and environment. Increases walkability of Wilsonville residents,

I only ride route 4 every weekday to the high school and train station. As long as wait times aren't any longer, I think it's fine.

This seems really complicated... What I want is to be able to take the bus to/from work on route 7 + 5/6 then take intercity during the weekend.

The 5/6 does not run often enough.

A better inner city export system would encourage residence in Wilsonville.

I generally only ride the 7, so obviously i'd like to see it run more.

blending routes 5/6 will dramatically increase my travel time, as I come and go from the City of Wilsonville

Villebois is not getting additional services which are needed due to growth. Not everyone has banker's hours and by cutting service to the transit center during the day it even takes the option of walking a mile there to pick up for work in late mornings and returning in

mid-afternoons.

## **Q. 6 Locally-Focused Alternative**

### **What specifically do you like or dislike about this alternative?**

#### **GENERAL COMMENTS**

The 5/6 does not run frequent enough to be useful for employees.

blending routes 5/6 will dramatically increase my travel time, as I come and go from the City of Wilsonville

If I am reading this correctly, the current service for the 6 would be the same with the addition of midday service. Because I commute to work by WES, I do not have access to a car during the day. This might make it where I can take the bus to get some lunch.

This provides more alternatives to people who live and work in the city of Wilsonville. I'm not sure if that outnumbers the workforce outside the city but I think the route that serves the greater number of riders and that supports the largest workforce should be considered first.

There is a Trimet bus leaving at 2:43 pm but no Smart bus 5 at 2:30 pm outside Microsoft office, can we have Smart bus run at this time for us to catch the 2:43 pm bus. How can I write to Trimet bus to change the time leaving from Commerce Circle from 2:43 pm to 3:15 pm?

Lack of access to the north outside limited WES/TriMet 96 hours.

This is easier for my family and me, as I don't need to leave Wilsonville often and have a car when I do. But it's horrid for accessing Tri-Met. Complete removal of non-WES options should not be considered, it basically locks us away from the larger Portland Metro area. At least one or two mid-day weekday buses, and at least 2-4 buses per day on Saturday and Sunday, to the Tualatin TC would be really, really an improvement.

Don't like the 5/6 combo, would not want to ride the loop, like it better with each side separate quicker to get to destination

4 is nice no stop at SMART central mid dal. Better use of 2x, strait shot north  
Maybe the 7 should be a larger loop in the large neighborhood?

(Like) The 5/6 increasing service.

I commute connecting to the WES, currently it is quick connection to WES, blending the 5/6 will cause extra commute time at the time I want to leave, I don't want to wait longer to get the short route to the WES station.

Regular service makes it more likely that people will use it. The times become well known and people are able to make use of the buses for cross-town trips outside of rush hour. So provides connectivity improvements in the city, and deals with the east-west divide that still challenges families, especially those with children.

I am concerned that limited service to Tualatin/Tigard on the 2 will damage connectivity. Saturday service to Tualatin TC should be labeled Bridgeport

I depend on the 4 and 7.

Buses that run later evenings. Currently, there's no option to take a bus home after dinner/drinks in the evening. Route 7 should also run on weekends and later in the evening.

I would like more frequency- right now lack of bus keeps me from using often. Too long of a wait after more work shift at Fred Meyer.

This would make transit time frame manageable, but some housing areas may have trouble reaching Wilsonville.

For residents who want to ride SMART cross-town to get to shopping, dining or school removing the trip to SMART Central will reduce how much time it takes to get across town. It should not take 40 minutes to go from Grahams Ferry Park to the High School and this should help with that.

Gives people more options to travel to their designated destination.

Faster Cross town service.

It will make peak rush our traffic way better

Shorter wait times on certain routes.

Shorter wait times.

Ok.

More mid day trips and the continued connection to Tri Met.

Again, since I only ride the 7, having it run all day every 60 minutes is better for me than having it run only during rush our at every 60 minutes.

I think having fewer stops further apart but more frequently stopped at by the buses is a good idea & better use of resources. Right now, the stops are very close together (especially on Wilsonville Road near Boones Ferry Primary School.)

I don't dislike this one. I don't currently ride so it's hard for me to have a strong opinion. I think this system is vital for residents who don't have cars or don't drive. Whatever works better for them should be priority. After that, getting cars off the road should be priority. Thanks!

I like that you would not have to wait as long so half an hour instead of like an hour or more.

This alternative would make it impossible to get to and from work at times when the WES and TriMet 96 are not running.

There is more to do outside of Wilsonville. I would need help getting to transit center though.

Wilsonville is compact city that is easy to get around on a bicycle. The bicycle gives me more freedom to travel when it is convenient for me and not have to wait for a bus. If I have a lot to carry then I take the bus around Wilsonville.

I love the expanded evening service.

Like the shorter waits.

I like that student have options if they don't own a car.

The Wilsonville TriMet should focus on home base first, making it more convenient for the people who live here.

Shorter wait times are always good. When I lived in Wilsonville, I didn't like not having any service on Sundays and very little on Saturdays.

YAY! More Frequency! :)

Shorter trips.

I like to get where I'm going with more than one option. As long as that doesn't change.

Shorter waits between rides would be wonderful!

Need to have more service during weekends and Holidays.

Dislike that this is unfriendly to Wilsonville low-income, please just use WES train and expand this option.

Nothing wrong about it.



I like that in town is the main focus and there are so many routes that I can use as backup if I miss my own.

I more frequently use public transit locally rather than commuting in the city. I live and work within Wilsonville and most of the places I need access to are in town.

I like being able to go places in town without using my vehicle. It is a great service.

The service to Rockwell Collins (Mentor Graphics Complex) has been perfectly adequate. The local alternative seems to preserve that service.

That the SMART is running not only during rush hour. I generally don't work those hours.

It seems more effective for the local people who would actually use the bus to avoid cars. My neighborhood is not in W-ville so won't be affected with either plan.

Looks good

Shorter in town trips better service for other areas in Wilsonville.

I really would like Sunday service. People like to go to church and other places everyday not just Monday to Saturday

I like the more direct routes - more efficient! No complaints.

I like the idea of more frequent bus pick ups locally, but I don't utilize them as often. I only took the 96 from Wilsonville once and it was quite a wait. I either got there too late or too early. I am sure it will benefit local riders, but not so much if you need to get out of Wilsonville, to connect to another city.

Better local service would allow me to use SMART more for local errands.

I like the more frequent coverage for day trips outside of commute

The availability.

The more frequent trips in town and to Canby opens up more available times for working a job and going to school. Which is a big plus for me and I'm sure others as well, which is also a boost to the local economy.

I like that there will be more availability.

The buses are mostly on time.  
Quicker service.

Easier travel within Wilsonville.

Lets me reach my work easily.

Shorter wait times would make it easier to get around the city in a timely manner.

It will make it easier for residents to get around Wilsonville.

Less time to wait.

Shorter waits.

I might be able to ride the bus more.

More frequent trips locally would increase my chances of using smart

I usually stay in town.

Porque esta major (Because this is better.)

Mas rapido (Faster.)

Me gusta que sea mas corto la espera. (I like that it makes the wait shorter.)

I'm not sure.

Do like better service locally to workplace.

Lame as other option.

I can walk to all of the shopping and businesses in the Wilsonville, city hall, library, post office, Safeway, Riteaid, etc.

It would eliminate my ride to school.

I am looking for 15-minute service even if it means I have to pay.

Doesn't apply to me.

I'm not local.

People need to connect outside of Wilsonville

Doesn't reflect demand.

I don't ride the local routes very often but I wouldn't know how many Wilsonville residents

ride locally so I'm not sure how much negative impact that would have on local residents. I don't take public transit within Wilsonville.

Not as diverse.

I don't live here.

It sucks.

Less options.

Lacks access.

Neutral on this alternative. Variable start and leave times do not make transit viable alternative.

Not specifically designed for a commuter to/from work.

This Locally Focused plan is a disaster. How is anyone supposed to get from Wilsonville to Portland / Beaverton, or vice-versa, at times the WES and Tri-Met 96 are not running? Which is most of the time by the way. I guess it might work for the old people living in Wilsonville, but it's a complete shambles of a plan, and a huge step in the wrong direction.

And if you listened carefully to the feedback from the first survey, people were asking for late night / weekend service to/from Portland, not late night service from the library to the old people's home.

I use SMART for work so this has no effect on me.

Less connecting routes to TriMet makes riding SMART unpractical for me personally.

I would not be able to use public transit for work at all.

This doesn't help commuters commute, just local residents.

Not so flexible for me.

As someone who commutes in from Portland, this offers no improvement.

It sounds as if very little would change.

I need the leg that takes route 4 to Convergys (also Transit Center).

I live outside the Wilsonville area. This would have no impact to me.

Coverage within Wilsonville already seems adequate. Connectivity to other communities

needs improvement.

Doesn't help those outside city.

Not going to use it

I do not use Smart for local trips.

I use Smart to connect to TriMet to go to downtown Portland for my daily commute to/from work.

It doesn't seem like it would be as helpful for getting people from outside Wilsonville into the city.

I live outside of Wilsonville.

Doesn't really help my situation and is unlikely to be as big benefit as more intercity options during the day.

Doesn't apply to where I live.

The less frequent trips through the bus service make it more difficult to get where I need to be.

I usually do not have time to use this during my work day

I almost never use locally focused. I can ride my bike anywhere in Wilsonville

I believe the need is greater to connect to other cities than it is within the City of Wilsonville itself.

Will not apply to me, as I will not be taking a bus to these neighborhoods within the city.

This alternative doesn't connect Wilsonville to the Portland metro area and makes the community exclusive to other areas.

As long as WES is only running during rush hours, this concept makes no sense. For me bus service of line 96 is too slow to consider it for a commute.

The buses are running down the exact roads that get super congested. What is the point? You are only adding to the traffic mess. You can't take public transit that takes you anywhere. It actually takes me less time to commute to Salem than it does up to Portland. It would take me 2+hrs to commute to Portland, plus I'd only be able to do that during the early morning or late afternoon hours. Forget about the rest of the time.

Don't use in the city bus at all, or rarely.

Option not relevant to me.

Very few workers could commute to work and back without having to drive to Tualatin

Most people in Wilsonville can drive.

Families will not be able to have one car. Wilsonville just approved Builders to build huge communities with one-car garages very close together. Most people must work somewhere and Wilsonville does not have enough jobs. It's too far to bike to Portland OR Salem or so there must be connections and more of them preferably

Since we live outside of Wilsonville (in Butteville) improving the intercity options makes it possible for us to leave our car behind when going to Portland. I wish the two could coordinate the pricing because it gets expensive when we have to pay for both separately. This alternative provides no new routes or services that I would use.

Same comment as above. I don't use SMART much, and have never used it locally, but I would be more likely to use it as an intercity resource. I can't speak for my neighborhood or the city, and don't think it would have much affect on my co-workers, so have left those lines blank. For local travel, I'm fine walking.

We have local transit now and no one that I know (family, friends, coworkers, neighbors) utilizes it. It is just as easy to jump in our car and it is faster too. Whereas avoiding traffic and having teenagers be able to go downtown or to Washington square which is a much more inconvenient back and forth drive to take and pick up would be much better.

The expanded hours on Saturdays and Evenings sounds great. I do NOT like avoiding the WES station for the 4. I work right by there, as do a number of my friends and coworkers, and the lack of convenience will be quite noticeable. And why would you want to take away from the centralized system of all the buses meeting at the WES? The rest of the cuts to 4 may cut down on my Pokestops, but I'm cool with it. :)

Disadvantage: it comes at the expense of expanded inter-city service.

Only useful for me if you make the LAST southbound 1X later (even if there are no additional trips), like at 9 am -- currently, it's 8 am.

## **VILLEBOIS**

Better service to Villebois

I will be moving into Villebois this month, and this gives me the option to take transit instead of drive

Connection from Villebois to the center of town ... students would be able to get in to town for school more easily.

Greater service to/from Villebois.

It serves Villebois.

Doesn't affect me but the Villebois increase seems better for the community

I live in Villebois, and more frequent service to the TC via the 7 route would eliminate the need for me to drive the short distance when I take the train. Having the 7 timed more or less to the departure of the WES trains and buses is critical to this working for me.

I would also use the 7 route with my kids, and my kids/caregivers could use the bus to get to the library during the day. Since it takes almost as long to get in the car and drive to the TC as it does to drive to downtown Wilsonville, not having bus access in the neighborhood keeps us from using the bus for intercity purposes.

- Increased service on the 7 would be a HUGE boost in livability for an already great place to live.

I want to see more Villebois buses and have them run on Saturdays

Not enough rides for Villebois, do not over accommodate for TriMet...remember? They hate WES! We accommodate plenty already

Like service to Villebois, especially the shopper shuttle. Dislike elimination of service to Tualatin transit center

Like the midday service to Villebois

Like: Midday service to Villebois on weekdays.

It gives me more options to use SMART at noon for trips to lunch or shopping. Also, coworkers living in Villebois who go home for lunch may be able to use it instead of driving.

No reason to go to Villebois

Midday service in Villebois.

Like the regular transit to Villebois.

More service to Villebois

## **CHARBONNEAU**

I live in Charbonneau so bus service is spotty. I would rather just take my car for local trips

I live in Charbonneau so it won't affect me much either way but I would like weekend service across the bridge.

Like more late night service to the library from Charbonneau

For those of us living in Charbonneau, the local trips don't benefit us at all.

## **SALEM**

I live in Salem and have to be at work at 7:00 AM and leave at 4 PM. More frequency for 1x.

I commute from Salem.

I live in Salem this does not meet my needs.

It does not improve mobility for those of us who travel from Portland to Salem M-F. We would benefit more from added weekend services between Portland, Wilsonville, and Salem.

## **OREGON CITY**

This wouldn't work. I drive from Oregon City.

Doesn't run to Oregon City.

What I really want is for my children to be able to get the Clackamas Community College.

## **COMMENTS ABOUT INTER-CITY ALTERNATIVE**

This alternative would do little to improve congestion on I-5, which should be a priority for the City of Wilsonville. If the City is serious about attracting businesses and residents, then it has to do more to alleviate congestion on I-5. This may include engineering solutions to the highway, but more importantly it should be frequent service to points north to make it possible for commuters, students, and seniors just wanting to get up to Washington County cities (Tigard, Tualatin, and Beaverton) for work, going to class, shopping, and medical appointments. Transit and carpool options should be explored more for the I-5/OR217 corridor when WES is not operating.

While it would be nice to have increased service on the 3x, I like that the local alternative leaves the timing of WES-3X connections unchanged.

The access to TriMet is extremely limited already, and this would just perpetuate the issue. WES connections.

I feel like we already have a lot of inner-city options. I'd like to see more alternatives for

people heading to Portland, especially on the weekend.  
No TriMet connection.  
WES times are great; shorter wait times are a plus!

I would like to see WES running during the day.

WES should be available at more times.  
I would like to see more WES on more evenings and weekends.

No access to Woodburn transit.  
I don't live here so I probably wouldn't need it much. I need more connections to TriMet

More frequent buses to and from Wilsonville

We need to be able to get out of town

I live in SW Portland and will probably no longer be able to use SMART at all except the very rare times I travel locally to/from my work at Xerox.  
I will probably need to drive or see if I can work from home and not come to Wilsonville due to the greatly extended travel time.

I dislike that there are less connections to the outside surrounding area. Local residents already get free service within Wilsonville and it's also already very difficult to coordinate transfers with existing routes.

I dislike the lack of connection to the metro area. WES's limited hours isn't enough.  
Less connection to Tigard.

There is none for the WES going into BTC.  
A connection to TriMet is primarily made by WES  
Weekday service to Tigard Transit Center every half hour at times when WES  
No difference in routes 2, 5 & 6 during rush hour.

No access to TriMet

No service to Tualatin Transit Center

I don't live in Wilsonville

Prefer more connections to outside Wilsonville

I attend classes in Wilsonville Monday thru Thursday from 7:50am to 1:50pm and I need the 2X for those times from the Barbur Transit center, or at the least from the Tigard transit center. Eliminating the morning connections on this route would end up forcing me to take an UBER and the cost would be prohibitive. Please do not change anything until at least the



first of the year. I depend on the 2X during these times.

I don't personally use SMART transit inside the City of Wilsonville. I might slightly more often under this plan, but for me, the town is small enough with light enough traffic to get around by walking, by bike, or driving when necessary. Inter-city connections are much more underserved in my opinion.

There is a short time between when I get off at work @ Xerox and when the 6 arrives at my stop. This causes me to occasionally miss my stop, and I need to wait until the next one. The proposed plan would cause this wait time to double, which will affect my ability to get home in a timely manner.

Shortening 2x route and not serving Tualatin park and ride on weekdays.

This alternative doesn't provide mid-day trips to/from Tigard making it difficult to get to points north. Traffic is the worst on I-5 south during the PM hours and this plan would encourage more people to drive who live in Tigard or Beaverton or Portland and work or go to school in Wilsonville.

I did not vote, as I don't use these bus routes; I only take the 1x and occasionally the WES. I do notice from bus alerts that the 2x seems to get stuck in traffic a lot and is often running 20 minutes late so changes to that route may help. When I drive to Tigard from Wilsonville in the afternoon, the fastest exit to Tigard to avoid traffic seems to be the Kruse Way or even Haines Street exit.

Dislike the negative impact it could have on the intercity-focused routes.

No access to Portland except for commuter hours with the WES. An unacceptable alternative for those who need a little more flexibility in their commute.

It would be VERY inconvenient for people who rely on public transportation a lot to get to other places if the connection to TriMet is primarily made by WES.

Again, it doesn't accommodate my specific commute needs.

You need to convince those of us using SMART to commute to work and school in downtown Portland that this does not make it more difficult for us to get to and from work than the current SMART programming.

People who are employed in Wilsonville but live outside would be stuck when WES is not running. :(

My only thing is that it's not very inviting for someone outside Wilsonville. Travel route is too limited.

Dislike unable to get out of town on weekends.

Don't like that there isn't additional connections to TriMet.

I would be entirely unable to get to or from work except during WES hours.

This doesn't address Convergys biggest need, transit during evenings and weekends.

I live outside Wilsonville.

Still limited by WES schedule for a real intercity connection for both commute and leisure. Like the longer service on the 4 and 2x (which intercity has also)

This option only benefits residents of Wilsonville. My employer employs many outside of Wilsonville.

Longer time.

TriMet only WES.

No Barbur drop off bad.

Connections to TRIMET are not great.

WES only runs during rush hours. I wouldn't be able to travel out of Wilsonville by mass transit at other times.

The current 3x bus works for me connecting to WES.

I drive myself locally, so no short-range service matters to me.

Dislike: It doesn't make a difference with regards to WES schedules, and limits access to Portland UNLESS it's a Saturday.

Routes to Tualatin TC Saturday only.

The 2X from downtown Wilsonville to Oregon Tech/Rockwell is ideal. The new 5/6 loop is weird- no need

Lack of connection to get to Portland

I ride to/from Hillsboro when WES isn't available. I don't ride much around Wilsonville town itself.

Saturdays only to Tualatin TC

I do not like the fact that the only connection with TriMet is the WES.

No Barbur TC connection. Again, a longer commute for me.

No mention is made of connections to the TriMet 96. This option might be okay, if connections with the 96 are more reliable than they are currently.

If the 96 crossed I-5, to what you call Zone 2, then this might, in fact, be the best option of the two. Perhaps Oregon Tech could be the end-of-the-line for the 96. Have you contacted TriMet to coordinate with them about the changes you are proposing?

There should be an option to get from Service Area 2 to WES every half hour. Is that going to be Route 2, since 5/6 will only run every hour? I am a little worried about the reliability of Route 2 to catch the WES train every day, since it crosses I-5 at Wilsonville Road, instead of Boeckman (as the current Route 6 does to get us to the WES station).

And, TriMet 96 is not even mentioned in your plan. What is your plan to improve connections with the 96?

Dislike no service to Tualatin and service to TriMet only through WES

I live in West Linn. Taking transit to Wilsonville requires connecting in Portland. That's not feasible due to time constraints (~90 min each way). Neither option will help make transit more available for me.

Prefer the Tigard TC connection above Tualatin or Barber Blvd.

Not late enough to go to events in Portland.

El tiempo de espera se mejoraría y no perdería mucho tiempo en un viaje a Portland. (The waiting time would improve and I would not waste much time in a trip to Portland.)

Does not apply to commute from McMinnville.

My commute is long no matter what,

Coworkers and I need to be able to get to and from Salem and/or more northern Portland suburbs. Reducing or eliminating service to those areas would leave us stranded.

The only connections to TriMet are WES and 96 which do not run late enough (9:30 pm) to get me downtown Portland

I like the concept of improving intercity connections.

I dislike that service to Barbur TC is being terminated.

It increases my commute time from ~40 minutes to over an hour. This is a show stopper for me.

I dislike that SMART service (5/6) intervals at my place of work is reduced to hourly at rush hours.

I don't like the lack of connection to cities north of Wilsonville. It can be very hard to travel outside of Wilsonville, especially since the WES train only operates during rush hours. I've only ridden the 2x and 4, so I won't be effected too much by other transit lines.

## **FINAL COMMENTS**

### **LOCALLY FOCUSED ROUTE COMMENTS**

Thank you so much for reliable, free in-town transportation! It's a huge boon!!

Both plans are good, however I believe the local focus would benefit Wilsonville more.

Please consider charging a nominal fare for routes within Wilsonville to generate additional revenue. :)

If there was a bus stop up Boones Ferry when the WES is not running would be more ideal and better connection to PCC.

The most perfect solution would be for SMART to have a bus that went up Boones Ferry during times WES doesn't run. (midday, after night rush hour) as late as 10:00 pm.

WES would not cost as much as multiple new bus routes. It is also faster. Please consider primarily expanding weekend service in Wilsonville and adding a few more hours to WES service.

Please keep the one part of the current 4 route that takes it to the WES station, so that I can get to my job easier.

I'm disappointed that the existing route 4 service to Mentor has been removed from both alternatives; if it were available during the morning rush hours I would use it much more often.

SMART needs to add a stop closer to Hartung Glass or Republic. Both employers have over 100+ employees working multiple shifts and having a bus stop near us would allow for more recruitment activities.

We have long advocated for a bus that would run along Butteville rd and loop back along Arndt Rd. That would pick up a lot of riders living along Butteville Rd. Retired people who have to stop driving could get into Wilsonville to the grocery stores, library etc.

There is no good transit service connecting where I live and work. Best I can figure it

would take 2 buses and about 45 minutes to make a 2 mile journey.

Currently we are using line 5 to help employees get to work. It does not run frequently enough to be useful. There is also not enough room on the bus for everyone to use it.

I work on the border of service area 5 and 2, to reflect that I can get there within one service area I chose to answer as 5.

As a student in Wilsonville I feel the transit should schedule around local businesses hours-like the college and places hours opening and closing for staff or student that use the transits a priority before adding outside boundary influences.

## **VILLEBOIS**

I use SMART for transportation to work, shopping and other activities around town. More local service would increase the ease of movement for me and allow me great freedom. I have loved the addition of the shopper's bus in Villebois and Renee is amazing. Villebois was designed as a community for walkers and transit users, let's make it a reality.

I work in Portland and I also chose to live in Wilsonville because it is a livable community, with easy access to green space and affordable housing. The one thing that would make the biggest positive difference in livability where I live is more frequent bus service to Villebois, so I can leave my car at home more often when I'm just running errands around town.

Need buses in Villebois on Saturday for Shopping.

I ride Smart to and from the Transit Center and then take the WES to Beaverton. More Smart service to Villebois is important to me.

Your map will not load but I live in the Villebois area.

We moved to Wilsonville because of ease in getting to shopping. Please do not eliminate the Villebois shopper shuttle.

Much better services in the Villebois area and through the Wilsonville business districts. This would be much more user friendly for work and shopping. One of the main reasons I purchased a home in this town is it transit system and ease of travel for a person without a driver's license.

## **INTERCITY ROUTE COMMENTS**

The reduction of service intervals from 30-min to hourly and the termination of Barbur Transit Center Service are a mistake. These increase commute time to intolerable levels, eliminating incentive to commute on SMART.

I need improved connections to Portland

As stated prior, we employ many employees outside of Wilsonville. It is challenging getting them to come to work with limited public transportation options. I really like the focus on inter-city.

Currently the latest in the morning I could go in is around 9 am and the earliest to go back is around 4pm. This requires a long wait in town to do certain things and could make me want to not use the bus for convenience despite environmental effects.

4 yrs ago you should have gone to the south waterfront put flyers out. Hopefully you'll have a few stops on the way to and from Tigard transit center. Maybe stay off freeway and go on roads to Tualatin. And maybe have a couple of spots at TriMet bus stops where you'll pickup people.

With the locally focused alternative, how will one get between Wilsonville and TriMet during the midday? Also, is it really that time-consuming to stop at Tualatin? It seems not doing so would be a detriment to many riders.

I work and take classes in Portland, and my commute matters more with less time to commute, and less transfers. Not everybody wants to be out of their house all day or stranded out of town because the only access to Portland is during the commuter hours.

I would love to see an express bus to downtown Portland to help commute and worsening traffic concerns.

Have you seen if TriMet would be willing to partner and have some Line 96 trips take the freeway between Tualatin and Wilsonville?

It is in the City of Wilsonville's best interest to go with more intercity service simply to help keep I-5 moving. Congestion is a huge cost and is a big reason businesses will choose or not choose to locate here. Also, the more we can help WES ridership increase, the fewer cars will be on the road. This will improve transportation safety and mobility for the entire south Washington County region. It makes the most sense.

Needs more TriMet service or SMART service to downtown to connect with light rail to PDX. Any route to help get people to PDX weekdays and weekend.

More access during weekends and evenings and to TriMet are better.

2X is a great service from Portland! Keep it up.

Need more flexibility and options outside of Wilsonville.

The trips to Tigard transit center during times WES isn't running

Any options that would connect to TriMet on the east side of the river would be great.

I attend classes in Wilsonville Monday thru Thursday from 7:50am to 1:50pm and I need the 2X for those times from the Barbur Transit center, or at the least from the Tigard transit center. Eliminating the morning connections on this route would end up forcing me to take an UBER and the cost would be prohibitive. Please do not change anything until at least the first of the year. I depend on the 2X during these times.

The nice thing about the current system is there is a way to go toward Portland during rush hour directly from Service Area 2 (via the current Route 2X). If you are taking that away, and forcing us to make yet another bus connection to get to the WES or the TriMet 96, then both options seem the same. They both have the same ability to get from Service Area 2 to WES or TriMet 96 during rush hour. I don't see any difference for someone travelling to/from Area 2 in rush hour. Both options have Routes 5/6 running every hour and Route 2 running every half hour.

Will appreciate if Smart bus offers 'bus-pooling' from Portland to Wilsonville and I volunteer to co-ordinate if Smart does not have the resource. Thank you!

Here's an idea: how about expanded service to Salem, but less than proposed above: namely, every 2 hours outside rush hour (e.g., 9 am, 11 am, 1 pm), while retaining some service to Portland. If we cut Portland out of the picture. That will make Wilsonville seem even more isolated than it is already being outside of the TriMet system. Compare to Sherwood, for instance, where you can take a one-shot bus to downtown Portland from the town.

Our primary use of public transportation is to get to the Portland Airport. The WES is such a help. It is would be helpful to have SMART in and out of Wilsonville on the weekends. For work, I am able to walk without using SMART.

Many Wilsonville residents live within walking distance of Wilsonville Road and also go to shopping, dining or school within walking distance of Wilsonville Road. Seems like a route that goes up and down Wilsonville Road regularly would increase ridership and reduce traffic.

Do you know to get to Washington square or downtown (pioneer square) on the weekends - which are both fairly mainstream destinations - you have to walk to the closest bus stop in Tualatin? This isn't even reasonable. Yes, I could drive to a transit center off of 217 to give myself more options, but I'm trying to avoid having to get stuck in traffic in my car. Besides if I drive that far in my car, I like most people, feel like I might as well just keep on driving to my destination. We need some weekend transit options from the Wilsonville transit

center.

If you're gonna kill Barbur TC service, then I hope you offer more trips to Tigard.

Getting rid of 2x to Barbur is CRAZY! We need better transit on the weekend, and we need to be able to get downtown earlier on weekdays.

More frequent buses to and from Wilsonville.

The problem with the current connections with TriMet is that they are all on the other side of I-5 from Zone 2. It adds at least 10 minutes to the commute each way. If the 96 came on west side of I-5, life would be much better.

I chose to live in Wilsonville even though I work in downtown Portland because the commute is possible via WES and I don't have to drive every day. Connectivity to Portland is very important to me.

If at all possible, being able to ride only SMART to Portland and Tigard would be great (i.e. not having to change to ride TriMet). It would mean lower riding cost, decreased travel time (and dependency on other agencies), and a greater likelihood of using SMART.

I would like service to downtown Portland.

I ride WES and that is good. Wish it would run more often. I could spend the day in Portland and come back commute time.

My family and I use lots of public transportation, both Smart and TriMet. We live here, but we work and study in Portland and surrounding areas. It is very important for us to have more connection to TriMet to get to other cities. I think it would also attract more people to use SMART transit if they find it easy to connect to TriMet. Thank you so much for your attention to this matter!

Although I live/work here- My family and co-workers need intercity connections. We are not an island!

Again, I believe with the increase in growth, Wilsonville is best served by connecting itself more frequently with larger hub cities. Once people are confident of it's reliability, more people will utilize it, etc.

I would like to be able to get a ride to the airport without having to go to Beaverton.

Would like to stop in PDX also.

I'd like to see easier connections to Tri-met to get downtown or the airport.



I work in Hillsboro so connecting to WES is important to me.

Ideally, WES would run a schedule similar to MAX - throughout the weekday, and on weekends. I know this would not be considered sustainable, but I feel that "if you build it, they will come!"

I used to live in Tigard, and took the 9x in the winter. In the spring the connections were not often enough in the evening to make it work.

More availability to get from Beaverton TC to Wilsonville midmorning/day and weekend.

I would prefer to use public transit over my own car. Being able to connect to through PDX is integral to that.

I can plan my outing locally around the current bus schedules, but I'd prefer the WES link to get around metro area, which means intercity service beyond their commuter hours.

What I want is a work around or route alternative for when the I-5 backs up. A few weeks ago there were 3 Smart buses on the same street at the same time. This needs to be addressed.

I live in Tigard and ride the WES train and bus here and the bus home. I need to be able to get to and from work.

Love the WES train.

I live in Washington state. C-Tran gets me to downtown Portland. TriMet gets me to Barbur transit center. Smart gets me to Wilsonville. If the downtown Portland to Wilsonville link is broken I will not be able to use transit to commute to work in Wilsonville.

Need to go to Barbur to go into City Center of Portland.

Continue to make the system work better for people who commute to and from Portland, they make up a very large base of riders.

I live in Tigard so I take the WES every day to and from work. It's been working just fine. If you want to add the stop to Tigard Transit Center, that would be nice if I needed to travel to and from work during non rush hour times Monday through Friday.

I work in downtown Portland.

Why is TriMet 96 not mentioned? Is it not going to continue?

I would suggest rather than completely eliminating the 2X it could be changed to hourly service in the am. I would say at the 1/2 hour (6:27, 7:27, etc in the am from Barbur to

Wilsonville). Maybe eliminate a trip or 2 in the afternoon. This would save some of the expense without adding so much additional commute time for those of us that currently depend on it. I think completely eliminating a route is not a fair solution for all current riders. Businesses contribute to the costs of SMART to assist their employees to be able to ride transit and the solutions offered assume many employees live in Wilsonville which is not the case. I also wonder as OIT student numbers continue to increase, this will limit the school as an option for some potential new students.

## SALEM

I have to be to work at 7 AM. I live in Salem and work in Wilsonville. The only way that would get me here in time leaves at 5 AM which gets me here way early. A bus leaving Salem at 6 would be perfect!

Thank you for the 1X (a lifesaver for me!) and for the opportunity to take this survey.

I work in Salem. My husband works in Portland. We chose to live in Wilsonville in large part because of the 1X bus for my commute to Salem. Thank you for your hard work to maximize this service!

I work in Salem.

I work in Salem and ride the 1X everyday I go into the office. Mid-day service would be nice.

More service to SALEM.

It seems smaller buses could be used at certain times for inter-city routes, if that saves some money. Salem-Wilsonville buses after 5:30pm, for example, are almost always carrying fewer than 5 riders.

I live in Tigard, drive to Wilsonville park and ride, and take the 1x to work in Salem. I could probably use the WES to Tigard but the morning WES doesn't line up with the 6:50 a.m. bus but also mainly because I don't want to pay an additional \$5 a day for TriMet when driving is so much cheaper. Maybe if there was a cheaper Smart bus from Tigard directly to Wilsonville 1x bus I might consider taking it.

I primarily use your system to bridge the gap between Portland and Salem. It is important that there be robust service between Salem and the Portland Metro area. Ideally, you would be able to provide some level of weekday service to Salem.

I ride the 1x bus to and from Salem, Monday-Friday. I think it would be great to offer midday service, so if people have appointments or feel sick, they know that they can get home. I would also like to see the WES rail open later in the evenings and on the weekends, so that I can go into Portland for events and not have to drive and park.

I work at 7 AM, if the 1x would arrive before my shift (Convergys) it would be a better service.

More Salem times to get to work on time rather than too early or late.

I live in Keizer, so neither plan actually will help me get to or from work. its a 20-30 minute drive to the bus station in Salem and only a 40 min drive to work directly from home so I still do not use public transportation.

I think a 1X connection at Keizer Station Transit Center should be explored.

### **CLACKAMAS/OREGON CITY**

I would like to get to Portland and Canby. Also to Aurora.

The biggest problem I see is that there's no good way for my children to get from Wilsonville to the community college that serve them in Oregon city.

It would also be nice to be able to get to the Oregon city transit station, which currently is ridiculously convoluted to attempt.

I live in Oregon City and drive to/from Wilsonville for the 1X.

Why can't Oregon City, Molalla, Canby, and Wilsonville get together and share the cost of a single bus that services all? There is a bus that goes from Molalla to Oregon City (unfortunately to Clackamas Community College, not the OC transit center; A bus goes from Oregon City to Canby; and yet another bus goes from Canby to Wilsonville. The current system with multiple busses is unusable. One bus should be used.

I would like to shop at Washington Square Mall during weekdays by transit.

Increased frequency and reduced travel times between Wilsonville and Oregon City would increase access to education for students who live in Wilsonville and take classes at Clackamas Community College in Oregon City and vice versa.

I drive from the Oak Grove area of Milwaukie. Too many connections and too much waiting to make mass transit a viable option. A bike path or bike safe route would be nice. I have good bike options up to Stafford road, but Stafford is pure death to anyone riding a bike.

I like the plan overall, but as stated earlier, please consider including Tualatin P&R, otherwise many transportation options will be eliminated for me. Thank you!

I live in Oregon city and work in Wilsonville and would ride if there were more buses.

I live in West Linn. Taking transit to Wilsonville requires connecting in Portland. That's not feasible due to time constraints (~90 min each way). Neither option will help make transit more available for me.

## **WOODBURN**

I work in Woodburn school district.

I work in Woodburn school district.

It would be wonderful to have a shuttle that goes to the Woodburn outlet mall.

Please petition SMART to stop 1x in Woodburn!

Also how about some trips/routes to the outlet mall?

Go to Woodburn. The outlets are there and that is where I want to go. I don't want to go to Salem only. Go places that are useful.

## **PROCESS**

Thank you for taking the time to express an interest in what the riders think and I hope you consider them upon making your decision.

Thanks for requesting stakeholder feedback!

This is a well-intentioned survey and the graphics are helpful but the questions and the response parameters are poorly designed. The data you get from this survey will likely mislead you.

Thank you for asking for our opinions - as a small business owner who pays SMART tax I appreciate having a voice.

Thank you for the survey!

You should have people who actually ride regularly on SMART and Tri-Met tell you what works and what doesn't. These consultants don't know much about our local bus services, and clearly do not use them. Eat your own dog-food folks!

This survey is not working well with an iPhone. You should fix it/make some adjustments to eliminate survey taker frustrations. I am using an iPhone 6+ and I cannot see what I type.

Thanks for allowing some rider input!

Thank you for thinking of me. Many seniors are in the same situation I am in.

Thanks for the outreach!

Thanks for accepting my opinion.

The survey was not really applicable to me because I cannot reasonably use public transportation between my home (outside of Wilsonville) and work (in Wilsonville). Since I need to drive to work, there are pretty much no circumstances where I can use public transportation in Wilsonville. Therefore, most of the questions were N/A for me. But, there were not really any N/A choices. So, I don't know how useful my inputs will be for you.

## **GENERAL**

I work at Rain Garden apartments. I strongly appreciate the new bus route that supports ADA individuals by providing hourly trips from Rain Garden and Charleston to Fred Meyer and Safeway. That new route/schedule has literally been life changing. It has significantly helped ADA individuals live more productive, active and enjoyable lives. Please don't ever get rid of that route/schedule! We love it!

I still have to get in my car to leave wilsonville. There are many things that residents do outside of the city you know

I don't use SMART because I combine trips by car, or walk, or bike. I support SMART for our citizens who want to commute to work or need public transportation to get around. I have used SMART for over 2 years and love the service.

I am self-employed and use public transit as my main source of transportation throughout the Willamette Valley and metro areas.

I was hoping you would be adding Sunday routes.

Late night pick up 11:30pm or later.

I think most of my difficulties with public transit lie with tri-met not you guys.

Public transportation is a wonderful service. I use WES then MAX to get to my job at St. Vincent

You should also consider keeping the website up-to-date. During a recent backup of traffic, almost an hour plus at each stop I took, the website said "up to 20 minutes" late. I would have walked to my destination instead. Also, mobile access to the web page is difficult for commonly searched options.

Gracias por mejorar el sistema transporte! (Thank you for improving the transportation system!)

SMART is a fantastic service with excellent drivers and staff. Keep up the good work!

Do not like any thing about "SMART" all I ever see is empty buses clogging up our streets.

It is great to have in Wilsonville.

I believe you should copy what Tri-met had to do a number of years ago and manage budget challenges. They eliminated the downtown 'fareless square'. Wilsonville looks to be expanding on their free services and maybe should consider at least charging adults a minimal fee to ride. Seniors and children still are free

I love this city! Please do not overcompensate for the influx of population...coming from L.A. whole transit "pretends" to exist, this is an important issue for me. Looking forward to the green buses coming soon! Thank you!

Thanks for this service. It is vital for those who don't have cars or don't drive, to get to work and services.

I am grateful for the transit system. I believe it promote a more efficient urban life.

I would love to have buses run just a little later. They are almost always on time and I love SMART so much for that! Keep up the great work!

I use DAR and think the bus system is fine the way it is.

I would only ride SMART if I was unable to drive.

I'm sorry none of this applies to me.

Don't make charge higher; keep the bus.

Keep up the great work!

I look forward to new changes.

Great idea!

I work at Convergys.

I would benefit from both alternatives.

I just want to get to school bro :(

The traffic mess and design of Wilsonville should make the city council and the mayor resign from their jobs. Who the hell thought it was a good idea to put a multi-story storage unit across from the post office? You can't even get a moving truck into that space. It's completely useless, not to mention an eyesore for the retirement center behind it. That should have been shops and cafes for the medical offices & residences behind it. What about the useless Subaru dealership? Like we need another car dealership! That Fred Meyer is one of the smallest in the state. You could have allowed them to expand. Now we've lost the best grocery store in town, Lambs, and it's an overpriced, generic Safeway. Nothing is walkable in this town. You wouldn't let a Dutch Bro's open a drive-thru coffee shop, but you let a crappy HumanBean open up. Dutch Bro's is an OREGON company. How about supporting the state's businesses?? What now for the empty Albertson's? How about a Trader Joes or a Whole Foods Market? Wilsonville is becoming a seriously worthless place to live, with overpriced low-quality housing (think Villebois), and serious traffic issues. There are no long-distance bike paths (take a look at Davis, CA and Fort Collins, CO for bike paths), no decent shopping, and no affordable housing. The industry jobs in this town DO NOT pay wages that allow for the workers to afford housing in this town. The people who bought farms for retirement have been overrun by the Villebois creeping disease, very similar to what happens to the elderly character in the movie, UP. Now to add to the traffic mess, you are destroying the Coffee Creek wetland and putting in ANOTHER road between Barber & Boeckman. It's insane. Do you realize that the Villebois housing that mowed down the farm behind the grade school destroyed habitat for pheasants and Bald eagles? There were pheasants in that field, among other animals and Bald Eagles roosting in the trees on that land. Now instead, we get gigantic crap houses, that have no yard or property that are like 3 feet from one another. There are many studies that have been done to show the more rats you pack into a cage, the more aggressive they become. You'd created the ideal conditions for just this to happen. The Villebois area has already seen a home invasion where a woman was tied up & robbed in her own home and another where a man barricaded himself in his house and had to have a SWAT team remove him. The crime will only increase and apparently we "rent" police from Clackamas. We don't even have our own police department. That is outrageous. The greedy politicians in this city should be ashamed of themselves. I have no idea how any of you wake up in the morning and feel good about yourselves. This town has been utterly destroyed. We can only hope for another disaster in the economy or for a Cascadia earthquake to remove all the bad decisions and return this place to something livable.

Increased transit service will help students at Oregon Tech.

Buy smaller buses. They are NEVER full so they are a waste of money to purchase, run, and maintain.

Keep almost getting hit at 95th and Ellington. Can the lights be reprogrammed to have 95th turning right on Ellington have a separate green light and have a sign no right turn on red light. We cannot get on 1-5 without almost getting hit when exiting the Commerce center south

It would be very useful to have a bus that goes from Woodburn to Wilsonville. Getting from Canby to OIT is impractical due to the fact that it requires three transfers. My current route would be Canby Transit Center->Oregon City Transit Center->Wilsonville Transit Center->OIT. Adding a bus that goes from Woodburn to Wilsonville (Woodburn->Wilsonville Transit Center->OIT) would simplify this significantly.

I live just down Airport Road from Charbonneau. Do everything in Wilsonville though and would love more service.

Todo eita bien. \$0\$ cero pago es supiciente (Everything is fine. \$0 zero payment is sufficient. )

Seria perfecto si mi mama pudiera tomar transito publico a esta ciudad. (It would be perfect if my mother could take public transit to this city.)

Lo que no me gusta es que entre Wilsonville y Tualatin no haya sexvicio de bus continue por e gemple los buses de Trimet dan servicio asta los domingos en las cisdades de Tualatin Tigard & Beaverton

(What I don't like is that there will not be bus service between Wilsonville and Tualatin. For example, the TriMet buses in service on Sundays to Tualatin, Tigard & Beaverton.)

cero pago. Muchisisis gracias (I pay nothing. Many, many thanks.)

no uso el transito publico en la ciudad. (I don't use public transit in the city.)

Not enough time to commute by transit to get to work and home. I was working and other places so that I would need a car and \_\_\_ of insurance \_\_\_airs, cars! (Note: \_\_\_\_\_ indicates could not read word)





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# TRANSIT MASTER PLAN

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## PUBLIC INVOLVEMENT PLAN

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December 2015

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## INTRODUCTION

South Metro Area Regional Transit (SMART) is updating the Wilsonville Transit Master Plan (TMP). Public input by residents, businesses, and employees about ways to improve service and future priorities for resources is essential to understanding and meeting community needs for transit and other transportation options.

Public engagement efforts will be tailored to reach people in practical and convenient ways so that the updated plan reflects learning from the diverse spectrum of existing system users and potential users. Gaining input from employee break rooms, neighborhood and community gatherings, and through mobile electronic kiosks are examples of the project's active public involvement approach resulting in system improvements that best suit identified needs and opportunities.

The following values will guide the TMP's public involvement:

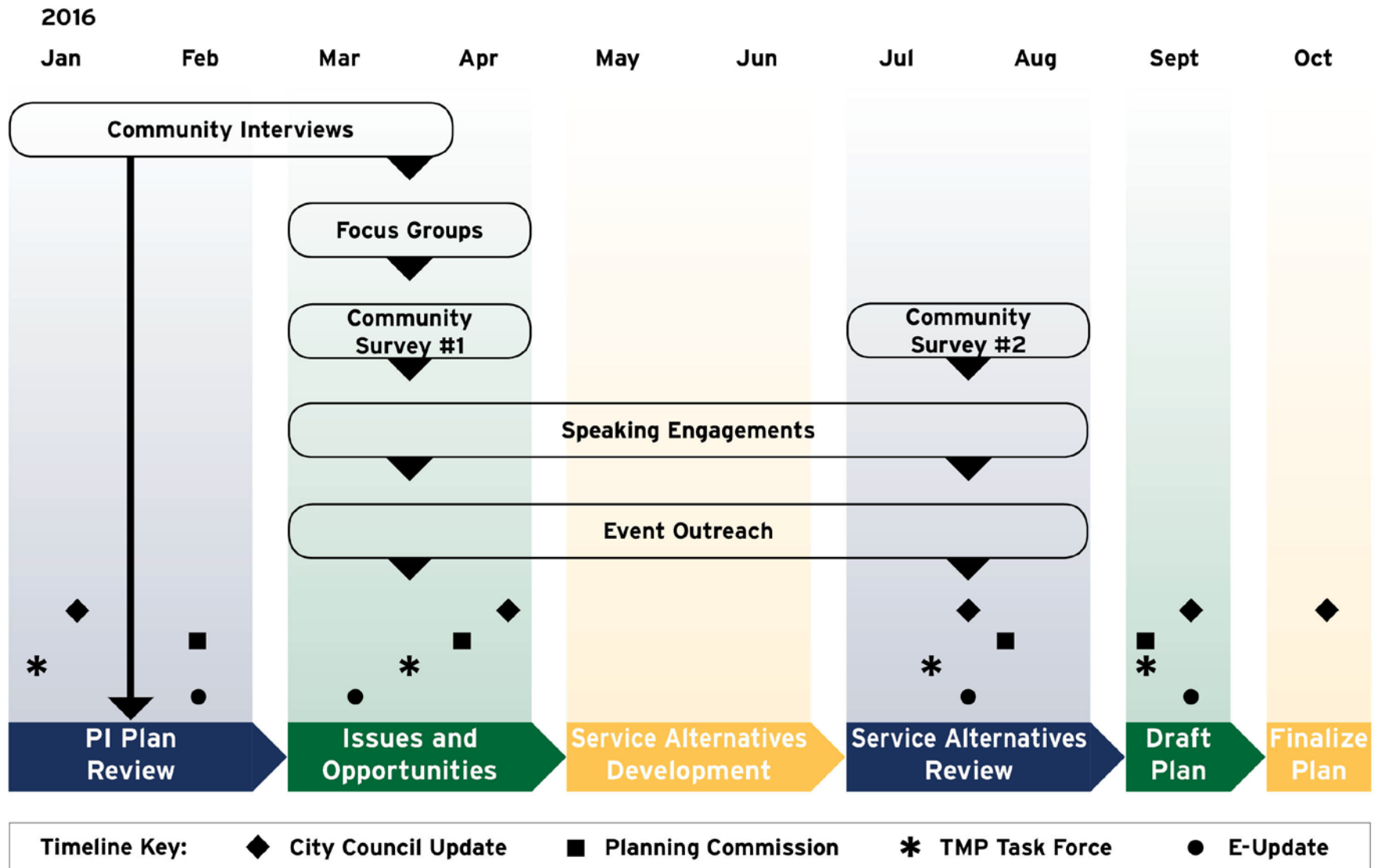
**Realistic:** be clear about the project constraints, objectives, and parameters

**Inclusive:** reach out to everyone, including those who don't use computers or face other barriers

**Meaningful:** provide timely information that is accurate and easily accessible

**Transparent:** make decisions public with key project materials available on the website and public City building

# PUBLIC INVOLVEMENT PROCESS



## **KEY ELEMENTS OF PUBLIC INVOLVEMENT PLAN**

- Consistent, reliable, accessible information and SMART contact person
- Thorough stakeholder analysis
- Vet Draft Public Involvement Plan with key stakeholders
- Representative stakeholder interviews from throughout Wilsonville
- Small issue-focused meetings, such as bicycle/pedestrian concerns
- Non-traditional outreach targeted to those with low-income, people with disabilities, young and elderly people
- E-updates to Interested Parties List
- Timely response to all submitted comments & questions
- Electronic kiosks with information and survey placed at high visibility locations
- Speaking engagements at civic organizations
- Updates to relevant committees and City Council
- Transit Master Plan website with comment and survey functions
- Media (print, radio, TV)

## **DECISIONS AND ROLES**

The Wilsonville City Council is responsible for the adoption of the Transit Master Plan Update. The project will provide public briefings to Council during the engagement process and expects to offer the plan for adoption in autumn of 2016.

The Transit Master Plan Task Force is an advisory committee of residents, transit riders, businesses and community organizations who will serve as a sounding board during the public involvement and update process.

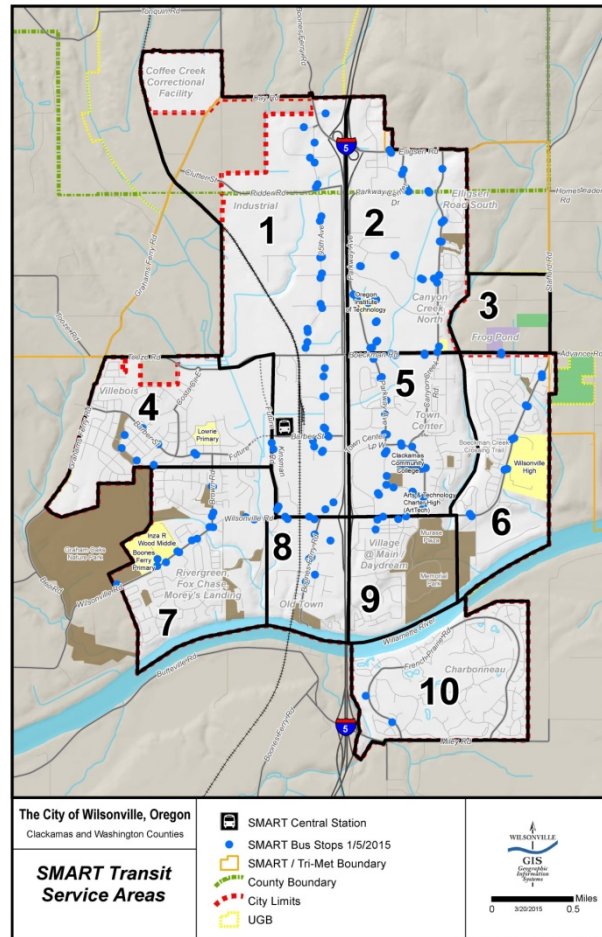
# TARGETED COMMUNICATIONS STRATEGIES

## Ten Identified Service Areas

Situated on I-5 between Portland and Salem, Wilsonville serves the area as a transportation hub and an important employment center. Large numbers of people commute to work in Wilsonville, and a large percentage of residents commute to other communities for their employment. SMART will tailor public involvement by neighborhood service areas, including residential, business and commercial zones, to optimize survey participation.

### Actions:

- Conduct interviews to best understand methods to reach each of ten specific geographic service areas.
- Do outreach with display to explain the project and collect input at identified high volume venues such as shopping centers and community festivals.
- Place electronic kiosks to collect survey input at designated locations. Advertise locations in local media.
- Add contacts into Interested Parties List and send E-updates.
- Contact employers to engage them with the project, including potential communications with their workforce either through displays, electronic kiosks, and break room posters with surveys or email.
- Reach out to residents and property owners outside City but in planned growth areas.



## **Wilsonville Civic Leaders**

Every community has leaders who connect with a variety of constituencies. Regularly updating those who serve on relevant Wilsonville committees, local service organizations, community non-profits, and business organizations is a good way to use social networks as conduits for accurate information and public input.

### ***Actions:***

- Network with Wilsonville organizations to place information in their newsletters and links to project website at major milestones.
- Arrange up to ten speaking engagements at civic committees and club meetings with the goals of sharing information, gathering input, and signing people up to receive future updates.
- Send e-Updates.

## **Title VI/ Environmental Justice (EJ) Populations**

The goal of this outreach is to ensure that the impacts and benefits of the Wilsonville Transit Master Plan are equitably experienced regardless of race, national origin, gender, disabilities, English language proficiency or being low-income. Materials will be made available in appropriately accessible formats, direct outreach in Spanish will happen at specific, identified locations, and times of day and locations will be a consideration in effectively seeking all community viewpoints.

### ***Actions:***

- Network with organizations that serve Title VI/EJ populations to learn best opportunities to reach constituents, including outreach at events and schools.
- Arrange display outreach at faith-based venues serving Latinos.
- Arrange survey input opportunities at low-income service providers and identified shopping venues.
- Incorporate Title VI/EJ perspectives in Focus Groups.

## **Workshop and Issue-Based Focused Groups**

SMART serves multiple constituencies with a variety of transportation needs. The project will organize discussions with specific types of users about the aspects most relevant to them to gain user insights on specific topics for the master plan.

### ***Actions:***

- Conduct a large community workshop with 30-50 community representatives to provide the necessary understanding of transit planning for well-informed decision-making and a community supportive Transit Master Plan for Wilsonville.
- Organize, recruit, and facilitate two meetings that include perspectives from bicycle and pedestrian users, people with disabilities, Title VI populations, youth and businesses.

### **Media**

Media reaches the most people with information, yet it is not a way to gather input. The project will use media to inform the Wilsonville community about the project, and about specific opportunities to give input prior to key decisions and project milestones.

### ***Actions:***

- Release Media Advisories at key times with project information, including how to participate, to The Wilsonville Spokesman and the Boones Ferry Messenger.
- Schedule radio interviews.
- Reach out to Spanish-speaking media outlets.
- Build media outlets into the Interested Parties List and send e-updates.

## **PROJECT COMMUNICATIONS MATERIALS & TOOLS**

- Project masthead
- Project description
- Process timeline
- Public Involvement & Communications Plan
- Service area map
- SMART contact name and number, business cards
- Website
- E-updates
- Electronic surveys
- Mobile kiosks
- Display boards, break room posters
- Leaflets for door-door outreach in some areas

**PLANNING COMMISSION  
WEDNESDAY, DECEMBER 22, 2016  
6:00 P.M.**

**Wilsonville City Hall  
29799 SW Town Center Loop East  
Wilsonville, Oregon**

*Minutes approved as  
presented  
January 18, 2017*

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**Minutes**

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**I. CALL TO ORDER - ROLL CALL**

Chair Greenfield called the meeting to order at 6:01 p.m. Those present:

Planning Commission: Jerry Greenfield, Eric Postma, Al Levit, Phyllis Millan, and Simon Springall; Peter Hurley, Kamran Mesbah, and City Councilor and Charlotte Lehan were absent.

City Staff: Chris Neamtzu, Amanda Guile-Hinman, Miranda Bateschell, and Stephan Lashbrook

**PLEDGE OF ALLEGIANCE**

The Pledge of Allegiance was recited.

**CITIZEN'S INPUT** - This is an opportunity for visitors to address the Planning Commission on items not on the agenda.

There was none.

**CITY COUNCIL LIAISON REPORT**

Chris Neamtzu, Planning Director, reported on behalf of Councilor Lehan, updating the Commission on two significant action items that took place at Monday's City Council meeting.

- In the work session, Council decided upon the City's new logo, which would be implemented in the New Year and branded appropriately. Council had undertaken another process, lasting about 14 months, to refine the logo. A number of artists presented at least 15 different choices and Council made revisions to the logo they preferred.
  - The selected City logo was a modern interpretation of the Hand-in-Hand with Nature theme of the current logo with a hand and seedling. The new logo was very scalable and had bright and vibrant colors with blues representing the river and the rain, green representing the trees and the land, and elements to reflect Wilsonville being a growing city. The artist had also told a story about that modern interpretation. He distributed a picture of the new logo.
- The second major action was the decision to connect the Brown Road Extension at 5<sup>th</sup> St because of better long-term suitability and stacking. Many of the engineering reasons behind the decision were based on the technical analysis.
  - A lot of testimony was provided on both sides of the issue. The neighborhood was not pleased and had significant concerns about the decision. It was a unanimous vote. Mayor Knapp stepped down from the dais and testified as a citizen, so the four remaining Councilors dealt with the final action. This was a fairly significant milestone decision considering the extension has been in the Transportation Systems Plan (TSP) since 1991, the choices being either 5<sup>th</sup> St or Bailey St.
  - Chris confirmed the discussion included redoing the plan at 5<sup>th</sup> St and Boones Ferry Rd, noting a number of additional elements were added to the decision. As the design progresses and surveying occurs, there would be numerous outreach opportunities and stakeholder involvement touchpoints with the neighborhood regarding the mitigation measures at 30 percent, 70 percent and he believed at 90 percent design. There would be a lot of communication and collaboration moving forward to make that connection as suitable as possible.



Commissioner Springall said the Planning Commission had a few reservations about the 5<sup>th</sup> St Connection, which included parking and the limited right-of-way on the east side of Boones Ferry Rd due to the old buildings. Those details would still need to be worked out.

- Mr. Neamtzu replied the parking was addressed in the revisions that came before Council. The project team figured out how to maintain the parking on the 5<sup>th</sup> St segment adjacent to the existing buildings. He understood parking was not eliminated on the existing segment of 5<sup>th</sup> St.
- He noted Councilor Lehan had made specific comments about the significance and importance of the old buildings on Boones Ferry Rd. There was no intention to damage or hurt the buildings. In fact, he intended to promote something to enhance them, perhaps a façade enhancement or partnering with the property owner to do something to complement the extension project.
- He added it would be worth watching some of the Councilor's summary comments, which were incredibly thoughtful. The Councilors all articulated the rationale behind their decision-making and all had different reasons. He believed the Council made a tough decision, but had provided a lot of logic behind the decision that was made.

## **CONSIDERATION OF THE MINUTES**

### A. Consideration of the November 9, 2016 Planning Commission minutes

Chair Greenfield corrected the last line of the seventh bullet on Page 3 to state, "...residential area was ~~unlikely~~ *likely* to occur since no I-5 onramp access existed."

The November 9, 2016 Planning Commission minutes were accepted as amended.

## **II. WORK SESSIONS**

### A. Town Center Plan Public Involvement (Bateschell)

Miranda Bateschell, Long-Range Planning Manager, briefly reviewed the context and background for the Town Center Plan project. The Town Center Plan was a goal set forth in both the Urban Renewal and Tourism Development Strategies, and it became one of City Council's goals in 2014. The City was awarded a \$320,000 grant from Metro at the end of 2015 for this project and established an intergovernmental grant (IGA) with Metro over the last year. The Town Center Plan contract was considered by City Council and awarded to MIG, Inc. in August 2016. The scope of work was refined and adopted in September.

- Since then, the project team has reviewed a lot of the history of Town Center and the City, in general. They toured the Town Center and adjacent areas and walked some of its segments to get a good feel of Town Center and learn what was working, where there might be challenges, and what was more easily accessible by car versus by foot.
- The project team was also gathering a lot of data to better understand the current infrastructure systems, traffic, Zoning Code, etc. to have a basis for the existing conditions work to follow and starting point for upcoming discussions with the public, Planning Commission and City Council.
- Much of the team's work thus far involved setting up the structure for the public engagement, which included creating the website and public involvement tools for the project, as well as developing the draft Public Engagement Plan. The Plan would act as a guide, laying out major events, as well as the different channels and times for the City's various communication methods.
- The intent of the Town Center Plan was to provide a community-driven vision for the Town Center and to establish clear actions toward achieving that vision. Therefore, the project team really tried to create a public engagement plan that was driven by the community.
- She briefly highlighted the extensive experience and professional background of MIG Consultant and Project Manager Alex Dupey, noting the City was excited to have his team and their experiences on this project.

Alex Dupey, Director of Planning Services, Consultant Project Manager, MIG, Inc, noted the draft Public Engagement and Communications Plan included in the meeting packet and developed with the City, was a

living document. Throughout the coming 18 months, the project team would be checking in with the Planning Commission, City Council, and the public at every stage of the process to evaluate what was and was not working and to consider the best and most effective ways to engage Wilsonville residents and businesses.

- MIG and City Staff worked together to create the logo for the project, which showed an active Town Center, the goal of the project, and a place for Wilsonville residents and businesses to come together and have a great space. The logo would become the brand for the project and be on all of the materials. Business cards with the logo, website, and a QR code were created for the project and the Commissioners were encouraged to hand them out as they talked with people about the project. The website would be continually updated throughout the project so everyone could access the current library of documents, list of next events, get updated about the current progress, and could learn how to provide input at a variety of scales. Though still pretty basic, the website was active now and would grow considerably as the project moved forward.

Mr. Dupey presented the Wilsonville Public Engagement Plan and Project Update via PowerPoint, outlining the goals and outcomes of the outreach process, as well as the methods and tools that might be utilized to gather input from Wilsonville citizens, businesses, and stakeholders about the redevelopment of Town Center.

Discussion and feedback from the Commission and responses to Commissioner questions were as follows:

- It was important that meeting attendees receive feedback about how their comments and feedback were or were not being incorporated into the project. How would people know their input or feedback was valuable? Some of the negative feedback related to public engagement stemmed from participants not knowing what became of their comments. There was some sense that people were not heard at some of the meetings.
  - Mr. Dupey responded that every comment, whether written, on a survey, or used in a wall graphic, would all be logged as public input and become part of the record for the project, and each logged result, whether from a public meeting or part of a process would have a response. Not every comment would be put into an alternative, but the response would identify what was done. The team would also want to learn what did or did not resonate with the Planning Commission, City Council, and public, as well Staff. This one process of documentation would facilitate understanding about where things were going while also providing the Commission information so decisions could be made about what was bubbling up as a key issue or theme as the process progressed.
    - For those attending meetings or more engaged in the process, meeting summaries, including visuals and materials, would be sent to every committee member after each meeting and to anyone who signed up for updates via mail or email regarding the process. Whenever an email address was provided, they will get information until the recipient tells them not to send anything further.
    - Information and materials would always be on the website as well.
    - He assured the project team wanted to be as transparent as possible. If something was not working, the team wanted to hear about it. If the Commissioners heard something from their constituents, the team wanted to be sure to understand it.
  - Working to avoid such negative feeling was a great thing to think about this early in the process. As a living document, course corrections could be made as needed if things were not working midway through.
- The project team was encouraged to reach out to the Chamber of Commerce CEO. The Chamber holds biweekly, and sometimes three times a month, networking meetings at various business locations. The Chamber would be happy to partner in getting businesses together, but also provide the opportunity to gather information, not just from people inside Town Center, but other businesses outside Town Center.
  - Mr. Dupey confirmed MIG had done that in the past and it was really successful. Doing a quick project presentation at a meeting provided a quick way to get input.
- Given the business licenses and property owners who were targeted for outreach in the Town Center, Staff believed there were about 350 businesses and about 50 to 60 property owners.
- Redevelopment of Town Center was dependent on the property owners' initiative. Addressing issues related to property owners not wanting to redevelop might need to be worked on in the Plan. The City wanted to do engage property owners, bring them to the table, and hopefully work with them on a shared vision based on

input from the community, as well as the property owners and businesses. Some property owners might want to develop in the short-term and some mid- to long-term. The Plan would include a set of action items related to the vision so when people were ready to come online, the development would occur in accordance with that shared vision.

- Mr. Dupey added that further in the process, Leland Consulting Group and Bob Gibbs, a national retail expert, would work on some catalytic site analyses to consider what would be needed to redevelop sites where the property owners might be more willing or interested, or even sites that make a lot of sense to consider for redevelopment, such as vacant spaces. What types of uses that were in line with the vision the project might make sense? Tools within the process would help address questions about what it might take to develop or redevelop specific locations that are to be determined through the process.
- Because the project was still in the discovery phase, the project team had just started the process and was gathering information about everything that currently existed. No one was talking about what the vision might be or what that might mean as far as retaining the existing buildings and infrastructure or tearing everything down and starting over.
  - There were a lot of great businesses and great infrastructure in Town Center, and the team did not want to negatively impact that either, but to think about how to make what was working continue to work and improve what was not working. How that evolved and the viewpoint of that process would develop throughout the process.
  - There was an opportunity for more connectivity. The City could probably not start from scratch and build a whole new street grid might be done somewhere else, but there would probably be talk about making extra connections that would not impact existing businesses negatively or at least in the most minimal way possible. All of this was to be determined and discussed through the process.
  - Mr. Dupey added Town Center was a very active place with a lot of businesses and some great new buildings. Part of the process was to acknowledge what was already working in the area, but also consider what things truly make this the center of the community. That discussion would be part of the vision process starting in early 2017 with the public and community.
- Regarding the boundaries of the study area, the majority of the team's focus and work would be within the primary Town Center designation, which was in line with the City's Commercial PUD zoning, and included all the property inside Town Center Loop and those immediately adjacent on the opposite side of Town Center Loop.
  - The study area would also include what are called influence areas, basically the neighborhoods to the east, north and south of Town Center, as anything being done along Wilsonville Rd would impact both sides of the street. These influence areas were connected to and a part of how Town Center operates, but not part of the existing discrete zone.
- The Land Use Map with Circulation (Slide 3) was not a vision type of deliverable, but for a master plan. When would the Commission see the high-level plan as a deliverable to be discussed? Even with the constraints with the property owners, perhaps the City was not thinking big enough. Wilsonville's Town Center could be pretty major, and there should be agreement about what the vision was before getting into details like circulation.
  - Mr. Dupey clarified the Circulation and Land Use Maps would be outcomes of the vision and become implementation measures. The vision builds throughout the process, from the first public meeting in February to ask the community about the grand scheme and what Town Center should look like in 20 years. From that, a vision statement and some goals would start being developed for the project. The vision statement should be the guiding principle for the project and the City needed to talk to the community about what that vision looked like.
    - There was something great about Town Center and they needed to figure out how to take advantage of that. As alternatives and analyses progressed, the grand vision would be more fine-tuned as well as how to actually implement all the pieces in a realistic manner. That was where specific types of financing and projects would be considered, as well as the right level of land use that made sense for the City of Wilsonville.

- The first half of the project involved the visioning. The second half involved figuring out what made sense to actually implement those pieces in a specific manner so that as a city, there were phased, incremental steps that made sense to support businesses and residents to achieve the vision for Town Center that the community developed early on.
  - Ms. Bateschell noted the first public event to start the visioning process would be in February, and Task Force meetings would run parallel with that in February and March for more discussions. The high-level vision would come to the Planning Commission and City Council in April 2017 to talk about the vision and goals for the project, which would set the stage for discussing different ideas over the summer about design alternatives, land uses, scale, etc. through community workshops and other planned events, such as the idea centers. The first draft land use plan would probably be presented toward the end of 2017 for review and refinement, aiming at that as a deliverable in 2018. The goal was to solidify the high level vision in the spring and then work to see how it might unfold on the ground throughout the summer and fall of 2017.
  - Given Wilsonville's tremendous traffic problems, concerns were expressed about the impact traffic would have on planning for Town Center. Oregon Department of Transportation (ODOT) would undoubtedly have something to say about any added traffic. Everyone needed to keep the City's traffic issues in mind throughout the entire planning process. While the vision for Town Center could be expansive, it was important to ensure it fit within the reality of what the City had to work with.
  - Mr. Dupey noted DKS Associates was on the team to evaluate traffic and, having done most of the traffic work within the city for many years, was well aware of the issues.
    - The vision elements that come through the public process to shape what Town Center would ultimately be would be looked at very closely to see not only how they affect traffic, but other infrastructure, like stormwater, sewer, water etc. as well. Discussions would consider what types of uses make sense, the traffic impacts, how they related to the community as a whole, and perhaps, a mix of land uses that could potentially reduce some trips. Big box commercial, for example, would have a huge traffic impact, while mixed-use would have a much different impact.
    - Being so close to the interchange, ODOT would certainly be part of the process.
- B. Frog Pond Area Plan (Neamtzu)

The following handout was entered into the record and distributed to the Planning Commission at the dais:

- One-page memorandum from Don Hanson, Otak, dated December 22, 2016 providing comments about the Residential Design Standards.

Chris Neamtzu, Planning Director, stated Joe Dills would continue the conversation on the Frog Pond Master Plan by walking the Commission through the revised Code language and discussing some new Comprehensive Plan language as well as a new topic on gateways and monumentation. The project team has been working hard on the Code and collaboration with the City's development partners continued as the partners reviewed and provided feedback on the Code versions. He noted a handout had been distributed from Otak, representing West Hills Development.

- He announced that just this week, the school district determined that its 10-acre property fronting on Boeckman Rd in the Frog Pond West Neighborhood would be the site of a future primary school, which changed everything for the land plan, density calculations, and the revenue assumptions made to this point regarding the Infrastructure Financing Plan. Though frustrated by this decision coming at this point, he believed the primary school would add a tremendous asset to the West Neighborhood and result in a tremendously cohesive community.
  - Given the school district's decision, he and Mr. Dills would need to relook at the entire project as a lot of rework and remapping needed to be done. For example, the transportation network, which assumed housing on all 25 acres of the school district property, would have to be redone. There would need to be collaboration with the school district to ensure any adjustments to the transportation network were

acceptable and suited the district to accommodate a future school site, all of which would take time and resources.

- The previous schedule of Commission and Council work sessions and public hearings would have to be re-calendared over the next several months. He had targeted a public hearing in February, but it would need to be moved to March to allow time to collaborate with the school district done and also get the revisions to the City Council.
- Work had already begun on the revised street grid, and there had been communication with school district Director Tim Woodley about the concepts. A series of meetings was being set up to collaborate on text and the revisions, all of which would return to the Planning Commission for more review and feedback.

Joe Dills, Angelo Planning Group (APG), noted the packet started with the updated draft Residential Neighborhood Code text and asked if the Commission had any questions about those revisions before he reviewed the Residential Design Standards, which began on Page 15 of 68.

Discussion and comments regarding the draft Residential Neighborhood Code text revisions were as follows:

- Section 4.127(.01)D (Page 3 of 68) included “cohesive neighborhoods”. Was all of Frog Pond the neighborhood or the subdivisions within?
  - The entire 180 acres was considered the Frog Pond Neighborhood. Any references to cohesion or “knitting together individual developments” referred to the entire neighborhood together.
- Section 4.127(.07)4 (Page 7 of 68) Concerns were expressed about the subjective language and the need for (.07)4 if variance provisions already existed in the Code.
  - Mr. Dills explained the intended review process was Plan Development Review (PDR) and that Code section was used as part of the model for this new zone. This type of language was actually found in several places in that part of the Code, so it just mirrored that as one style that was being done. The other part was on the open spaces, the project team realized they were not going to think of everything at this stage of code-writing. Developers would have good ideas that should be considered other than what had been proposed in this 4,000 sq ft standard.
  - The burden or authority was passed onto Development Review Board (DRB) in a number of areas. Words like ‘adequate’ were not defined and had no criteria. Section 4.127(.08)A (Page 8 of 68) stated “The purpose of these standards for the Neighborhood Zone is to provide adequate light, air, open space...” How was adequate to be understood or interpreted? Did any guidelines exist for what was adequate?
    - Mr. Dills responded no, not in this section. This was a discretionary review in which the discretion did lie with the DRB to make that particular judgment. Quite a bit of responsibility was on the DRB in this section because of the linkage to the PDR process and the language mirrored that type of authority and decision-making. In this instance, it was for when somebody asks for something that was different from what was more cut-and-dried in the Open Space Standard, such as for an idea that was not listed in explicit words in this particular set of standards.
    - The personality of the Code was entirely the City’s to direct. The team could go through and tighten up the Code language and include guidelines for the purposes of this section if that was the direction of the Commission.
- The Commission agreed it would be good to tighten up such words, provide guidelines. Because the DRB’s membership changes, Board members did not know what had been decided on previous projects or a part of a project, so it was better to have guidelines to provide more consistency, especially between phases of a development.
  - Some plans were not built as approved because the Planning Department had to accept some things that the developer just could not do, so there was also variation at a later stage in the process than just the DRB decision.
  - Subjective terms provided wiggle room and the opportunity to argue for things. Commissioners and DRB members sought objective, easy-to-measure predictability in the Code, but such provisions provided too

much leeway for someone to get outside of the original goals of what the Commission and City Council wanted because there was too much room without an objective measure.

- Parameters were absolutely needed for the DRB because it was often a matter of how well the applicant presented an alternative to sway the Board when there were no good guidelines or parameters to work within. That could be good for the applicant, but not for sticking with parameters that ensure the quality that the City wanted.
- The City of Wilsonville divided the quasi-judicial and legislative processes. Writing Code belonged to the legislative side and applying the Code was the quasi-judicial part. The language allowed the quasi-judicial to act in place of Code-makers.
- Mr. Dills confirmed the direction was to revise the language toward more clear and objective standards.
- Section 4.127(.03)F (Page 4 of 68) What might this provision mean for anyone who may or may not want to have home-sharing or Airbnb type options for their homes? There had been a lot of conversations recently about the subject, so it might be worth considering in this process to see if it should be revisited.
- Section 4.127 (.02)I. Mr. Neamtzu explained conversations about manufactured homes had been fairly minimal over the years, though a lot occurred around the time of the Thunderbird Mobile Club Park closure. State law required the City to accommodate manufactured housing as a permitted housing type under the Building Code, so this Code section simply mirrored what was already in the PDR standards for permitted uses and ancillary uses. If someone wanted to put a manufactured home in a single-family subdivision, there was a process for that through the Building Department. He did not believe the number of manufactured homes could be limited in an area as doing so might be a violation of fair housing rules.
  - Mr. Neamtzu confirmed the proposed residential design codes would apply to any manufactured home, adding the fundamental difference was whether a home was stick-built on the site or stick-built in a factory and brought to the site.
- Concerns were expressed about the quality of manufactured versus stick-built homes and having a concentration of them in Frog Pond.
  - Mr. Neamtzu stated no one he has spoken to was even contemplating manufactured homes in Frog Pond. He appreciated the concern, but given the lot sizes, price of land and cost of development, it would seem unlikely for a developer to choose that housing type, particularly in the West Neighborhood compared to other parts of Wilsonville.
- On page 1 of 68, Commissioner Springfield noted the very first point on the memo from Don Hanson talked about attached single-family units. He recalls the Commission saying no more than two items to be attached in the single-family category. He noted they were asking four-to-five. He asked whether the Commission just wanted to say no to this or should the Commission consider incorporating this.
- Section 4.127 (.02)C. (Page 1) Mr. Neamtzu clarified that a conscientious decision was made last November after lengthy discussions that the West Neighborhood would be a single-family, detached subdivision, so the Plan did not accommodate four to five attached units as requested in the memo from Otak. He noted the parties were not at the table during those discussions in 2015. He did not believe incorporating that housing type would be genuine to the Concept Plan at this point in time as it would involve going back and reopening the Concept Plan and having those conversations over again.
  - He noted that Staff just received the memo today and he intended to write responses to all of Mr. Hanson's written comments, and explain to him and Mr. Grimberg how the City had arrived at the decision to limit attached dwelling units to two.
  - He clarified duplexes were considered single-family, so as proposed the Code allowed for two, but three attached units was a multi-family housing product that the Plan did not accommodate, so there would be no row homes in the West Neighborhood.

Mr. Dills outlined the revisions to draft Code text and policies presented in the meeting packet, which included Code and Residential Design Standards; Monuments, Gateways, and Signs; and Comprehensive Plan Text Amendments, as noted in the work session agenda provided on Page 1 of 68 of the packet.

Discussion and comments regarding the revised Code language to implement the Frog Pond Master Plan were as follows with responses from the project team as noted:

#### Code and Residential Design Standards:

- Section 4.127 (0.15)G.a (Page 18 of 68) Vinyl siding was listed as a prohibited material because higher quality than vinyl siding was desired.
- Section 4.127 (0.15)E.q Objective standards were needed if there were variance standards elsewhere. Having the DRB making decisions was one thing, but giving the planning director some leeway was another.
  - As with the other references regarding the DRB's discretion and the need for clear and objective standards, the subject language added a layer of discretion that could actually bypass the DRB. Such subjective standards should not be granted to a single person when the DRB was available, and there was still some discomfort with the DRB having such discretion without objective standards.
    - Mr. Dills explained the language was included to give the director the authority to address individual issues like whether a 46.5 inch front stoop was acceptable as opposed to the 48-inch standard.
  - Giving the director that authority would be like a single property type of variance rather than overall design-type variances for projects. However, the subject Code section addressed a big-picture, multi-home type development and approval process, so the language did not seem to fit.
  - (0.15)E.q should reference (0.15)A, the purpose statement for the Residential Design Standards, "Other items meeting the purpose of this section..." Purpose statements were very important within the Code to express the issue of clear intent the Commission had been discussing.
- Even though much of the proposed Code offered the opportunity for variation, but most of the homes would look pretty similar. Existing homes in the area looked the same, and homes that look very different from the normal Northwest style, like in Villebois, did not sell very well. Other parts of the country had broader variations in architecture.
- In Table 2 on Page 20 of 68, Note I was corrected to state, "...the minimum combined side yard setbacks shall be a minimum of 10 ft."

#### Monuments, Gateways, and Signs:

- The project team did not have a strong opinion about what the monument at the Boeckman Creek Bridge would state. It could say East Wilsonville. "Frog Pond" applied to the entire 500-acre area of the project. Frog Pond seemed an appropriate way to mark the entrance; but perhaps, it could be done in combination with Boeckman Creek, because it was such an important crossing and connection down toward Town Center and Memorial Park. Any monument language would be consistent with language in the Vision Statement "Integration with the community and a part of Wilsonville".
  - The Commission discussed suggestions about what the monument should state, noting the proximity of Boeckman Creek and the proposed Boeckman Trail. Coffee Creek did not reference Villebois, but the creek.
  - Since Boeckman Rd actually divided old and new development north and south, just focusing on the Frog Pond part, would ignore the people in the other developments on the other side.
  - Having the sign note one was driving into Frog Pond would create confusion since those living in Meadows used the same road to get to Meadows.
  - Boeckman Creek would be great. Villebois probably did not care that about the Coffee Creek monument, but there could be a problem with it stating Frog Pond because might irritate those in Meadows who already had an identity.
  - There were also other opportunities to identify Frog Pond with the two neighborhood gateways.
- Mr. Dills clarified that the Trail Plan included a trailhead for the Boeckman Trail was planned in conjunction with the linear park, which he indicated on the map on Page 24 of 65. From a site-specific sense, it was difficult to know how trail users or pedestrians on Boeckman Rd would get down to the creek until the bridge was redesigned with a set of stairs or sidewalk. The common theme was that there should be a pedestrian access at that juncture, but how that would be done was still unknown.
  - Although no trailhead would be located at the bridge, it was worth signing that one was going over Boeckman Creek, and it would also provide opportunity for environmental education.

- The Welcome to City sign should have some uniformity with other Welcome to Wilsonville signs. The existing Welcome to Wilsonville sign on Wilsonville Rd coming from Sherwood was a wooden monument sign that just stated “City of Wilsonville” and had the old logo.
  - Mr. Neamtzu suggested addressing monuments, gateways, and signage in a more comprehensive way through the wayfinding project currently in the City’s budget. The new City logo could provide a fresh opportunity to brand the City’s monumentation in a great way. Universal Health on the north end of town wanted to identify something there, and the wooden routed monuments at Fox Chase on the west side and next to Landover on Wilsonville Rd were pretty outdated and could use a fresh look. There were also two huge monuments at I-5 and Wilsonville Rd, but nothing at Elligsen Rd.
- At the major Stafford Rd/Boeckman Rd intersection, trees and a more natural approach (Landforms and tall vegetation, Page 35 of 68) was preferred for the enhanced landscaping because it blended better and was more pleasant looking than a big hard structure. One concern was to make Frog Pond another part of Wilsonville and using a large monument there would cut it off from the rest of the city.
  - The visuals of a more natural approach might take away from power lines, too; although there would be some limitations due to the power lines.
- Subordinating subdivision identities within the neighborhood was suggested to avoid detracting from the overall Frog Pond identity. Each development or developer should not decide on their own name and have that as a prominent identification. Neighborhood identities should be subordinated consciously and carefully to the overall identity of Frog Pond so a subdivision’s identity did not overpower the larger neighborhood identity.
  - Villebois provided a parallel example as various builders had sub brands during the development and marketing of their properties, but once built out, it was all Villebois, not a lot of individual neighborhoods. Like Villebois, the City wanted Frog Pond to be a unified, cohesive neighborhood.

#### Comprehensive Plan Text Amendments:

- Implementation Measure 3.1.11.e (Page 53) When the language in regarding neighborhood parks was written, the idea was that neighborhood parks would be provided and maintained by Home Owners Associations (HOAs) and would not be the City’s responsibility. The Infrastructure Funding Plan for Frog Pond, which was still being developed and would be reviewed by the Commission, took a slightly different direction. The revised language regarding “other entities” reflected that there would be two publicly-maintained parks. Because 15 to 20 developments would be knitted together over time, no single HOA would be created to care for them.
  - The subject language was one of several references where the Code language was broadened to allow for a public strategy in addition to the HOA strategy.
    - Mr. Dills explained that while there was a practical need to broaden the language to allow the Wilsonville Code to be interpreted by future Planning Commissions and City Councils, it could be narrowed to say “HOA or the City”. “Other entities” would allow the City to delegate to some other party, such as a parks district or private party.
- Implementation Measure 4.1.4.u (Page 57) Mr. Neamtzu explained the gaps in the densities were created by a large rewrite of the Code about 15 years ago. Staff’s plan for addressing this complex problem was the Density and Consistency Project. A consultant from APG had helped Staff considerably in teasing apart the City’s standards and building a new process that filled the gaps. Because the Frog Pond project was tied to the actual density standards in the Development Code, the density gaps could not be fixed in the subject language. All of the work to address the issue would be presented in the future as one package.
  - Mr. Dills added a different approach was taken in Frog Pond to streamline the process and be clear about the subdistricts to find minimum and maximum numbers of units.
- Implementation Measure 3.1.11.e (Page 53) The two neighborhood parks and the smaller, open-space tracts in the higher-density, small-lot subdistrict would be maintained by HOAs. It seemed unbalanced that the people with the biggest homes would benefit from the parks, but not have any responsibility for maintaining parks. The smaller tracts and neighborhood parks would not be where the larger homes were located, but those homes would have an HOA and still benefit from the parks.



- Mr. Dills agreed HOAs for small lots with open-space set-asides would have responsibilities that others would not. Open space, stormwater, or other tracts of land were likely to appear in other parts of the community because of design intent by a particular project. There was a proposal for a co-housing project with a central open space. There would undoubtedly be tree grove site design direction and regulation by the City that would place the grove in an open space tract. There would be ownership and maintenance responsibilities in other areas that came from those types of decisions.

Mr. Dills stated the project team was preparing a draft West Frog Pond Master Plan that would be adopted by the City as a supporting document to the Comprehensive Plan. It would contain all the topics discussed by the Commission, from residential design through public realm recommendations. The project team would be working over the next couple of weeks to incorporate the new school proposal and present a 90 percent draft for the Commission's review and discussion at the January meeting. Additional topics or final refinements could be discussed at a work session in February in preparation for a public hearing in March.

- The major revisions resulting from the school's new location would involve changing the central part of the Boeckman Rd frontage, which would no longer be residential on the maps. The project team had gone through the refinements to the Street Demonstration Plan, so only refinements would be needed since the grid the project team set up served the group of properties there very well. The grid was used as the base for the Street Tree Plan, which would change a little bit. The changes would trickle through about 15 different graphics, but the real substance was a different land use at that location.
- He indicated the location of the 10-acre tax lot owned by the school district, and noted the adjacent tax lot, which extended to the Willow Creek swale, was not part of the school district's school designation.

Commissioner Millan asked about the impact of losing some housing on the funding mechanisms for some of the infrastructure.

- Mr. Neamtzu replied Staff had begun researching the System Development Charges (SDCs) paid by schools to be able to recalibrate the analysis that was done. Staff would pull data from employee counts and recently built schools for comparison and show the Commission how everything might impact the bottom line.

Chair Greenfield called for public comment.

James Wolfston, Frog Pond resident, asked how the district's new school would impact the completion date of the plan.

Mr. Neamtzu responded the Master Plan would go to the Planning Commission in March and City Council the following month. He anticipated it would take more than one meeting for the Planning Commission to conclude the public hearing and adopt the Master Plan, but could be adopted by Council as early as May. The City would continue responding to citizen concerns and comments throughout the process.

Ben Altman, Pioneer Design Group, representing Mr. Wolfston, said he supported some of the recommendations from Arbor, in terms of the menu approach to the architectural design, which had been incorporated. It was important to provide flexibility.

- He explained there were two types of manufactured housing, mobile-homes, which were now all called manufactured, and modular designs that were stick-built and assembled in a factory, and then brought to a site to be put together. He anticipated seeing some modular designs because the criteria in the menu would not fit a typical manufactured, mobile-type unit very well. A custom design would be needed to fit into those criteria, so he did not believe it would be a concern.
  - Under the Goal 10 Housing Rule, manufactured housing was a needed housing type and therefore the City must provide for it. It could not be precluded, but specific standards could be adopted. There were already some standards in the Code about mobile-type units, but no standards addressed modular units, which fell more under the conventional stick-built home.
- The language of the Code language must provide for "rigid flexibility." The Code had some rigidity to it in terms of the overall guidance needed, which was how the current PDR section came about. The standard R5/R7 zones were very prescriptive and some flexibility was needed. The planned development section was

created to allow less rigid requirements, but development had to stay within the provided guidelines, which came back to the Purpose Statement that drives all that. The Commission was headed in the right direction, but some flexibility had to be maintained.

- The current menu had enough choices to provide variety. The reason everything looked the same was because many of the code standards were too tight. If only five, six or eight options were provided, a lack of variety would result, and every third house would look the same. More flexibility in the menu criteria would result in a better variety.
- He supported the idea of maintaining Frog Pond as the overall name. There would need to be some ways to create additions, phases, or neighborhoods within Frog Pond because a plat name was part of the platting requirements; for example, Frog Pond Phase 1, 2 and 3, or Addition 1, 2, etc. It made sense to have an overall name, like Villebois, because the Frog Pond neighborhood would have the same character.
- The new primary school site essentially added a third neighborhood park, which should be considered in the context of the small lot to see how much open space needed to spill over. Accessibility to open space was an issue, but the smaller lots were also closer to Boeckman Creek, which was all open space. He was concerned about adding open space on top of open space. He suggested revisiting the open space in the context of now having the school within the neighborhood.

Don Hanson, Otak, representing West Hills, stated he would work through the items in his memorandum with Staff, but would not go through them in detail this evening. He had proposed that more than two attached units might be a possibility, but now sensed that was not the case, adding he was fortunate to not have been involved in the 2015 discussions.

- He believed Mr. Altman's comments about open space and the school site were well taken. The school would be in a good, walkable location. Young students living in the neighborhood could get there safely. They were looking forward to the new school location and hoped open space could be revisited, not from square one, but in terms of refinements.
- He found Commissioner Postma's comments on objective standards for the DRB very compelling. The more clear and objective the standards, the better for the Panel as well as the applicants. They would know what they were responding to and how to design their projects. He noted there were always exceptions and that was why the Purpose Statements in the Code were relied upon.
- He loved the ideas regarding monument signs for the neighborhood and agreed that the more it could be cohesive and just identify the neighborhood, the better. It was okay as long as there could still be some marketing signs during the build out. Having it all under the Frog Pond banner was the way to go as Frog Pond was a cool name.
- He was happy to hear about the new timeline and now that the school district had committed, Staff could move forward in collaboration with them.

### C. Transit Master Plan (Lashbrook)

Stephan Lashbrook, SMART Transit Director, stated he was primarily before the Commission to set the stage for the public hearing in March on the new Transit Master Plan (TMP). Staff had been working on the TMP for about four years, and this last year, a self-appointed and very dedicated citizen task force had been involved. There was a lot of potential for controversy at the public, especially among serious transit riders, as SMART was discussing making changes to routes and schedules.

- The TMP was a subset of the City's Transportation System Plan (TSP) which was a subset of the Comprehensive Plan. The TMP was not regulatory, but a guidance document for SMART, City Council, and the Budget Committee when making decisions about SMART's investments. In some ways, the TMP was more of a financial document than a typical planning document.
  - One key aspect was that the TMP put the City in a position to secure grant funding. SMART was funded primarily through local payroll taxes but also received grants from Clackamas County, Metro, ODOT, and sometimes from federal highway money, but usually Federal Transit Administration money.

- Almost invariably, grant applications discussed local planning and public outreach processes, so it was very important to show that the TMP was adopted by City Council through a specific public process and identified how grant money would be used.
- He reviewed the roles of the Planning Commission in the review process, which included acting as the Committee for Citizen Involvement (CCI) to ensure adequate public involvement and determining whether the new Master Plan complied with the TSP and Comprehensive Plan or created any conflicts that needed to be resolved. Some conflicts, such as those related to route and schedule changes, could be forwarded for City Council to resolve.
- He noted more than 1,300 comments were received and summarized two key suggestions that were guiding the TMP as it was being finalized as follows:
  - Increase ridership versus coverage of the transit system by having more buses focused on main avenues and connection points for more frequency on routes and fewer stops dispersed throughout the community. This would result more people having to get to the buses, but he did not see drastic changes to the system as Dial-a-Ride provided services to any house in town if the rider qualified.
  - Increase the connections between Wilsonville and other cities rather than service within the City.
- As a result of increased PERS costs, SMART would have an \$85,000 hit to the budget next year, increasing to at least \$100,000 within two years; a substantial amount for a small transit agency as \$100,000 paid for bus and driver for an eight-hour shift, which was a lot of service.
  - Fortunately, payroll taxes in the community were stable and gradually on the incline. More jobs in the community would fund more service for transit in the future. The redevelopment of the Xerox site and the potential for new development on the Mentor Graphics property south of Boeckman Road meant new revenue potential for SMART.
- There was also complete uncertainty about revenue sources. No one could anticipate the level of transportation funding from the federal government. In any given year, that could mean a \$1 million impact on SMART's budget.
- Staff's approach to the TMP was different from most transit plans, which were usually a compilation of intended tasks and projects for when more money was available. Instead, the new TSP would also provide specific recommendations to City Council for cuts to be made in a certain order if less money was available.
  - If there was a steady stream of money, and the PERS obligations could be covered, very few changes would be made. If more money became available through a State transportation package or a new, large industry moved in with 5,000 highly paid employees, transit service would be increased for the community.

#### Commissioner Postma:

- Asked if the TMP included plans to increase funding by taxation or other avenues.
  - Mr. Lashbrook responded the TMP would include a menu of possible funding sources but no specific recommendation about utilizing any particular source. He added one of the clearest statements from the 1300 comments was from the local business community strongly stating they had done their share and to look elsewhere for additional funds.
- Stated a big concern among the business community was the impetus toward more service, which meant more money, and the easy target was those who did not have a direct vote, which had always been his concern. There were all these plans for business growth and construction in places by Day Road, Coffee Creek and Basalt Creek, but to what effect if their taxes were increased with the notion that they would not be distributed elsewhere.
- Noted were other discussions with Mr. Lashbrook about the inefficiencies of fare boxes, etc. but there were routes other than expecting businesses to foot the entire bill if services were increased. Small businesses were a big engine for the community. He was concerned that increased services without conscientiousness toward small businesses would have a negative impact on the community's ability to attract small businesses to Wilsonville.

Mr. Lashbrook confirmed the SMART payroll tax applied to all businesses and some units of government uniformly, regardless of their size, traffic volumes, etc.; however, the set rate did not apply to non-profits, schools, churches, etc. SMART's payroll tax rate was .5 percent. TriMet's rate was .737 percent.

- Wilsonville's job base was one reason why SMART existed and why it had been able to provide services for so many years. He believed the taxable payroll in Wilsonville was more than \$900 million.
- He concluded that he hoped to have the draft TMP for the Commission in a few weeks, adding he would move forward with scheduling the public hearing as soon as possible.

### III. OTHER BUSINESS

#### A. 2017 Planning Commission Work Program

Commissioner Levit recalled that City Council was trying to come up with some plans to alleviate traffic on Wilsonville Rd, suggesting the installation of cameras. He noticed recently that more drivers blatantly running red lights and block intersections. It was getting to a point where drivers running red lights were an expected outcome. He asked what the City planned to do.

Chris Neamtzu, Planning Director, stated there had been a lot of discussion on the topic, which City Council raised regularly in work sessions. He knew a staff member was assigned to work on the red light problem and report back to Council for a much more comprehensive look at the camera program. The cameras in Sherwood on Hwy 99 had significantly cut back on red light running incidents.

- The City did add a lot of additional signage stating, "Do not block intersection", such as at the Boones Ferry/Wilsonville Rd interchange.
- There had been a lot of conversation with the police force to increase enforcement. He had just met the new police chief, who would be brought up to speed on the issue.
- He agreed to communicate the Commission's concerns to Council.
- He clarified that the existing cameras in the City were ODOT cameras that monitored traffic and signaled the lights; they were not used for enforcement. He described the camera system used in Sherwood at two Hwy 99 intersections. The thousands of photos taken were fielded by a company in Arizona. The system created a tremendous amount of work because somebody had to determine the validity of the violation at the local level, and then coordinate with the national companies running the cameras and doing the actual distribution of the violations through the mail.
  - The reduction in red-light running was clear, but a tremendous amount of effort and investment was involved. Some areas installed the cameras for revenue, but revenue decreased as drivers stopped running red lights.

Amanda Guile-Hinman, Assistant City Attorney, added that the Wilsonville contract with Clackamas County Sheriff's Office had a dedicated traffic officer, but other deputies could still issue citations. More monitoring had also been occurring.

- A possible concern was that when traffic cameras were implemented, an officer had to sign off on it. It needed to be determined whether Wilsonville needed to get an additional person from Clackamas County to cover that or if Wilsonville's traffic officer could handle it.
- She offered to provide a report for the Commission to review.

Commissioner Millan preferred having a short update during a Commission meeting. Red light running had become a common problem and many citizens were very concerned. She would like to understand the options being considered, knowing it would help the Commission, especially given the Town Center and Frog Pond Master Plans as well as the TMP. The hope was to reduce traffic, but if that did not happen, the TMP was lacking something.

Commissioner Springall noted that throughout the election cycle, people felt they were not being heard. Traffic was a huge issue and some have asked what forum was available to explain to concerned citizens about what was being done. The Planning Commission was the CCI, and they were not talking about the issue.

Commissioner Levit believed there were two separate issues. Traffic could be reduced, but that would not stop drivers from running red lights. People violated laws to avoid the traffic congestion.

Chair Greenfield commented that he hoped Wilsonville would not have a celebrated road rage case.

Mr. Neamtzu thanked the Commissioners for their flexibility in meeting so close to Christmas.

#### **IV. ADJOURNMENT**

Chair Greenfield adjourned the regular meeting of the Wilsonville Planning Commission at 8:43 p.m.

Respectfully submitted,

By Paula Pinyerd of ABC Transcription Services, Inc. for  
Tami Bergeron, Administrative Assistant - Planning

**PLANNING COMMISSION  
WEDNESDAY, MARCH 8, 2017  
6:00 P.M.**

**Wilsonville City Hall  
29799 SW Town Center Loop East  
Wilsonville, Oregon**

*Minutes  
Approved as corrected off  
the record  
April 12, 2017*

**Minutes**

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**I. CALL TO ORDER - ROLL CALL**

Chair Greenfield called the meeting to order at 6:02 pm. Those present:

Planning Commission: Jerry Greenfield, Eric Postma, Peter Hurley, Al Levit, Kamran Mesbah, Phyllis Millan, and Simon Springall.

City Staff: Chris Neamtzu, Amanda Guile-Hinman, Steve Adams, Dwight Brashear, Miranda Bateschell, Eric Loomis, and Zach Weigel

**PLEDGE OF ALLEGIANCE**

The Pledge of Allegiance was recited.

Chair Greenfield announced Staff would be requesting a continuance of the Transit Master Plan public hearing to a time certain.

**CITIZEN'S INPUT** - This is an opportunity for visitors to address the Planning Commission on items not on the agenda. There was none.

**UPDATE ON LIAISON ASSIGNMENTS**

Chris Neamtzu, Planning Director, stated that at the City Council's retreat, Council decided to change how it handled the Council liaison assignments due to the increasingly busy and complicated schedules of the Councilors. Going forward, City Councilors would not attend Planning Commission meetings to provide a report; instead, written summary minutes from each City Council meeting would be provided in the Commission meeting packets. He would field Commissioner questions on behalf of City Council, since he attended most of the meetings, and research any questions as necessary and report back to the Commission. Conversely, his written summary reports of the Planning Commission meetings would be included in the City Manager's monthly report, instead of being read at City Council meetings. Finally, any City Councilor was willing to attend any Planning Commission meeting upon request by the Commission. This new program was implemented yesterday.

- Additionally, City Council was working to schedule an annual summit so the Planning Commission, Development Review Boards, and City Council could share ideas, concepts, and concerns in a larger venue. Such meetings have been held in the past and more details would follow.
- Future City Council/Planning Commission joint work sessions would continue to focus on long-range projects, such as the upcoming joint work sessions on the Town Center Area Plan.

**CONSIDERATION OF THE MINUTES**

A. Consideration of the February 8, 2017 Planning Commission minutes  
The February 8, 2017 Planning Commission minutes were accepted as presented.

**II. LEGISLATIVE HEARING**

A. Transit Master Plan Hearing

Chair Greenfield read the legislative hearing procedure into the record and opened the public hearing at 6:13 pm.

Chris Neamtzu, Planning Director, verified no audience members were present for the Transit Master Plan public hearing. He introduced Dwight Brashear, the new South Metro Regional Transit (SMART) Director, and Transit Operations Manager Eric Loomis and made the following comments:

- The project team prepared an additional chapter of financial information, which was distributed to the Commission that City Council discussed at its work session Monday night.
- The Transit Master Plan has been in a state of flux, but the financial analysis had been prepared, which was important information for the business community and the Chamber of Commerce, in particular.
- With his March 1 starting date, Mr. Brashear needed time to review the Transit Master Plan to ensure the Plan was reflective of the direction he saw the Transit Department going. Therefore, Staff was requesting that the public hearing be continued to a date certain, which would fulfill the City's legal noticing requirements. He noted anything else could be entered into the record at this time.

Dwight Brashear stated he had only been with the City eight days and commended Mr. Loomis and City Staff for bringing him up to speed on SMART. He confirmed he needed time to review the Transit Master Plan and asked to continue the public hearing to May 10, 2017 and schedule a Planning Commission work session on April 12. He believed it was important not to rush through something so vital to the City of Wilsonville.

Eric Loomis presented an overview of the new financial chapter, titled "New Service Priorities" with these comments:

- Public and City Council input on the first draft of the Transit Master Plan indicated no cost analysis was included for the new service priorities or changes. The project team wanted to ensure everyone involved in the process that no new costs were being added to the Transit Master Plan.
- The New Service Priorities Chapter provided a cost analysis of the current service costs, as well as the new service priorities costs, once the Transit Master Plan was implemented. The total at the bottom of Page 79 showed that the current and projected annual costs were very close, demonstrating that the Transit Plan was not adding any service or additional costs.
  - Transit Staff developed the lists of priorities on the next two pages, based on internal discussions about what would happen if additional funding came in or if funding was reduced. The first priority list showed the priority level Staff set for different service scenarios along with the estimated cost so Staff could add a certain priority level for customers should extra funding come in. The second priority list showed what to scale back for a service level should the funding be cut or fall short in some way.
  - In the remainder of the chapter, priority lists were developed for each individual route based on an increase or decrease in funding.
- Transit Staff had no inclination of any funding sources going away or fiscal cliffs in the next year, but assumed the Department would run on the same budget. Staff just wanted to make people aware of the different priorities that SMART might come across and keep the Master Plan available for different options.
- He clarified DAR stood for the Dial-A-Ride or Paratransit program.

Mr. Neamtzu suggested the Commission review the new chapter over the next couple of weeks and ask any questions of Mr. Brashear or Mr. Loomis in preparation for the April work session. He noted the Master Plan would undergo some reorganization and the new financial chapter would be inserted, likely with some new text, so Transit Staff would be highlighting a new Master Plan in April.

Commissioner Postma thanked Staff for the cost analysis, which he had been requesting for some time, as he did not like changing plans without knowing the costs, which was incredibly important for transparency. His personal views as a Planning Commissioner were the same as his views as a Board Member and Public Policy Chair for the Chamber of Commerce. Both he, as a Commissioner, and the Chamber were sensitive about the

sustainability of the model of how the funding was currently happening. Having just under 80 percent of the City's current model funded primarily from payroll taxes with a very small portion coming from the fare box and almost 20 percent of the funding reliant upon federal grants was a scary prospect for businesses. While this involved discussion might fall outside the Transit Master Plan, he personally could not separate the two. He wanted a conversation about the funding model at the work session and the public hearing.

Commissioner Mesbah asked to see a benefit analysis in addition to the cost analysis. He noted in the City of Madison, the largest growth in bus ridership came from employees who lived in the downtown, but worked at a software company located 15 miles to the southwest, so the company subsidized the bus route because it benefitted them. He assumed businesses in Wilsonville also benefitted from the availability of free rides on the bus system. If the City was going to zero in on the costs the businesses were footing, then it would also be good to compare what benefits the businesses were receiving.

- He agreed with Commissioner Postma it would be unfair if all the costs were continuously shoved into one or another pocket without the community participating. The fairness would be good to look at, but also the benefits the system provided for employers.
- As far as a metric, he suggested monetizing the benefits would make for an easy comparison of a dollar-to-dollar metric. Considerations could include what facilities businesses would have to pay for to compensate for having no bus system. If quantification was not available, then a qualitative analysis of the kinds of benefits the businesses were receiving should be done.

Commissioner Postma said he would leave the metrics to the experts. He assumed ridership data could assist with the analysis. The fare box was approximately one percent of the funding, which demonstrated that riders also saw a benefit making for a unique cost-benefit analysis.

- Mr. Brashear noted past and recent studies demonstrated that every dollar invested in public transit returned up to ten dollars. A recent study by the American Public Transit Association spoke about 90 percent of transit system boardings having some level of impact on the city providing the service, which was significant, so there was gain through more than the fare box.
  - He had worked in cities like San Diego that invested heavily in transit systems, which resulted in a huge boom in business and economic development. Portland did a model study where a half mile of light rail corridor showed a return of a couple billion dollars of investment within that corridor. He would share the information from the studies with the Commission.

Commissioner Mesbah cautioned about mixing apples and oranges because the densification and value added to properties around a light rail station did not occur with a bus stop. He was familiar with the studies and wanted to make sure the analysis fit Wilsonville's conditions to the best of Staff's ability.

**Commissioner Postma moved to continue the public hearing on the Transit Master Plan to May 10, 2017 date certain. Commissioner Mesbah seconded the motion, which passed unanimously.**

#### B. Frog Pond Master Plan

Chair Greenfield read the legislative hearing procedure into the record and opened the public hearing at 6:33 pm.

Chris Neamtzu, Planning Director, expressed his pride in the work done on the Frog Pond West Master Plan by the Planning Commission over the past 16 months. The process had been very open, collaborative, and welcoming with eight work sessions and 15 to 20 hours spent talking about the details around all aspects of the Master Plan. The collaboration and shared ideas from the property owner and developer partners resulted in a better plan, which the project team and community worked very hard on.

- The Planning Commission action tonight, should the Commission reach a formal decision this evening, would be a recommendation of approval of the package of improvements to City Council. He clarified for the



public that the Planning Commission was an advisory body to City Council and did not make decisions. The City Council public hearing was scheduled for April 17<sup>th</sup>.

- The Frog Pond West Master Plan was proposed to be adopted as a sub element of the City's Comprehensive Plan. The Master Plan applied only to the West Neighborhood, which was approximately 181 acres in area and bounded by Boeckman Creek to the west, Boeckman Rd to the south, and Stafford Rd to the east, with the northern boundary being the lots running on the north side of Frog Pond Ln. The Frog Pond West Master Plan built upon the Frog Pond Area Plan, often referred to as Phase 1, which the City Council adopted in November 2015. The Master Plan was consistent with the vision established in the Area Plan containing all single-family detached homes in three primary categories: R-10, which were Large Lots of 8,000 to 12,000 sq ft; R-7, Medium Lots of 6,000 to 8,000 sq ft; and R-5, Small Lots of about 4,000 to 6,000 sq ft in size.
- The two school district properties were now proposed for changes after being programmed for residential uses throughout most of 2016. Late in 2016, the District's 10-acre parcel fronting on Boeckman Road was programmed for a future school site, and the District has agreed to work with the City on possibly siting a neighborhood park on a portion of their land-banked parcel.
- The Frog Pond West Master Plan contained a maximum of 571 dwelling units at build out, which would take many years to achieve. With the many small parcels involved, one main goal of the Master Plan was to knit together a cohesive and complete community as opposed to what could be upwards of 10 to 15 different individual development reviews.
  - The 571 units in the proposed Master Plan were 39 units less than the adopted Area Plan and a couple hundred fewer homes than modeled in the 2013 Transportation System Plan (TSP). Staff did updates to the TSP in 2016 and all of the City's assumptions had been less than expected throughout the process.
- The Master Plan document contained sections on Purpose and Scope, the Vision, Principles and Intent, Land Use, Residential Community Design, Transportation, Parks and Open Space, Lighting, Street Trees, Gateways and Signage, and Implementation. The appendix included a draft Infrastructure Funding Plan as well as other items.
  - The lighting section was new and proposed a Dark Sky compliant approach to street lighting, utilizing new technology and LED lights with attractive functional fixtures and based on the functional classification of the streets. Some thought was given to how to light paths, trailheads, and parks while being sensitive to the natural areas. The Master Plan included three to four new pages in the Lighting Chapter along with a new map addressing lighting.
- While the project team had been working hard with the development partners on the Infrastructure Funding Plan, the Planning Commission did not need to resolve the issues surrounding infrastructure funding tonight. The framework memorandum in the Commission's packet discussed many of the tools the project team would like to pursue. The project team had been actively engaged with the development community trying to find common ground but this interactive process would take time.
  - The Frog Pond West Master Plan was not likely to be adopted until there was agreement on the Infrastructure Funding Plan, but the funding was not something the Commission needed to worry about. Input from the Commission regarding general direction and things for City Council to consider in their decision-making process would be helpful and useful.
- The Master Plan established and applied a Residential Neighborhood Comprehensive Plan Map designation to the residential areas and applied a Public Facilities designation to the two western most parcels that the Wilsonville-West Linn School District owned.
  - The Master Plan did not apply zoning to the property and zoning was not under consideration tonight. Zoning would be done as part of the individual property owner initiated land use applications that would come forward for residential development consistent with the Master Plan and requests for annexation into the City of Wilsonville.
- He highlighted via PowerPoint (Slide 2, Exhibit R) the five proposed amendments with these key additional comments:

- A Comprehensive Plan Map would establish the Residential Neighborhood and Public Facilities designations.
- The Comprehensive Plan Text Amendments (Exhibit C) created goals, policies, and implementation measures around the Residential Neighborhood designation and the combing of the Comprehensive Plan to ensure compliance with those changes, such as changes to the Parks and Recreation sections of the Comprehensive Plan as they related to neighborhood parks.
- The amendment to the Significant Resource Overlay Zone (SROZ) map would add the small creek just north of Willow Creek Ln, which had not been previously mapped in the City's 2000 inventory.
- Amendments to the Development Code text would create a new Residential Neighborhood Zone (Exhibit D), which included all the necessary associated changes to support and ensure consistency as this zone was adopted.
- Finally, the Frog Pond West Master Plan itself would be adopted as a sub element of the City's Comprehensive Plan, containing the full force and effect of the Comprehensive Plan.
- The Commission spent a considerable amount of time reviewing and discussing all the proposed amendments in work sessions.
- The voluminous public record was provided on a flash drive to the Commission, but also posted on the City's website for more than seven days, which met all the State statutes for public record review. Public hearing notices were posted throughout the community and mailed to property owners in the study area and within 500 ft of the study area, as well as to agencies with which Staff regularly engaged and the City's extensive interested parties list. Notifications were also published in the Spokesman Journal, Boones Ferry Messenger and via social media leading up to this hearing.
- He reviewed the Frog Pond Master Plan Records listed in the updated Frog Pond Master Plan Record Index dated March 8, 2017, which replaced Page 10 of 10 in the Staff report, with the following additional comments and entered new exhibits received this week into the record as noted:
  - Exhibit F: The Master Plan Findings Report contained the applicable criteria Staff identified as needing to be satisfied in order to adopt the Master Plan. It was an extensive set of written findings speaking to compliance with state, regional, and local rules, regulations, and policies. Specific items included:
    - A revised Traffic Impact Analysis report, which was written after the school district made its announcement, so the 571 dwelling units could be reviewed from a traffic modeling standpoint.
    - Statewide Planning Goal 12 Findings for the Transportation Planning Rule (TPR), which were critical for Oregon Department of Transportation (ODOT); ODOT provided Exhibit O, which stated they had no concerns and that adopting the Master Plan would have no associated impacts to the State highway system.
    - Metro's Urban Growth Management Functional Plan, Title XI Concept Planning for New Urban Areas, and a compliance report and findings in support of the Functional Plan and the requirements around concept planning and master planning the Frog Pond Area.
    - The SROZ and related economic, social, environmental, and energy analysis and findings required under Statewide Planning Goal 5, Protection of Natural Resources, to include the small drainage ditch north of Willow Creek Lane in the Frog Pond Study Area.
  - New Exhibit N: Letter from Ben Altman, Pioneer Design Group dated March 6, 2017. He would respond tonight to some of the issues Mr. Altman raised.
  - New Exhibit O: Email exchange between Marah Danielson, Senior Planner at ODOT, and Planning Director Chris Neamtzu, in which Ms. Danielson stated there were no problems with the Oregon TPR.
  - New Exhibit P: Technical memorandum from Scott Mansur, DKS dated March 7, 2017 regarding the Frog Pond Willow Creek Drive Alignment Transportation Evaluation. Councilor Starr had requested Staff review the memo, which involved analyzing an offset intersection at Boeckman Rd and Willow Creek Dr as Willow Creek's extension north into the West Neighborhood. The memo discussed the challenges of offsetting intersections and some issues associated with them.
  - New Exhibit Q: Letter from property owner Amy Thurmond dated March 6, 2017, in support of the Master Plan.

- New Exhibit R: Staff's Frog Pond Master Plan PowerPoint presentation dated March 8, 2017

Project Manager Joe Dills, Angelo Planning Group, continued the PowerPoint presentation, reviewing the Frog Pond Master Plan with these additional comments:

- The Frog Pond Area Plan was the starting point for the Frog Pond West Master Plan. The 181-acre West Neighborhood was in the context of a three neighborhood areas of 500 acres that wrapped around the existing city. Work done for the Area Plan knitted together, in the broadest sense, the land uses, transportation, infrastructure, and open space frameworks.
- The Vision Statement about planning a cohesive place was an important theme driving the project team and Planning Commission's detailed discussions about standards and specific proposals. The intent was to plan a cohesive neighborhood where individual private developments and public realm investments would fit together, look nice together, and function well, as opposed to a collection of individual developments that did not connect into a coordinated whole.
- He reviewed the amendments proposed for the Planning Commission's recommendation to City Council:
  - The first requested action was to adopt the actual Comprehensive Plan Map designations, which would add the Residential Neighborhood designation as well as the Public Facilities designation to the two school district properties, as shown on (Slide 7), to the Comprehensive Plan Map.
  - The Comprehensive Plan text amendments involved a new two-page section describing the Residential Neighborhood Designation and providing the enabling language that created the actual Map designation. The language defined the purpose, which was carried forward into the Code. It required area plans and master plans as the basis for when the Residential Neighborhood designation was applied. The section specifically discussed the integrated and comprehensive type of planning the Commission and project team had been doing where land use, transportation, utilities, open space, and natural resources were considered together and coordinated into a whole.
    - The supporting Administrative Amendments included broadening some neighborhood park language to retain past language about recognizing that private development provided neighborhood parks but that public neighborhood parks could be created in Wilsonville, specifically through the legislative master planning process.
  - The SROZ Amendment would add a small unnamed tributary to the SROZ. Though not currently mapped as SROZ by the City, the tributary was consistent with the City's criteria and would be added as a resource area. There were no changes to the Boeckman Creek SROZ.
    - To resolve access issues to a couple properties immediately adjacent to this additional SROZ, the project team mapped one street crossing. (See Street Demonstration Plan)
- The Residential Neighborhood Zone was a new zone for Wilsonville that applied to Frog Pond West and was intended to apply to the East and South Neighborhoods should those areas be added to the urban growth boundary (UGB). In addition to doing all the things that zone districts typically did, the new zone also created neighborhood and residential design standards as the test and standards that developers must meet. Currently, this was done either through the CC&Rs of individual developments, conditions of approval, or the Village Master Plan Standards in Villebois.
  - Permitted and Conditional Uses were adjusted from those of the Planned Development Residential Zone as a new definition was added in the Definition Section of the Code to be specific about cohousing.
  - The density method in Frog Pond West used mapped subdistricts as the basis for the minimum and maximum densities allowed. The subdistricts also were used to geographically reference where certain lot standards applied. One intent was to take the guesswork out of how much density would be designated for-each property and to provide a range of housing units that was simple, matched the overall master planning, and kept the promises made in the Area Plan.
    - The proportional acreage method was used to provide flexibility in determining the density on a portion of a subdistrict. Natural resources, existing homes, and other site-specific conditions were also accommodated in the methodology.

- The Residential Design text was drafted using a set of principles called the Ten Essentials, which were now in the specific language of the Development Code. He highlighted several of these key principles as follows:
  - The new Code was intentional about having a strong relationship between the street life and various elements of the semi-public space of the home, such as the house front, front door and entryways, and front yard. This relationship provided a connection and greater safety for children playing in the front yard. The aesthetic of the homes was more like the house on the left in Slide 16.
  - Garages were regulated for the same reason: to create a quality streetscape. The text provided maximums for garage widths as part of the front façade, as well as restricting how close the garage door could be to the front of the house. The garage door needed to be recessed for the smallest of lots.
  - The Residential Design Standards worked as a package of requirements for facades facing streets and pedestrian connections and trails, also known as “enhanced elevations.” Specific elements required for the front facades included: a minimum 10 percent window requirement; articulation and interest, meaning a portion needed to be offset, have bay windows, or a roof form to provide variety; a design menu requiring 5 of the 17 options be used to add detailing and variation; and house plan variety, so that no two adjacent or opposite homes could have the same façade.
    - During its Code briefing, City Council raised a question about the 10 percent minimum window requirement.
  - In addition to the Residential Design Standards, there were four ways to comply with the actual layout of Small Lots in the Small Lot subdistricts: main entrances face a pedestrian green, cluster housing, alleys, or street-facing garages recessed from the front of the home.
- A lot of discussion regarded the Boeckman Rd and Stafford Rd frontages, which needed to be a welcoming front door to Frog Pond West, but the reality was private lots and yards formed the edge on the north side of Boeckman Rd and the west side of Stafford Rd. The solution was to design the public part of the street cross-section in concert with the private land that would be comprised of a landscaped buffer tract and required ornamental wall. The consistent brick wall with an ornamental railing on top would be interrupted by the required pedestrian connections, street grid, and the school district property frontage.
  - A similar approach was used on the Boeckman Creek frontages where a see-through fence painted in dark colors, similar to Villebois fencing, was required to be more compatible with the natural setting adjacent to it.
- The project team did not add any new regulations for tree preservation, but provided guidance for how site planning could work with the existing tree groves and how to integrate trees in with development. The City’s existing tree preservation regulations would apply to Frog Pond West just as they did in the rest of the city.
- One vision for Frog Pond West was having a close physical and visual relationship to the broad Boeckman Creek Corridor, which would have a public trail. The standard stated the streets should be oriented to the east and west and terminate not with houses or fences at the end, but with an openness to the creek corridor as shown in Slide 24.
- The Street Demonstration Plan was a key tool in knitting the neighborhood together. The local street layout done for the entire neighborhood was considered “guiding, not binding” by the Code, so there was flexibility to work on an individual level with site-specific constraints. However, the standard did require substantially equivalent connectivity, so if an applicant was not going to connect a street in one way, they needed to show how it would be connected or how the pedestrian connectivity was an adequate substitute.
  - He indicated the one crossing of the SROZ on Slide 26, noting the property immediately west of the SROZ could not get access from Boeckman Rd because the access would not meet the City’s standards along Boeckman Rd and would not allow for safe turning movements, especially with the school frontage now directly adjacent. The SROZ crossing was deliberately put into the Street Demonstration Plan to show that particular access could be allowed and would access those lots. The access would also provide public access to the edge of the park should the park go in to the north.

- The proposed street cross-sections were tailored to be consistent with the TSP and included many tree plantings. Green streets were anticipated in Frog Pond West, so the storm drainage would be part of the public street system within the planter strips. While applicants could propose tailored solutions to their property, this was the starting point for that conversation. The Master Plan allowed for a variety of different street cross-sections, including a cross-section for pedestrian connections.
  - In alleys, the homes needed to be set back so that a car could be parked between the garage door and alley without hanging over into the alley.
  - In areas of extremely low traffic volumes, a Woonerf street could be proposed, subject to Development Review Board (DRB) review.
  - Dead end streets would use a hammerhead turn around, as opposed to a cul-de-sac, which benefitted the private properties because hammerheads were smaller than cul-de-sacs.
    - Mr. Neamtzu explained the hammerhead diagram was the standard diagram in the City's Public Works Standards. (Slide 30) In response to Mr. Altman's questions, he said modifications to the hammerhead design were possible, but Staff would consult with Tualatin Valley Fire & Rescue (TVF&R) on the design.
- The Boeckman Trail was conceptually aligned at the edge of the neighborhood and up in the living part of the neighborhood, as opposed to down in the creek bottom. The trail would traverse the western edge, come to the bridge and, at some point, come down the hill to take a more creek/forest environment alignment clear to Memorial Park. Eventually, people would be able to go from Memorial Park to Frog Pond West, wrap around to Frog Pond East and South, and return to the high school within a connected trail system.
- Parks and Open Space were an integral part of the Master Plan with two formal parks, natural areas, pedestrian connections, and tree-lined streets forming an integrated network of green spaces.
  - The Master Plan took a different approach to Parks and Open Space than the typical City requirement for individual developments to contribute 25 percent of their land to open space. An analysis found that the Master Plan itself would deliver a minimum of 27 percent open space across the neighborhood.
  - As part of implementing the City's Park Plan, which called for two public parks in this area anyway, the recommendation was that the two parks and the rest of the network would meet the open space standard, which was typically achieved through private implementation elsewhere in the city. The exception was that in the Small Lot Subdistricts, which had greater density and smaller yards for individual homes, 10 percent open space was required. Per the Commission discussion, an applicant could request a reduction or waiver and three factors were defined to guide that decision-making process.
- A Public Lighting Plan was developed and added to the Master Plan to provide a unified set of standards that considered the different lighting levels required for different street types, such as along arterial streets where the most light was needed and small hammerhead turnarounds near Boeckman Creek where the least light would be wanted.
- The Frog Pond West Master Plan also provided guidance for street trees in an effort to provide some coherence as well as a lot of leaves and branches to the neighborhood.
- Consideration was given to gateway features at several levels, including the entrance into Wilsonville near Kahle Rd at Stafford Rd, crossing the Boeckman Bridge, which was a transition to the eastern part of Wilsonville, and smaller neighborhood gateways at Willow Creek Dr and Frog Pond Ln. The Master Plan captured the idea that City gateways needed to be consistent with the other City gateways in the Wilsonville.
- With respect to monuments and signs, the recommendation was not to have individual subdivision monument signs within the Frog Pond West Neighborhood, but rather, a more unified type of subtle signage, such as the gateways and street sign caps would indicate it was all one neighborhood.

Chair Greenfield confirmed there were no questions from the Commission and called for public comment.

Doris Weher, 6855 SW Boeckman Rd, stated this had been a long process, but it had a huge amount of public input and many revisions. She thanked City Staff, Mr. Dills and his staff, and especially Mr. Neamtzu, all of whom had been fabulous. She thanked the Planning Commissioners for the many hours spent listening and reading all the documents. She believed this would be a beautiful plan with its integration of the transportation grid, parks and trails, and various neighborhood densities. The Commission and Staff have done an excellent job.

Andrew Karr, 7700 SW Carriage Oaks Ln, North Wilsonville, appreciated the diligence the Planning Commission, City and Staff had put into this process to make sure the Master Plan projected the beauty and efficiency of the city. He had concerns that were beyond the scope of the project, but could have a direct impact. He recommended installing a two-lane roundabout at the Elligsen Rd/SW 65<sup>th</sup>/Stafford Rd intersection, similar to the roundabout at the Stafford Rd/Borland Rd intersection.

Commissioner Levit replied a roundabout at that location was already a potential project in the County ~~Transit~~ **Transportation System Plan**.

Don Hanson, OTAK, representing West Hills Development, stated both he and Dan Grimberg of West Hills Development wanted to be on the record with their complete support for the project. They hoped the Planning Commission would recommend forwarding the Master Plan on to City Council for an April 17<sup>th</sup> public hearing. He thanked the Commission for a great process and for listening and collaborating with them.

Ben Altman, Pioneer Design Group, 9020 SW Washington Square Dr, Portland Or, 97223, stated he was representing Jim Wolfston, the owner of 16 acres in the southwest corner of Subarea 1 who was also present. He noted he had provided written comments to the Commission. He asked about the duplex or attached unit requirement in the Code for Small Lots. The general lot standards for Small Lots of 4,000 to 6,000 sq ft in the Code required 10 percent of the Small Lot area be attached or a duplex, but it did not give any specific lot standards. While an attached unit, like a townhome, would be fine on a 4,000 sq ft lot, a duplex was a tight fit. He requested more clarification or direction in the standards to address that situation, though he did not have any suggestions at this time.

Chair Greenfield asked if the requirement of a maximum permissible footprint and setbacks was sufficient to meet Mr. Altman's need.

Mr. Altman replied the codes he worked with typically had a square foot per unit criteria. If the City wanted duplexes, it would be something like 2,500 or 3,000 sq ft; whatever that number was, but the standard had to be something other than 4,000 or 6,000 sq ft. Otherwise, nothing would be accomplished if each unit required 4,000 sq ft. The unit per lot area had to be a tighter area to gain density. He assumed the City wanted higher density by including duplexes as a variety of housing type and an opportunity for a smaller lot unit mix.

- The other big issue was the infrastructure financing, although they realized it was not finished. Some concerns were regarding how the costs would be proportioned out. For example, the bridge's cost would be proportioned out based on the percentage of traffic from the Frog Pond West Neighborhood, but once the bridge was crossed, 100 percent of the costs went to the frontage properties fronting on Boeckman Rd, which is a drastic change.
  - The 16-acre property in Subarea 1 he was working on had a density range of 66 to 82 units with 1,450 linear feet of street frontage. Adding a turn lane, curb, and sidewalk was one thing, but the 10-ft landscape buffer and decorative brick wall added a significant additional cost to the street design that would be borne by those 66 to 82 units, ranging from \$5,000 to \$8,000 per unit. The developer did not believe that was fair; the cost should be spread out beyond the properties fronting the street.
  - He suggested the City consider the same proportional traffic volume considerations for the improvements because basically, it was a beautification component that added no traffic capacity at all. The cost of the 10-ft buffer and decorative wall should be borne by the entire city or even the entire West Neighborhood, not just the properties fronting on Boeckman Rd. The minimum would be to

spread the costs across the whole neighborhood, but even that would increase the unit costs for something that added no traffic capacity whatsoever.

Chair Greenfield:

- Confirmed that City Council, not the Planning Commission, was responsible for figuring out the funding.
  - Mr. Neamtzu added the City was in the middle of complex negotiations. All of the public input would be closely reviewed and be the subject of additional negotiations, hopefully, resulting in agreements by all parties on how to pay for the infrastructure. Paying for the wall was a new concern this week and not raised in any prior conversations.
  - West Hills Development had proposed the design concept for the wall. The City was seeking a high quality, cohesive, and consistent neighborhood, which drove the Master Plan. The neighborhood should look incredibly attractive.
- Asked what advice City Council would be looking for from the Planning Commission.
  - Mr. Neamtzu replied the Commission could provide helpful direction on high-level policy recommendations regarding whether:
    - Frog Pond West was a place where the City should consider doing something different than done in the past.
    - There was an important public need being met in Frog Pond West that might need different tools, or did the Commissioners have preferences for tools they had seen utilized successfully in other places.
    - The Commission had been provided a memorandum outlining many possible approaches. A reimbursement district should be used with the public fronting the money and payback over time by the developers or was that cost an obligation of the development community. The Boeckman Rd improvements would not be needed without the improvements to the Frog Pond West Neighborhood.
    - He noted the City's long-standing policy has been outlined as part of these ongoing negotiations, but he was hesitant to go into any detail, since it was still all up in the air.
  - Believed the question of equity was very important and that the Commission should have input about it, but he had more questions than ideas right now. One fundamental principle was how to strike a balance between the aesthetic improvement to the town and the property value to the neighborhood, both of which had strong implications for apportioning the costs. He did not know exactly how the apportionment should be devised, but he firmly believed there needed to be an apportionment. The City needed to take into account the broader impact of beautification and the quality of life in town, which were not separate from, but rather an overlay on, the neighborhood's interests.
  - Asked if Wilsonville had been in a similar situation with other embedded developments where this principle might have been considered or worked in other areas.
    - Mr. Neamtzu replied the general City policy has often been that the half-street requirement was the obligation of the development community when the development fronted on a road like Boeckman Rd. The City could legally negotiate 24 ft of improvements, regardless of the right-of-way width, through the development review process. Going beyond the half-street improvement into offsite improvements got into disproportionate takings and Dolan arguments, which were all legal aspects of land use and development that became very complicated very quickly.
    - With Boeckman Rd, the developer 's required 24-ft improvement would go out toward the street median, while the City was responsible for the entire south side of the entire Boeckman Rd frontage. The City was already working with the school district to improve the entire corner and had significant resources dedicated to improving the Four Corners intersection with streetlights, widening, new sidewalks, ramps, striping, etc. Development paying for that 24-ft of improvements was the City's long standing policy and standard starting point for negotiations with all new development.
    - Steve Adams, Development Engineering Manager, clarified the Code measurement was 24 ft from face of curb, which provided the two travel lanes of roadway required to provide travel to and from

the development. This City's standard has been enforced for both residential and commercial development for the 15 years he has been at the City. Everything behind the curb, such as landscaping, sidewalks, streetlights, etc., was an improvement required as part of the development process, for which the developer was responsible on their side of the street, but not as part of the 24 ft.

- Mr. Neamtzu noted the Infrastructure Funding Plan technical memorandum from the Leland Consulting Group included a diagram (Figure 2, Appendix D) showing a cross-section of the entire street with a yellow highlighted portion that illustrated the City's standard practice for the developer's obligation.
  - He confirmed beautification elements included more than transportation elements, such as the planting strip, enhanced sidewalks, and ornamental wall, and were the property owner/developer's responsibility as shown in the diagram.
- Understood Mr. Altman was raising a principle that these elements went beyond what would necessarily fall to a property owner because of the development's size and the City's need to make it look good. The question, as he saw it, was whether the City had a responsibility to help offset the cost of these elements.
  - Mr. Neamtzu stated the City had no obligation to offset the cost of private subdivision walls that served as property line fences for private homes. He emphasized there was no public obligation there.
- Replied that would seem clear and asked about the planting strip.
  - Mr. Neamtzu explained the 10-ft area outside of the wall was a public utility easement for a wide variety of undergrounded public utilities going in along Boeckman Rd that needed to be accessible. In addition, the 10-ft area provided more privacy for residents, as well as separation and buffering for the homes from the roadway noise.
  - Mr. Adams recalled the developer of the old mobile home court property provided the fence facing Parkway Rd and the 10- to 20-ft landscape buffer between the right-of-way and the fence, as well as choosing to create offset lots from the busy road; none of which was required by the City. The 10- to 30-ft wide linear greens fronting on Boeckman, Tooze, and Graham Ferry Roads were part of the Villebois Master Plan. The nice brick and wrought iron fences in Villebois were never a sore point for the developers building homes along those roads.
- Stated, while Mr. Altman might be technically correct to say these beautification measures would not provide any significant direct benefits to the abutting lots, there were certainly many indirect benefits to the property owner in terms of property value, attractiveness, resalability, and the pleasure in living in a beautiful place. These other benefits needed to be taken into account along with or the absence of any direct benefits.
- Urged, however, that the Council to take into consideration the general public benefit of doing a good job, particularly along the approach to Frog Pond West on Boeckman Rd. The City needed to look at this issue sensitively and carefully.

Commissioner Postma pointed out that public comment had not been closed. Acknowledging indications from West Hills for comment, he said he was amenable to hearing further testimony from West Hills Development on the issue, since they had created the dilemma.

Dan Grimberg, West Hills Development, stated West Hills has been very involved in Frog Pond and appreciated all the hard work that gone into the Master Plan. He had called Mr. Neamtzu earlier today to say that West Hills Development was 100 percent supportive of getting the Master Plan approved, adding it had been a great process and acknowledging that Staff had listened to the developers' concerns and made some revisions. While West Hills was concerned about the finance plan, he understood it would not be discussed at tonight's the public hearing. However, since it had come up, he felt it was appropriate to provide input from the developer's perspective.

- City staff had been very open to their concerns in their three meetings so far, although they had not yet reached a workable plan. He cited West Hills' involvement in four other UGB expansion areas noting such developments were unique because of the small properties were master planned as one development. In other areas, West Hills Development combined two or three 50-acre properties and created a nice master



plan within the master planned community, but Frog Pond was very unique in that the average property size was five acres.

- West Hills Development was looking for a fair mechanism to finance the infrastructure improvements, which they did believe were beautifications in many instances, such as the medians and wide landscaping. As an example, West Hills was in contract to buy four pieces of property, the largest of which was five acres. The combined frontage on Boeckman Rd was about 1,300 lineal ft, which was about half of Frog Pond. Under the current program, West Hills would be responsible for a half-street improvement, including the median, landscaping, and brick wall. He confirmed West Hills did propose a brick wall as an appropriate solution for providing privacy, instead of an earlier proposal to have the front door fronting on Boeckman Rd.
- West Hills also wanted a nice community, but also a fair way to share the cost. In a letter to Mr. Neamtzu, written in late February, West Hills proposed that every lot in Frog Pond West pay roughly a 1/500<sup>th</sup> share of the Boeckman Rd improvements as part of the development fee. The Frog Pond West residences took their driveway access from a local subdivision street and not from Boeckman Rd directly. Boeckman Rd was used by everybody in the area; therefore, West Hills was asking that all lots in Frog Pond West pay an equal share of the Boeckman Rd and Stafford Rd improvements. West Hills believed that was fair, as this was the same apportionments in other areas, such as in North Bethany, South Cooper Mountain, Bonnie Slope, and River Terrace.
- He contended Villebois was entirely different, as it was a large-scale development that had a master developer for its hundreds of acres and thousands of homes. In contrast, Frog Pond West involved small-scale development so the infrastructure costs needed to be spread across a broader range of homes rather than just a small portion of the development.

#### Commissioner Postma:

- Asked what West Hill's proposal did for the existing property owners who would not likely see development, such as the church property that had frontage subject to the proposal.
  - Mr. Grimberg noted the roads were funded through collected fees. When private development built roads, it received credits against the fees normally paid with each house, or, the money was collected, and as it became available, it could be used to do improvements, like those along the church property if the church was not going to redevelop.
    - The entire cost of the Boeckman Rd and Stafford Rd improvements would be totaled and divided by the number of lots in the neighborhood.
- Confirmed the methodology worked in Frog Pond West because the improvements were not slated for all of the lots.

Don Hanson, OTAK, believed Mr. Grimberg's comments were pretty thorough. He observed a framework plan for financing was included in the report this evening and confirmed with Staff there was time between now and City Council to discuss it further.

#### Commissioner Levit:

- Understood a timing issue was involved if the cost was to be spread over all the houses because the wall would be built and Stafford Rd developed long before all the houses were built so, the money might never show.
  - Mr. Grimberg replied the money would come in, but agreed it was a timing issue. He explained in other areas, incremental improvements were done because the whole road did not have to be built up front. In West Hill's case, they did not need to improve all of Boeckman Rd when they developed their 50 lots. Mr. Wolfston's development might trigger a small improvement somewhere; perhaps interim turn lanes might be provided until the collected fees were adequate to fund the major improvements.
    - These discussions usually started out with the idea that the roads would all be built up front, which raised the question of where the money was going to come from. There was no money. Typically, a city told the developer to make the improvements and they would be reimbursed down the

- road. The developer wanted to know how far down the road and asked about the city's money, but the city had no money. There was no big pot of money.
- He clarified that, in other areas, doing the improvements incrementally typically did not result in a hodge-podge of road improvements and constant construction for years. While it was a long discussion, incremental improvements did work.
  - Asked about the system development charges (SDC) for the south side of Boeckman Rd from Meadows and the other developments there.
    - Mr. Neamtzu confirmed the City would contribute SDC money, which has already been collected, towards its obligation to construct the entire south side of Boeckman Rd.
    - He confirmed that 14 percent of the road development was for the bridge.

Commissioner Springall observed that apportioning the funding by number of households would seem to add a higher cost on the Small Lots, whereas apportioning the funding by lot size added a higher cost to the larger lots. He asked if the Commission should give some direction on that matter or if Council determined that policy.

- Mr. Neamtzu stated it was impossible to prove that a large lot had more impact on the system, since one person could live in a 50,000 sq ft house and 14 people in a 2,000 sq ft house. Because of the need to normalize the impact, it was calculated on a per dwelling unit basis.
  - While the idea was attractive on the surface, he had never seen a study able to demonstrate that impact, nor could they demonstrate that bigger lots would have a bigger impact in Frog Pond West, and therefore, should legally pay a higher proportion of the cost. He was not certain the idea was legally defensible.

Commissioner Postma:

- Understood current conversations were occurring with the owners in the hopes that they would come up with a methodology between them to equitably distribute the funds.
  - Mr. Neamtzu clarified the City was engaging with individuals, floating ideas back and forth, and trying to find common ground. Once both West Hills and the City were in a comfortable place, Staff would then talk with Mr. Altman's group, who has been involved in the infrastructure interviews.
    - It was a complicated matter. Not everyone agreed and people wanted different things, but at some point, the City would have to say, "This is as far as we're able to go financially." The unintended consequence might be disagreement or an inability to bridge the gap, and the project might not happen for a while. That was the reality.
    - He reminded that the Commission discussed the per door costs during its density discussions and the need to pay for the infrastructure projects. The Planning Commission was told in no uncertain terms and repeatedly through the testimony on the Area Plan that this was "no big deal" and "to build it and they will come." Now, at this point in the process, it was not easy.
- Suggested including the word "equitable" in the Infrastructure Plan section in hopes that those who came to the table could have a reasonable conversation about it. The simple solution might be to punt with some idea of equity.
  - While 'equitable' was about the same word as 'fair,' lawyers like himself preferred to use 'equitable.' To him, 'equitable' tried to encapsulate the concepts of fair and some semblance of equality, while 'fair' was a subjective term. However, the key was how did one measure equal. Equitable attempted to be a little more objective, although it seldom was.
  - Mr. Hanson added and possibly 'proportional'. It started steering toward the amount of land one owned and the amount of land one was developing.
- Believed that could be incorporated by saying the apportionment should be with an eye towards an equitable distribution proportionally among the lots. However, it begged the question that Mr. Neamtzu raised, how did one really measure the impacts, if that was the intent.

Commissioner Mesbah agreed the element of equity was completely missing here, noting the issue of takings raised by Mr. Neamtzu. The City had no obligation to annex this land, so the fact that the City was developing

the area provided value added to the landowners, developers, and so forth. That part of the equity also needed to be counted. The City had the option to require that it be done a certain way, which cost money. The wall was an aesthetic element, but so were all the rest of the design standards, which cost the developers and property purchasers money. The City was saying that in order for the City to provide the services, “This is what we want it to look like”, which was part of the equity.

Chair Greenfield observed it was a cost/benefit analysis. The cost to the City and homeowner needed to be balanced with the benefit to the City and homeowner.. It was a complex matter that he was not sure the Planning Commission could do anything about, but he might attend the City Council meeting when the issue came up, as he had some sensibility about the issue.

Mr. Neamtzu appreciated Commissioner Postma’s simple and elegant idea.

Commissioner Springall:

- Noted the density designation of Subdistrict 3 in Table 3 of Appendix C-4 should be corrected to state R10, not R3 as noted. The R3 Zone did not exist in Frog Pond West.
- Asked to include former Commissioner Marta McGuire and current Commissioner Kamran Mesbah on the Acknowledgements Page in Appendix A-1.
  - Mr. Neamtzu said he had caught that earlier and Staff would make the noted corrections.
- Expressed concern about the lighting levels shown on the Public Lighting Plan (Slide 37) with respect to pedestrian connections, trailheads, hammerheads, and their connection to the creek. He confirmed the pedestrian connections had the lowest lighting levels, but noted conflicting requirements regarding the trailheads and hammerheads and their connection to the creek given concerns about personal safety and the wildlife issue. Lighting adjacent to an SROZ should be much dimmer and nonintrusive because wildlife would primarily use the creek corridor at night, and not people. He could not visualize how bright the lighting would be, but it should not be to the same standard as the roadway lighting.
- Asked for clarification about what trailhead lighting meant before he suggested any potential changes.
  - Mr. Dills noted the intent statements on Page 80 of the Master Plan discussed pedestrian, trailhead, and path lighting. The bottom paragraph addressed the SROZ lighting noting that the SROZ buffers and trailhead areas would be the lighting Zone LZ 1 as specified in the City reference provided, which called out specifically the unique character and wildlife considerations. He confirmed the language accomplished the outcome Commissioner Springall was suggesting. The project team was comfortable with the lighting standard accommodating the SROZ.

Chair Greenfield asked what the lighting standard was for the pedestrian connections.

- Mr. Neamtzu explained typically, it was more of a transition lighting zone with a smaller scale pedestrian light. The parks in Villebois had a similar lighting fixture. Pedestrian lighting was at a smaller scale of 10 ft, rather than 12- to 15-ft high, so it was closer to the ground to provide comparable ground level lighting for pedestrians. The lights were likely placed a little further apart than safety lighting for cars, but still provided consistent lighting without dark patches.
- Bollards were not recommended due to the high likelihood of vandalism and serious maintenance issues in these types of areas, especially along trails.
- He described the transition of lighting from the dark of the canyon for wildlife to the lighting of the trailhead zone and then to the regular street lighting.

Chair Greenfield stated for the record that he strongly supporting spreading the cost of the Boeckman Rd and Stafford Rd improvements to the whole neighborhood. He agreed with Mr. Altman’s argument on Page 3 of his memorandum that it was not reasonable or equitable for a limited number of lots to be burdened with this large additional cost.

- He agreed with Commissioner Levit that the Master Plan had come together pretty well. It was an excellent document and he recognized the input from the Commission and the community.

Andrew Karr commented that given the discussion on spreading the cost equitably across housing units, he reminded there was a minimum and a maximum, so the City needed to be careful to balance the SDC Fund every year. If the cost was spread across 571 units, but only 452 units were built, then not enough SDCs would be collected to pay for the improvements.

- Mr. Neamtzu noted Mr. Karr was a Budget Committee member and a former DRB member and was very familiar with the City's process. He clarified the project team's methodology assumed an 80 percent build out for the purposes of spreading the cost across the neighborhood, which would address Mr. Karr's concern.

Commissioner Millan thanked the City for including the Dark Sky elements in the lighting piece. Regarding the equitable distribution issue, it was good to hear there has been some contingency planning, and the City was not looking at spreading the cost out over all the possible things that could be built. She commented it had been good and interesting process, and it was remarkable to see the interplay between the landowners', developers', and City's interests coming together.

Chair Greenfield thanked the consultants and Staff. He confirmed there were no further comments and closed the public testimony. He called for any further comments from the Commission.

Commissioner Postma suggested including in the motion references to items the Commission felt should be changed or included, such as potential inclusion of language in the infrastructure funding portion regarding a reference to equitable allocation for things that benefit the entire project. He wanted to be sure the Commission consented to that inclusion.

Amanda Guile-Hinman, Deputy City Attorney, advised on how the motion should be stated.

The Commission consented to the inclusion of the word "equitable" in the Infrastructure Financing Plan.

**Commissioner Postma moved that the Planning Commission recommend approval of Resolution LP17-0001 Frog Pond West Master Plan, associated amendments to the Comprehensive Plan, Text and Map, Planning and Land Development Ordinance Text, and SROZ Overlay Zone, to City Council, correcting the density designation in Appendix C-4, Table 3, Subdistrict 3, to R10, not R3, and including a reference within the Infrastructure Financing Plan to equitable cost allocation for improvements that benefit the entire Frog Pond Development. Commissioner Mesbah seconded the motion, which passed unanimously.**

Chair Greenfield closed the public hearing.

### III. OTHER BUSINESS

#### A. Town Center Plan Kick-off Event Update - Miranda Bateschell

Miranda Bateschell, Long-Range Planning Manager, reported on the Town Center plan Kick-Off Event held on Tuesday, February 28th, in City Hall with these key comments:

- Activities for engaging families prior to the formal events starting included project boards and maps, where people could place stickers to show where they lived and where they went in the Town Center. There was also a visioning exercise using a card that said, "My future Town Center is \_\_\_\_\_."
- The consultants' presentation began with a review of the existing conditions in the Town Center and trends the consultants were seeing in town centers across the country. In presenting the different alternatives, the consultants asked whether these trends were part of the attendees' vision and which pieces citizens were interested in seeing or not seeing in the Town Center.
- The approximately 80 attendees, both children and adults, then broke into seven small discussion groups, including a youth table and a Spanish-speaking group. At the end of the event, many very good ideas were reported out and people were excited about the project.

- She explained that the break out groups were being recorded so everything discussed would be part of the record. Videos were also made of the small group report outs. As with any other map-based activities, Staff has offered to do the same facilitated exercise at schools or with other groups. All of this information, including the survey results, would be compiled into a report for the Task Force and then for the joint Planning Commission/City Council meeting as part of the public input received to date in helping to inform a vision statement and goals for the Town Center project.
- She encouraged the Commissioners to take the survey and share it with their network. Within two days, the survey had 41 responses, so she looked forward to seeing the response by the end of the week. The survey was available in both English and Spanish, and other translations or resources were available upon request, so anyone could take the survey, which would be on the Town Center project website until the end of March.

Feedback, suggestions for future events and comments about any key issues that arose within the Commissioners' particular focus groups were as follows:

- The Kick-Off Event was a productive start and there was a sense that the community felt involved, which was important.
- The enthusiasm of an engaging young woman from the high school, who was on the Task Force, was contagious. Her input on how her age group wanted a Town Center that created spaces where the kids could socialize and interact with different age groups was an important piece beyond the typical shopping element. Her energy about creating a space in Town Center and what it could be for her age group provided a very enlightening perspective.
- One area business owner was willing and open to seeing change in the area, while another area business owner was completely against it.
- It was an engaging process and the consultants did an excellent job, especially in bringing back information from the small groups better than other consultants have done over the years. The consultants probably would have done even more, but there were time constraints.
- All the excellent charts on the mind map drawn by Consultant Alex Dupey from MIG would be captured in high resolution and included as part of the report.
- Bringing in the bilingual perspective and inviting both kids and families to contribute was appreciated.
- Having multiple means to gather feedback and connect with people at the Kick-Off Event was valuable and encouraging for community engagement. Stepping away from the traditional open house event was beneficial.
- The Kick-Off Event was well done, especially the variety of citizen input and attendance.
- Staff was aware that the Question of the Week on the website only allowed one choice instead of all one might want as requested. They had been troubleshooting the problem for about four days and identified the problem. In theory, it had been fixed, but it would be helpful if someone who had not already responded tested it and let Staff know if it was working now. Deleting computer cookies might eradicate the problem.

Commissioner Springall noted the French Prairie Bridge online survey closing in a few days and he encouraged the Commissioners to fill in their comments soon.

#### B. 2017 PC Work Program

Miranda Bateschell, Long-Range Planning Manager, stated the work program was already outdated with the Transit Master Plan now going to public hearing in May with the work session in April.

She expected to bring the 2016 Housing Report forward next month and perhaps, an update on the Task Force's work on the Town Center Plan.

Commissioner Levit reported seeing outdoor burning last week behind the Wiedeman House and asked about the City's policy on outdoor burning within the city limits.

- Commissioner Hurley recalled asking TVF&R about a similar situation and learning outdoor burning was allowed in unincorporated sections of the city.
- Ms. Bateschell said she would have one of the planners follow up with Commissioner Levit about the City's outdoor burning policy

Commissioner Levit asked how one could find out when a project would be on the Capital Improvements Plan (CIP). The sidewalk project at the end of his street, Willamette Way West, was in the Pedestrian Master Plan. The sidewalk on the east side of the street was not complete. The sidewalk was a Safe Route to School and right across the entrance to Graham Oaks, but there was no way for people to stay out of the busy road. He inquired when the sidewalk project might be done. He could not find any recent information on the City's website.

- Ms. Bateschell believed all the Pedestrian Master Plan projects had been transferred to the Capital Improvements Plan (CIP). She would find out from the engineers when the Willamette Way West project was scheduled and follow up with Commissioner Levit.

#### **IV. ADJOURNMENT**

Chair Greenfield adjourned the regular meeting of the Wilsonville Planning Commission at 8:20 p.m.

Respectfully submitted,

By Paula Pinyerd of ABC Transcription Services, Inc. for  
Tami Bergeron, Administrative Assistant - Planning

**City Council Meeting Action Minutes  
February 6, 2017**

| COUNCILORS         | STAFF            | STAFF              |
|--------------------|------------------|--------------------|
| Mayor Knapp        | Bryan Cosgrove   | Mark Ottenad       |
| Councilor Starr    | Barbara Jacobson | Jon Gail           |
| Councilor Akervall | Jeanna Troha     | Stephan Lashbrook, |
| Councilor Stevens  | Sandra King      | Eric Loomis        |
| Councilor Lehan    | Susan Cole       | Michelle Marston   |
|                    | Nancy Kraushaar  | Cathy Rodocker     |
|                    | Delora Kerber    |                    |

| AGENDA ITEM   | ACTIONS   |
|---|---|
| <b>WORK SESSION</b>   |   |
| <ul style="list-style-type: none"> <li>Transit Master Plan</li> <li>Frog Pond Master Plan</li> <li>Metereaders Contract Questions</li> </ul>  | <ul style="list-style-type: none"> <li>Staff presented a synopsis of the Transit Master Plan and distributed copies of the draft Plan and comments to the Council.</li> <li>An overview of the Master Plan was given to Council.</li> <li>Staff responded to questions about the cost effectiveness of electronic meter reading.</li> </ul> |
| <b>REGULAR MEETING</b>  |   |
| <u>Mayor's Business</u> <ul style="list-style-type: none"> <li>Board and Commission appointments<br/>A list of the appointments is attached.</li> </ul>   | <ul style="list-style-type: none"> <li>Council ratified the appointments to fill vacancies on the Boards and Commissions.</li> </ul>  |
| <u>Communications</u> <ul style="list-style-type: none"> <li>Bob Gibbs, Town Center Retail Analysis</li> </ul>  | <ul style="list-style-type: none"> <li>Mr. Gibbs presented retail purchasing information gathered about Wilsonville and talked about additional retail establishments the city could support.</li> </ul>  |
| <u>Consent Agenda</u> <ul style="list-style-type: none"> <li>Resolution 2613 – awarding a contract to Metereaders, Inc.</li> <li>Minutes of the January 5<sup>th</sup> and 19<sup>th</sup> Council meetings.</li> </ul> | The consent agenda was approved 5-0.  |
| <u>Public Hearing</u> <ul style="list-style-type: none"> <li>Ordinance No. 801 and 802</li> </ul>   | Both ordinances were continued to the February 23 <sup>rd</sup> Council meeting.  |
| <u>Continuing Business</u> <ul style="list-style-type: none"> <li>Ordinance 804 – establishing a residential parking zone program</li> </ul>  | The ordinance was adopted on second reading unanimously.  |

|                                |   |
|--------------------------------|---|
| <u>City Manager's Business</u> | Council was reminded about their retreat and goal setting session scheduled for Feb. 10 and 11. |
| <u>Legal Business</u>          | No report.  |
| Adjourn                        | 8:45 p.m.   |

## BOARD AND COMMISSION APPOINTMENTS

February 6, 2017

### BUDGET COMMITTEE:

- ✓ Sam Scull to a 3 year term beginning 2/6/17 and ending 12/31/19

### DEVELOPMENT REVIEW BOARD

- ✓ Joanne Linville to a 2 year term beginning 2/6/17 and ending 12/31/18
- ✓ Jennifer Willard to a 2 year term beginning 2/6/17 and ending 12/31/18
- ✓ James Frinell to a second 2 year term beginning 2/6/17 and ending 12/31/18
- ✓ Ronald Heberlein to a second 2 year term beginning 2/6/17 and ending 12/31/18
- ✓ Richard Martens to a second 2 year term beginning 2/6/17 and ending 12/31/18
- ✓ Shawn O-Neil to a second 2 year term beginning 2/6/17 and ending 12/31/18
- ✓ Aaron Woods to a second 2 year term beginning 2/6/17 and ending 12/31/18

### PARKS AND RECREATION ADVISORY BOARD

- ✓ Jim Barns to a 4 year term beginning 2/6/17 and ending 12/31/20
- ✓ Diana Cutaia to a 4 year term beginning 2/6/17 and ending 12/31/20
- ✓ Katharine Johnson to second 4 year term beginning 2/6/17 and ending 12/31/20

### PLANNING COMMISSION

- ✓ Phyllis Millan to a second 4 year term beginning 2/6/17 and ending 12/31/20

**RECORDED BY: SCK**



**CITY COUNCIL MEETING ACTION MINUTES  
MARCH 6, 2017**

| <b>COUNCILORS</b>          | <b>STAFF</b>     | <b>STAFF</b>    |
|----------------------------|------------------|-----------------|
| Mayor Knapp                | Bryan Cosgrove   | Nancy Kraushaar |
| Councilor Starr-excused    | Barbara Jacobson | Jon Gail        |
| Councilor Akervall         | Jeanna Troha     | Chris Neamtzu   |
| Councilor Stevens -excused | Sandra King      | Delora Kerber   |
| Councilor Lehan            | Mark Ottenad     | Eric Mende      |
|                            | Dwight Brashear  | Eric Loomis     |
|                            |                  |                 |

| <b>AGENDA ITEM</b>  | <b>ACTIONS</b>  |
|---|---|
| <p><b>WORK SESSION</b></p> <ul style="list-style-type: none"> <li>• Cost of Services Analysis for SMART</li> <li>• Frog Pond West Maser Plan Residential Neighborhood Zone</li> <li>• Water Treatment Plant Master Plan and Willamette Water Supply Program Coordination</li> </ul> | <ul style="list-style-type: none"> <li>• Staff presented the financial issues outlined in the proposed Transit Master Plan and the service priorities with and without new funding.</li> <li>• An overview of the new development zoning code created as part of the Frog Pond West Master Plan was provided.</li> <li>• Staff briefed Council on the recently completed Willamette River Water Treatment Plant 2015 Master Plan Update, and identified the next steps for a more focused look at the existing WRWTP, as well as the efforts between the City and the WWSP for near term projects.</li> </ul> |
| <b>REGULAR MEETING</b>  |   |
| <u>Mayor's Business</u>   | <ul style="list-style-type: none"> <li>• Mayor Knapp presented his 2017 State of the City Address</li> </ul>  |
| <u>Consent Agenda</u>   |   |
| <ul style="list-style-type: none"> <li>• Minutes of the 2/23/17 Council Meeting</li> </ul>  | Consent Agenda adopted 3-0.   |
| <u>New Business</u>   |   |
| <ul style="list-style-type: none"> <li>• 2017-19 Council Goals</li> <li>• Council Protocol Manual Revisions</li> </ul>  | Both of these items were continued to March 20 <sup>th</sup> so all Councilors could attend.  |
| <u>City Manager's Business</u>  | <ul style="list-style-type: none"> <li>• Reminder of the March 15<sup>th</sup> Mid-Year Budget Review Meeting</li> </ul>  |
| <u>Legal Business</u>   | No report.  |
| Adjourn   | 8:55 p.m.   |

Prepared by SCK

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**Task Force Kick off meeting  
South Metro Area Regional Transit (SMART)  
Transit Master Plan Update**

Thursday, April 9, 2015, from 6:00 to 8:00 pm has been scheduled for the official kick-off meeting of SMART's new Planning Task Force. We will use that meeting so that volunteers and staff can get to know one-another and begin to talk about goals and objectives, discuss schedules and to pencil out the work plan for the upcoming year.

Earlier this year, SMART began seeking volunteers to serve as Task Force members to help guide the update of the City's Transit Master Plan (TMP). City Councilor Julie Fitzgerald agreed to chair the Task Force and we received over a dozen inquiries from Wilsonville's citizens and employees wanting to know more about what would be involved with being a Task Force member.

The Task Force will guide the process for the TMP update that will take place over the course of this next year. Task Force members are committing their time to meet, on average, a couple of hours every other month – and this schedule will ebb and flow as topics and conversations become more detailed and also when input will be sought from the broader community. Ultimately, the new plan will be reviewed by the City Planning Commission and City Council in 2016.

The TMP is SMART's guiding planning document and as such, will lay out the framework for what types of transportation options the City of Wilsonville wants and at what level of transit service the community would like to see and support.

Current operations will be analyzed to find efficiencies so that SMART can offer relevant and quality service that has the potential to retain and attract more riders of all ages and backgrounds. Besides providing transportation options for the general public, a modern and streamlined transit system can spur economic development by helping to attract new businesses to the community.

Public participation is a crucial part of preparing a strong and successful Transit Master Plan. The citizens and the business community of Wilsonville will be asked to provide input and help the Task Force in the process.

SMART's website will contain information about the Task Force meetings as well as opportunities for public input and related TMP activities and events. Later this month, you can look forward to learning more at [www.ridesmart.com/tmp](http://www.ridesmart.com/tmp)

**TMP task force notes from 6-23-2015**

In attendance:

Staff:

Stephan Lashbrook

Jen Massa Smith

Michelle Marston

Steve Allen

Task Force Members:

Councilor Julie Fitzgerald, Chair

Gary Fleenor

Peter Raply

Pat Rehberg

Paul Diller

Miriam Ludlow

Tobi Crooks

Chair Fitzgerald reviewed the role of the Task Force:

To provide useful advice to City Council and staff without requiring a long-term time commitment. Advice from the Task Force will help to shape the new Transit Master Plan, with the City Council approving the document after making final changes.

We are looking for:

- how can SMART increase ridership?;
- what does SMART do well, and what could we do better?; and
- making sure everyone feels heard in the process

Stephan Lashbrook reviewed the contents of the 2008 Master Plan:

- Focus of the old plan was on WES (commuter rail) coming to town; this is now more than six years in the past.
- Pages 49-53 (Implementation Measures) – a list of the actions that were to be taken to implement the 2008 Master Plan. Many have been completed; some have not.
- How can SMART achieve our goals in the new plan?

Members of the Task Force asked a number of questions about, and discussed, existing SMART services, including:

How would SMART go about improving the quality of service and/or improving access for more riders?

How do people learn about SMART? Staff talked about the following:

- Boones Ferry Messenger;
- Newsletters;
- Info booths or “tabling” at various events;
- Visits by staff to local businesses or other organizations;
- Social Media;
- Free Ride Tickets;
- Home Owners’ Associations;
- Mom’s clubs/exchanges;
- Senior living facilities;
- Senior/Community Center outreach; and
- Email lists.

How are non-English speakers engaged?

- SMART currently has at least two drivers who are fluent in Spanish.
- SMART’s phone system allows for immediate translation of more than 200 languages. The only users thus far have been speaking Spanish.
- One summer intern speaks Spanish.

Loaner bicycle program? Loaner car program?

The 2008 Master Plan envisioned both at some point in time.

SMART has two bikes for staff use but has not yet actively pursued a bike-share program for the public. Jen said that the Enterprise car rental company does offer some possible car-share or van-pool options and that the staff could do more research on potential use of their services.

Legislative goals – there was a brief discussion about the current uncertainties about transportation funding at both the state and federal levels. Stephan indicated that the state’s proposed transportation bill could provide substantial funding for expanded SMART services. *(That bill subsequently died without ever getting to the floor of the state Senate.)*

A discussion of TriMet services followed. If TriMet’s revenues continue to increase, WES service may be expanded within the next few years, with weekday service through the mid-day and later at night. Eventual Saturday WES service is also being discussed at TriMet.

TriMet Route 96 could be extended to SMART Central. This has been under discussion for some time, with SMART actively supporting the idea. Those negotiations are continuing but TriMet staff has expressed concerns that, by providing fixed route service within Wilsonville they could be required to also provide dial-a-ride service for ADA-qualified riders. SMART has said that it will assure that all required ADA services are provided, and is willing to enter an inter-governmental agreement with TriMet about those services. (Stephan noted that the fact that TriMet already provides service into Wilsonville at Commerce Circle does not seem to resonate with TriMet staff.)

Stephan said that there is a bill introduced in every legislative session to extend WES service to Salem. The City of Wilsonville supports that idea but it never appears to gain much traction at the Capitol. There are also some significant logistical issues with a single track carrying both passengers and freight.

When asked why WES has not been replaced by a MAX line, Steve and Stephan pointed out that it would require a completely different kind of infrastructure because WES is “heavy rail” and diesel powered, while MAX is light rail, running on electricity. Michelle also pointed out that any new public rail improvements on the current alignment would probably require the support of Tigard voters.

Questions were raised about the gaps in service in some areas through the middle of the day. Staff noted that quite a bit of the service provided by SMART is really geared to commuters, with a focus on each end of the typical work-day. Steve talked about plans for SMART to augment TriMet service to Portland in a way that would assure that the total fare paid by a rider (to SMART or to SMART and TriMet combined) would be \$3.00. A rider who transfers from SMART to TriMet in Tualatin currently pays \$4.00 (\$1.50 to SMART and \$2.50 to TriMet).

Councilor Fitzgerald asked if Citizen letters in support of this change would help? Staff responded that letters of support can be helpful for any proposed changes in service or fares.

A question was raised about the potential for service along the I-205 corridor, to West Linn and/or Oregon City. Stephan responded that it would be the kind of increased service that could be possible if SMART had more funding.

A question was raised about WES ridership. Steve said that it averages about 1800 rides/day, but not all of those come as far south as Wilsonville. That number is only a fraction of the ridership that was originally anticipated for WES. Steve also noted that when WES ridership is down, SMART’s ridership drops as well.

A question was raised about SMART’s Emergency Ride home program- It is mentioned in the Master Plan, but what is it? How does someone access it? Steve said that currently our Supervisors respond to those requests and take riders home in an emergency situation, but they can only do it when they have the time available. SMART does not have any stand-by drivers who are on duty but just waiting to be dispatched. Steve also said that it is difficult for a transit agency to provide emergency rides home without having it become a free taxi service for anyone who just wants a ride and does not want to pay for a taxi or wait for a bus.

What is potential for Uber in Wilsonville?

Councilor Fitzgerald asked if Uber has a local business license. Stephan agreed to find out. *(Note that the City Finance Department subsequently reported that Uber has not applied for a City business license.)* Uber seems to elicit strong reactions, pro and con, from everyone who knows about them. It was agreed that Uber is a rapidly growing company with the potential to provide more transportation alternatives to Wilsonville residents. Staff will do more research and return to the Task Force with more information about Uber and similar companies.

Staff talked about the Existing Conditions Report, pointing out that it

will soon need to be finalized, but comments are welcome while it is still in draft form. The report explains what is happening now in terms of local transit services. It also provides history, facilities, current services, community profile, with neighborhood by neighborhood information.

In the report the community is divided into 10 neighborhoods. The staff intends to reach out to each neighborhood to find the major opportunities and constraints in terms of transit service to each.

Task Force members had questions about some of the neighborhood information in the Existing Conditions Report. Steve pointed out that we have only anecdotal information about the number of riders getting on and off a bus at any given location. Planned technology updates should help to address that. Under the current system, the only way to generate those numbers is to have someone, other than the bus driver, keep track of all of the passengers boarding or disembarking from buses. This is something that SMART does infrequently and when it happens, it creates only a snapshot view of ridership patterns.

Pat mentioned the history of bus service in the Fox Chase neighborhood, where some neighbors convinced the City to stop providing service in the area. As with the ends of most bus lines, people reported seeing empty buses. Steve pointed out that most bus service includes peaks and valleys of ridership, but the ends of most lines generally have the lowest ridership. Steve mentioned the Old Town neighborhood, and the fact that residents there have been very divided about transit service, with some wanting all bus service there to be curtailed. Stephan pointed out that SMART may be able to gain more neighborhood support as we transition to more CNG or battery-electric buses, which are considerably quieter than their diesel counterparts.

Much of the remaining discussion went to possible service enhancements that Task Force members want to have considered. These included:

- Providing a direct connection to Woodburn;
- Adding more frequent mid-day crosstown service on Route 4;
- Providing a park-and-ride on the east side of the freeway to augment the one on the west side;
- Improved service to Canby;
- Better scheduling and route configuration on Route 4. Another option was to have two crosstown routes, one that continues the current #4 and another that sticks primarily to Wilsonville Road and just goes back and forth as quickly as possible;

- Work out a compromise to address concerns of Old Town neighbors; and
- Better and more efficient turn-arounds at each end of Route 4.

The Task Force talked more about the Existing Conditions Report and the information it contains about various neighborhoods. The fact that roughly 90% of people working in Wilsonville commute in, while 90% of Wilsonville residents who are employed commute out, has a major impact on highway congestion and on demands for transit services.

Task Force members talked about ways to increase public awareness about SMART and the services it provides. Ways of building community were discussed, including encouraging people to try the bus for the first time. This could include simple trips to yoga, hair appointments, post office, etc. It was noted that kids from all over the community go to Boones Ferry Primary School during summer for day activities and other programs. There may be ways that SMART can reach out to those parents and children. Pat suggested that the City needs to recommit to providing transit services within walking distance for residents (1/4 mile or 1/3 mile of every neighborhood).

Stephan talked about Metro's Climate Smart Strategy, now approved by the State. It is intended to reduce greenhouse gas emissions by 20% by 2035. The adopted Strategy assumes that there will be huge increases in transit funding as part of the implementation process, but the details have not even been discussed at the regional or State level. Obviously, it will not be implemented without State or federal funding assistance. Stephan talked about the fact that the Strategy assumes that there will be more frequent bus service all around the region – and it will need to also address more fuel-efficient buses in the process. To encourage more people to use transit, fares will need to be kept to a minimum and there needs to be fare reciprocity from one transit agency to another. Stephan noted that someone commuting from within the Portland area to Salem would have to pay separate fares to TriMet and SMART, and might end up also paying a fare to Cherriots.

Task Force members were asked: "If you could add one new SMART route or service, what would it be?"  
Answers included:

- Lake Oswego service, including service along Stafford Rd.;
- Frog Pond area service;
- Orange Line connection in South Waterfront area (already in the works for later this year);
- Sellwood bridge service;
- Route 9X expansion to all-day service, with a focus on access to Downtown Portland;
- Later morning service to Salem – 8 am as the last bus is too early for many travelers;
- All day service on TriMet's Route 96, along with an extension of the 96 to SMART Central;
- Increased hours of operation for weekend service in Wilsonville;
- Create a Canby loop service;
- Create a Villebois service loop with more frequent service than the current Route 7;

- Add service to Yamhill County, maybe in cooperation with Yamhill County Transit;
- Add service to Sherwood;
- Emphasize Safe Routes to Schools;
- Convey the message that buses are safe zones for people (especially children) needing to escape a hostile environment;
- Finding ways for employers to support non-traditional work schedules to reduce peak-hour commuting;
- Do more outreach programs for children at schools; and
- Profile Riders- in the Boones Ferry Messenger and let them tell their stories.

At the close of the meeting Stephan encouraged everyone to review the Southwest and Southeast TriMet service enhancement plans, noting that our Transit Master Plan will include our own service enhancement ideas.

Task Force members suggested that the next meeting not be so far in the future. Stephan agreed that it would be a good idea but also noted that it is difficult to pull meetings together during the summer. The idea of a transit field trip for Task Force members was discussed – with rides including SMART, a TriMet bus and a WES train ride. The staff agreed to look into scheduling that trip.



**Transit Master Plan Task Force meeting of 11-03-15**

*attendance:*

Staff:  
Stephan Lashbrook  
Jen Massa Smith  
Michelle Marston

Consultant: Patty Fink

Task Force Members:

Councilor Julie Fitzgerald, Chair; Jean Tsokos; Stephanie Yager; Pat Rehberg; Barb Leisy; Lynnda Hale; Katrina Koppe; Peter Rapley; Paul Diller; and Ginger Fitch

The meeting opened at 6:04 pm, with a welcome to new members. Stephan explained that the Task Force operates on a “first name basis” and that everyone has agreed to stand their name-tags upright in order to get the attention of the Chair before speaking. In response to a request from Julie, Stephan explained the role of the Task Force as an ad-hoc advisory committee -- giving advice to staff, consultants and the City Council. Stephan added that after the evening’s discussion of the proposed Master Plan Goals, those Goals will be reviewed by both the City Council and staff at the Oregon Department of Transportation (ODOT).

**Minutes for meeting of September 22, 2015:**

No one wished to make any changes to the minutes that had been sent out in advance. Stephan suggested that the September meeting be regarded as “informal” because of the low attendance (3 Task Force members) and the fact that no official actions were taken.

**Overview of public outreach effort:**

Jen showed a brief Power Point and talked about the consultant team that has been retained to help with public outreach for the Master Plan. She said that three different firms will be involved, each firm with its own particular expertise. Jen told Task Force members that they will be asked to work closely with the consultants in the months ahead. Jen added that the consultants will be placing electronic kiosks at different locations in the community, to gather public comments.

Patty said that the outreach consultants will be seeking advice on the best places to locate the kiosks and suggested that Task Force members help to make those choices.

**Draft Goals for the new TMP**

Stephan handed out an updated version of the draft Goals for the Master Plan, as developed by the staff, with input from the three Task Force members who attended the September meeting. Stephan reviewed the draft briefly, pointing out that the Task Force will have the opportunity to make adjustments to the language as the planning process moves forward.

**Draft Service Standards to implement the proposed Goals for the new TMP**

Patty reviewed the draft fixed–route Service Standards that were distributed with the draft Goals. She went through each of the proposed Standards, explaining how they would be applied. There were numerous questions and comments for Patty.

## EXHIBIT K

Jean said that she had received a call from someone who said they worked for Clackamas County and that they were doing research on transit services. However, the caller was apparently unaware that SMART even existed. Stephan said he would contact the office at the County that provides some funding to SMART to see if they know anything about the call.

Pat said that the public outreach effort should include outreach to youngsters. She suggested a “ride the bus” day promoted by SMART and the School District.

Task Force members suggested that the Goals or Service Standards should also address:

Environmental concerns; affordability (especially in terms of fares paid by riders who transfer from one system to another); usefulness; flexibility; shelters at bus stops (and other amenities – especially lighting); the economic development importance of SMART;

Barb said that the bus stop at Shari’s Restaurant is a problem for numerous riders. Stephan said that SMART staff and City Engineering staff continue to discuss how best to address the problems there.

Paul suggested that SMART really needs to focus on where riders want/need to go and when they need to get there.

Jean reminded everyone that good transit service equals “freedom” for many people who have no other means of getting from point ‘A’ to point ‘B’.

Katrina suggested having a bus available at night to pick up people who would otherwise be driving drunk.

Peter said that some people do not like riding on smaller buses. They may feel like it’s not a genuine transit experience unless they are on a full-size bus.

Julie pointed out that one of the values of transit in a commuter-oriented community like Wilsonville is that it takes cars off the road and, thereby, helps to reduce congestion. She added that ridership will be the most important consideration for the future of SMART and the Master Plan will benefit from any ideas that will encourage more people to ride the bus rather than drive.

Ginger said that more needs to be done to serve special needs riders, especially those with mental health problems. Jen responded that SMART provides quite a bit of dial-a-ride service to those special needs riders but agreed that more outreach to them could help.

Lynnda suggested establishing an “adopt a shelter” program to reduce bus shelter maintenance costs and allow for the placement of shelters at more stops. Jen said that the transit system in Denver has used that approach with great success. Discussion followed about adding lighting at shelters or making other improvements to increase safety for riders. It was agreed that if people do not feel safe, they will not ride the bus. Patty added that she thought improved ADA access to some bus stops could also be considered an important “amenity enhancement.”

Katrina pointed out that many commuters to/from Wilsonville do not take the bus because of the “double fare” situation between SMART and TriMet. It was agreed that fare reciprocity will continue to be an issue for SMART. She also added that the lack of mid-day service on many SMART routes is a hindrance to ridership.

Peter said that he appreciates SMART’s focus on business commuters. Paul added that he thought SMART could do more to promote the services it offers to commuters.

Stephanie and Katrina said that they could help to provide information, including meeting times with new employees at Convergys, where SMART could explain the services it offers.

## EXHIBIT K

After some discussion about the tech upgrades being made at SMART, someone asked if the new system would show prospective riders how many bicycles are already on a given bus. Stephan said he did not know but he liked the idea. He agreed to contact ETA Transit Systems and find out if they can provide that service.

At the end of the discussion of Goals and Service Standards Stephan asked if the Task Force would feel comfortable having the staff move forward with the Goals, provided that an additional Goal is added dealing with “environmental protection.” The consensus of the group was supportive.

Patty and the staff also indicated that the draft Service Standards would be amended to address: State Employee Commute Options (ECO) Rule compliance; support for car-free employees and residents; business access via SMART; bicycle load factor on buses; improved transit options for those with special needs; fare reciprocity; and assuring that fares remain below the cost of driving a car. (Stephan added that the actual cost of car operation is often far more than what drivers think it is.)

Stephan noted that the Goals and Service Standards are biased heavily towards bus operations, with more language needed to support other means of reducing driving by single-occupant vehicles.

### **Service to new growth areas.**

Stephan used a map to show the current SMART and TriMet service boundaries and highlighted where there could be future gaps in service or conflicting interests between the two transit agencies. Stephan added that the City is expected to make a public statement indicating that when land is annexed into the City, it should become part of SMART’s service territory, regardless of whether it was previously within the TriMet boundary.

### **Closing comments.**

Patty reminded the Task Force that SMART has a commitment to the State (which provided a grant to support this planning process) to have the Goals approved by the Task Force in November and reviewed in December by the City Council. The Goals are to be submitted to ODOT by the end of calendar year 2015. Stephan and Julie thanked the Task Force for a helpful and positive conversation.

### **Proposed date for next meeting:**

Stephan said that, since the public outreach consultants have not yet started work, it would make sense to work with the consultants to pick the next meeting date. He suggested that the Task Force meet again in mid-December if the consultants are ready for committee input.

Staff agreed to work with the outreach consultants and look at dates in mid-December and mid-January, to determine a good target date for the next Task Force meeting.

## TMP notes from 9-22-15

attendance:

Staff:

Stephan Lashbrook

Jen Massa Smith

Michelle Marston

Patty Fink

Members:

Julie Fitzgerald

Jean Tsokos

Stephanie Yager

### Brainstorming

- More frequent service, to that it is faster than walking.
- Make flyer for Convergys- let them know about 9X late night route and early 8x route
- Shopper Shuttle for seniors, more convenient.
- Make little videos for cable TV commercial style, storytelling examples to run late night rather than reruns of City Council meetings. Possibly have these infomercials run at work sites and community areas.
- Information into the Sr. Gazette, to Convergys and other businesses that have video displays to share information, and other Senior type messaging.
- Add comments in in emails about vital input in participants to encourage those in the task force to continue to come to meetings. Add dinner to the agenda for meetings make it something folks want to come to, they are volunteering their time.
- Provide a “golden nugget” for participants. Such as they will get first hand test experience with our new technology APP before we go live, to provide feedback.
- Post card flyers to be mailed/ handed out to gain more interest?
- How about a New Year’s Eve shuttle till 1 am?

### Breaking down the Goals

Do we combine the Efficiency and Reliable categories? Much discussion about making too long of sections, better to read in smaller chunks.

### How do we add a new route?

What is the process? What is the service enhancement plan for Wilsonville? Do we request increase in Payroll taxes, possible survey to have riders state what they really want. Prioritize options rank them.

### Homework

Review the goals and message for a formal meeting in November possibly for final approval in December by Council.

Map review

Tonkin area, Wilsonville and Tualatin blended expansion. North of Day road, TriMet service area. Petition TriMet ?

### Proposed dates of next meeting:

The week of October 8-14, or November 3



**DRAFT notes – Transit Master Plan Task Force meeting of 4-12-2016**

attendance:

Staff:

Stephan Lashbrook  
Jen Massa Smith  
Michelle Marston

Consultant: Christian (Chris) Watchie



Task Force Members:

Councilor Julie Fitzgerald, Chair;  
Jean Tsokos; Pat Rehberg; Peter Rapley; Lynnda Hale; Stephanie Yager; and Paul Diller.

The meeting opened at 6:08 pm, with members introducing themselves.

**Minutes for meeting of January 12, 2016:**

No one wished to make any changes to the minutes that had been sent out in advance. Stephan asked if any corrections were needed, Julie asked for a motion to approve the minutes, Jean made the motion and it was seconded by Stephanie and approved without dissent.

**Public Involvement Update and Discussion led by consultant Chirstian Watchie:**

The public outreach effort started in March was described as “March Madness.” Chris began by commending SMART staff, saying that the connections, dedication and leadership of Michelle and Jen has made this process a huge success.

Chris said that, while gaining input in the early phase of the TMP, we wanted to be realistic with our goals and values about what was ahead. She pointed out that there was nothing to gain by raising unrealistic expectations about the services that SMART will be able to provide, but it is still important that people realize that SMART’s services will change over time, and the public can influence those changes. People are being asked to be very clear about what choices they want to make for their community. Inclusivity was very important, reaching out to those whose voices are often underrepresented, as well as those without access to computers. Getting accurate early input will help to provide transparent information to all decision makers.

Chris and Jen reviewed the overall process chart for TMP outreach and public involvement:

We are now beginning the analysis of public comments received to date, primarily from community interviews, focus groups and surveys. In May, June, July, and August, there will be speaking engagements and event outreach for service alternative development and then going back out to the community to hear thoughts about whatever proposals emerge.

Staff and the Cogito consultants will be working with Michelle Poyourow from Jarrett Walker Associates to review and recommend potential service changes, with a report available in July.

The Plan will then be wrapped up in October for final public review, with adoption hopefully to follow in November or December.

Chris then reviewed what we learned in the March 10<sup>th</sup> workshop. She explained that maximizing either ridership or coverage will result in costs – forcing transit agencies in general to choose between the two or opting for a mixture of

both. In many ways, SMART's current system attempts to balance demands for productivity with demands for coverage. With a limited budget, it is not possible for SMART to meet every public expectation about service.

Participants at the March 10<sup>th</sup> workshop included a variety of stake holders including businesses, elected officials, city staff, community groups, educational institutions and home owners. Many who were unable to attend the workshop or the focus group meetings were given the opportunity to be interviewed.

**Focus Group meetings, conducted March 28 and 29:**

- One with a focus on Spanish-speaking and lower income people; and
- One with a focus on older adults and people with disabilities

Jen pointed out that these focus group meetings were especially valuable because new partnerships were created through which we should be able to reach broader communities of people who otherwise might not take part in the planning process.

**Broad level themes that were heard through first round of outreach:**

- 1-Support the economy, local businesses matter;
- 2-Target service times at major employers to shift workers and weekend service (weekends and evenings for non 8-5);
- 3- Economic development for tourism;
- 4- Our payroll tax is system is not well understood by our tax-payers, especially those who pay both TriMet and City (SMART) payroll taxes;
- 5- Enhanced connectivity;
- 6- Need to get to Oregon City for social services, and not just the transit center, but up the hill to the Red Soils area for many different kinds of appointments (pointed out by Stephan);
- 7- More effort is needed to get the word out to Spanish speaking people, public schools, temporary employees coming in to Wilsonville for work; and
- 8 – More partnerships with groups of potential riders of all kinds.

**Jen reported on the online survey that was launched March 1:**

About 825 responses received thus far. More paper copies are still anticipated.

- 1- Website has triggered some responses
  - 2- Electronic Kiosks have been moved from place to place -- City Hall, Community Center, CCC, OIT Lowrie Primary School, Wilsonville High School, Wilsonville Library, and Lambs grocery store
- 3-About a tenth of input so far has come from Kiosks. Paper surveys have been the most productive way to capture input.

Jen added that Jean and Barb turned out to be great ambassadors, especially the day they were at the community center capturing surveys. They made announcements and encouraged folks to fill out the surveys to make sure everyone's opinion was heard.

Resident coordinators at housing complexes were also able to get the information out to the occupants.

Written surveys also went out to:

CCC

Oregon Tech

Wilsonville Chamber of Commerce

Mentor Graphics

Gillespie Graphics

Vision Plastics

Convergys

Dynacast

Fred Meyer

Costco

Lamb's grocery, in front of store and at the checkout lanes.

Express Employment (Tualatin – but with over 300 workers in Wilsonville), placed into all paychecks

Wilsonville Transit Center, during commute hours one evening

Poster and flyers inside all buses

Every Dial-a-Ride, especially for the new Villebois shuttle

Qdoba, stapled them to all to go orders and placed information on catering orders as well as provided in restaurant for surveys to be taken.

Jen said that the staff and consultants will be building on the outreach as we move into this next round of work on the TMP.

SMART has released 3 E-updates to an "interested parties" list of over 1200:

1- Introducing the plan;

2- Introducing the survey; and

3- A follow up reminder to the survey.

#### **Advertising Media**

The Boones Ferry Messenger has been hugely helpful, as well as the Wilsonville Spokesman, where Jen gave a great interview that became a story to highlight the survey. Even though much has been done to this point, there will always be room for more outreach.

#### **Wrap up:**

Jean mentioned that it was wonderful to hear so many from the Senior Center feel like somebody was really listening to them.

Also at the very end we were able to reach some people in the faith-based community. Paper surveys were made available at St. Cyril's Church.



The difficulty in getting HOAs involved was discussed. It was suggested that someone might have to go door-to-door to really get input in some residential neighborhoods.

**Discussion/ Questions:**

There will be follow up with those providing housing for lower income residents, which includes: Creekside Woods; Autumn Park; Rain Garden and Charleston. Another facility run by another organization is Windfield. Some are family oriented and some are senior oriented, others are mixed.

Julie asked what questions will be asked on the next survey? Chris responded that the next survey will be focused on service options. What we are seeing today is a very preliminary view of the results of the first survey. That includes requests for more service, more stops, and regional connectivity.

Jen shared the following survey responses:

*“What do you currently use transit for?”* The majority responded “work commute.”

*“What would you like to use transit for but cannot.”* Again, “work commute” seems to be the most common response.

Jen pointed out that “work commute” doesn’t necessarily mean peak 8-5 hours. It could be evening and weekend shifts that are referred to here.

*“How would you describe yourself?”* The majority of respondents said they live in Wilsonville and/or are employed in Wilsonville.

*“Are short or long transit trips more important to you?”* It was pretty clear from the results that they are equally important: quite a bit of analysis yet to be done with this information.

*“At what times should SMART primarily target service?”* All day, everyday, (even on weekends) edges out commute hours.

*“Where should we concentrate transit service?”* Ridership v coverage model, most people say it doesn’t matter either way but typically if folks are using transit one is chosen over the other.

Through the survey we received many comments, and we will be reviewing as we move forward. The majority of respondents are appreciative of SMART, even though there are some that will never get on a bus.

A lot of analysis is yet to come. Consultant Michelle Poyourow will be presenting different service alternatives at the next meeting.

Pat opened a discussion about whether buses should be going into neighborhoods or not. Who voices opinions the loudest is often able to get their positions prioritized? Stephan suggested that people will need to be willing to voice their opinions folks to voice opinions at City Council meetings. The Council will soon be at a decision point about how the survey results are to be interpreted. It was noted that SMART now runs buses that are cleaner and quieter than the buses that were used ten years ago. That could change some opinions about when buses should go into neighborhoods to provide service. Stephan said that SMART’s newer CNG-powered buses run cleaner and quieter than diesels but he is especially hopeful that SMART will soon begin to add battery-electric buses that have no tailpipes and run more quietly than most cars.

Julie said that SMART will need to broaden the base of transit supporters who have more understanding of the value/benefits of transit for a community as Wilsonville grows and changes in the next 20 years.

Stephan started a discussion about things that we now know the Master Plan will need to address:

- 1-the #4 bus (crosstown) is too slow. Some sort of "crosstown express" is a possible way to address that concern
- 2-later service available to schools, for kids to get home from after school activities after the last activity bus leaves.

SMART may need to have "ambassadors" to work with the school district to educate kids to ride transit, almost like learning how to ride a bike.

SMART's buses could be labeled like safe houses: safe havens for those who may need help quickly, given that bus drivers have radios. Many young kids in the community do ride the bus and some may feel vulnerable.

If TriMet makes midday or weekend service on WES available, that would increase SMART's ridership. This could help Wilsonville residents reach weekend events in Portland.

Julie pointed out that the Mayor has written letters for long range funding for the Region's Southwest Corridor project. This may be something needing more City Council discussion in the future.. It was noted that the Mayor and Stephan will be meeting with FTA folks in May in Washington DC.

Stephan mentioned a point to be aware of: long-time Operations Manager Steve Allen will be retiring this summer. When we get to the portion of the Master Plan that deal with changing SMART's routes Steve will no longer be available to give us advice or to help implement the changes.

Stephan said that some members of the local business community are worried that changes to the Master Plan are being suggested to justify increases to local payroll taxes. That concern is understandable, but taxes have not really been talked about in the planning process thus far.

**Next Meeting Dates**

Tuesdays: July 12, & September 13, 2016

Meeting adjourned at 7:30 pm



**Transit Master Plan Task Force Meeting Notes**

**December 6, 2016 APPROVED**

*Task Force Members:* Chair Julie Fitzgerald, Kristin Akervall, Lynnda Hale, Pat Rehberg, Paul Diller, Peter Rapley

*Staff:* Stephan Lashbrook, Eric Loomis, Nicole Hendrix

Meeting began at 6:04 pm with introductions and a welcome to Kristin Akervall who will be the new liaison from city council for the TMP Task Force (to replace Councilor Fitzgerald).

Minutes from November 1<sup>st</sup> meeting approved.

Stephan asked the task force to help order potential services changes if future SMART funding shifts to increased operating expenses or if more revenue becomes available. Attachment A provides the outline of the meeting and information that Stephan explained in detail to the task force.

For reducing geographic area for out-of-town medical (Dial-a-Ride) trips if there are increased operating expenses, Stephan stated that would require a new round of public outreach to include stakeholders currently using Dial-a-Ride services. For overall increased operating expense service changes, Pat states that the possible cuts listed counter to public opinion from the outreach SMART has completed so far.

If SMART has more revenue, Peter questioned if added Saturday service to Tualatin could reach farther, possibly to Portland. Peter also commented about improving connections to TriMet 96; the only two connection times that are troubling is the last TriMet 96 run in the morning which is slightly late to get the 2x and the first TriMet 96 in the afternoon that has no SMART connection.

Pat speaks to the importance of training riders from a young age and more outreach at schools; specifically mentioned was the bike on bus demonstration.

Next meeting is temporarily planned for January 10<sup>th</sup>, 2017. A follow up e-mail from SMART staff will confirm time and place.

## Attachment A

**Potential Service Cuts or Enhancements****December 2016**

- If we have to make cuts because of increased operating expenses, our first cuts will be:
  - a) Reduced geographic area for out-of-town medical (Dial-a-Ride) trips.
  - b) Dropping one morning trip on the 1X, returning to 30-minute headway.
  - c) Reduced services to Tualatin, or to Tigard, on the 2X.
  - d) Reduced service to Canby on route 3.
  
- If we have more revenue, our first service enhancements will be:
  - a) Added Saturday service and hours of service on the 2X to Tualatin and/or Tigard.
  - b) Added Saturday service and hours of service on route 4.
  - c) Added mid-day or late-morning service on the 1X.
  - d) Added frequency of service on the 5/6 loop, or more service to improve connections with TriMet's route 96.
  - e) Expanded service to Villebois.
  - f) Added service to Coffee Creek and Frog Pond growth areas.
  - g) Begin service to Downtown Portland.
  - h) Begin service to Woodburn.
  - i) Conversion to battery-electric buses, especially for in-town use.

**Transit Master Plan Task Force meeting of 1-12-2015**

*attendance:*

Staff:

Stephan Lashbrook  
Jen Massa Smith  
Michelle Marston  
Eric Loomis

Consultant:

Patty Fink  
Christina Watchie

Task Force Members:

Councilor Julie Fitzgerald, Chair; Jean Tsokos; Pat Rehberg; Barb Leisy; Peter Rapley; and Paul Diller.

The meeting opened at 6:10 pm, with a welcome to Mayer Knapp in the room for a brief time. Stephan explained that Eric Loomis a Field Supervisor for SMART would be guest speaking to inform members about the Tech upgrades and changes that SMART is in the process of updating to.

Members introduce themselves.

**Minutes for meeting of November 3, 2015:**

No one wished to make any changes to the minutes that had been sent out in advance. Julie asked if any corrections were needed. Jean made motion to approve the minutes; they were seconded by Barb and so approved.

**Stephan goes through review of recent meetings:**

Recap of what group has accomplished. Review existing TMP plan. First draft of Wilsonville by neighborhoods, about 4 updated versions since.

Approved Mission statement and goals.

Engaged in service enhancement brainstorming, how to improve services.

**Tech Upgrades, many in place and more coming**

Eric highlights tablets on all buses are installed. The goal is to help with passenger counting and reporting. Counting how many passengers get on at each stop and where they get off at. This will help with routing, determining which routes are more popular and which stops on each route are more utilized.

The public will be able to see where their bus is or where to get on from their mobile device, tablets or desktops. Live updates / real time arrivals will be available to learn where delays may occur and plan accordingly. The company will customize our counters and be available in a month or so.

Patty reminds members they will have the opportunity to try in beta testing phase to test the software. Stephan mentions that data collection was noticed to be incorrect with old software and that the new system is more accurately tracking and counting SMARTs DAR trips. Peter asked if 3<sup>rd</sup> parties could develop aps from the open data. Data will be available; however we will have an associated app created by ETA vendor.

## Public involvement and timeline

### Jen explains an overview public process

Chris Watchie's team on the ground public engagement, interviews with various stake holders via Urban Interactive Studios will be the consultants who will provide backup with website, mobile kiosks, and surveys. We are also working with Jerret Walker and Associates who will help lead a transit planning interactive workshop to participate in on March 10. Seeking 30-50 participants to participate. We will be asking the Mayor and Council members to invite 2-3 people to represent a cross section of the entire community. Time will be 5:30-8:30 or 6-9 pm.

The other main component is to be ambassadors and suggest best locations for Kiosks.

### Chris Watchie explains:

They are a small firm that does public involvement strategies for land use for transportation and economic development. Goal is to assist communities to create a process with a high level of integrity and respect to create a sustainable outcome. Accessible to all, asking key questions- going to the public, rather than having public come to you. Our process makes it convenient for people give input.

TMP website *plansmartwilsonville.com*, is not live yet. It is an interactive website with a lot of opportunities to give feedback in multiple languages. This will be one of the primary outreach tools, getting people to sign up for updates.

Very focused 10 month long process with 4 key phases.

Community interviews, 2 rounds. Events, venues for Title VI and environmental justice for race, national origin or color, low income or minorities to not be excluded.

Focusing on businesses, events, neighborhoods, employers

Identifying issues and opportunities

Electronic updates will be sent out 4 times during this process. FEB project is started, currently have a list of 400 interested parties, hope to expand that to over 1k.

What is a Kiosk? I pad on a stand; we will get 2 of these which will move around Wilsonville. We may be asking available parties to stand next to Kiosks and explain what they are about to help bring in survey takers.

Let's look at meeting dates and check staff availability.

April 11, July 12, Sept. 13 of 2016.

Dates to bring back survey responses to share with taskforce. Sept 13, would be the draft plan to share with task force members.

What type of questions will we be asking? The questions will be phrased tastefully about tradeoffs. Such as frequency and where routes go. EX: Are you comfortable waiting for a bus or would you rather walk two blocks and know a bus will be there every 10 minutes? We will have the ability to move the kiosks around to ensure we get a quantity of responses, so if one area is not getting much traffic, we can quickly move it to another location. Speaking opportunities and event outreach will reach a large cross-section of folks.

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Julie: When is the last date we can provide possible locations? Chris reponds: This will be moving date as we move along.

Pat asks about familiarity with “Nextdoor App” communications for neighborhoods. A social networking site for neighbors, states Paul Diller. Sites like this to explain awareness. We will provide links to our Ambassadors to provide links in your neighborhood and associations, so that it comes from inside rather than a city staff posting on the site.

Large apartment properties have not been contacted but they are on the list to be notified. Working with City staff to contact these facilities. We may not take a kiosk to every facility but plan to have a handsome cross section.

Churches are a great way to reach Latino population as well as schools. Other task force groups will also be added to the list of interested parties as well as leadership academy folks.

Julie asks ...Chamber of Commerce as well, would they be interested in selecting some locations to get credit as being a significant partner.

Bike groups and Walk Smart have lists of contacts as well. Website contacts will be added to contact lists.

### **Ways to be involved recap**

Hours of time to help staff outreach events.

Buttons to wear to show involvement.

Be an ambassador, share info with neighbors and your circles of contacts.

Think Smart. Ride Smart. Plan Smart- volunteers to wear ask me buttons?

Incentives to provide tokens for those who pass on the information, rhumba chain, creative ways to get information moving.

### **Next meeting dates proposed.**

April 12, July 12, Sept 13

Meeting closed at 7:20 pm.

**Transit Master Plan Task Force meeting of July 12, 2016**

Staff: Consultant: Michelle Poyourow (JWA)

Stephan Lashbrook  
Jen Massa Smith  
Michelle Marston  
Eric Loomis  
Nicole Hendrix  
Brad Dillingham

Task Force Members:

Councilor Julie Fitzgerald, Chair; Stephanie Yager; Pat Rehberg; Barb Leisy; Lynnda Hale; Caroline Berry; Peter Rapley; and Paul Diller

The meeting opened at 6:05 pm, with introductions.

**Minutes for meeting of April 12, 2016:**

No one wished to make any changes to the minutes that had been sent out in advance. Julie said the minutes would be accepted without dissent.

Stephan asked that agenda item #6 be moved up so Eric Loomis could make his presentation next. Stephan explained that Eric was on duty as evening supervisor and Eric could receive a call at any time that would require him to leave.

**Technology Update**

Eric used the computer system in the room to demonstrate some of the features of SMART's new "SPOT" technology, including showing bus locations and arrival times at different stops. Eric also explained advantages of having more data on ridership as a result of the technology upgrades. He said that research is on-going about a possible change to a different cell phone carrier, in order to eliminate "dead spots" where the bus seems to disappear briefly while en route.

A number of questions and comments from Task Force members followed.

**Overview and Recap of Process to Date**

Stephan reviewed the planning process over the last year. This included the following:

1. Formation of the TMP Task Force;
2. Review of the existing TMP (drafted approximately ten years ago);
3. Review of "Existing Conditions Report;"
4. Adoption of draft "Mission Statement and Goals" for the new TMP;
5. First round of public outreach (Stephan noted the importance of the March 10 public workshop facilitated by Michelle Poyourow and the work of focus groups).

Stephan then introduced Jen to talk more about the public outreach effort.

**Overview of public outreach effort**

Jen showed a brief Power Point and talked about all of the efforts of the staff, consultants and volunteers over the last few months. Highlights included:



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- SMART collected broad based input from residents, employees, businesses, visitors, with special focus on Title VI and environmental justice populations.
- Over 800 survey responses
- E-updates to over 1,200 individuals, businesses, public and private organizations and services
- Over 8k informational tear offs distributed through community partnerships
- Display outreach at SMART Central Station and Shopping Centers
- Workshops / focus groups with businesses, Spanish-speaking/low income, older adults/ADA, and community members/city staff
- Individual interviews with businesses and civic organizations

### Second round of public outreach approach (July – September):

Based on the success of the initial round of outreach, SMART will use similar strategies to reach Wilsonville employees, businesses, visitors, and residents via:

- Bi-lingual online and paper survey (longer survey time to capture educational institutions and school-aged families)
- Kiosk stations placed at highly visible locations (City Hall and Public Library)
- Display outreach at community events and key locations (retail, SMART Central, Community Center)
- Presentations
- Bookmark distribution

### Service Alternatives

Michelle Poyourow walked through two alternative service scenarios intended to elicit a reaction from the Task Force (draft graphics attached). As she put it, the two alternatives illustrate a range of possibilities, with one showing an increased focus on local service, while the other emphasizes inter-city (or out-of-town) service. Michelle said that both alternatives were designed with the assumption that revenue for SMART operations would stay relatively flat, noting that there would be no way to increase both local and out-of-town service without additional funding (and that has not been part of the service planning to date).

One of the potential major changes shown for enhanced out-of-town service would delete the use of the Barbur Transit Center in favor of having the 2X route connect to TriMet at the Tigard Transit Center. Michelle noted two possible advantages of that change: first, the 2X would then connect with more different frequent service routes provided by

TriMet; and second, it would place more emphasis on WES as the best north-south transit system by having SMART essentially replicate that part of the WES service during mid-day times when WES does not run. Not surprisingly, that generated quite a bit of discussion.

Other topics included:

TriMet's refusal (so far, at least) to extend their route 96 from Commerce Circle down to SMART Central, the irregular schedule of route 96, leading to difficulties as SMART attempts to match up with it.

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The potential for a new looped version of SMART's routes 5 and 6, with improved connections with TriMet's 96.

The potential for SMART to provide limited peak-hour service to Downtown Portland, possibly including evening service on Fridays and Saturdays for people wanting to go to dinner or shows in Portland.

The prospect of working with Canby Area Transit (CAT) to create a route from Wilsonville to Oregon City via 99E (Michelle said it would not necessarily move people between O.C. and Wilsonville any quicker than having service on I-205, but it should be able to maintain a more consistent schedule.) It was agreed that coordination with CAT will clearly be needed.

The need for improved cross-town service in Wilsonville, including:

Better connections from the east side to Commerce Circle (to connect with TriMet's 96);

Quicker service along Wilsonville Road (that does not have all of the "route deviations" of the current route 4);

Having route 4 only go to SMART Central at times when WES is running; and

Improved Villebois connections to the rest of the community.

Peter talked about schedule and route changes being planned by TriMet, including the plan to extend TriMet's route 44 service from the PCC Sylvania campus down to the Tualatin Park & Ride. Stephan noted that if, and when, that happens it will open another north-south connection for SMART as we connect to TriMet in Tualatin.

Questions were raised about the frequency of service on TriMet's route 76. That could be increasingly important if SMART is to shift service away from the Barbur Transit Center in favor of the Tigard Transit Center.

### **Closing comments.**

Pat said that she appreciated the open and interactive way the Task Force operates. Several people agreed.

Stephan encouraged Task Force members to attend the City Council work session on July 18 to support Julie and the team who will be presenting much of the information discussed in tonight's Task Force meeting. Stephan added that the Council will be reviewing much of the same information that has now been considered by the Task Force.

### **Next Meeting:**

Stephan said that, since the public outreach consultants have not yet started work, it would make sense to work with the consultants to pick the next meeting date. He suggested that the Task Force meet again in mid-December if the consultants are ready for committee input.

**Note target date for next meeting: September 13, 2016.**

## DRAFT STAFF RECOMMENDATIONS

Revised October 25, 2016

## Routes &amp; Schedules

Short-term actions:

- Start the first morning Route 3 trip to Canby from SMART Central, rather than leaving directly from SMART's offices on Boberg Road. (Already being implemented.)
- Verify availability of planned turn-arounds at both east and west ends of Route 4 (Graham Oaks and Meridian Creek School). Note that some lobbying may be required (Metro – Climate Smart, Vision Zero and limited parking at Graham Oaks.) (Already being implemented.)
- 1. Determine, using actual bus trips, whether replacing the Barbur Transit Center stop with the Tigard Transit Center on the 2X is viable and efficient. If so, recommend making that major route change to begin in October, 2017. Research is now under way.
  - 1a. Evaluate the potential to include stops at the Tualatin Park & Ride with trips to/from Tigard on the 2X.
  - 1b. Provide trips to/from the Tigard Transit Center on 30-minute headways during the mid-day on the 2X.
- 2. Minimize the time taken to go cross-town on Route 4 by eliminating stops on Town Center Loop in favor of stops along Courthouse.
- 3. Evaluate the potential to reduce redundancy of Route 6 with 2X service in town (noting alternative routing of 2X and 5 now under consideration). This option will only work if the northern 2X terminus is changed from the Barbur Transit Center to the Tigard Transit Center.
- 4. Change the direction of travel on Commerce Circle for the SMART bus on Route 5 (will require a new stop with a shelter and pedestrian improvements). Eliminate the Pioneer Court turn-around.
- 5. Negotiate to equalize trips with Cherriots on the 1X (SMART currently provides 8 of 13 weekday trips). Given the demand for service between Wilsonville and Salem, it can be hoped that Cherriots will increase service in order to balance with SMART. Simply reducing the number of trips provided by SMART could be expected to result in an increase in the number of standing riders on the 1X, due to capacity constraints – a safety concern and a failure to meet service standards.
- 6. Work with Cherriots to explore the option of reducing the number of stops in downtown Salem on the 1X, in order to create time in the schedule for regular Woodburn stops.
- 7. Remove Charbonneau from fixed route service on Route 3 and replace with a pilot project shuttle service like the one currently being used in Villebois. Re-evaluate the effectiveness of the shuttle service on a monthly basis after an initial six-month period, with the potential to: extend the pilot project; renew the service long term; or curtail it.
- 8. Coordinate with Canby Area Transit (CAT) to determine the potential for increased service between Canby and Wilsonville on Route 3, as well as connecting to Oregon City. This could also involve having SMART provide some of the service between Canby and Oregon City that is currently provided by CAT, possibly in exchange for having CAT provide some of the service between Canby and Wilsonville that is currently provided by SMART.
- 9. Shift Route 7 service away from Brown Road to focus on Barber Street.

10. Continue to provide Route 6 service through northeast employment center (including Xerox campus), rather than shifting service to adjoining public streets.
11. Expand the size of the Route 7 loop through Villebois (Costa Circle) or limit service to Barber Street, with more frequent service. Note that the latter idea would support a policy shift towards emphasizing ridership over coverage.
12. Evaluate the staff plan to combine Route 7 (Villebois) and Route 4 into two cross-town routes. Structure the routes and schedules to assure there is no increase in cost beyond current service. (Note alternatives under consideration for Route 4.)
13. Evaluate the potential for replacing Route 5 with the 2X, running 30-minute headways. Note that a reduction in east-side service may occur.
14. Evaluate additional Route 5 service loop along Day Road.
15. Explore the option of a fare increase on the 1X (increase could potentially go from \$3.00 to \$5.00). Cherriots would have to agree in order to avoid multiple logistical problems between the two agencies.
16. Work with TriMet to improve the timing of connections between Route 5 and TriMet Route 96.
17. Work with Microsoft and other businesses in the northwest area along Route 5 to enhance service to meet their needs.
18. Work with developers of the former Xerox campus and other businesses in the northeast area to enhance Route 6 service to meet their needs.

**If additional funding becomes available:**

1. Provide more frequent Route 7 service through Villebois.
2. Expand Route 7 service area in Villebois.
3. Increase evening (until 10 p.m.) and Saturday service on Route 4 from one-hour to 30-minute headways, and adding Saturday evening service.
4. Increase service to Canby on Route 3 with the goal of improving local access to the main campus of Clackamas Community College and to social service agencies in Oregon City.
5. If re-routing 2X through the Tigard Transit Center is found to save money, use that revenue to provide evening service to Tualatin Park & Ride and Tigard after WES service ends for the evening (11 p.m.).
6. Add service and expand geographic service area (Frog Pond and Advance Road areas) to Route 4.
7. Add service and expand geographic service area on Route 5 (Day Road, Coffee Creek and Basalt Creek).
8. Add mid-day trips to the 1X schedule (via SMART or Cherriots).
9. Expand Route 6 service area to cross freeway and include Commerce Circle in order to improve connections with TriMet Route 96.
10. Provide Route 6 service on 30-minute headways (or better, all day).
11. Add service to Tigard on Saturdays.

**If cuts become necessary:**

- Retain 1X service at current levels if at all possible. Last resort cuts.
- Retain Route 7 service at current levels if at all possible. Last resort cuts.
- 1. Decrease out-of-town Dial-a-Ride service area significantly. Replace with medical shuttle if that is found to be a less expensive alternative that meets riders' needs.
- 2. Eliminate the Charbonneau stop on Route 3 before curtailing other Route 3 service.
- 3. Shut down Route 3 and cease all service to Canby.
- 4. Implement the staff concept of replacing Route 5 in town with 2X service.
- 5. Combine Route 6 with either Route 2X or 5 to reduce costs.
- 6. Provide out-of-town service on the 2X only during the mid-day, when WES is not running.
- 7. Reduce weekday frequencies on Route 4 from current 30-minute headways.
- 8. Reduce frequency of Route 6 service.
- 9. Eliminate the 2X and replace with focus on WES and Routes 6, 4 and 7.

**Future considerations:**

- Plan to provide expanded Dial-A-Ride service, including outside the City limits, as fixed routes are expanded in Frog Pond, Coffee Creek and Villebois areas.
- Plan to provide service along new roads to be constructed west of Boones Ferry and south of Wilsonville Road.
- Provide service on Canyon Creek when either the Xerox campus undergoes redevelopment or new development occurs on the vacant property owned by Mentor Graphics, south of Boeckman Road.
- Provide battery-electric buses for in-town routes.
- Consider PERS budgetary impacts on the City, and thereby, on the service SMART is able to provide.
- Reductions in local employment levels (thereby reducing payroll tax revenues) could necessitate reductions in service.
- If voters approve a new community/aquatics center, SMART will need to provide service to it.
- Provide more amenities, at stops and on buses, to encourage ridership. Noting that SMART's buses are frequently stuck in the same traffic congestion as private vehicles, it is important that riding the bus has some added value for passengers.
- Note the number of Wilsonville workers who have been displaced from rental housing in Wilsonville or Tualatin, now living in Molalla, Woodburn, Newberg and other outlying areas that do not have direct transit access to or from Wilsonville. Providing service to them will be increasingly important, although expensive, in the future.

**Transit Master Plan Task Force Meeting of November 1, 2016****SMART Staff:**

Stephan Lashbrook, Director  
Eric Loomis  
Nicole Hendrix  
Michelle Marston

**Consultants:** Michelle Poyourow, JWA  
Chris Watchie, Cogito

**Task Force Members:**

Councilor Julie Fitzgerald, Chair  
Stephanie Yager  
Pat Rehberg  
Lynnda Hale  
Caroline Berry  
Peter Rapley

**Citizen:**

Robyn Blake

Julie opened the meeting at 6:05 pm, with everyone present introducing themselves.

**Minutes for meeting of July 12, 2016:**

Julie asked if anyone wanted to make any additions or corrections to the minutes. There being none, Julie said the minutes would be accepted as submitted.

**Processing for completing work on the Transit Master Plan:**

Stephan and Chris provided a brief overview of the remaining steps to adopt a new TMP. Stephan pointed out that, once a draft plan has been assembled, there will be work sessions and then public hearings before both the Planning Commission and City Council. Stephan emphasized that the City Council will want to receive a recommendation from the Task Force before the Council takes action.

Julie pointed out that the process will continue after she leaves the Council. She said that she intends to continue as a citizen-member of the Task Force and she will ask for a new City Council liaison to take her official role.

**Overview of public outreach effort**

Chris showed a brief Power Point and talked about all of the outreach efforts of the staff, consultants and volunteers, with the second round just completed in October. Chris emphasized the far-reaching efforts that have been made to be as inclusive as possible in generating public involvement. She talked about how that was done and she praised Michelle M and Nicole for their efforts. Chris reminded the Task Force that the outreach efforts dealt with:

- 1-How to balance ridership and coverage goals;
- 2-How to balance rush hour service against service at off-peak times;

3-Finding the right balance between local and inter-city service; and

4- How to make it all work within a fixed budget.

Highlights of the second round of outreach included:

- Broad based input from residents, employees, businesses;
- Over 1,100 total survey responses;
- E-updates sent to over 1,200 individuals, businesses, public and private organizations and services;
- Most second-round respondents said that they did not participate in the first round;
- Consistent preference for focus on inter-city (rather than local) travel.

Chris spoke of the four recurring themes from public comments (most often heard):

1-Improve connections with TriMet, with more focus on Beaverton and Hillsboro. This would involve using the Tigard Transit Center rather than Barbur, and running the 2x more frequently. The importance of this is that it would take advantage of the more frequent transit service already being provided by TriMet. It also involves connecting at more of a transit node, rather than along the line;

2- Work with Canby to create all-day connections to Oregon City; which serves as the county seat, Clackamas Community College, headquarters of a lot of social services and a place where people receive health care. It is very hard for people to get from Wilsonville to the Red Soils District of Oregon City via transit without spending the whole day;

3- Mid-day service to/from Salem needed via Route 1X; and

4-Longer in-town service is needed weekdays and Saturdays.

### **Service Alternatives**

Michelle Poyourow reminded the Task Force of the two alternative service scenarios that have been used to elicit a reaction from the public. Michelle said that both alternatives were designed with the assumption that revenue for SMART operations would stay relatively flat, noting that there would be no way to increase both local and out-of-town service without additional funding (and that has not been part of the service planning to date).

### **Route 2X**

It was pointed out that the most controversial change that has been discussed would delete the use of the Barbur Transit Center in favor of having the 2X route connect to TriMet at the Tigard Transit Center. Stephan said that, although many comments support the change, there are a number of regular 2X riders who are strongly opposed to it. Michelle P showed a graphic indicating that the 2X has relatively high costs without high ridership, when compared to most of SMART's routes. While the 2X does help people get to and from Portland, it does not help the significant number of commuters from Washington County. Michelle P again showed the graphics indicating where Wilsonville workers live and where Wilsonville residents work – both of which show an obvious connection between Wilsonville and land to the northwest of the City. Eric said that he and one of the supervisors had driven two different buses between Wilsonville and those two transit centers to the north (Barbur and Tigard). That trial supported the consultants' conclusions about timing and connections with TriMet. Stephan said he intends to have the staff do more research on this.

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Pat said that she has had good experiences riding WES and that the fact that TriMet's Route 12 runs frequently through the Barbur Transit Center to the Tigard Transit Center also would support making the change that is being considered. She said that people who currently get off the 12 to board SMART's 2X could do it at Tigard, rather than Barbur.

Michelle P reminded the group that a substantial infrastructure investment has already been made in the WES system and that it makes sense to take advantage of that investment as much as possible. She pointed out that one of the ideas being explored is putting more emphasis on WES, with the 2X duplicating the WES route during off-peak times.

Robyn Blake (citizen who sat with the Task Force at the dais) said that her husband, who works in downtown Portland, does not like relying on TriMet's Route 12 because it stops so frequently along its run. She pointed out that that problem will be even worse for people riding all the way from Tigard to Downtown Portland on Route 12, when compared to the current situation where commuters to/from Wilsonville only have to deal with the frequent stops between Barbur and Downtown.

Peter talked about his experience as a regular transit commuter from downtown Portland to Xerox. He remains convinced that maintaining the current system will be more efficient with less travel time than the proposed change. In response to a question from Stephan, Peter indicated that many of his concerns about the proposed change could be addressed if SMART's Route 6 made better connections with TriMet's 96 at Commerce Circle.

Caroline said that, from everything she has heard, using the Tigard Transit Center will still provide the best service to the largest number of travelers.

Pat asked about Tualatin's shuttle bus service and how it connects. Stephan said that that service being provided by Ride Connection under a contract with TriMet. He said that, unfortunately, Tualatin's shuttle does not make the connection between the park & ride where SMART connects and the WES station in Tualatin.

Michelle P pointed out that frequency of service and ridership really go together – and that switching to the Tigard Transit Center will require fairly frequent service in order to be effective.

Stephanie said that she is now a regular WES rider, working in Washington County. She said that she is convinced that more people would use it if WES had longer service hours.

### Route 3

Michelle P then spent some time talking about Service to Canby and the potential to improve connections to Oregon City. She noted that Canby currently makes 20 trips/day to/from Oregon City, while SMART only serves Canby 8 times/day. Chris emphasized that the focus groups placed a lot of importance on improving access to Oregon City from Wilsonville.

Stephan and Eric talked about the possible elimination of the Charbonneau stops from Route 3, in favor of a shuttle service, modeled on the shuttle that has been successful in Villebois. That could provide more flexibility in scheduling for SMART's buses to better connect with CAT buses in Canby and still connect with WES trains in Wilsonville. Stephan said that a Charbonneau Shuttle could be a pilot project to see how it might work out. It was pointed out that SMART will continue to provide Dial-a-Ride service to Charbonneau regardless of any changes made to Route 3.

### Route 1X



## EXHIBIT K

Michelle P talked about the importance of providing mid-day service to out-of-town locations (such as Salem) because many people will not ride transit unless they feel like they have a way to return in the middle of the day to deal with emergency situations. The Task Force talked about the importance of the 1X for many commuters and about the fact that Cherriots is not providing as much service per day as SMART. Stephan said that ODOT is just beginning the conversation about ways to pay for inter-city service all over Oregon.

### Route 4

Stephan acknowledged that increasing hours for Route 4 is one of the highest priorities for respondents to our surveys but said that he is concerned about how those increases would be paid for. It was noted that the current Saturday schedule (8 hours, total) does not serve local people who would like to use it to get to and from work, unless they are working part time.

Michelle P suggested that the changes planned for Route 4, and potentially the changes being considered for the 2X, could free up ample funding for increased service on Route 4.

### Overall Route & Schedule Discussion

Caroline said that she feels that there is an obvious need for more connections between Wilsonville and Washington County and she thinks that the switch to the Tigard Transit Center is one way to help promote that.

Pat said that she feels that inter-city connections will be increasingly important for SMART in the future.

Stephanie said that the use of the Tigard Transit Center would be a big help to her and other people she knows commuting from Wilsonville to the Beaverton area.

Peter urged a cautious approach, with no drastic changes.

Julie said she supports the direction that the majority is talking about, but she anticipates a long time before SMART experiences major increases in ridership.

Michelle P pointed out that, in general, it is likely that SMART will move to less of a "pulse" focus on WES, where we have buses waiting for each arriving train and schedules intended to bring riders to the train shortly before it departs.

Paul Diller's comments arrived by email and were read by Michelle P:

*"Although I am not personally affected in a major way, I still worry about cutting off SMART's only direct connection to Portland. The lack of a direct connection may hurt Wilsonville's desirability as a place to live for persons who work in downtown(ish) Portland."*

*"Bridgeport Village and Club Sport, among other attractions, a connection to this stop is key."*

*"I was hoping for more streamlining of this route, like eliminating the stop in Old Town or the diversion to the transit center (I proposed a "dinky" before -- a quick shuttle from the transit center to the 4 line on Wilsonville Road)."*

*"Please don't cut out the stop at the capitol/Willamette! Also, I hope that if there is Woodburn service, only SOME 1X's would stop there. A Woodburn stop would add 10 minutes in each direction, making it much less of an "X"press."*

## EXHIBIT K

*"\$5! This is getting really expensive! When I first moved here (2005), I think it was free. Is there a way to charge more to the non-Wilsonville residents? Why should the Portlanders get a "free" (or cheaper) ride on a service funded by Wilsonville businesses?"*

*"Hear, hear! Can we please get just 1 later southbound 1X morning bus, like, say, at 9 AM? Last southbound 1X currently leaves at 8:05 AM."*

Stephan handed out comments received from Judy Dancer (a regular 2X rider) and Paul LaBarre (seeking service to Woodburn) as well as all 34 pages of verbatim public comments in response to the last survey.

Staff is now developing a draft service plan to present to the Task Force for review as soon as possible. We will also notify all 1,200 people on our contact list, encouraging them to be involved before the process is finished.

Julie will complete City Council term in December and get the next councilor to next meeting. Julie will continue as a member, but not run the meetings after December.

### **Next Meeting:**

Stephan asked that the Task Force schedule a work session to really dig into the different route and schedule changes that are now under consideration. Julie said that she would like to do that as soon as possible. **After some discussion the date for the next meeting was set: 6 p.m. November 14, 2016, at SMART offices.**

## Transit Master Plan Task Force Work Session Notes

November 14, 2016

6:00pm-8:00pm

Attending: Chair Julie Fitzgerald; Paul Diller; Peter Rapley; Stephanie Yager; Lynnda Hale; Pat Rehberg and Caroline Berry

Staff: Michelle Marston; Nicole Hendrix; Stephan Lashbrook and Eric Loomis

Peter began the discussion by saying that, although the minutes accurately reflected what was said at the November 1 meeting, the summary of public comments should have included the fact that a number of people strongly objected to losing the current 2X service to the Barbur Transit Center and service to the Tualatin Park & Ride. Stephan agreed that those comments were part of the record from the public outreach.

### **Overview**

Julie said that the intent of this work session was for the task force to review and discuss the major staff recommendations and public comments for the Transit Master Plan. The main topics that became the focus of the meeting were:

- Tigard Transit Center versus Barbur Transit Center as the most northerly stop on the 2X route;
- How to maintain service to the Tualatin Park and Ride (near Bridgeport Village); and
- Potential Dial-A-Ride (DAR) savings and the prospect of using those funds for other service.

Other topics that were discussed included:

- A brief history of SMART;
- How SMART services are funded;
- SMART's current and potential connections with TriMet;
- Mounting pressure for SMART and Cherriots to provide service to Woodburn;
- Proposed changes to SMART's crosstown Route 4 configuration; and
- Public comments about the need for more Villebois service.

### **Tigard versus Barbur Transit Center**

The main issue with the service shift to Tigard from the Barbur Transit Center is that current riders who commute to Wilsonville from the neighborhoods near Barbur would have to pay an extra fare and make more transfers. It was also noted that with the proposed new service the 2x would only run when the WES is not running (which would be midday/evenings).

*Possible solutions discussed:*

- The possibility of a fare discount or no fare for those traveling south to Wilsonville from Barbur Transit Center if SMART moves to Tigard TC (possibly as a pilot project to see what effects it has on ridership).
  - Free fare to Wilsonville (one way or both ways)
  - Discounted fare
- Speak with current 2x riders to ensure they know that TriMet's Route 12 buses that go to Barbur also go to Tigard transit center.
- Incentives for van/carpool by employers who pay payroll tax

### **Tualatin Park and Ride**

The Task Force agreed that there were a large number of public comments that clearly did not want SMART to stop service to the Tualatin P&R. People use this stop for health services, transfers to TriMet, Bridgeport retail services. The overall consensus was that Tualatin P&R service should stay and, if possible, provide service there throughout the day, not just when WES isn't running. The current lack of transit connections between the Tualatin WES station and the Tualatin Park & Ride also posed the idea of a potential shuttle to connect the two. Also receiving considerable discussion was the need for improved connections from SMART services to TriMet's Route 96.

### **Dial-A-Ride (DAR)**

A discussion came up about the resources DAR currently uses. Eric Loomis mentioned that, on average, out-of-town DAR costs SMART \$43 per ride. Eric said that staff is still verifying the exact amount, but it appears that DAR is roughly 30% grant funded and the remaining 70% comes from local payroll taxes (with a very small portion from fares). In any case, it was mentioned that changes made to this service would need to be phased in over long periods of time to make sure that riders who depend on SMART DAR services have ample time to make other travel arrangements or switch to medical providers within newly defined service boundaries. Lynnda reminded the group that some medical appointments can take months to be scheduled.

#### *Possible solutions discussed:*

- Create a task force specifically for a DAR service conversation/solutions
- Reduce SMART's current service area
- Provide specifically designated trip shuttles for dialysis, chemo, medical
- An established schedule that riders can work around and make rides more efficient
- More transfers to TriMet shuttles
- An improved connection point between SMART's DAR buses and TriMet's lift system. (It was generally agreed that the parking lot at the Coffee Creek Prison is not a desirable location.)

## **City Council Work Session Talking Points**

- Funding hardships with fixed budget, (increasing PERS costs, growing population and expanding service area, and declining state and federal support for transit)
- Although new service might become less convenient for some folks, the new service can then be available to a greater number of people (Beaverton, Tigard, Barbur not just Barbur)
- Maximize use of TriMet existing resources
- Researching shared-use mobility options (ridesourcing, ridesharing, shuttles, etc.)
- The role transit can serve when talking about the rise in congestion
- Intercity connections- Canby, Oregon City, Salem

## **Upcoming Dates**

November 21, 2016 5:00pm City Council work session, City Hall

December 6, 2016 6:00pm Task Force Meeting, City Hall

December 14, 2016 6:00pm Planning Commission work session, City Hall

**Transit Master Plan Task Force Meeting Notes**

**January 17, 2017**

*Task Force Members:* Chair Kristin Akervall, Pat Rehberg, Paul Diller, Stephanie Yager, Peter Rapley (on phone)

*Staff:* Stephan Lashbrook, Eric Loomis, Nicole Hendrix, Michelle Marston, Brad Dillingham

Meeting began at 6:07 pm.

**December 6, 2016 task force minutes were reviewed and approved.**

Stephan begins by stating that this TMP is different than the last due to the funding challenges ahead, that SMART must prioritize changes with the money available.

The task force then goes on to analyze each proposed route as seen below:

1X – Paul pointed out that there are two morning WES trains that do not have a SMART bus to pick them up. He would like to see one more morning bus go southbound.

2X – Peter is not a fan of the move from Barbur to Tigard Transit Center because it creates a longer trip for current riders trying to get to Portland.

Stephanie likes that it connects to commerce in town

Paul points out that this would create mid-day isolation of Wilsonville.

Peter questions the 30 minute service to hour service

Eric explains that cost is the reason for the service chosen and that parking lots are unsafe and timing is worse

3 – Cost-wise this was a choice between WES connections or Oregon City.

Stephan states that additional outreach in Charbonneau should happen in the summer to determine if a Charbonneau shuttle would be successful and what people would want.

4 – The turnaround at Graham Oaks Nature Park is ideal but the gates to the park close at dawn and dusk. The task force would like to have conversations with Metro at City Council to see if this can change. If it cannot, the turnaround would occur at Fox Chase because the other turn around (the school) is extremely unsafe for drivers and pedestrians.

Pat mentioned that the newer buses would be very quiet on the route 4. Stephan follows up that electric buses would be ideal for route 4.

5 – Not having a loop will provide more frequency. The commerce circle will go clockwise instead of counterclockwise so it is quicker for the drivers. Peter points out that if that change were to happen, there will need to be sidewalks added.

6 – Parkway Woods not Xerox. Route duplicating? No.

7 – No feedback

What the task force would like to see more of:

The decision making process for proposed routes: is it based on funding? Outreach? Ridership?

Taxes to generate revenue

Cost changes from old to new routes

Retail at transit center (coffee shop)

More on how high ridership reduces congestion in Wilsonville and on I-5

Advertisement on back of buses

Understand/advocate for transit: a call to action

This Task Force meeting will be the last. Further communication will be through e-mail. The task force was invited and encouraged to attend the March 6 public hearing.

Please see <https://www.plansmartwilsonville.com/> for the Draft TMP and opportunity for public comment.



# PLANNING COMMISSION

## WEDNESDAY, MAY 10, 2017

### III. INFORMATIONAL

- A. City Council Action Minutes: (04.03.2017 cancelled) and 04.17.2017  
(Staff) (15 minutes)



## City Council Meeting Action Minutes April 17, 2017

| COUNCILORS         | STAFF            | STAFF              |
|--------------------|------------------|--------------------|
| Mayor Knapp        | Bryan Cosgrove   | Mark Ottenad       |
| Councilor Starr    | Barbara Jacobson | Jon Gail           |
| Councilor Akervall | Jeanna Troha     | Mike McCarty       |
| Councilor Stevens  | Sandra King      | Miranda Bateschell |
| Councilor Lehan    | Susan Cole       | Jordan Vance       |
|                    | Nancy Kraushaar  | Amanda Guile       |
|                    | Delora Kerber    | Adam Phillips      |
|                    | Dwight Brashear  | Mike McCarty       |

| AGENDA ITEM   | ACTIONS   |
|---|---|
| <b>WORK SESSION</b>   |   |
| <ul style="list-style-type: none"> <li>• Resolution No. 2626 Inclusion</li> <li>• Parks and Recreation Master Plan</li> <li>• Wilsonville Road Truck Traffic Count Results</li> <li>• Frog Pond Infrastructure Funding</li> <li>• Basalt Creek Concept Plan Update</li> </ul>   | <ul style="list-style-type: none"> <li>• Council reviewed a red-line version of the resolution, and made further changes. The Resolution will be on the May 1<sup>st</sup> Council Agenda for adoption.</li> <li>• The consultants for the Parks and Recreation Comprehensive Master Plan were introduced.</li> <li>• Staff presented the results of the latest traffic count and speed data collections. Council felt the “No Thru Truck” signs should remain in place until next February.</li> <li>• Information about the funding challenges was presented. The scheduled public hearing will be continued to June 5<sup>th</sup>.</li> <li>• A development feasibility analysis for the central subarea was contracted to determine what employment uses were achievable. The findings will be brought to Council May 1<sup>st</sup>.</li> </ul> |
| <b>REGULAR MEETING</b>  |   |
| <u>Mayor’s Business</u> <ul style="list-style-type: none"> <li>• Wilsonville High School Basket Ball Team</li> <li>• Appointment of Emily Bryant Utz, Group/Tourism Specialist for World of Speed Museum, for the Tourism Promotion Committee for Position No. 1 with a term ending 6/30/2019. She will be eligible to re-apply to serve another full three-year</li> </ul> | <ul style="list-style-type: none"> <li>• The Mayor read a proclamation recognizing members of the Wilsonville H.S. Basket Ball team for their outstanding performance this season.</li> <li>• Ms. Utz was appointed to the Tourism Promotion Committee Position # 1 by a vote of 5-0.</li> </ul>  |

|  |   |
|--|---|
| <p>term.</p> <ul style="list-style-type: none"> <li>• Construction Safety Week Proclamation – Bill Kalapsa Safe Building Alliance</li> <li>• Arbor Day Proclamation (staff – Scola)</li> </ul>   | <ul style="list-style-type: none"> <li>• The Mayor read both proclamations into the record.</li> </ul>  |
| <p><u>Consent Agenda</u></p> <ul style="list-style-type: none"> <li>• Resolution No. 2627 Authorizing A Change Order With Northstar Electrical Contractors Inc. For Additional Street Lighting Improvements.</li> <li>• Minutes of the March 6, 2017 and March 20, 2017 Council Meetings.</li> </ul> | <ul style="list-style-type: none"> <li>• Resolution No. 2627 was removed from the agenda.</li> <li>• Adopted 5-0.</li> </ul>                            |
| <p><u>Public Hearing</u></p> <ul style="list-style-type: none"> <li>• Transit Master Plan Public Hearing</li> <li>• Frog Pond Master Plan Public Hearing</li> </ul>  | <ul style="list-style-type: none"> <li>• Continued to June 5, 2017 by a vote of 5-0.</li> <li>• Continued to June 19, 2017 by a vote of 5-0.</li> </ul> |
| <p><u>City Manager’s Business</u></p>  | <p>No report.</p>   |
| <p><u>Legal Business</u></p>   | <p>No report.</p>   |
| <p>Adjourn</p>   | <p>8:55 P.M.</p>  |



**PLANNING COMMISSION**  
**WEDNESDAY, MAY 10, 2017**

**III. INFORMATIONAL**

B. Basalt Creek Concept Plan (Bateshell) (10 minutes)



## PLANNING COMMISSION MEETING STAFF REPORT

|   |  |   |
|---|--|---|
| <b>Meeting Date:</b> May 10, 2017   | <b>Subject:</b> Basalt Creek Concept Plan<br><br><b>Staff Member:</b> Miranda Bateschell<br><br><b>Department:</b> Community Development Department                                |   |
| <b>Action Required</b>  | <b>Advisory Board/Commission Recommendation</b>  |   |
| <input type="checkbox"/> Motion<br><input type="checkbox"/> Public Hearing Date:<br><input type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date:<br><input type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date:<br><input type="checkbox"/> Resolution<br><input type="checkbox"/> Information or Direction<br><input checked="" type="checkbox"/> Information Only<br><input type="checkbox"/> Council Direction<br><input type="checkbox"/> Consent Agenda | <input type="checkbox"/> Approval<br><input type="checkbox"/> Denial<br><input type="checkbox"/> None Forwarded<br><input type="checkbox"/> Not Applicable<br><br><b>Comments:</b> |   |
| <b>Staff Recommendation:</b> N/A  |  |   |
| <b>Recommended Language for Motion:</b> N/A   |  |   |
| <b>Project / Issue Relates To:</b> <i>[Identify which goal(s), master plans(s) your issue relates to.]</i>  |  |   |
| <input checked="" type="checkbox"/> Council Goals/Priorities<br>Basalt Creek Concept Plan   | <input type="checkbox"/> Adopted Master Plan(s)  | <input type="checkbox"/> Not Applicable |

### ISSUE BEFORE COMMISSION:

The staff report and attached feasibility study provide an update on the Basalt Creek Concept Plan.

### EXECUTIVE SUMMARY:

Since 2011, the Cities of Wilsonville and Tualatin, Washington County, and Metro have been working together to implement an Intergovernmental Agreement (IGA) to concept plan the

Basalt Creek Planning Area. After five joint Council work sessions and two Public Open Houses, a preferred Basalt Creek Land Use Concept Plan was completed in September 2016, presenting a proposed jurisdictional boundary in conjunction with ten considerations for success.

On February 13, 2017, the Tualatin City Council, at a council work session, provided Tualatin city staff with direction to modify the previously agreed upon preferred Basalt Creek Land Use Concept Plan to show the Basalt Creek central subarea as residential. On March 20, 2017, Wilsonville City Council expressed concern over the residential designation, remained committed to providing employment opportunities in the Basalt Creek planning area, and directed city staff to communicate the Council discussion with the IGA partners.

Since the March 20, 2017 City Council meeting, city staff has conducted two site visits to properties in the “central subarea.” The City has commissioned a development feasibility analysis for the central subarea to determine what types of employment uses, if any, would be achievable. Staff has coordinated with Metro on this study and shared the scope with all partners to the IGA. Staff presented a draft of the employment development feasibility study to Council on May 1 (Attachment A). At work session, the City Council discussed the findings of the report and concluded: confidence in the ability for employment uses to develop in this area and a continued commitment to provide employment opportunities in the planning area including this subarea. Council directed staff to work with the IGA partners to establish a jurisdictional boundary north of Tonquin Road and north of this central subarea and to designate employment and industrial land uses in those areas on the Basalt Creek Concept Plan Map.

**EXPECTED RESULTS:**

Planning Commission informed of Council discussion and next steps for the Basalt Creek Concept Plan.

**TIMELINE:**

Staff will communicate the results of the development feasibility analysis with IGA partners and discuss next steps in concluding the Basalt Creek Concept Plan.

**CURRENT YEAR BUDGET IMPACTS:**

The City of Tualatin received approximately \$350K from Metro’s Construction Excise Tax (CET) grant program to perform concept planning. The current scope of work and budget with the consultant and as outlined with Metro under the CET grant program does not include additional funds for analysis of additional land use alternatives. The City of Wilsonville has, and will continue to, invest staff time into the process.

**FINANCIAL REVIEW / COMMENTS:**

n/a

**LEGAL REVIEW / COMMENT:**

n/a

**COMMUNITY INVOLVEMENT PROCESS:**

The project includes participation from affected residents, businesses, and property owners. Two

open houses were held to engage and inform the public about the project. Additionally, the website is updated to reflect the most recent work and staff sends out monthly updates to an interested parties list and property owners via email and U.S. postal mail.

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY:**

The Basalt Creek area is important for the long-term growth of Tualatin, Wilsonville, and the Metro region. Conducting a thorough and thoughtful planning process will identify and resolve each city's vision for the area and potential impacts on the community. The Basalt Creek area presents an opportunity to maximize assessed property value, integrate jobs and housing, develop efficient transportation and utility systems, create an attractive residential and business community, incorporate natural resource areas, and provide recreational opportunities as community amenities and assets.

**ALTERNATIVES:**

n/a

**ATTACHMENTS:**

Attachment A: Basalt Creek Development Feasibility Study



CITY OF WILSONVILLE

# Basalt Creek Concept Plan - Feasibility Study

MAY 1, 2017

# Intent

The Basalt Creek Concept Plan Existing Conditions report was prepared in 2014 to look at future development of the 847 acres between the City of Wilsonville and City of Tualatin as part of the Urban Growth Boundary (UGB) inclusion process. In this report, potential land uses were identified for the region based on economic factors, land use compatibility, and projected growth (see Figure 1). The intent of this feasibility study is to take a further look at approximately 60 acres within the Basalt Creek Concept area to **evaluate the potential to develop these properties to support increased employment opportunities in the region.**

To meet this goal, three schemes have been developed in this feasibility study. The schemes have varying degrees of development to preserve open space and natural habitats on the site. The schemes are:

- Scheme A** – maintains all of the open area in the northern portion of the site and development is primarily on the southern portion of the site (below the PGE easement)
- Scheme B** – maintains some open space in the northwest corner of the site but develops more of the site than Scheme A
- Scheme C** – fully develops the site to the maximum extent feasible and has the least amount of open space

Various building prototypes are used in the schemes from general office buildings to industrial/warehouse facilities. These prototypes are based on local buildings and provide guidance on building footprint sizes and parking counts. Architectural renderings have been developed for 3 building prototypes to provide a graphical representation on how these buildings may fit the overall look and feel of the study area.

# Site Overview

KPFF is studying in detail a portion of the Basalt Creek area that is approximately 60 acres in size and located east of Grahams Ferry Road, south of Helenius Road, west of Basalt Creek, and near the intersection of Grahams Ferry Road and Tonquin Road. The current use of the site is agriculture with single family homes and forested area near the creek. To the south is the Chick-A-Dee nursery and to the north of the site there is an open parcel of land and then residential housing. The site is comprised of 9 tax lots varying in size from 1.46 acres to 11.68 acres.

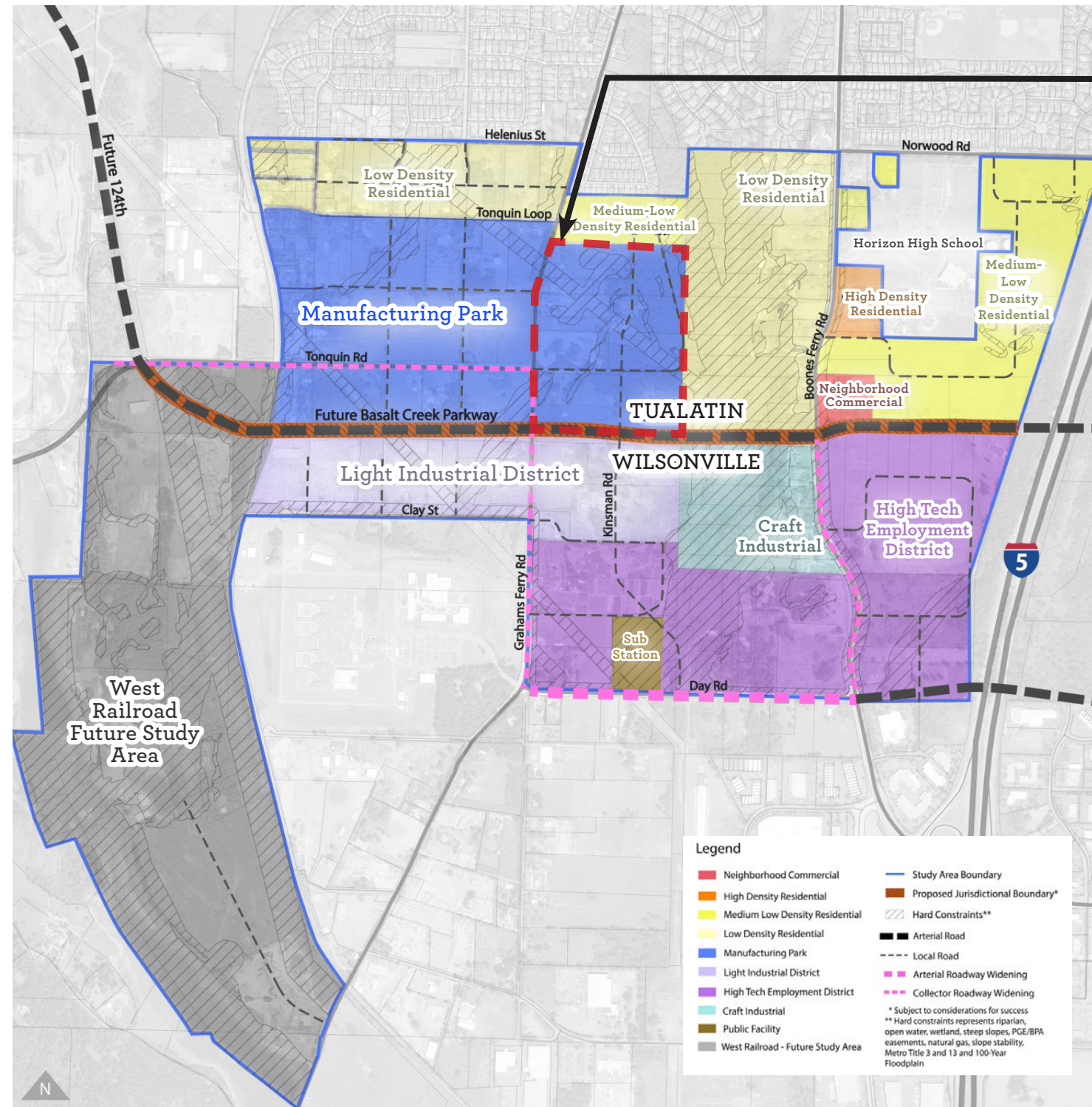


Figure 1 - Preferred Basalt Creek Land Use Concept Map (October 2016)

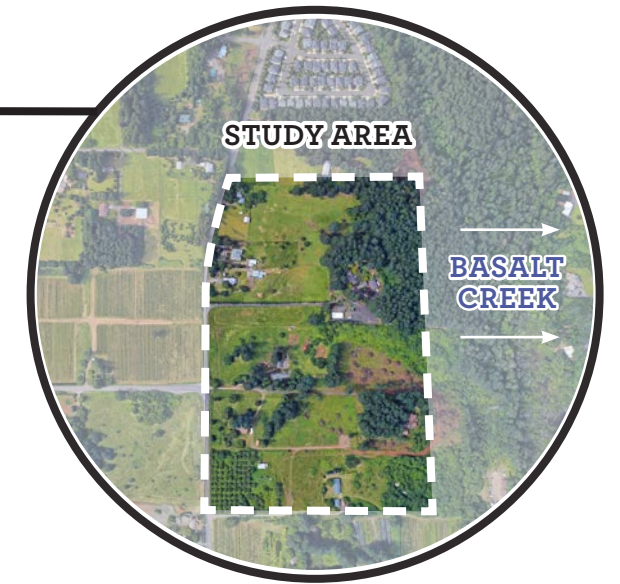
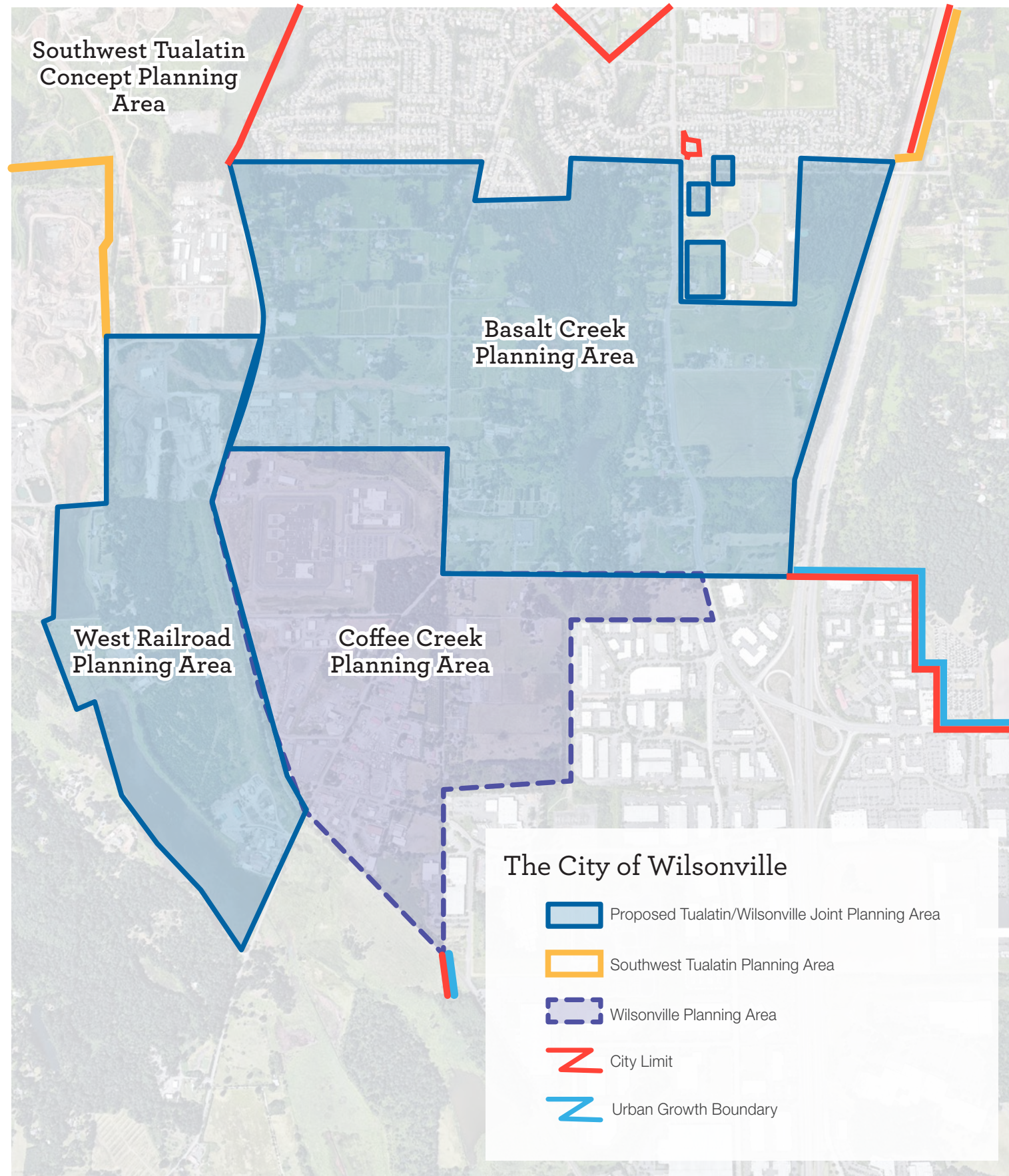


Figure 2 - Study Area





Aerial of the Planning Area



Site Photos

# Design Standards

## ZONING

The site is currently zoned FD-20 per Washington County. FD-20 covers land added to the Urban Growth Boundary after 1998. This feasibility study is based on the assumption that the building prototypes indicated in the schemes are allowed uses.

## BUILDING SETBACK

Building setbacks used to locate the structures on-site are based on a comparison of Washington County, City of Wilsonville and City of Tualatin zoning standards. The building setbacks are 30 feet from lot lines or right-of-way lines. This setback is for the building location only and parking is indicated in the setback.

## SITE ACCESS

On-site parking is a key component of an industrial and manufacturing area to promote employee access and commuting. The Basalt Creek area has freeway access to I-5, Hwy 99W, Hwy 217 and I-205. The primary mode of transportation to the site is assumed to be single occupancy vehicles. Construction is currently ongoing to improve SW 124th Ave to provide a freight route and east/west connection through the site from I-5 to Tualatin Sherwood Road. This work includes the expansion of Grahams Ferry Road south of Tonquin Road from a 2-lane rural road to a major collector. The current roadway width is approximately 24 feet with an existing right-of-way that varies from 74 feet south of Tonquin Road to 40 feet north of Tonquin Road. With the SW 124th Ave extension, the ROW is being increased to 74 feet to approximately 225 feet north of the intersection. This is where the 3-lane roadway section finishes its taper to match back into the 2-lane road. Parcels north of the taper will require additional ROW dedication when Grahams Ferry Road improvements continue to the north or when these properties go in for development. The additional ROW dedication that will be required is approximately 17.5 feet along the frontage. Figures 3 and 4 indicate the lots that will be impacted by the additional ROW dedication and the typical street section from Washington County for a collector roadway.

Per Washington County, the future Basalt Creek Parkway along the southern frontage of the study area will be an arterial and will have access restrictions for new driveways. To accommodate this, the schemes have all assumed that access from Basalt Creek Parkway to the site is not feasible and no driveway entrances are shown here.

Public roadways within the site are assumed to have a pavement width of 50 feet and a right-of-way dedication of 64 feet. The City of Wilsonville standard may vary from this but Washington County standards have been used to establish roadway widths in the concept plans as shown in Figure 5. Private access drives for parking lots and buildings are a minimum of 20 feet wide and truck circulation paths around the buildings are 50 feet wide. Loading docks and storage areas are shown to be 50-100 feet wide to allow semi-truck parking and access around the loading bays. At the terminus of the public roadways, either a hammer head or cul-de-sac are provided to allow for fire truck turn-around. These locations are indicated on the concept plans.

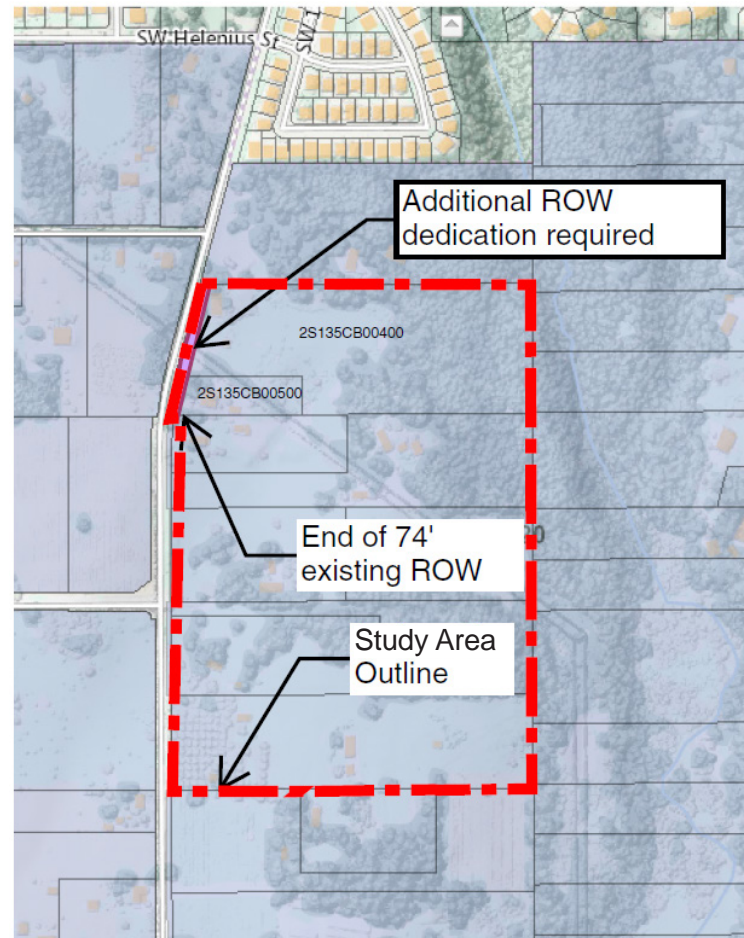


Figure 3 - Additional ROW Dedication Required

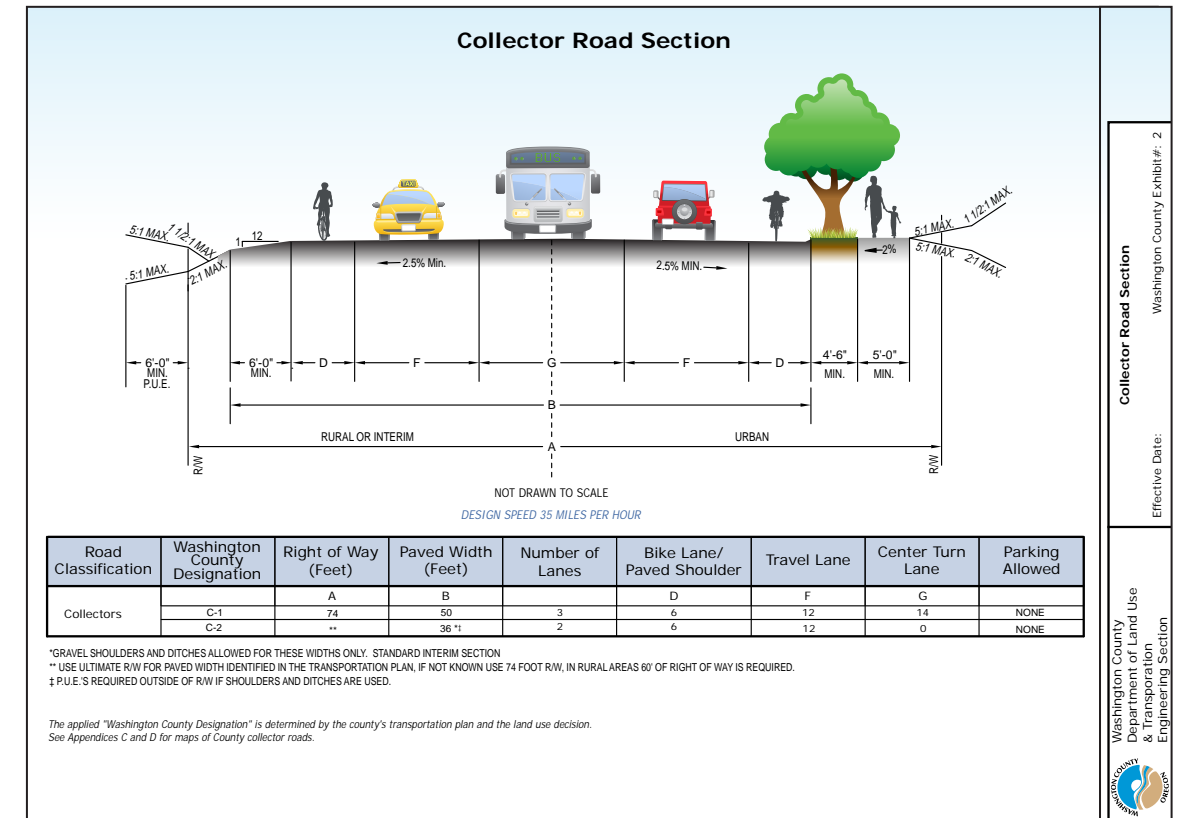


Figure 4 - Washington County Standard Street Section for Collector Roadway

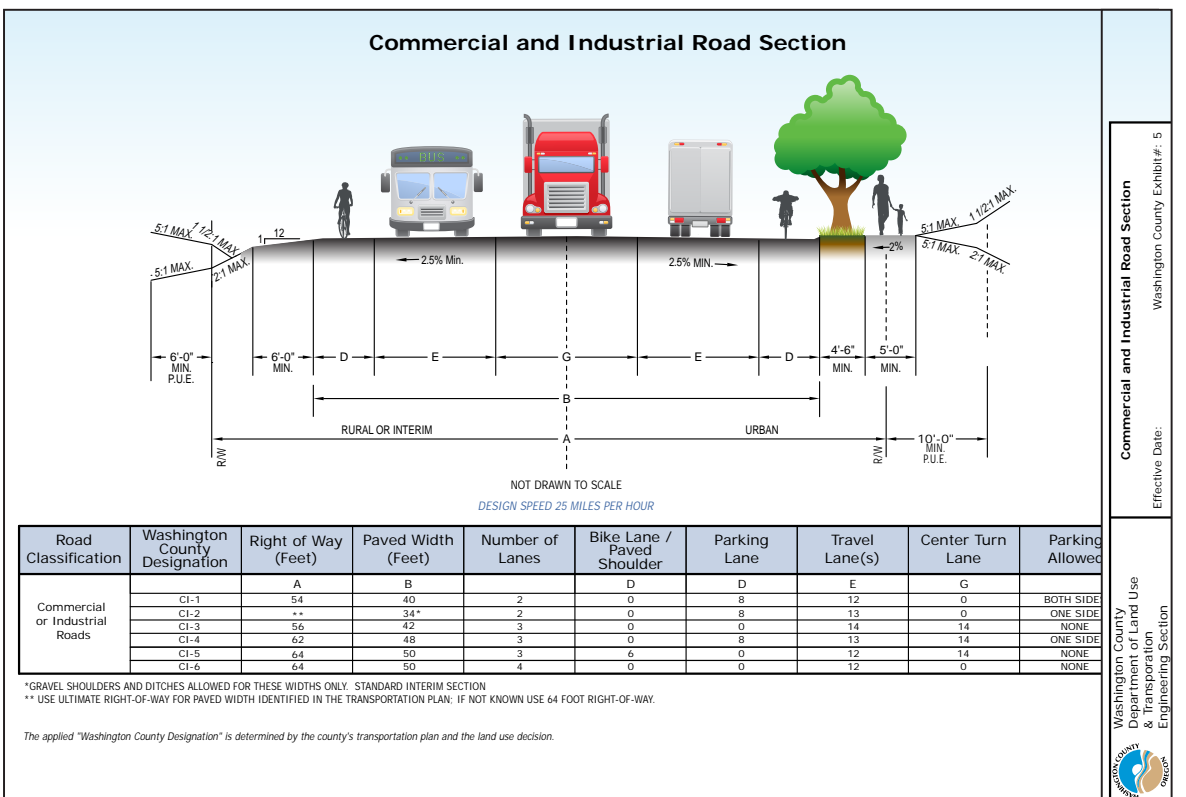


Figure 5 - Commercial and Industrial Road Selection

# Parking Requirements

To accommodate the vehicles accessing the site, on-site parking will be required. Based on the City of Wilsonville's zoning code section 4.155 Table 5, the following parking standards are used for the concept plans. The assumption is that larger building footprints shown in the concept plans are manufacturing type facilities and will require less parking and more loading dock/storage space. The office building prototypes will be more general office and medical/dental type of facilities. For these 3 stalls per 1,000 gsf and 4 stalls per 1,000 gsf are used respectively. For the manufacturing type of facilities 1.6 stalls per 1,000 gsf has been used. The City of Tualatin has the same parking requirements for the assumed uses based on their Development Code.

|                      | Parking Requirement   | Notes   |
|----------------------|---|---|
| City of Wilsonville* | 1.6 per 1000 sf   | Per City of Wilsonville Zoning Code section 4.155, Table 5 for manufacturing establishment  |
|                      | 2.7 stalls per 1000 sf (min) and 4.1 stalls per 1000 sf (max) | First requirement is based on other commercial use (not medical/dental) per City of Wilsonville section 4.155, Table 5 and the second parking requirements are based on medical and dental use. |
|                      | 3.9 stalls per 1000 sf (min) and 5.9 stalls per 1000 sf (max) |   |

Table 1 - Parking Requirements

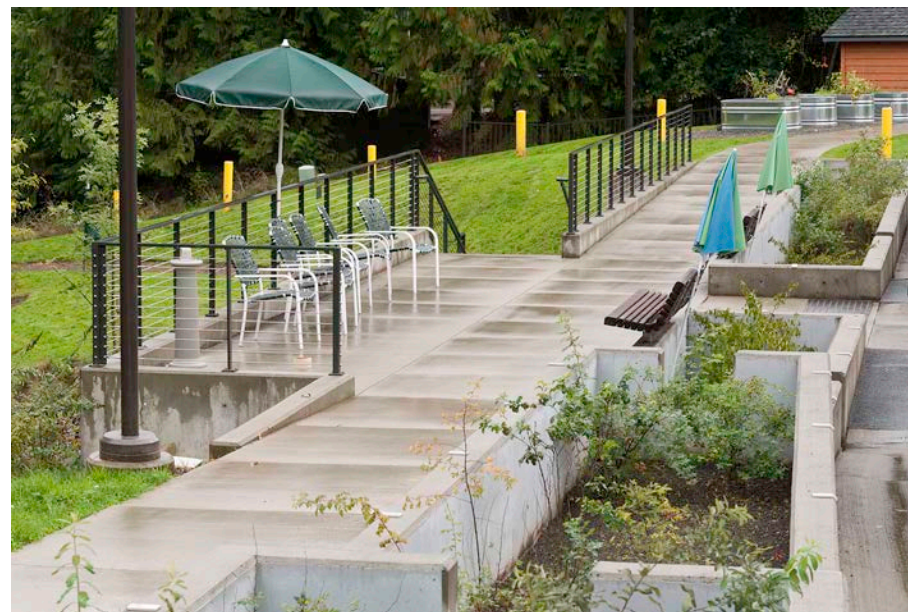
\* - City of Tualatin parking standards are the same as the City of Wilsonville for the zoning noted in this table

# Utilities

## STORMWATER

The Basalt Creek Concept Plan and follow-up studies by CH2M indicate that a regional stormwater management facility is not being evaluated for the site and drainage will need to be handled on an individual development basis. The concept plans represent these two options. One scheme indicates some regional stormwater management ponds that can be used to serve portions of the site while the other schemes assume that stormwater will be treated and detained on an individual development basis. The individual lot systems are not shown on the concept schemes but the systems could be combined with the open space indicated to enhance the natural area while meeting the standards. For smaller lots, subsurface systems may need to be considered due to the limited open area on these properties. Low impact development concepts should be considered in the design since they provide an aesthetic and functional benefit.

Based on the existing studies, drainage from a portion of the site can be conveyed north to the City of Tualatin public system and the remainder will flow to Wilsonville. The City of Tualatin has 12-inch storm drain lines that discharge to Basalt Creek. The City of Wilsonville has storm drain lines to the south that discharge to the Willamette River via the Coffee Creek Basin. New storm drain lines are being installed in Grahams Ferry Road as part of the 124th Ave Extension work per the Washington County construction drawings. These lines are 12-inch and 15-inch pipe which collects the roadway section between Basalt Creek Parkway and north of Tonquin Road. The drainage is collected using catch basins and conveyed to a water quality swale at the intersection of Tonquin Road and Grahams Ferry Road to treat the run-off as shown in Figure 6. A 24-inch and 15-inch culvert are being installed along the feasibility study area in tax lots 2S135CC00600 and 2S135CC00800 to collect roadway run-off in this area as the road is sloping towards the site. These culverts cross under Grahams Ferry Road and discharge to the new water quality facility. With the proposed development in this area, these culverts may need to be revised if the grades are modified at the culvert inlets.



Wilsonville Creekside Woods Swales

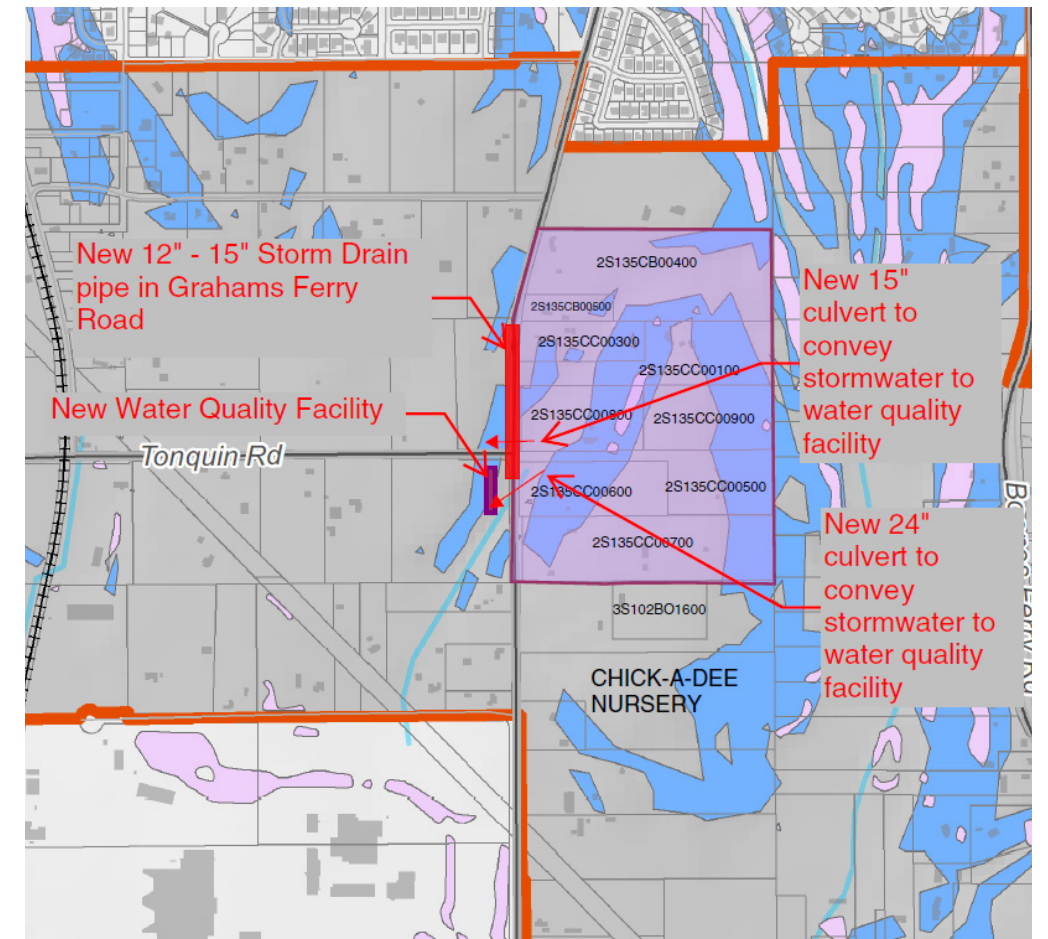


Figure 6 - Storm Drainage

The soil types in the area are silty loam and well drained at the surface. This would indicate that infiltration is a feasible option for disposing of storm drainage and would reduce the amount of run-off leaving the project site. The site is not located in a FEMA floodplain.

# Utilities

## WATER

Currently there is no water service to the site, but the CH2M utility study in 2016 indicated that water can be provided to the site either from the City of Tualatin or the City of Wilsonville. The City of Wilsonville gets their water from City of Wilsonville (Willamette River Water Treatment Plant). The current capacity in the plant is 15 MGD but it can be increased to 70 MGD. The City of Wilsonville water study for the region accounted for 0.75 MGD of water being required for industrial use. The increased water demand for the proposed schemes can be served by the planned improvements in the area. The utility layout for each scheme assumes that water lines are installed in Grahams Ferry Road and these lines are sized for the capacity of the proposed development. The water line installed in the future Basalt Creek Parkway will provide a looped system in the area but the study area will not connect to this line. Water lines will be installed within the public roadway shown on the schemes.

## SEWER

There is currently no sewer service to the site but sanitary sewer service connections are available to the north via the City of Tualatin or to the south via the City of Wilsonville. The City of Tualatin sewer lines are conveyed to the CWS Durham Treatment Plant for treatment and the lines are managed by CWS. The City of Wilsonville has a treatment plant south of the site and most flows from the study area can gravity flow to the Wilsonville treatment plant. To connect to the CWS and Tualatin system in the north, a lift station will be required near the intersection of Grahams Ferry Road and Day Road. This will pump the sewage to the existing main at Grahams Ferry Road and Helenius Street.

Assuming the Grahams Ferry Road are sized to accommodate the proposed development, sewer service is available to the site. Public sewer lines will be installed within the public roadway sections shown on the schemes. Due to the uncertainty regarding utility lines being installed in the future Basalt Creek Parkway and ability to connect; the sewer connections for the study area are shown to Grahams Ferry Road only and no connections are shown to the future Basalt Creek Parkway.

Water reuse strategies should be considered for manufacturing and industrial facilities. These facilities often use a large quantity of water for functions such as cooling towers, air handling units, or process water. This water could be recirculated on-site for non-potable use or a closed loop system could be designed to both help reduce the amount of water required and the sewage flow from the property.



*Scheme B Stormwater Pond*

# Site Factors

## ENVIRONMENTAL

The Basalt Creek planning area primarily contains agriculture and undeveloped forest land. Basalt Creek runs along the eastern edge of the site and there are multiple wetlands and riparian corridors within the site based on the RLIS data provided by the City of Wilsonville and the Basalt Creek Concept Plan Existing Conditions Report. The Land Suitability Study completed in 2015 indicates that the tax lots within the project site are modestly constrained for development since these are medium sized lots that are mostly vacant or undeveloped (see Figure 7). The Environmental Constraints Exhibit (Figure 8) in this report indicates the site constraints that will impact the developable area. Per documentation from PGE, restrictions within their easement include the construction of any permanent feature such as a building, planting trees or heavy vegetation that may impact the clear height to the poles, constructing permanent obstructions that may impact their maintenance access, and extensive grading. Parking lots, roads and low height vegetation are allowable uses within the easement.

The Site Constraints table summarizes the setbacks associated with the environmental constraints and lists the implications to the developable area using Clean Water Services standards. The City of Wilsonville standards may vary from these, and some of the environmental constraints noted can be mitigated through the use of mitigation banks or mitigation at other locations. Washington County indicated for the 124th Ave extension work, mitigation for wetland impacts was done by purchasing credits for the Mud Slough Mitigation Bank.

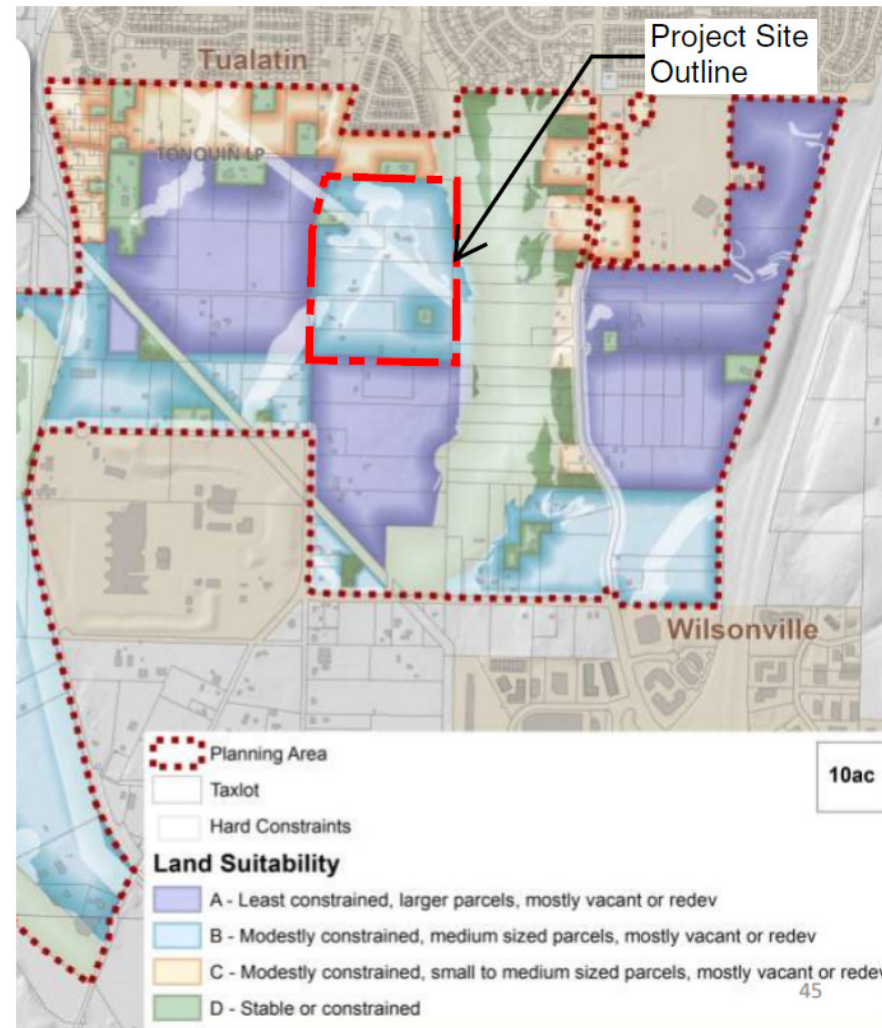


Figure 7

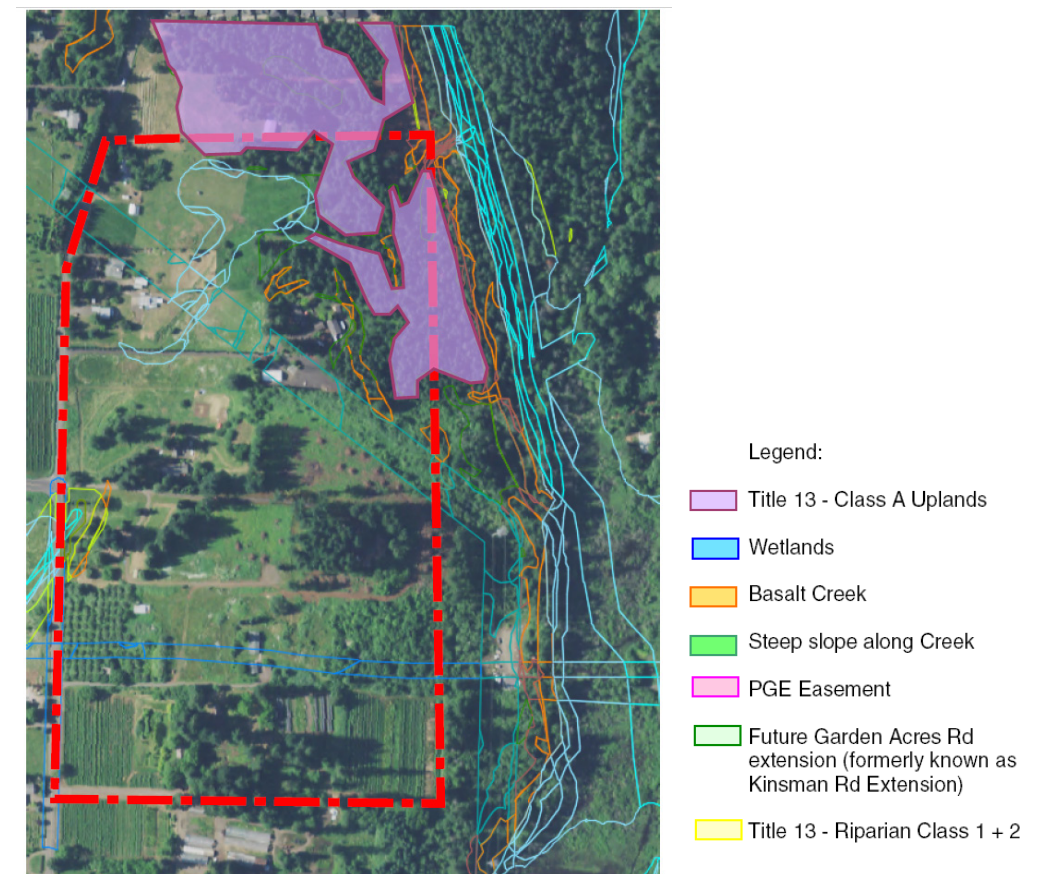


Figure 8

| Constraint                        | Description                                     | Setbacks*  | Development Limitations  |
|-----------------------------------|---|--|--|
| Title 13 – Class A Upland         | Area defined as high value for wildlife habitat |  | Can do routine maintenance and repair of structures within this zone as long as the upland area is maintained. Design to incorporate minimum impact. |
| Title 13 – Riparian Class 1 and 2 | Supports 1-3 riparian functions                 | 100'   |  |
| Basalt Creek                      | Perennial Creek                                 | 50' (per CWS Table 3.1)  |  |
| Wetlands                          | Wetland   | 50' (per CWS table 3.1)  |  |
| Steep slope                       | Slopes greater than 25%                         | The vegetated corridor will extend 35' beyond the break in slope along the creek | Steep slopes are considered unstable and not desirable for development.  |
| PGE Easement                      | Overhead transmission lines                     | 125' easement  | PGE has limitations for use within the ROW including tree locations and permanent structures to allow maintenance of the lines.                      |
| Future Roadway Extension          | Future Basalt Creek Parkway                     | 90' ROW  | Per Washington County TSP Figure 3.10 this is a 4-5 lane arterial.   |

Table 2 - Site Constraints

\* - Vegetated corridors within CWS are measured from edge of the sensitive area and are for each side of the corridor.

# Site Constraints

## ELEVATIONS AND ROCK EXCAVATION

The project site is currently relatively flat in the southern and northwestern region but has slopes towards the middle and east that are in the 15-25% range. Adjacent to Basalt Creek, the slopes are above 25%. The site slope map (Figure 10) indicates the approximate slopes on the site based on GIS data provided by the City. The steeper slopes can cause challenges for development due to additional cut and fill earthwork that is required to create a level building pad and site. Industrial facilities typically require large, flat areas for the building pad and adjacent storage and loading/unloading zone. As a result, in the schemes the industrial facilities have been set in the flatter portions of the site and alternative building types have been shown in the steeper sections such as office space. Office buildings are more conducive to split elevations and access at varying levels to accommodate grade. Parking lots can also be used in the sloped portions of the site with retaining walls to create level parking areas. Concept grading schemes have been developed for each of the three schemes to show potential finished floor elevations.

The geotechnical report by GRI dated March 19, 2015 and the CH2M study of this area indicate that rock excavation can be expected in the eastern portion of the site. Moving further west from Basalt Creek and towards Graham Ferry Road, the amount of rock expected to be encountered with construction decreases. The rock in the area is basalt per GRI's findings and it is decomposed rock that is soft to very soft. However, the boring equipment did experience drill chatter while excavating the 2 deep boring holes within the feasibility limits, suggesting the rock does provide resistance to excavation and may require heavier construction equipment to remove or blasting. Rock excavation can be a costly undertaking for some developers and options for raising the grades in this area would need to be considered.

Per the SW 124th Ave Extension plans, the intersection of SW Grahams Ferry Road and Tonquin Road will be raised approximately 9 feet from existing grade. To match existing grade on the east side of the roadway (along the project site frontage) a cut/fill slope will extend approximately 20 feet into the site. Per Washington County, the slope easement is a permanent easement. However, if the project development raises grade to match the roadway grade in this area, then the easement can be removed and this land is available for development. The slope easement is shown on the concept site plans for reference.

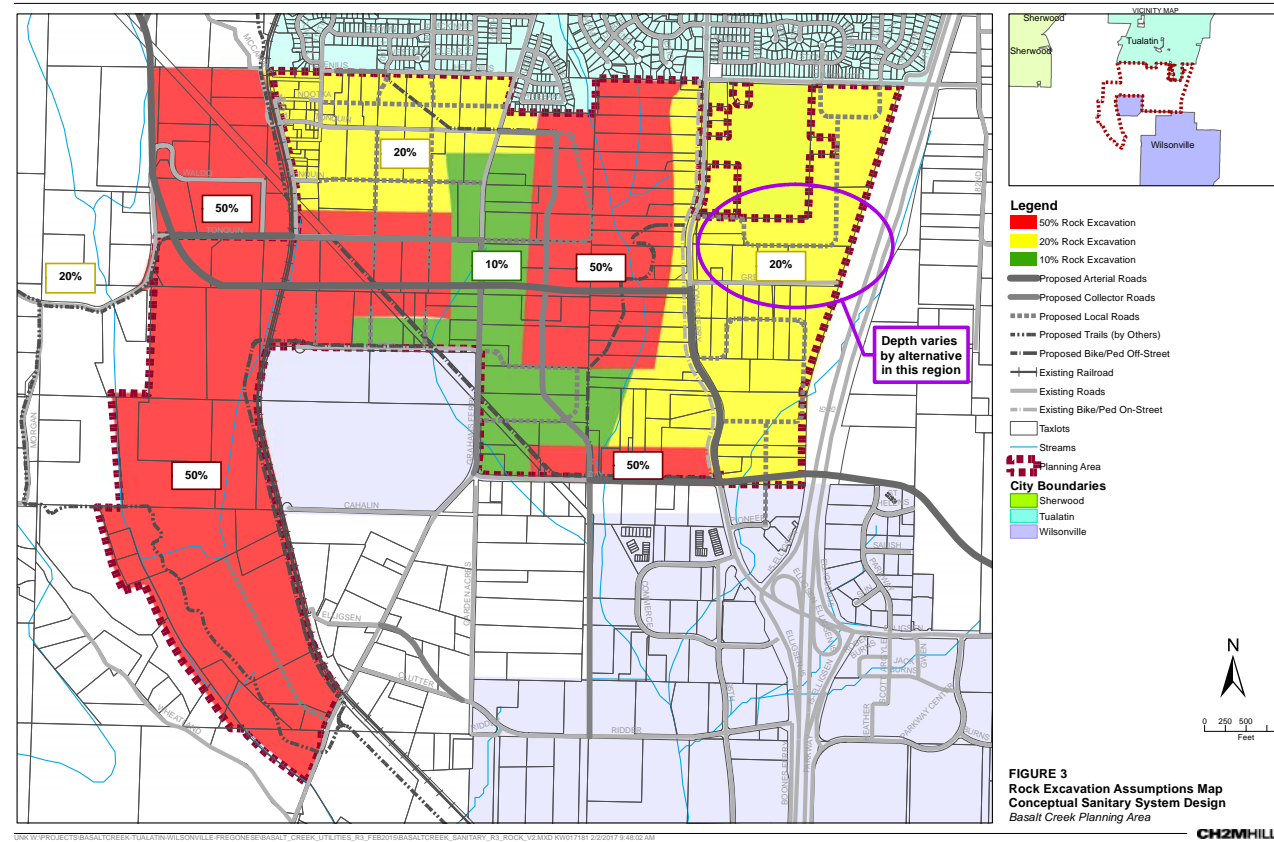


Figure 9 - Rock Map by CH2M

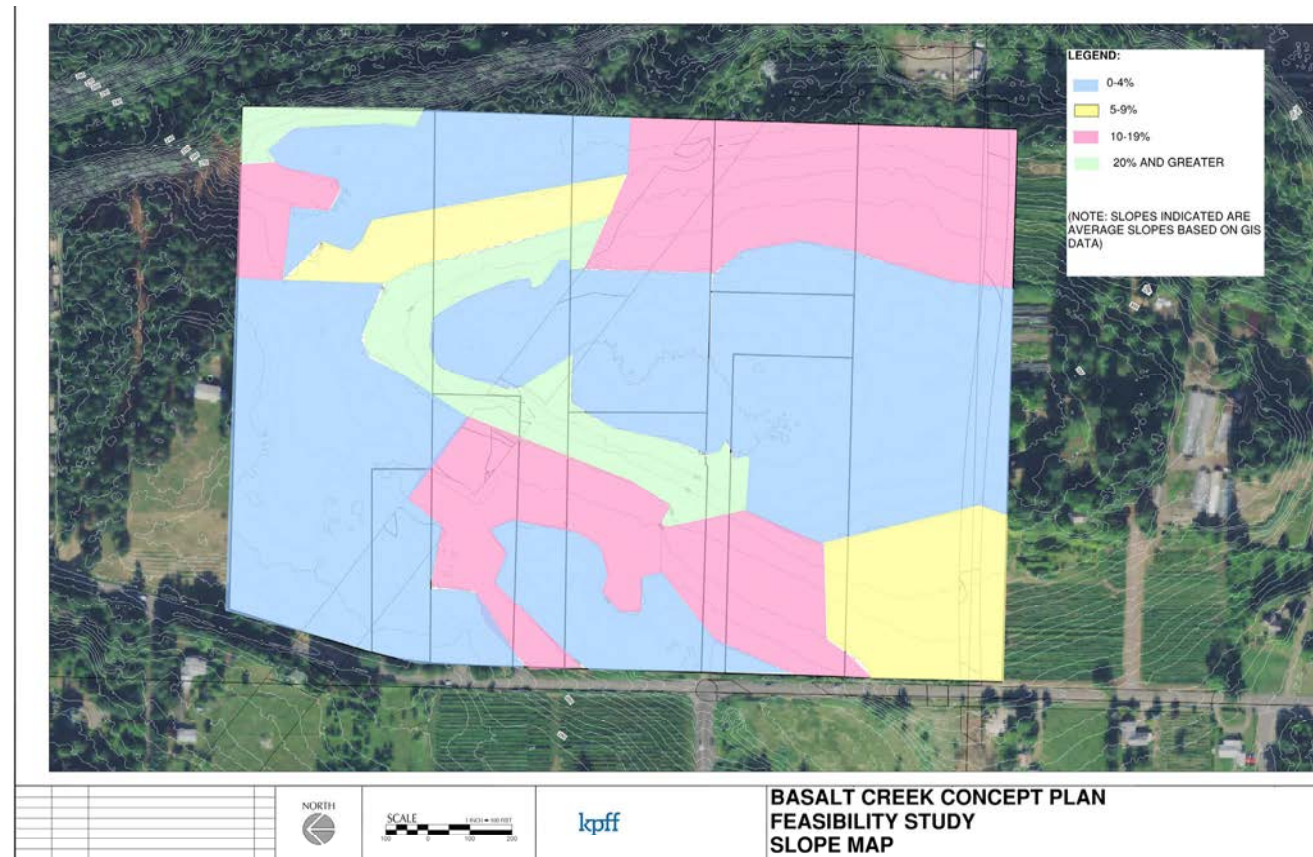
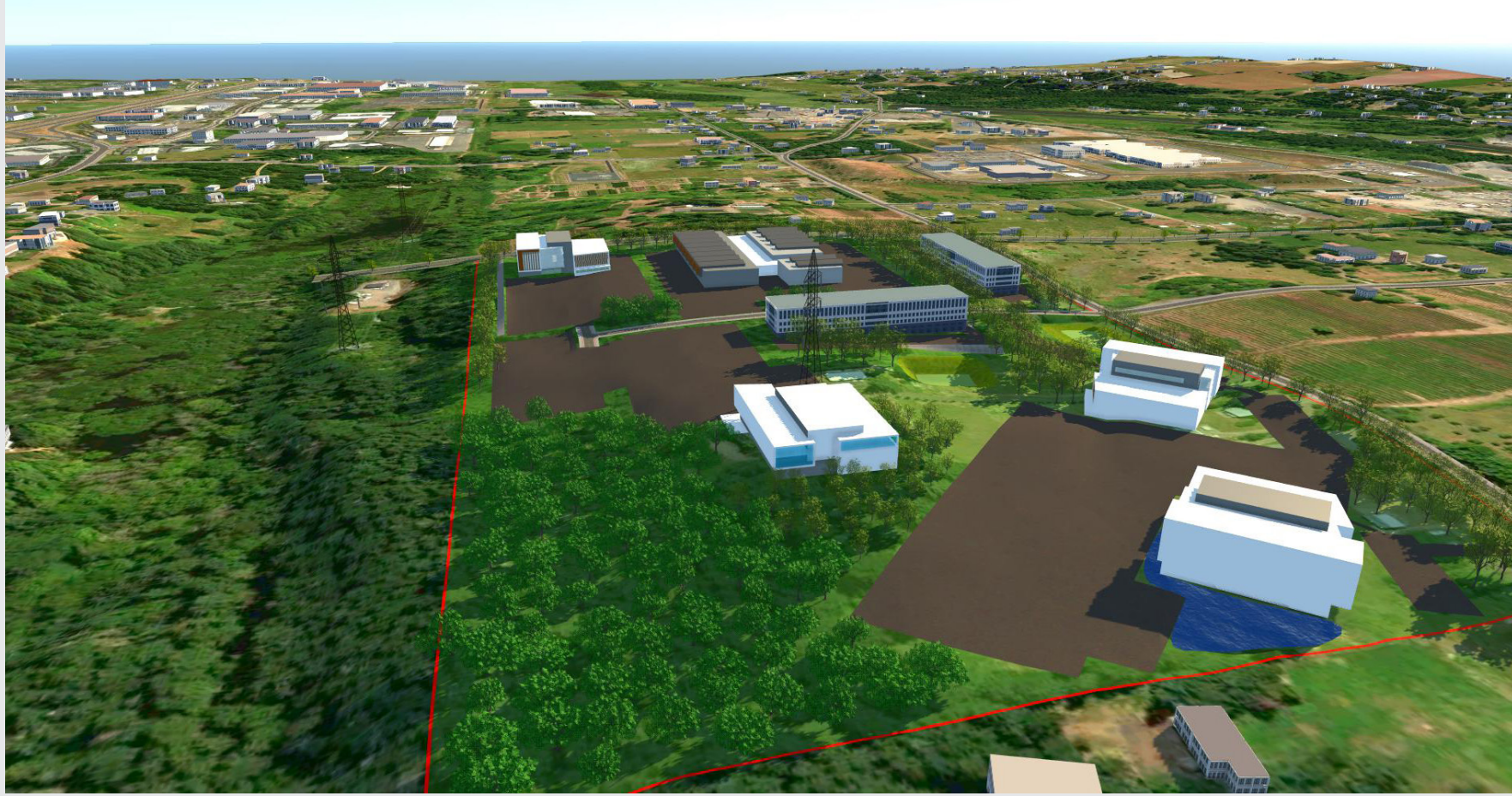


Figure 10 - Slope Map



# Schemes

# Building Prototypes

The building sizes and footprints shown in the concept plans are based on the following building prototypes. These prototypes were chosen based on their location in the local area (so they are representative of local facilities), variation in footprint size, and to provide a variety of building uses. The table to the right corresponds to the letters on the concept plans and in the renderings:

| Building Designation | Building Type                   | General Description   | Approx GSF             | Similar facility type   |
|----------------------|---------------------------------|---|------------------------|---|
| A                    | Office and industrial mixed use | This is the largest of the building footprints and is a combination of office space in the front and manufacturing/industrial in the back.  | 215,800                | Fujimi Corporation  |
| B                    | Office                          | Office building (assumed to be 3 stories for the parking count)   | 120,000                | Levetron Drive office building component shown in image for E1-E4 below |
| C                    | Office                          | Office building with a split bar so can accommodate grade changes (assumed to be 3 stories)   | 120,000                | Providence Bridgeport   |
| C1                   | Office                          | Office building with a split bar to accommodate grade changes. This is a slightly smaller footprint than C (200' x 100' vs 200' x 200') to fit the smaller lots (assumed to be 3 stories)   | 80,000                 |   |
| D                    | Manufacturing/Industrial        | This building is more of a manufacturing/industrial facility with limited office space and more loading bays and exterior storage.  | 90,000                 | Hesler Industries   |
| E1-E4                | Craft Industrial                | Buildings E1-E4 are craft industrial type of facilities with limited office space and more workshop type of facilities over large manufacturing. These have vehicle access around the site and some storage area but do not require as much yard space as building D. | Varies 10,800 – 82,500 | Industrial Park on Leveton Drive  |





# Scheme A

Scheme A – maintains all of the open area in the northern portion of the site and development is primarily on the southern portion of the site (below the PGE easement)



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## BASALT CREEK CONCEPT PLAN FEASIBILITY STUDY SCHEME A - SITE PLAN

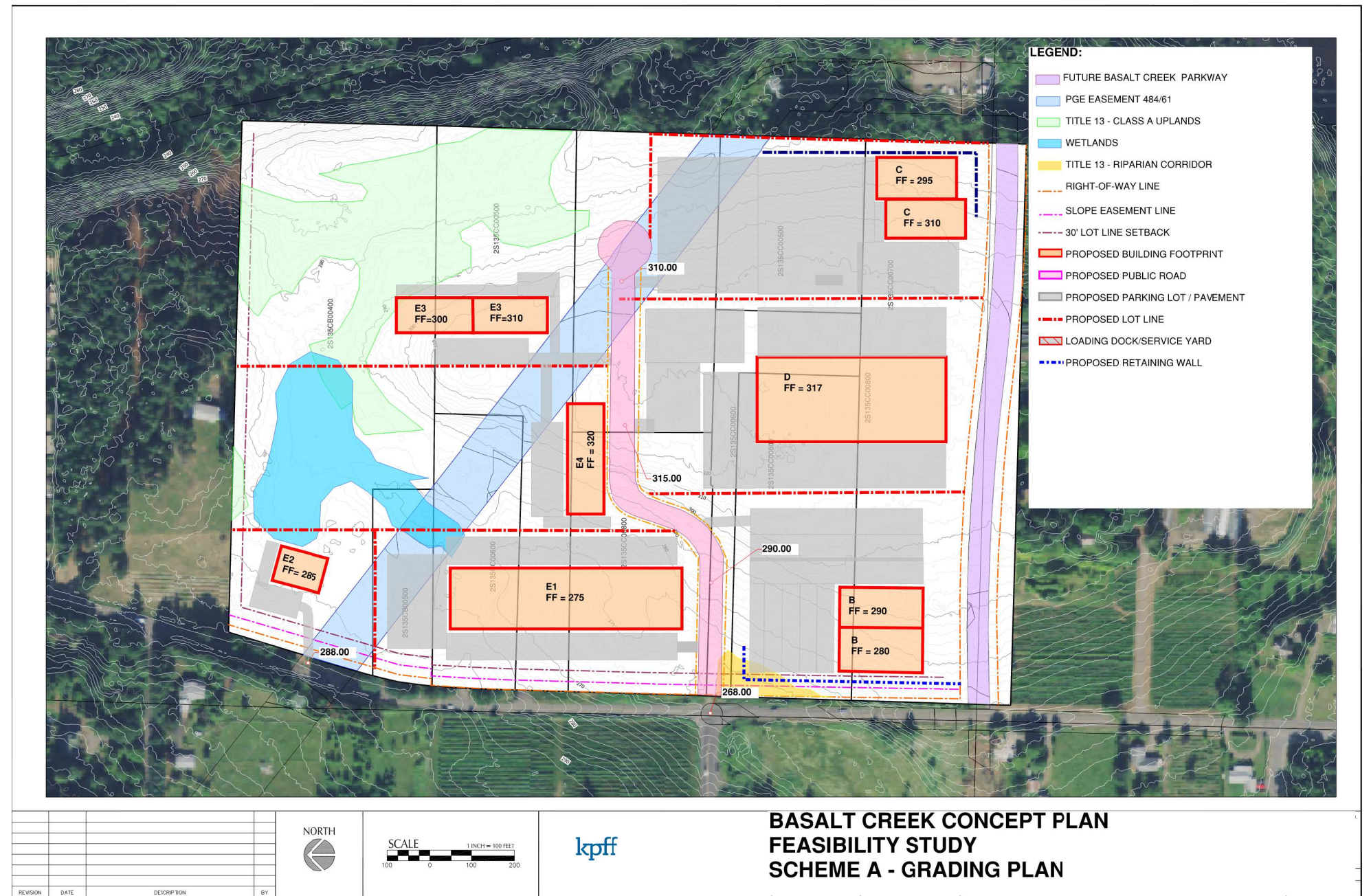
## LAYOUT

Scheme A is developed to preserve as much of the open space as feasible on the north side of the site including the upland habitat and wetlands. The development is primarily focused south of the PGE easement and includes building prototypes C, D, B, and E. Building C is located on the northwest corner of the site so it is the face of the development when someone is traveling west on future Basalt Creek Parkway. From here, the site begins to accommodate more of the industrial use by locating building D on the flatter portion of the site near the future Basalt Creek Parkway and buildings E1-E4 to the west near the environmental regions and PGE power lines. The lots for buildings E3 and E4 are large to include the open space areas. At the corner of Grahams Ferry Road and future Basalt Creek Parkway, Building B is located to anchor this corner and provide office space and a visual marker. Access to the facilities is via a single public road connection at Tonquin Road. This public road is assumed to be similar to the Washington County industrial/commercial roadway section described previously. The parking lot sizes indicated are based on the assumed parking counts previously indicated for the gross square footage of development. Parking lots and loading areas can be screened in this layout with trees planted along Basalt Creek Parkway, Grahams Ferry Road and along the public roadway within the site. A secondary access point will be required off of Grahams Ferry Road for the E2 building in the northwest corner of the site. Parking for Scheme A is located close to the buildings and there is some parking in the PGE easement, but of all the schemes this has the least amount of parking in the easement.

For Scheme A the total built area is 480,000 sf, the total parking count is 1,230 stalls, and the potential open space is 14 acres.

## GRADING

The finished floors indicated represent potential finished floors for the buildings taking into account existing grade and the roadway grading being completed for Grahams Ferry Road at Tonquin Road. Building B in the southwest corner will be a split building to accommodate the slopes in this region. Retaining walls will be required on the west side of this building to meet the roadway grades and the building will be sitting higher than the adjacent roadway. Buildings E1-E4 are set close to existing grade to minimize the earthwork for these structures and to maintain the existing grade at the open space areas. E3 has a split elevation in the east/west direction to accommodate the grade change north of the PGE easement where the site currently slopes down to the upland habitat area. Building D is located near the plateau area on the south side of the site where the grades are relatively flat. This allows for level areas around the building for storage yard and loading bays. Building C in the southeast corner of the site is designed to accommodate the grade change here as the site begins to slope into Basalt Creek. The existing grade change in this area is 40 feet. The design concept shows a 15-foot grade change between the two building bays which would allow access at two different floors. Changes to grade in the parking lot will need to be accommodated by retaining walls in this option. A retaining wall will be required along the eastern property line near building C. Of the three options, Option A works the best with existing grade since improvements north of the PGE easement and east near the creek are limited.



## Scheme A Grading

Scheme A

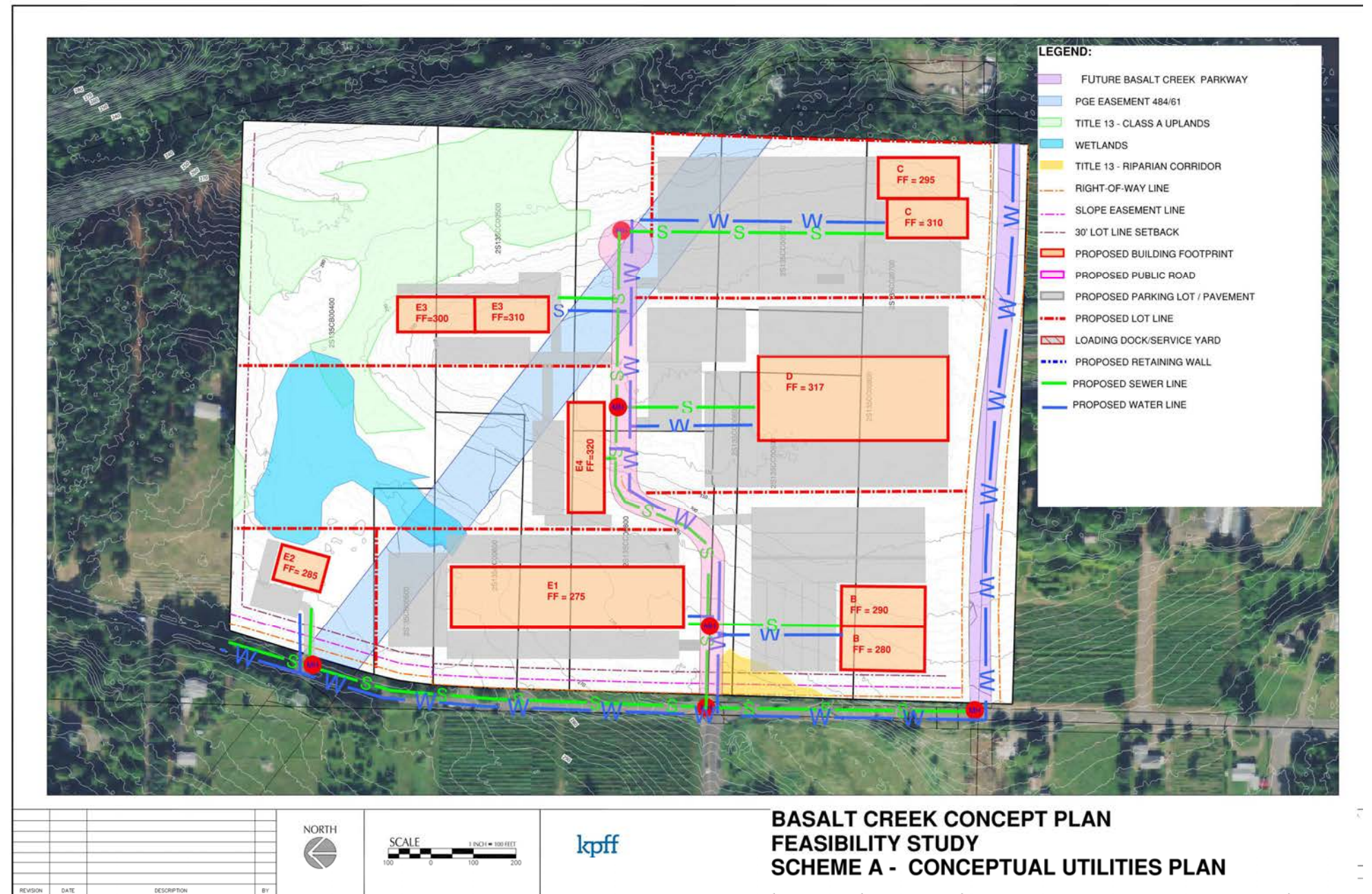


## UTILITIES

Utilities for Option A will include sewer and water service to the buildings from Grahams Ferry Road and via the public roadway within the site. There is no regional storm drainage concept in this scheme and the intent is for storm drainage to be accommodated on each lot through the use of subsurface or low impact development options. Buildings E1-E4 are either adjacent to or include the open space in their lots which provides the opportunity to incorporate the drainage systems with these areas and to gravity drain down to the open space. The building grades within the site are raised from the roadway grades in Grahams Ferry Road in Scheme A, with the intent to gravity drain the sewer from the site to the public lines. Drainage for the other buildings (B, D, and C) will also be able to gravity drain to Grahams Ferry Road.

## ENVIRONMENTAL IMPACTS

Scheme A of the three schemes preserves the most amount of open space and has the least impact on the existing natural resources. This option preserves both the wetland and upland habitat area on the north side. The Title 13 riparian corridor along Grahams Ferry Road is impacted under all 3 schemes and this can be mitigated similar to what the County did for work on 124th as described above. Based on the condition of the wetland onsite, there is a potential to enhance this area to act as a transition from the development to the residential uses to the north.

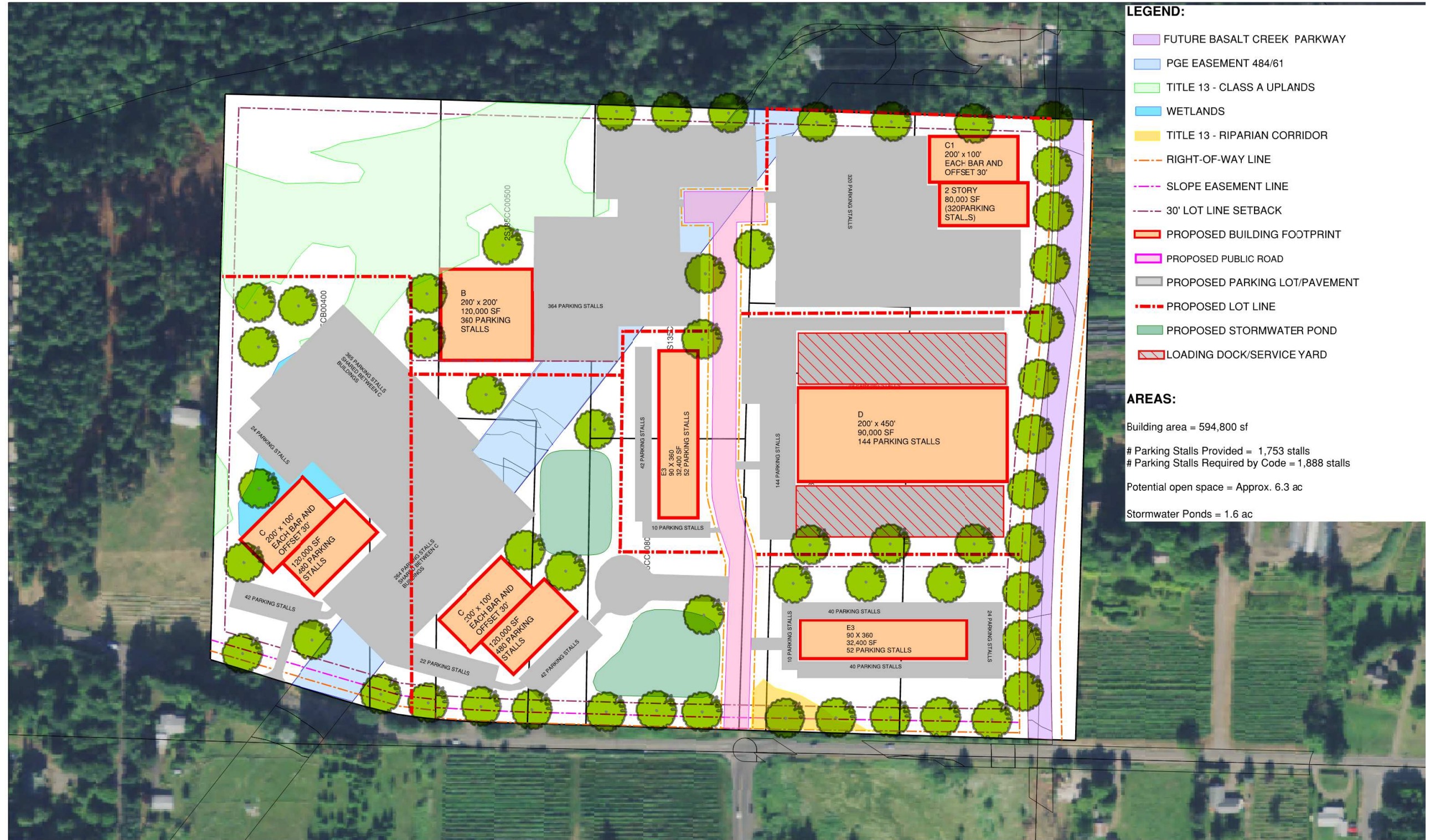


Scheme A

Utilities

# Scheme B

Scheme B – maintains some open space in the northwest corner of the site but develops more of the site than Scheme A



- LEGEND:**
- FUTURE BASALT CREEK PARKWAY
  - PGE EASEMENT 484/61
  - TITLE 13 - CLASS A UPLANDS
  - WETLANDS
  - TITLE 13 - RIPARIAN CORRIDOR
  - RIGHT-OF-WAY LINE
  - SLOPE EASEMENT LINE
  - 30' LOT LINE SETBACK
  - PROPOSED BUILDING FOOTPRINT
  - PROPOSED PUBLIC ROAD
  - PROPOSED PARKING LOT/PAVEMENT
  - PROPOSED LOT LINE
  - PROPOSED STORMWATER POND
  - LOADING DOCK/SERVICE YARD

**AREAS:**

Building area = 594,800 sf

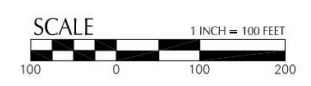
# Parking Stalls Provided = 1,753 stalls

# Parking Stalls Required by Code = 1,888 stalls

Potential open space = Approx. 6.3 ac

Stormwater Ponds = 1.6 ac

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## BASALT CREEK CONCEPT PLAN FEASIBILITY STUDY SCHEME B - SITE PLAN

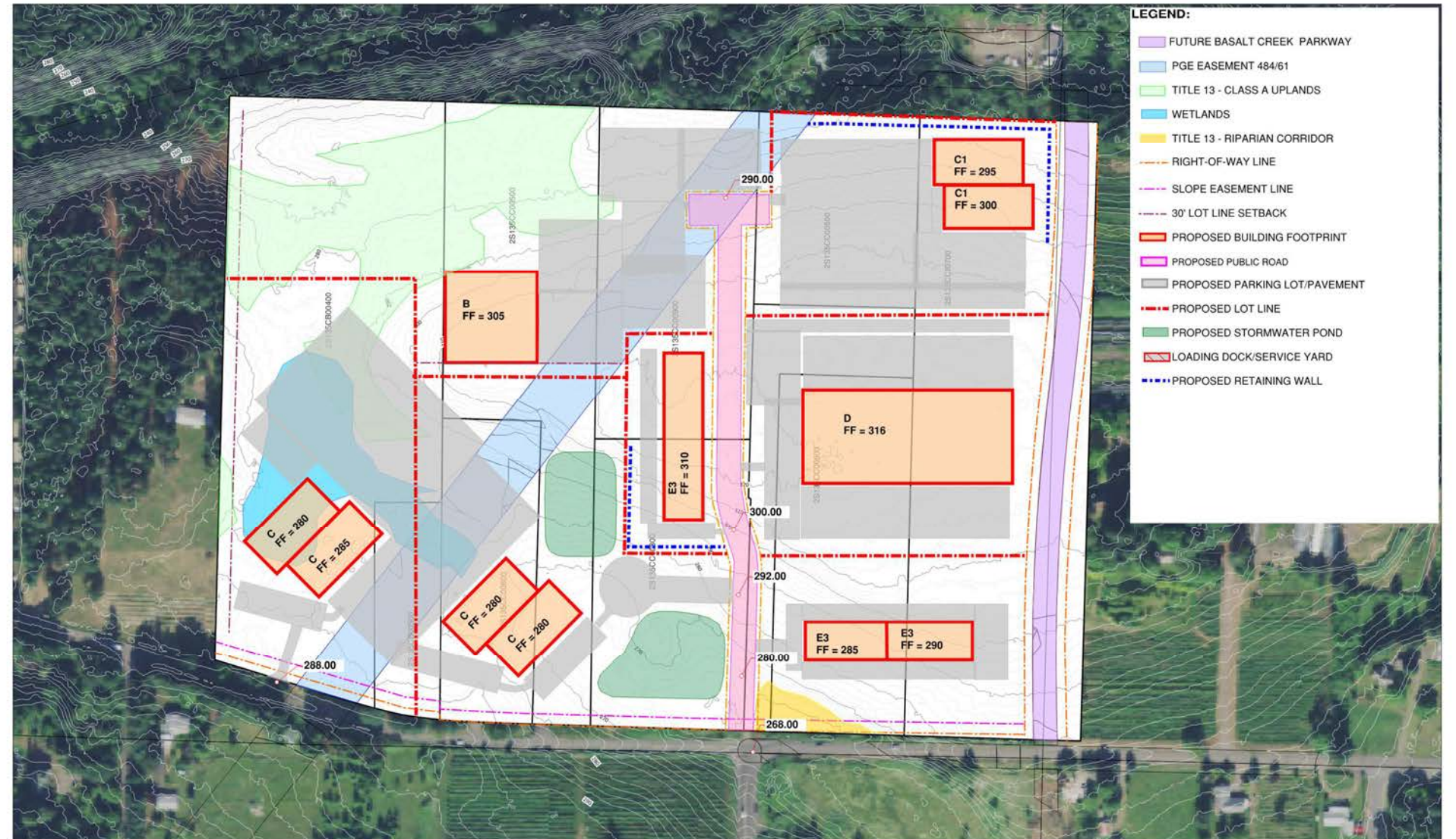
## LAYOUT

Scheme B is the moderate scheme between Schemes A and C. It utilizes more of the open space for development than Scheme A but it also maintains some open space unlike Scheme C. Scheme B has a combination of office and manufacturing/warehouse space and has the largest number of office-only buildings (prototypes B and C) of all three schemes. In Scheme B, buildings E and D are located along the future Basalt Creek Parkway in the southern portion of the site. Building D is located on the flatter portion of the site, similar to Scheme A. Building E3 is located in the southwest corner of the site and both parking and vehicle access are provided around all sides of this building. Building C1 is located in the southeast corner as in Scheme A to anchor this location and to provide a visual presence for the site as drivers travel west on the future Basalt Creek Parkway. The C1 footprint was used here to reduce the parking area and to allow Building D to be located on the flatter portion of the site. The public roadway heading east from Tonquin Road intersects the site internally both north and south. To the north of this road is a smaller craft industrial building (E3) and then closer to the open space are office Buildings B and C. Building B is adjacent to the upland habitat area and the open space can be incorporated into the building layout and design. The two type-C buildings located on the northwest portion of the site will have a shared parking lot. Access to these buildings is from Grahams Ferry Road and the internal public roadway. Scheme B utilizes the PGE easement for parking more than Scheme A. Scheme B is also the only scheme that looks to incorporate regional stormwater systems into the layout. These ponds are shown near the main entrance to the site.

For Scheme B the total built area is 594,800 sf, the total parking count is 1,753 stalls, and the potential open space is 6.3 acres.

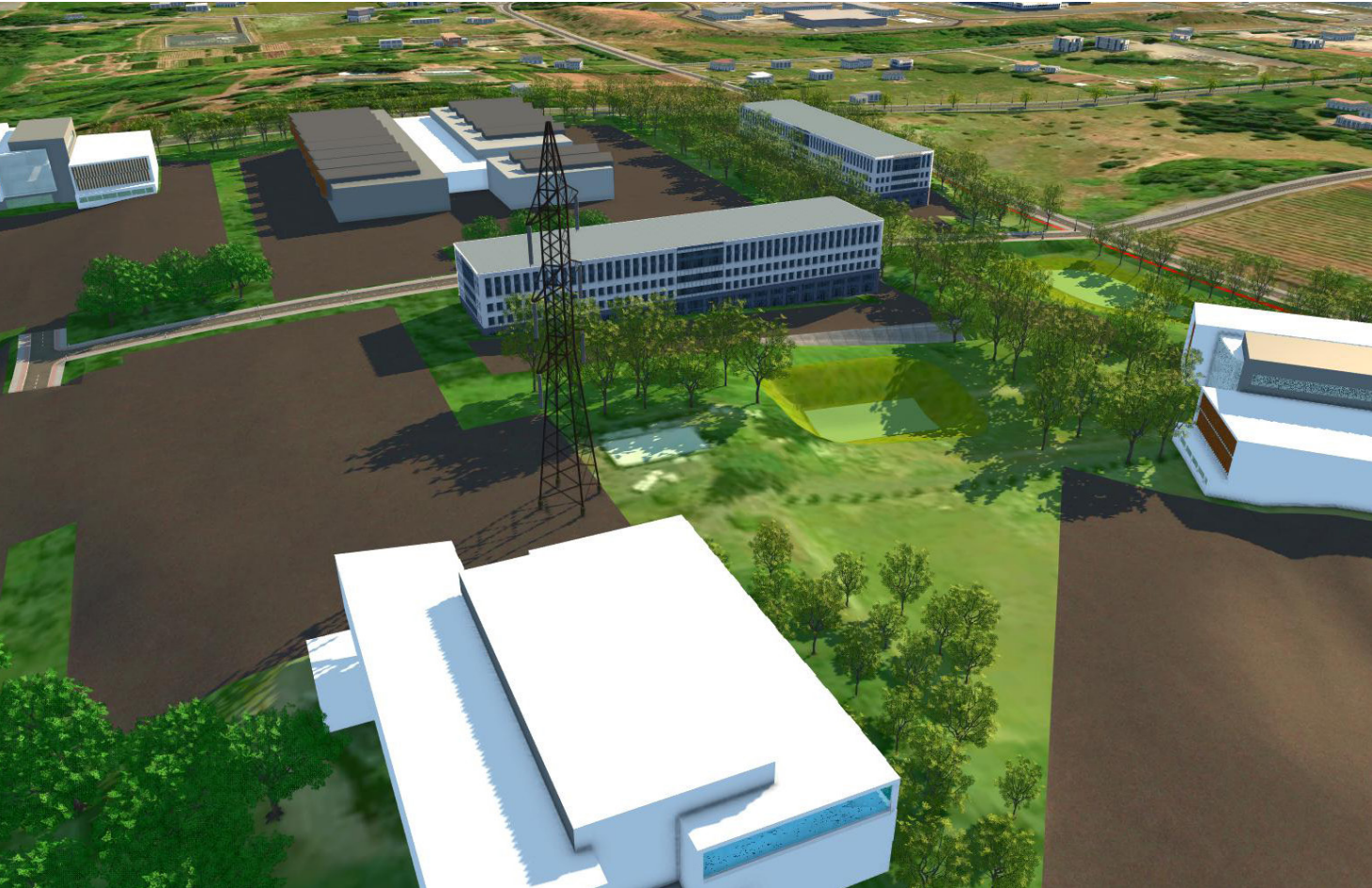
## GRADING

The finished floors indicated represent potential finished floors for the buildings, taking into account existing grade and the roadway grading being completed for Grahams Ferry Road at Tonquin Road. Building E3 in the SW corner of the site will be split in elevation to accommodate the existing grade here. Retaining walls will be required along the eastern and southern side of these lots to match the roadway grades. Since building E3 will be filling in the slope easement, this easement can be removed providing more developable area between the building and Grahams Ferry Road. Buildings D and C1 are located in similar locations in Scheme A and B. Building D is located near the plateau area on the south side of the site where the grades are relatively flat. This allows for level areas around the building for storage yard and loading bays. Building C1 in the southeast corner of the site is designed to accommodate the grade change here as the site begins to slope into Basalt Creek. The grade change in building C1 is shown as 5 feet and a retaining wall will be required along the eastern property line. Retaining walls will be required in the parking lot for C1 and the adjacent building B to accommodate the slope in this area. Building B is located at the top of the slope near the upland habitat and will require some cut in this area to create a level building pad. Buildings C near the western portion of the site steps down with grade which has 5- to 10-foot grade variations between the levels. This is to accommodate the existing slope in this area. Access is also provided from Grahams Ferry Road to these building which impacts the finished floor elevations.



## Scheme B Grading

Scheme B

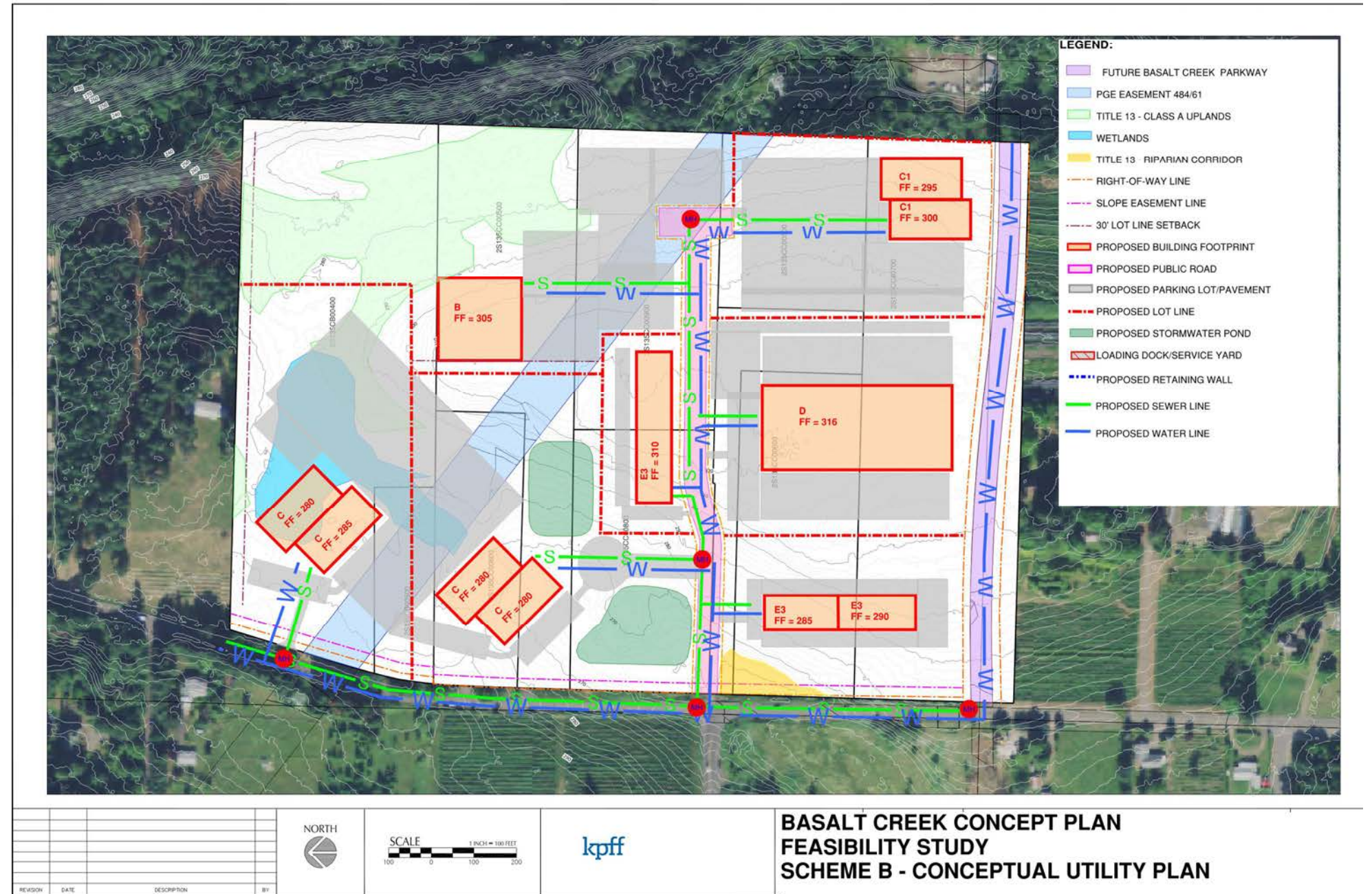


## UTILITIES

Utilities for Scheme B will include sewer and water service to the buildings via the public roadway within the site and Grahams Ferry Road. There are two regional storm drainage ponds shown in this concept that provide approximately 1.6 acres for water quality treatment and detention. To accommodate the full build out of development, this will not provide enough volume and storm drainage will also need to be accommodated on each lot through the use of subsurface or low impact development options. Buildings B and C are either adjacent to or include the open space in their lots which provides the opportunity to incorporate the drainage systems with these areas. The building grades within the site are raised from the roadway grades in Grahams Ferry Road, with the intent to gravity drain the sewer and drainage from the site to the public lines.

## ENVIRONMENTAL IMPACTS

Scheme B is the middle ground option for natural resource protection. The upland habitat area in the northeast corner of the site is maintained in this option but the wetland north of the PGE easement is utilized for development. The condition of this wetland will need to be evaluated as the region is developed but visual observations during the site walk of the area indicated this wetland may not be in "good" condition and could be mitigated elsewhere. By utilizing the wetland area it allows the northwest portion of the site to be developed more than in Scheme A providing more building square footage and employment opportunities.



Scheme B

Utilities



# Scheme C

Scheme C – fully develops the site to the maximum extent feasible and has the least amount of open space



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## BASALT CREEK CONCEPT PLAN FEASIBILITY STUDY SCHEME C - SITE PLAN

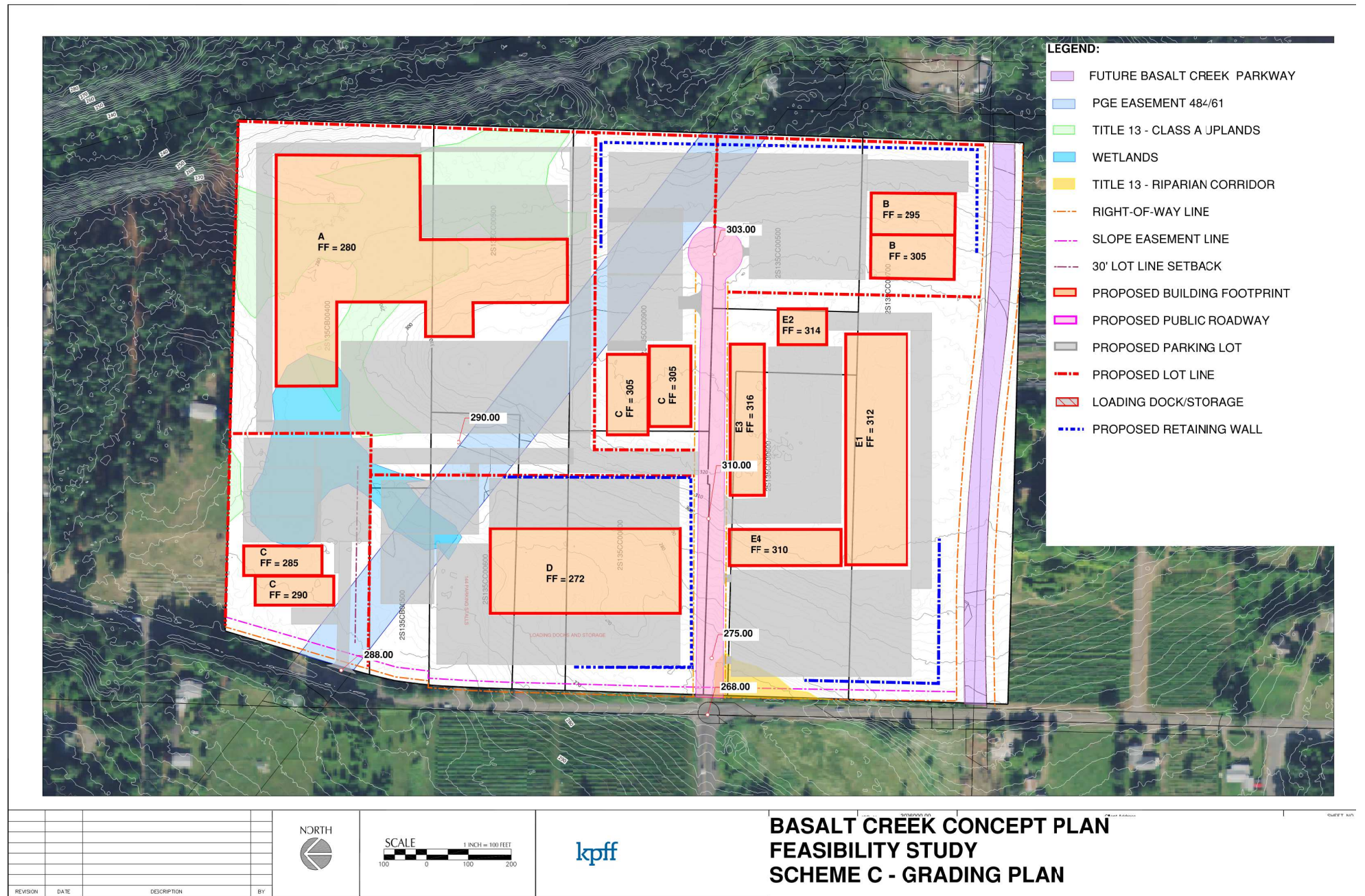
## LAYOUT

Scheme C utilizes the most amount of area on the site for development and preserves the least amount of open space of the three schemes. In Scheme C, buildings E1 – E4 are located along the southern portion of the site and have a combined courtyard area to allow for storage, utility yards and other uses that can be screened from the public. Parking for these buildings is combined and located in a lot at the southwest corner of the site. In the southeast corner of the site is Building B. Building B is located further away from the Basalt Creek steep slope in this scheme so it can be closer to the flatter portion of the site. This building still requires a step in the finished floor elevations but there is more room to grade out from the building to the east, which helps to reduce the retaining wall height here. A public roadway is located east of Tonquin Road and provides the north/south split within the site that is common between all three schemes. On the north side is Building C which is accessible from the site public roadway. Building C shares a combined parking lot with Building B. North of the new public roadway is Building D. A second building C is located in the northwest corner of the site, similar to Scheme B. Scheme C is the only one to utilize the Building A footprint since this is the largest of all the building prototypes and requires infilling the natural resource area to accommodate the building. Parking and roadways are located within the PGE easement in Scheme C and this scheme utilizes the PGE easement the most. In Scheme C there is no anchor building at the southeast corner of the site to provide the visual connection from Grahams Ferry Road since this area is utilized for parking.

For Scheme C the total built area is 781,350 sf, the total parking count is 1,542 stalls, and the potential open space is negligible. The parking stalls provided is less than the required count by code since the parking lots are shared in Scheme C for Buildings B and C. There is not enough room on the site to meet the parking count for each building individually.

## GRADING

The finished floors indicated represent potential finished floors for the buildings taking into account existing grade and the roadway grading being completed for Grahams Ferry Road at Tonquin Road. For Scheme C, buildings E1-E4 are located on the flatter portion of the site near the future Basalt Creek Parkway and the parking lot in the southwest portion of the site will need to be tiered to accommodate the 35 feet in grade difference between the roadway and buildings. Building B is stepped in the southeast corner similar to buildings C in Schemes A and B to accommodate the grade change here. The combined parking lot for buildings B and C will be located on the slope heading down to the upland habitat area and will require retaining walls to accommodate the grade change. Building C has a constant finished floor in Scheme C and the grades slopes north from Building C to Building A. Building A is located in the lower portion of the site and is at the elevation of the upland habitat area. This allows the building to create level areas on the east and north side for storage and vehicle access. From Building A, the site slopes back up to the west to set the finished floors for buildings D and C. Building D is located on the flatter area near Tonquin Road and Building C is stepped to accommodate the grades to the north. Scheme C will require internal retaining walls to accommodate the grade changes within the site since this scheme has the most amount of impervious area and the least amount of open space to accommodate grade changes in undeveloped areas.



## Scheme C Grading

Scheme C

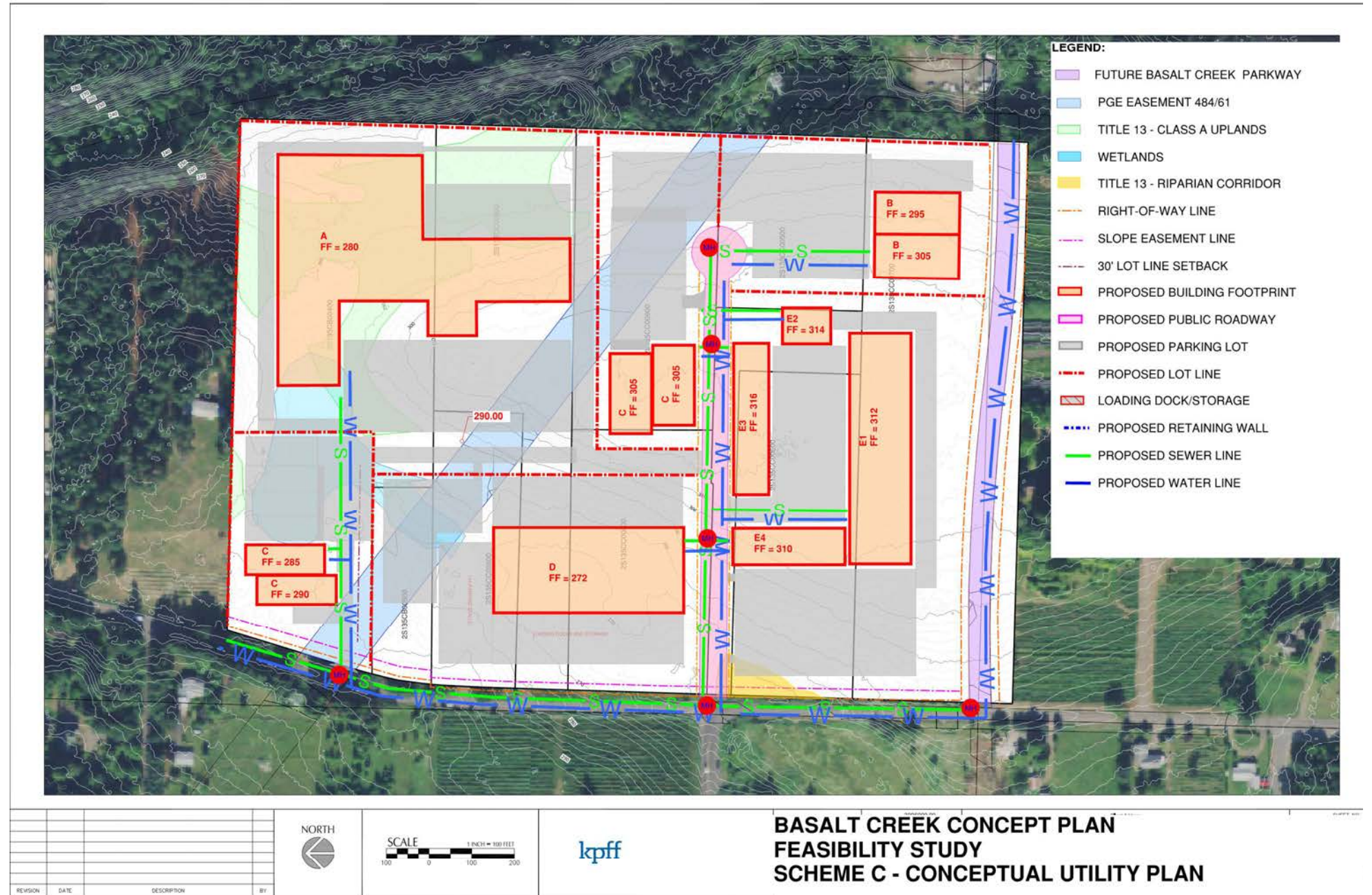


## UTILITIES

Utilities for Scheme C will include sewer and water service to the buildings from the public roadway within the site and Grahams Ferry Road. This scheme does not show any regional stormwater systems and storm drainage will need to be accommodated on each lot through the use of subsurface or low impact development options. The building grades within the site are raised from the roadway grades in Grahams Ferry Road, with the intent to gravity drain the sewer and drainage from the site to the public lines.

## ENVIRONMENTAL IMPACTS

Scheme C has the most impact to the environmental resources on site and preserves none of the upland habitat or wetlands. These areas will need to be mitigated either through mitigation banks or improvements off site.



Scheme C

Utilities



## Building Renderings

*Building renderings provided are representative conceptual images of what Buildings B, C, and D could look like*

# Building B



# Building C



# Building D





# Summary

## SCHEMES

Each scheme presented provides unique design opportunities for the region that capitalize on the goal of increasing employment opportunities. The three schemes take different approaches on how to develop the site and provide test fits for design concepts. The existing grades on the site pose a challenge for development but the concept grading schemes show the grade changes can be accommodated through the use of stepped finished floors and retaining walls. Grades within the parking lots for some of the schemes may be challenging and require the use of walls and terraced parking to accommodate the slope. The material for the retaining walls can be chosen to be aesthetically pleasing and the walls may be terraced to break up the grade and allow for planting in this area. Consideration should also be given in some of these locations to structured parking which can accommodate grade changes better than surface parking can. The table below is a side by side comparison of the three options to show their similarities and differences. "1" indicates that the scheme is closest to meeting the objective and "3" is that is least of all schemes to meet the objective.

| Objective   | Scheme A | Scheme B | Scheme C |
|---|----------|----------|----------|
| Provides the most building square footage   | 3        | 2        | 1        |
| Provides the most number of parking stalls  | 2        | 1        | 3        |
| Meets or exceeds the code parking count based on the assumptions outlined in this study | 2        | 1        | 3        |
| Provides the most potential for open space  | 1        | 2        | 3        |
| Transition to residential area to the north   | 1        | 2        | 3        |





# PLANNING COMMISSION

## WEDNESDAY, MAY 10, 2017

### III. INFORMATIONAL

C. 2017 Planning Commission Work Program (5 minutes)

## 2017 WORK PROGRAM Planning Commission

updated: 5/1/2017

| DATE  | AGENDA ITEMS   |   |   |
|---|--|---|---|
|   | Informational  | Work Sessions   | Public Hearings   |
| <b>JAN. 18</b> (rescheduled from Jan. 11 - weather) | PC Chair & Vice-Chair Election   | Frog Pond West Master Plan (Neamtzu)  |   |
| <b>Feb. 8</b>                                       |  | Frog Pond West Master Plan (Neamtzu)<br>Water Treatment Plant Master Plan (Mende) |   |
| <b>Feb. 22</b>                                      | French Prairie Bridge Open House hosted by the Committee for Citizen Involvement (CCI)           |   |   |
| <b>Feb. 28</b>                                      | Town Center Plan Public Kick-Off Event - City Hall   |   |   |
| <b>March 8</b>                                      |  |   | Frog Pond Master Plan (Neamtzu)<br>Transit Master Plan (Lashbrook) Continued to May |
| <b>April 12</b>                                     | 2016 Housing Report (Scola) (20 Min)<br>Basalt Creek Concept Plan Update (Bateschell) (20 Min)   | Transit Master Plan (Brashear)(90 mins)   |   |
| <b>May 10</b>                                       | Basalt Creek Concept Plan Update (Bateschell) (10 Mins)  |   | Transit Master Plan (Brashear) (90 mins)  |
| <b>May 15</b>                                       | Joint Planning Commission / City Council Work Session (5 PM): Town Center Plan                   |   |   |
| <b>June 14</b>                                      |  | Old Town Development Code (Pauly)   |   |
| <b>July 12</b>                                      |  | Industrial Form-based Code (Rybold / Vance)                                       |   |
| <b>August 9</b>                                     |  |   | Old Town Development Code (Pauly)   |
|   | Tentative: Joint Planning Commission / City Council Work Session: Town Center Redevelopment Plan |   |   |
| <b>September 13</b>                                 |  | French Prairie Bridge (Weigel)<br>Water Treatment Plant Master Plan (Mende)       |   |
| <b>October 11</b>                                   |  |   | Water Treatment Plant Master Plan (Mende)   |
| <b>November 8</b>                                   |  | Citywide Wayfinding & Signage Plan (Scola)  | Industrial Form-based Code (Rybold / Vance)   |
| <b>December 13</b>                                  |  | Town Center Plan  |   |

### 2017

- 1 Frog Pond Master Plan
- 2 Basalt Creek Concept Plan
- 3 Town Center Redevelopment
- 4 Transit Master Plan
- 5 Coffee Creek Industrial Area Form-Based Code
- 6 French Prairie Bike/Ped Bridge
- 7 Parks & Rec MP Update
- 8 Code Amendments
- 9 Parking Code Update
- 10 Water Treatment Plant Master Plan